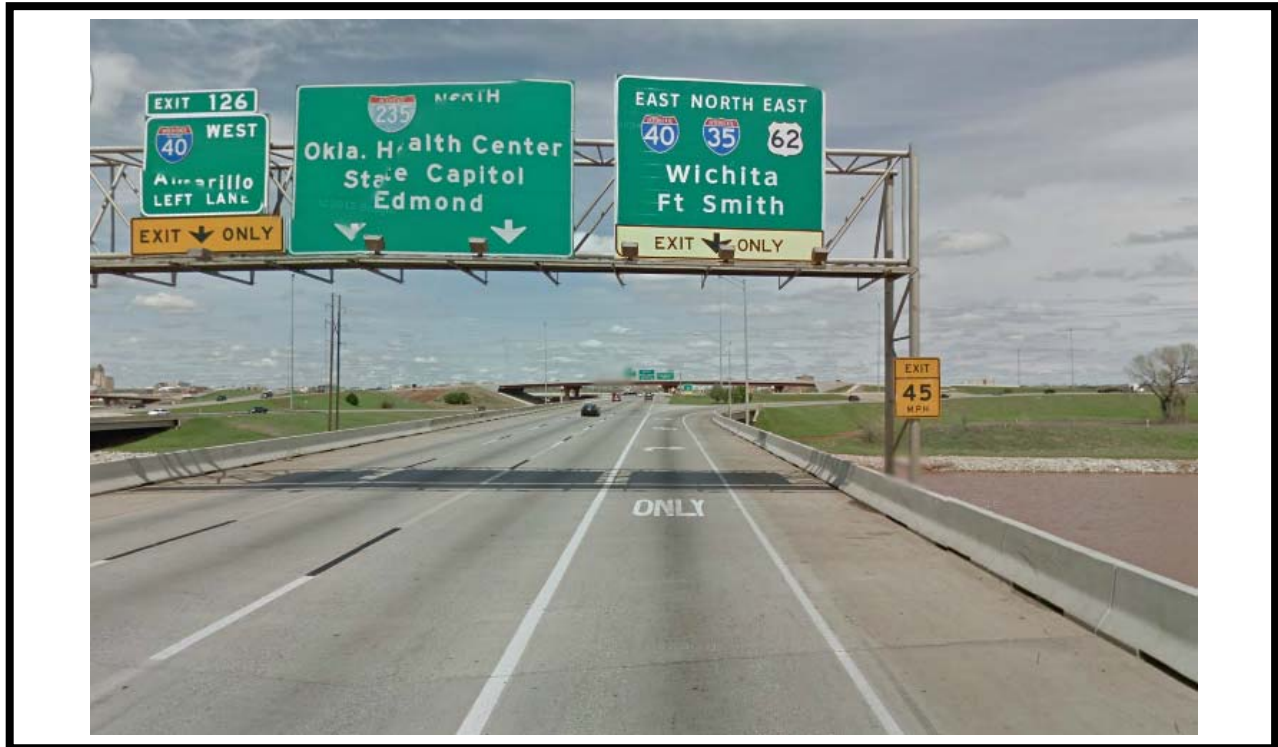


**STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION
RECONNAISSANCE REPORT**



**I-35: ADD RAMPS FOR I-35 NB/SB AT I-40 JCT.
OKLAHOMA COUNTY
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096,
23635, 26269 & 27869
JP 30444(04)**



OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, & 27869
OKLAHOMA COUNTY
J/P 30444(04)

TABLE OF CONTENTS

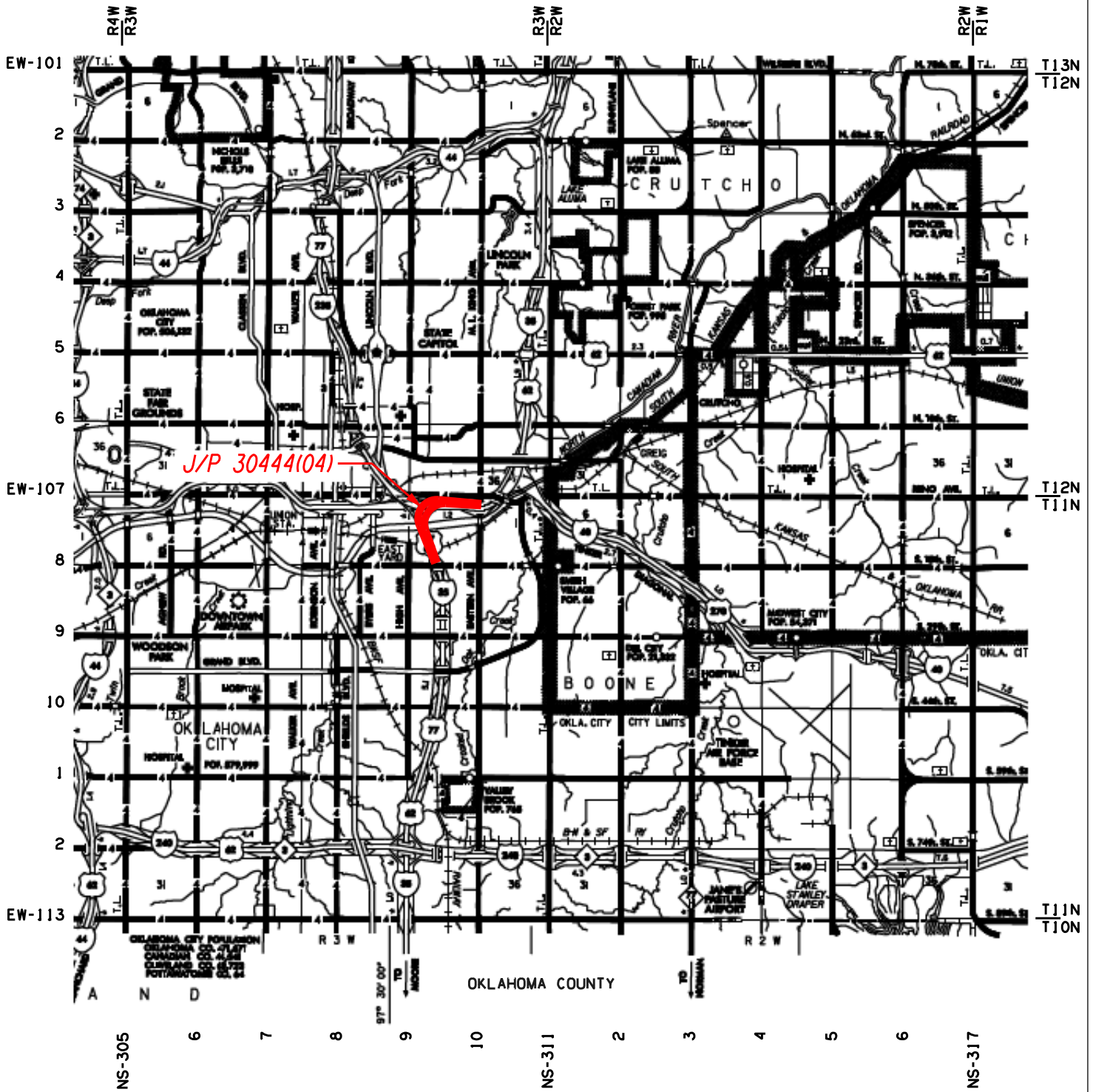
| | |
|----|---|
| 1 | GEO-REFERENCED GRAPHICS |
| 2 | AS-BUILT PLANS |
| 3 | PROPERTY IDENTIFICATION |
| 4 | UTILITY & TRADEFIXTURE (BILLBOARD) INFORMATION |
| 5 | ACCIDENT HISTORY |
| 6 | EXISTING BRIDGE CONDITION AND HYDROLOGICAL DATA |
| 7 | CULTURAL RESOURCES |
| 8 | HAZARDOUS WASTE/LUST SITES |
| 9 | NATURAL RESOURCES |
| 10 | EXISTING FACILITY DATA |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, & 27869
OKLAHOMA COUNTY
J/P 30444(04)

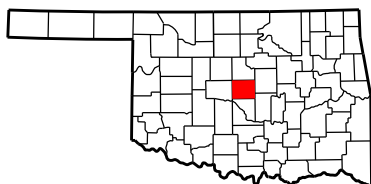
SECTION 01 – GEOREFERENCED GRAPHICS

Sheet(s)

- 1 County/Project Location Map
- 2 Study Area Plan Sheets- 400 Scale - USGS
- 3 Composite Map – 400 Scale
- 4 State Highway Detour Map

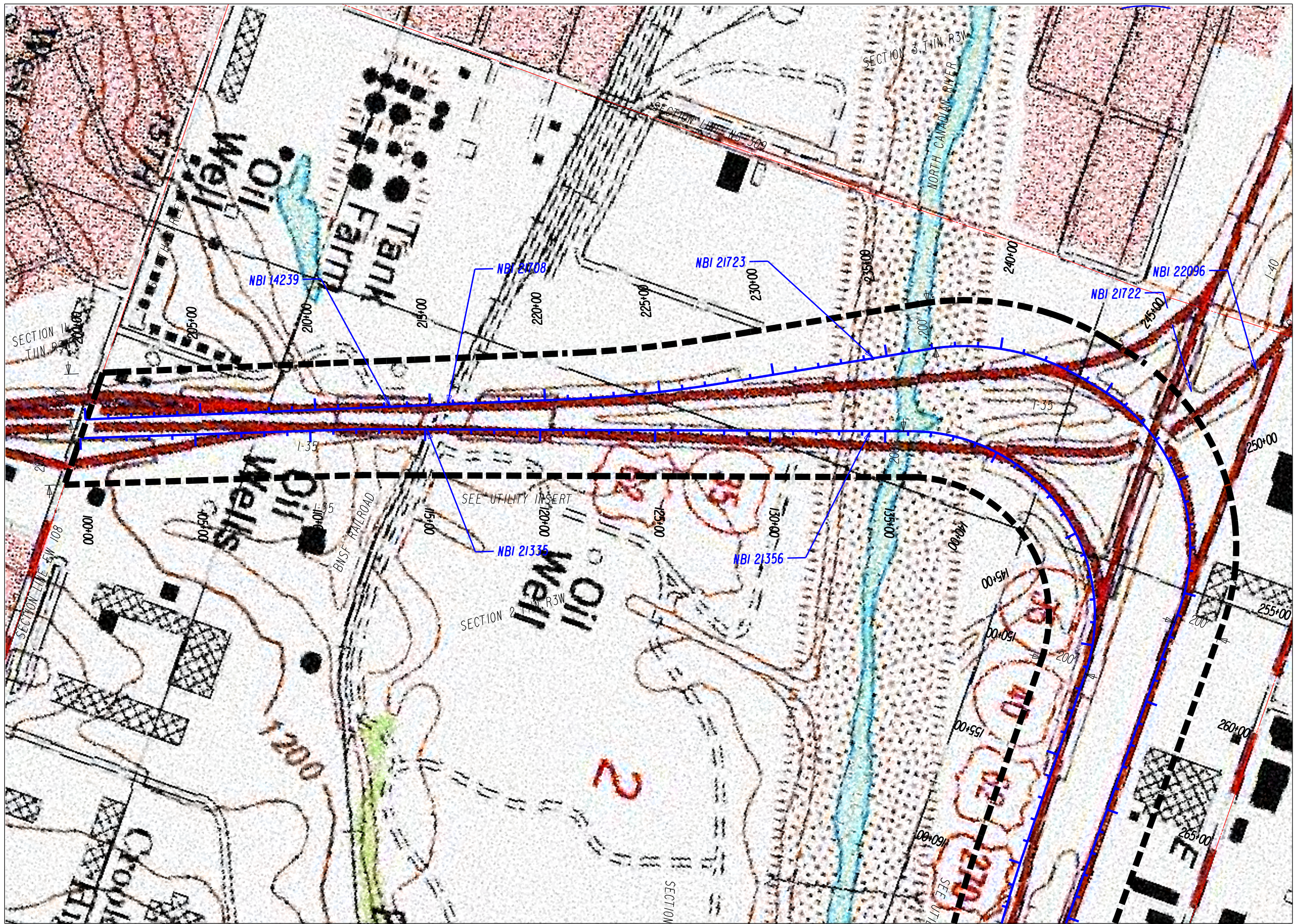


OKLAHOMA COUNTY

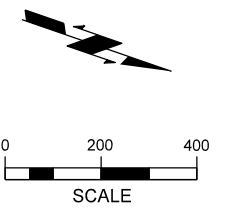


SCHEMMER
 ARCHITECTS | ENGINEERS | PLANNERS
 COUNTY / PROJECT LOCATION MAP
 I-35: ADD ADDITIONAL RAMPS FOR
 I-35 NB & SB AT
 THE I-40/I-35 JCT
 Division 4 - Oklahoma County
 J/P 30444(04)
 Sht 1 of 6

1" =
2 MI.

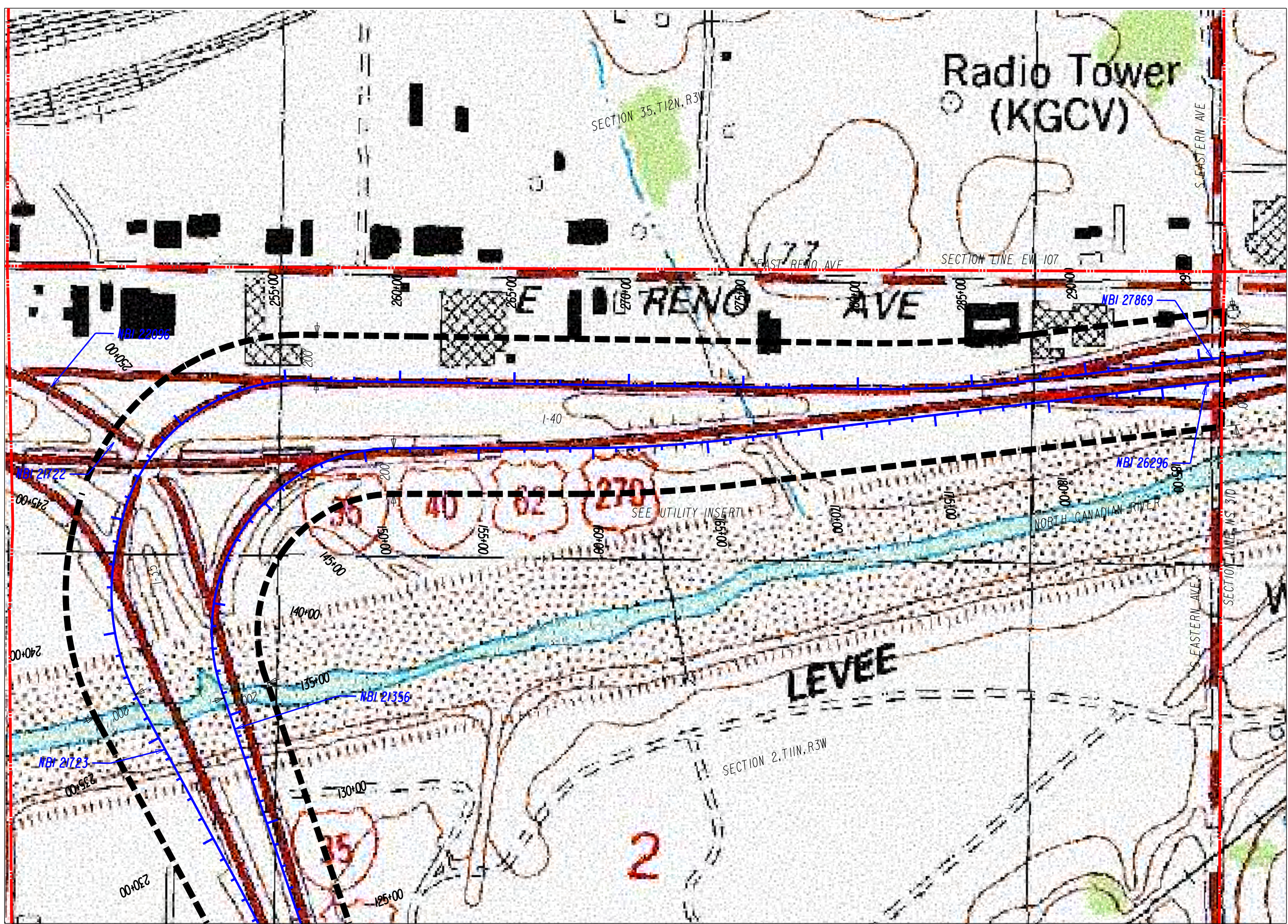


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 - - - - - STUDY LIMITS

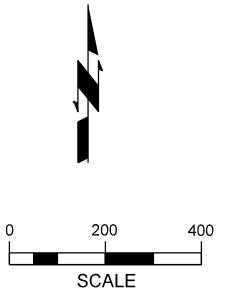


SCHEMMER
 ARCHITECTS | ENGINEERS | PLANNERS

STUDY AREA LOCATION
 I-35: ADD ADDITIONAL RAMP FOR
 I-35 NB AND SB AT I-40/I-35 JCT
 DIVISION 4, OKLAHOMA COUNTY
 JP 30444(104)
 SHT 2 OF 6

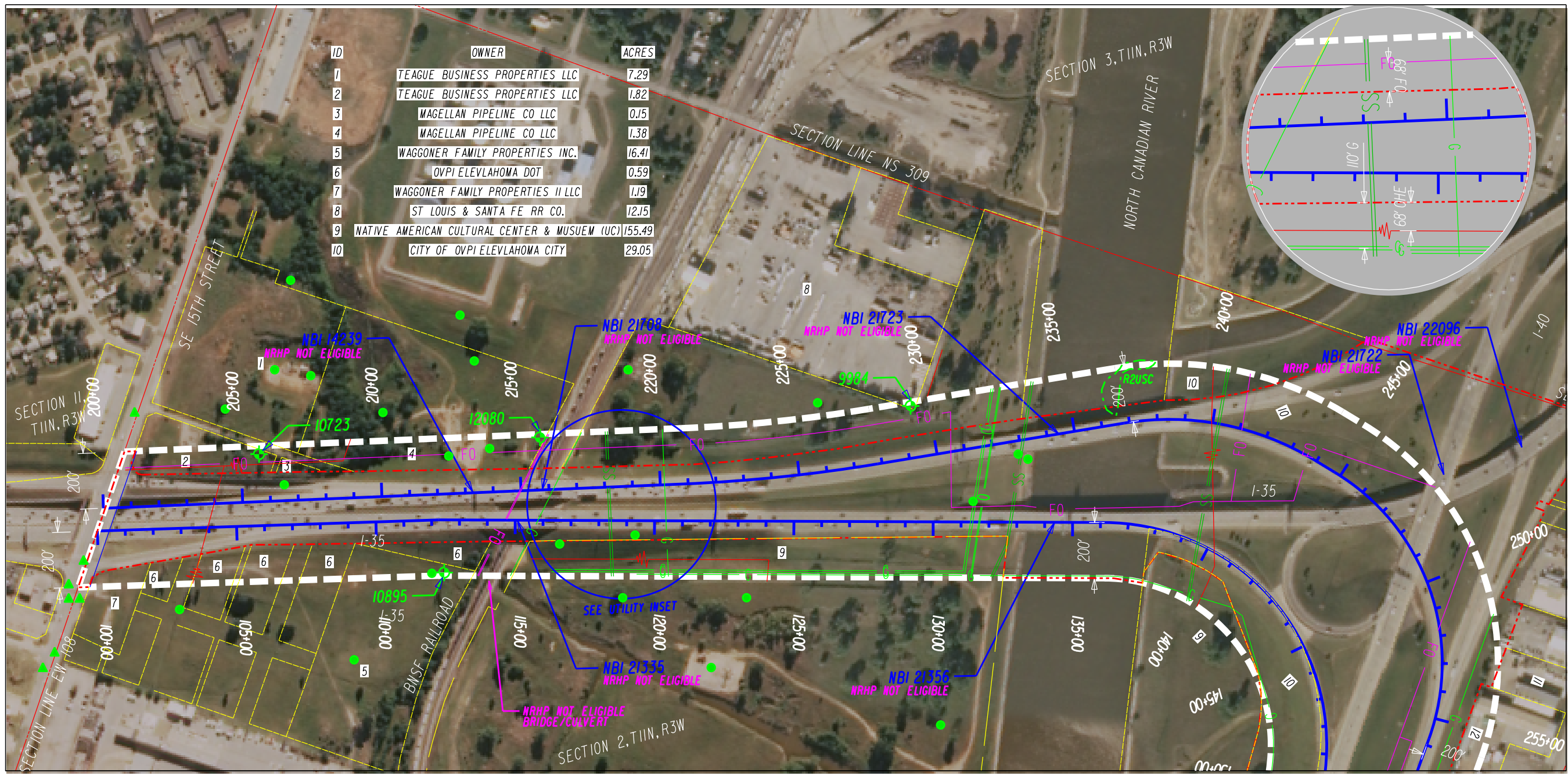


LEGEND
 - - - - - STUDY LIMITS



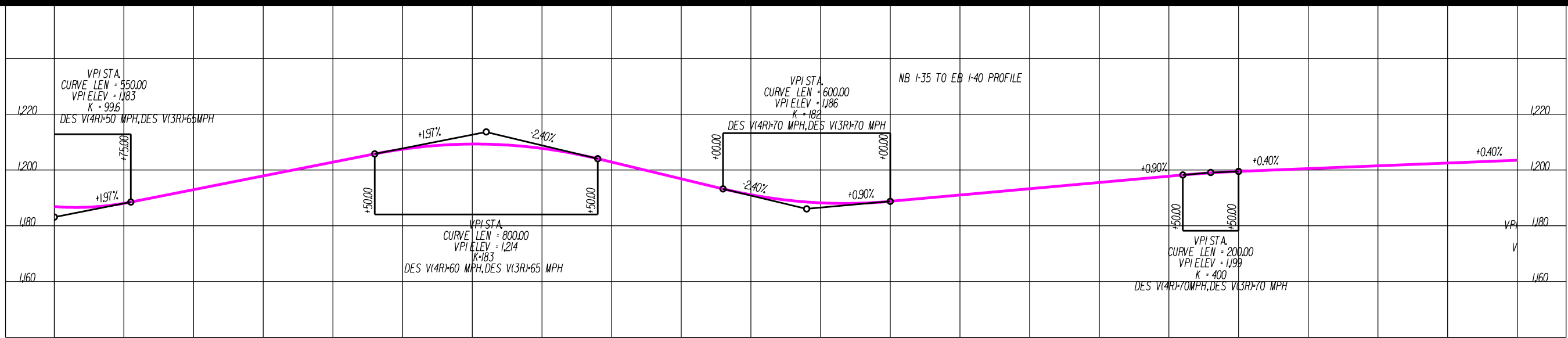
SCHEMMER
 ARCHITECTS | ENGINEERS | PLANNERS

STUDY AREA LOCATION
 I-35: ADD ADDITIONAL RAMP FOR
 I-35 NB AND SB AT I-40/I-35 JCT
 DIVISION 4, OKLAHOMA COUNTY
 JP 304441041
 SHT 3 OF 6



- LEGEND**
- STUDY LIMITS
 - SECTION LINE
 - PROJECT CENTERLINE
 - TRIBAL OWNERSHIP
 - PROPERTY LINE
 - PROPERTY ID
 - EXISTING ROW
 - FO FIBER OPTIC
 - P PETROLEUM PIPELINE
 - TVUG UNDERGROUND TELEVISION (NONE LOCATED IN PROJECT LIMITS)
 - SS SANITARY SEWER
 - G GAS
 - PUG UNDERGROUND POWER
 - OVERHEAD POWER
 - TUG UNDERGROUND TELEPHONE (NONE LOCATED IN PROJECT LIMITS)
 - T OVERHEAD TELEPHONE (NONE LOCATED IN PROJECT LIMITS)
 - W WATER LINE
 - CULTURAL RESOURCE
 - PJWA - POTENTIAL JURISDICTIONAL WETLAND
 - ▲ HAZARDOUS WASTE SITE
 - WELL SITE
 - ⊕ BILLBOARD

STATIONS & PROFILE BASED ON PROJECT
 FAP FBIY-35-3(279)126, SHTS 18, 19, 22
 AND FAP --351(3), SHT 34

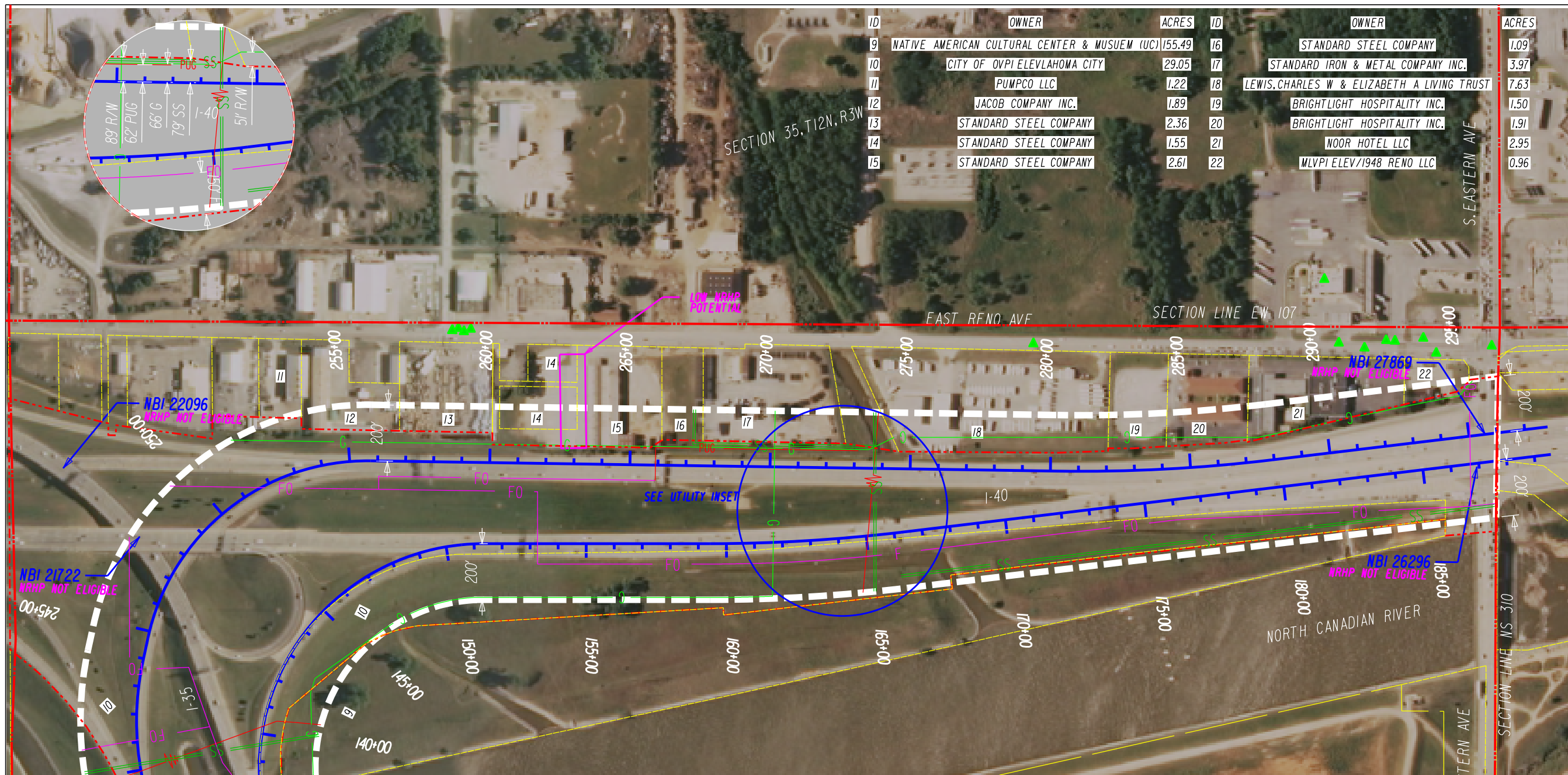


North arrow pointing up and slightly right.

Scale: 0, 200, 400. SCALE

SCHEMMER
 ARCHITECTS | ENGINEERS | PLANNERS

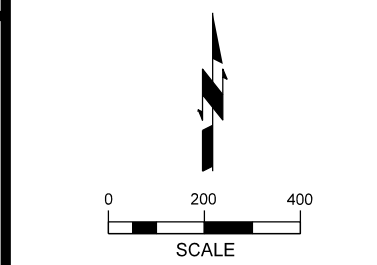
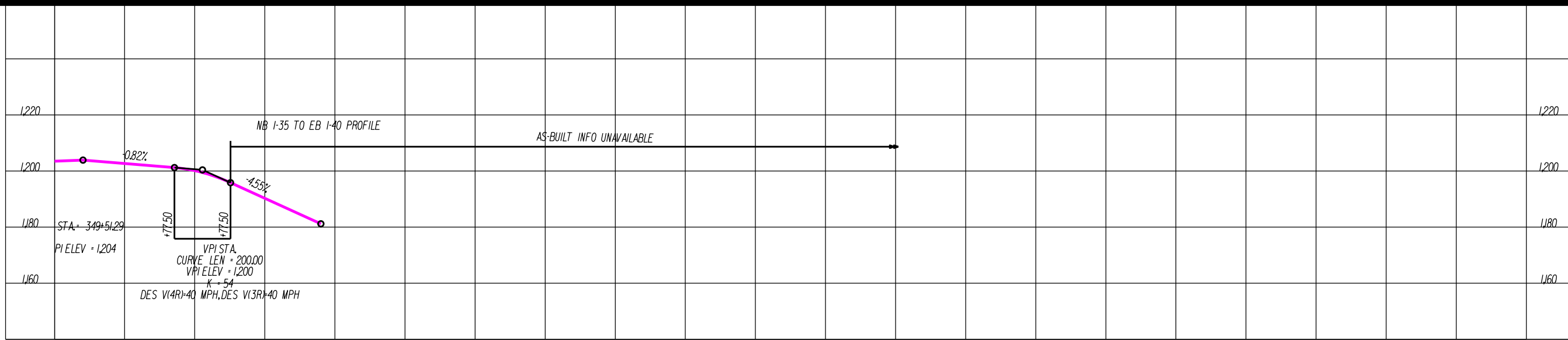
STUDY AREA LOCATION
 I-35: ADD ADDITIONAL RAMP FOR
 I-35 NB AND SB AT I-40/I-35 JCT
 DIVISION 4, OKLAHOMA COUNTY
 JP 30444(04)
 SHT 4 OF 6



| ID | OWNER | ACRES | ID | OWNER | ACRES |
|----|---|--------|----|---|-------|
| 9 | NATIVE AMERICAN CULTURAL CENTER & MUSUEM (UC) | 155.49 | 16 | STANDARD STEEL COMPANY | 1.09 |
| 10 | CITY OF OVPI ELEVLAHOMA CITY | 29.05 | 17 | STANDARD IRON & METAL COMPANY INC. | 3.97 |
| 11 | PUMPCO LLC | 1.22 | 18 | LEWIS, CHARLES W & ELIZABETH A LIVING TRUST | 7.63 |
| 12 | JACOB COMPANY INC. | 1.89 | 19 | BRIGHTLIGHT HOSPITALITY INC. | 1.50 |
| 13 | STANDARD STEEL COMPANY | 2.36 | 20 | BRIGHTLIGHT HOSPITALITY INC. | 1.91 |
| 14 | STANDARD STEEL COMPANY | 1.55 | 21 | NOOR HOTEL LLC | 2.95 |
| 15 | STANDARD STEEL COMPANY | 2.61 | 22 | MLVPI ELEV/1948 RENO LLC | 0.96 |

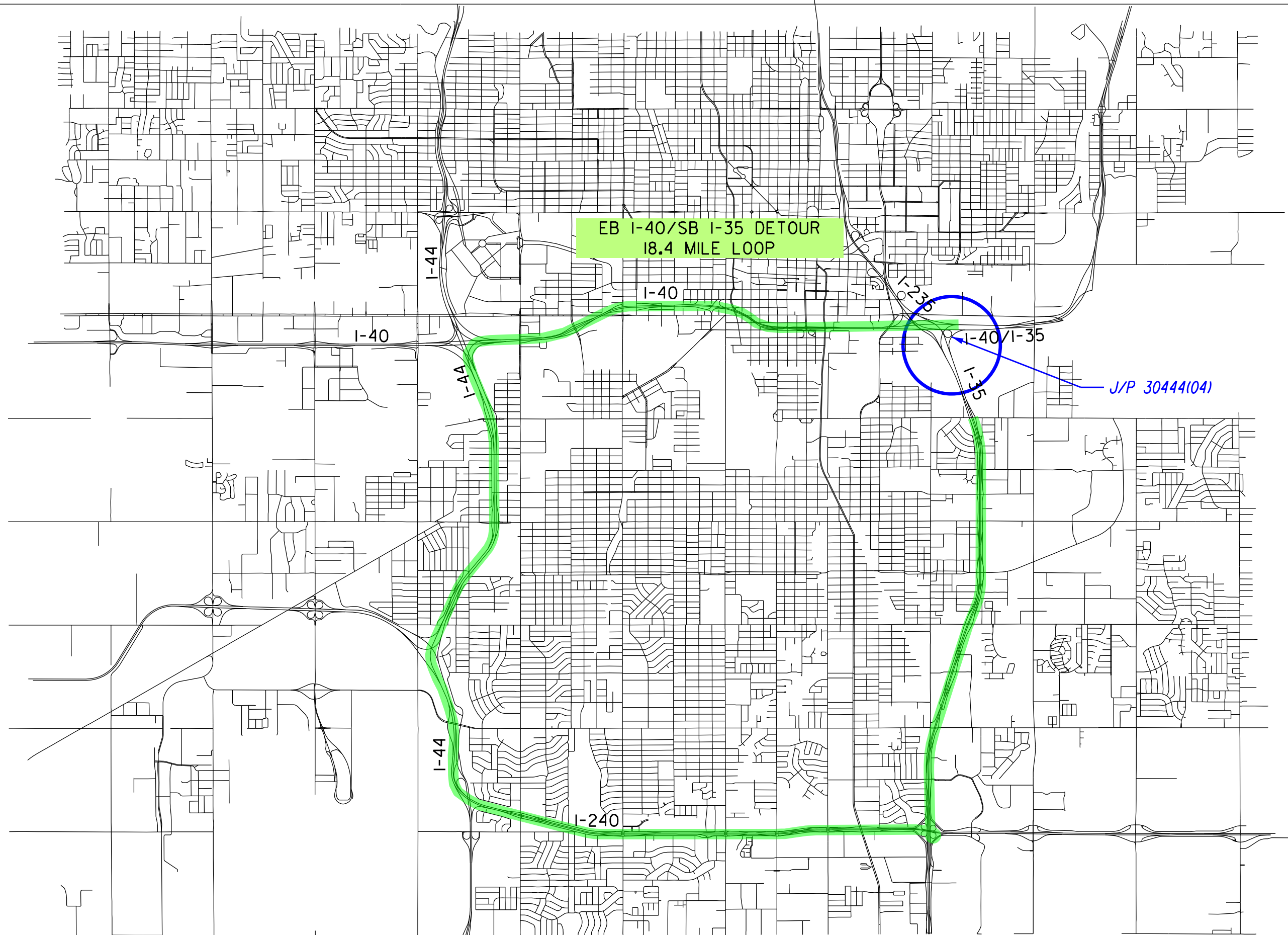
- LEGEND**
- STUDY LIMITS
 - SECTION LINE
 - PROJECT CENTERLINE
 - TRIBAL OWNERSHIP
 - PROPERTY LINE
 - PROPERTY ID
 - EXISTING ROW
 - FO FIBER OPTIC
 - O PETROLEUM PIPELINE
 - TVUG UNDERGROUND TELEVISION (NONE LOCATED IN PROJECT LIMITS)
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 - PUG UNDERGROUND POWER
 - OVERHEAD POWER
 - TUG UNDERGROUND TELEPHONE (NONE LOCATED IN PROJECT LIMITS)
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 - W WATER LINE
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 - PJWA - POTENTIAL JURISDICTIONAL WETLAND
 - ▲ HAZARDOUS WASTE SITE
 - WELL SITE
 - ◆ BILLBOARD

STATIONS & PROFILE BASED ON PROJECT
 FAP FBIY-35-3(279)126, SHTS 18, 19, 22
 AND FAP --351(3), SHT 34



SCHEMMER
 ARCHITECTS | ENGINEERS | PLANNERS

STUDY AREA LOCATION
 I-35: ADD ADDITIONAL RAMP FOR
 I-35 NB AND SB AT I-40/I-35 JCT
 DIVISION 4, OKLAHOMA COUNTY
 JP 30444(04)
 SHT 5 OF 6



1.0 MILES



SCHEMMER
ARCHITECTS | ENGINEERS | PLANNERS

DETOUR MAP
I-35: ADD ADDITIONAL RAMPS FOR I-35
NB & SB AT
THE I-40/I-35 JCT
Division 4 - Oklahoma County
J/P 30444(04)
Sht 6 of 6

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, & 27869
OKLAHOMA COUNTY
J/P 30444(04)

SECTION 02 – AS-BUILT PLANS

As-built plan for the following projects were provided by ODOT Reproduction and Field Division 4:

FAP BHIY-35-3(279)126, JP 20869(04), Sheets 1 & 6.

FAP I-35-3(107)126, Sheets 19, 20, 22, 49, 55-61, 67-74, 83, 93, 94 & 103.

FAP I-35-3(279)126, Sheets 1 & 6.

FAP I-235-1(019)000, Sheet 1, 15, 52, 53, 63-69.

FAP I-235-1(020)000, Sheets 23, 54-56, 74-86.

FAP I-381(3), Sheet 34.

FAP OKCY-XTWN(015)TI, JP 17428(34), Sheets 1, 254-257, 275-285.

FAP OKCY-XTWN(047)SS, JP 17428(59), Sheets 1, 2, 453-456, 476-490.

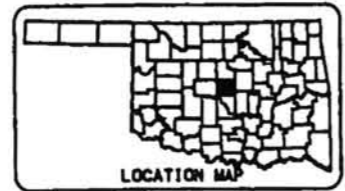
JP 02689(11), Sheets 39, 40, 203, 212, 216, 217, 220, 229, 232 & 233.

Sub 10-13-05

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED
STATE HIGHWAY
FEDERAL AID PROJECT NO. BHIY-35-3(279)126
DIVISION FOUR URBAN JOINT REPAIR PROJECT
OKLAHOMA COUNTY
STATE JOB NO. 20869(04) BRIDGE 'A' & 'B'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | | | | | |
| DESCRIPTION | | REVISIONS | DATE | | |



TRAFFIC CONTROL
PROJECT ENGINEER: F. DHAMAN
SOLID SUPERVISOR: D. KOCH

BRIDGE DESIGN
ENGINEER MANAGER: STEVEN JACOB
SOLID SUPERVISOR: MAX NELSON
SOLID MEMBERS: L. L. CLEARLEY, K. ENGER, M. PEARSON, K. RICHARDSON, I. BORRELLA & J. ALLEN

INDEX OF SHEETS

| SHEET NO. | TITLE |
|-----------|--|
| 1 | TITLE SHEET |
| 2 | GENERAL NOTES AND SUMMARY OF PAY QUANTITIES (BRIDGE) |
| 3 | GENERAL PLAN (BRIDGE 'A') |
| 4 | GENERAL PLAN (BRIDGE 'B') |
| 5 | TYPICAL SECTION (BRIDGE 'A' & 'B') |
| 6 | DETAILS OF PARAPET AND JOINT REPAIR |

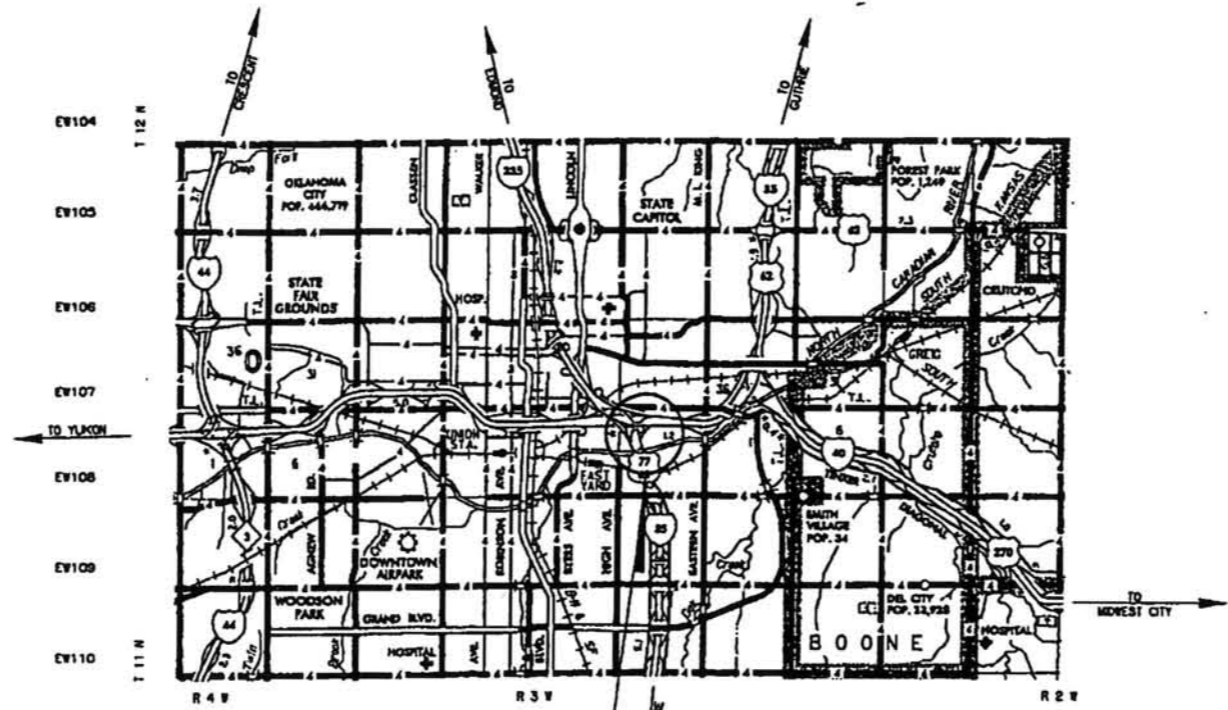
THE FOLLOWING STANDARD DRAWINGS WILL BE REQUIRED

| | | |
|------------|------------|-------------|
| EJ-30-01E | TC31-1-00E | TC314-1-00E |
| EJ-3K-01E | TC32-1-00E | TC315-1-00E |
| EJ-07L-00E | TC33-1-00E | TC368-1-00E |
| | TC34-1-00E | TC368-1-00E |
| | TC33-1-00E | TC370-1-00E |
| | TC38-1-00E | TC371-1-00E |
| | TC37-1-00E | TC372-1-00E |
| | TC38-1-00E | |
| | TC39-1-00E | |

CONVENTIONAL SYMBOLS

- PROPOSED ROAD
- RAILROADS
- RANGE & TOWNSHIP
- SECTION LINES
- QUARTER SECTION LINES
- FENCES
- GRAVEL LINE
- EXISTING ROAD
- BASE LINE
- GRADE LINES
- TELEPHONE & TELEGRAPH
- POWER LINES
- BUILDINGS
- OIL WELL
- DRAINAGE STRUCTURES - IN PLACE
- DRAINAGE STRUCTURES - NOT
- RIGHT-OF-WAY LINES - EXISTING
- RIGHT-OF-WAY LINES - NEW
- CONTROLLED ACCESS
- RIGHT-OF-WAY FENCE

1999 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION - ENGLISH VERSION, APPROVED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION, SEPTEMBER 21, 1999.



BRIDGE 'A'
LOCATION NO. 5515-0566X CONTROL SECTION NO. 35-55-15
FACILITY CARRIED: I-35 FEATURE INTERSECTED: N. CANADIAN RIVER
LOCATION: I-35 OVER N. CANADIAN RIVER, 0.2 MILES SOUTH OF I-40
ADT 2003 = 49,200

BRIDGE 'B'
LOCATION NO. 5515-0566YX CONTROL SECTION NO. 35-55-15
FACILITY CARRIED: I-35 FEATURE INTERSECTED: N. CANADIAN RIVER
LOCATION: I-35 OVER N. CANADIAN RIVER, 0.2 MILES SOUTH OF I-40
ADT 2003 = 49,200

PREPARED BY:
OKLAHOMA DEPARTMENT OF TRANSPORTATION
BRIDGE DESIGN DIVISION

DATE: 3-3-05
BY: STEVEN JAMES JACOB

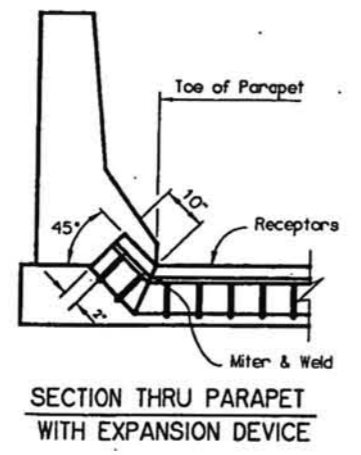
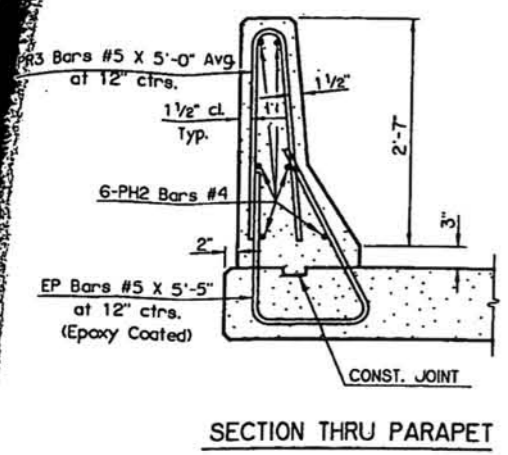
OKLAHOMA DEPARTMENT OF TRANSPORTATION
DATE APPROVED: _____
BY: _____
F.A. PROJECT NO. BHIY-35-3(279)126

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

DATE APPROVED: _____
BY: _____
DIVISION ADMINISTRATOR

STEVEN JAMES JACOB
REGISTERED PROFESSIONAL ENGINEER
16018
OKLAHOMA

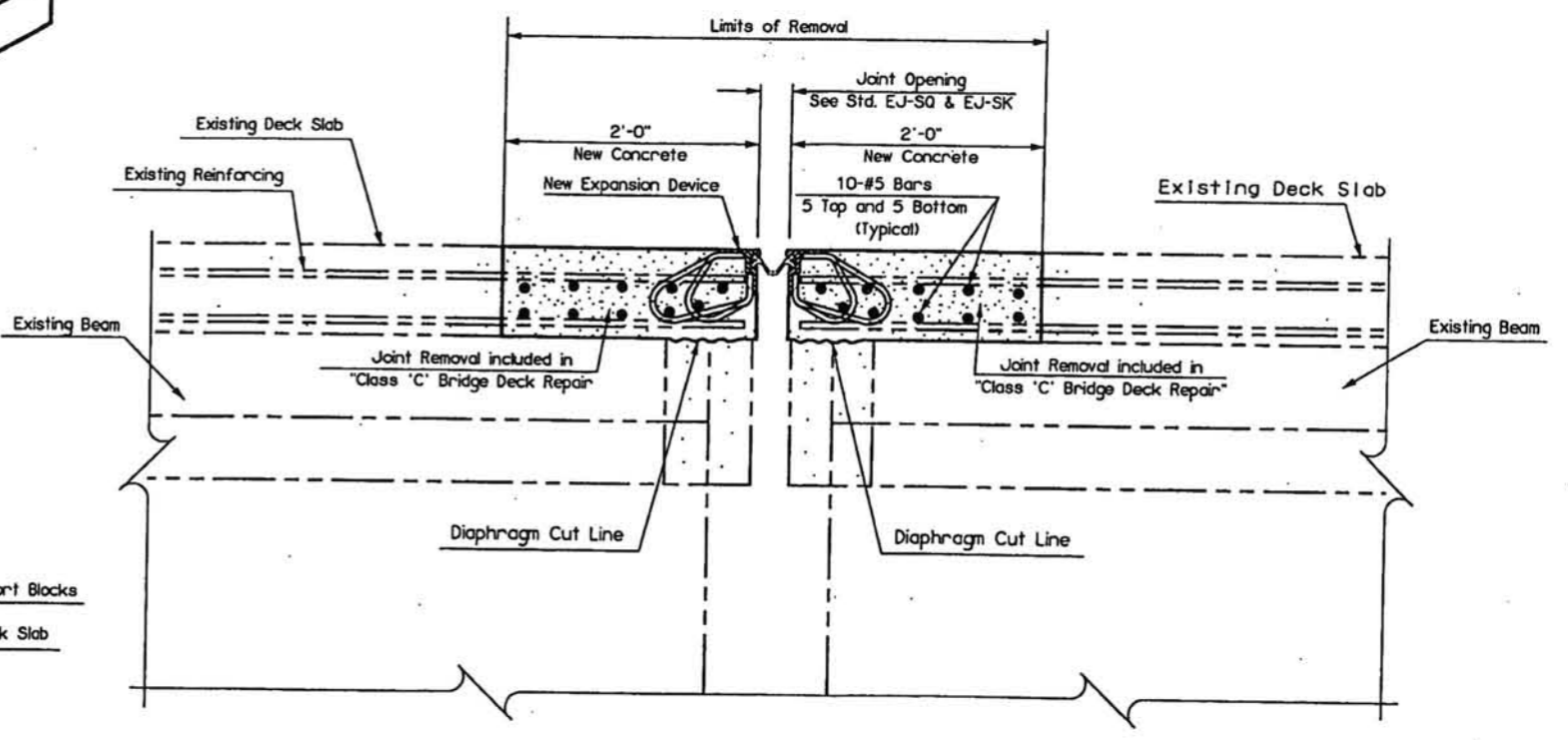
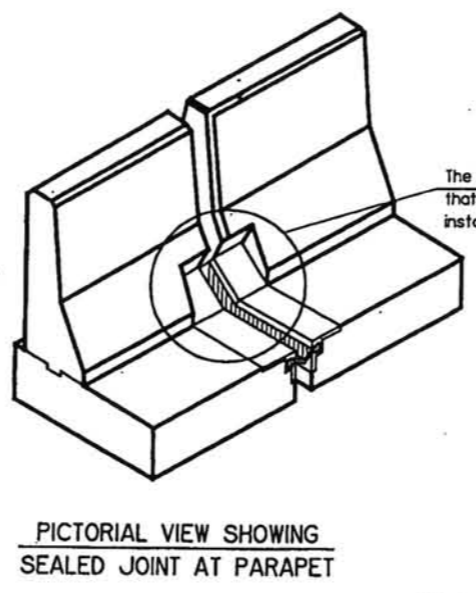
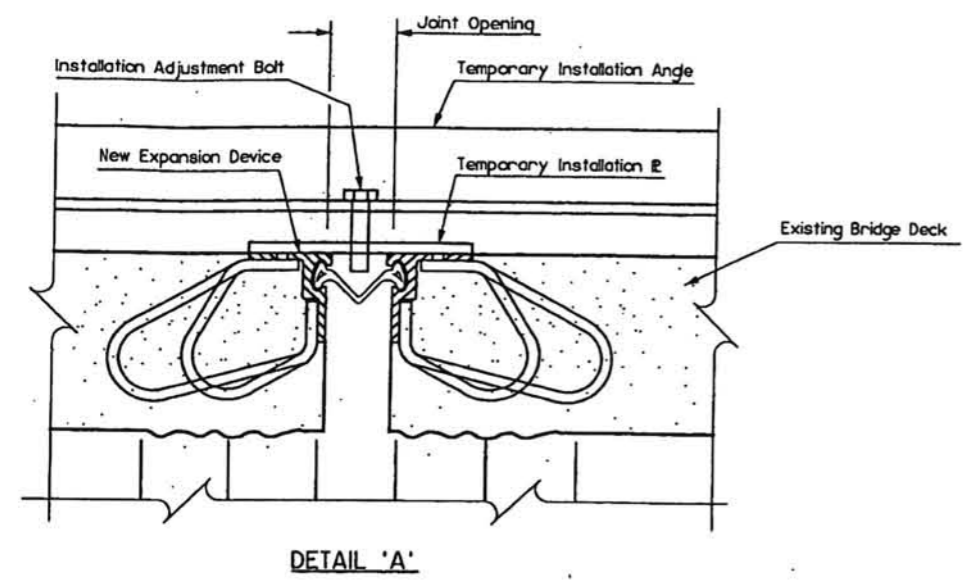
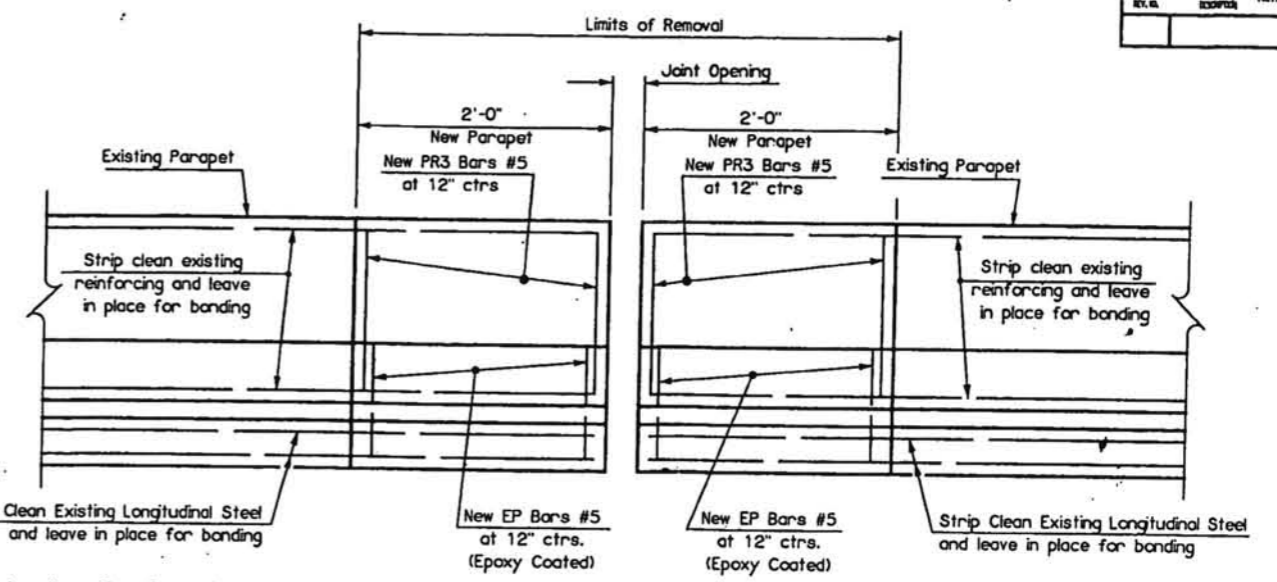
SHEET NO. 1



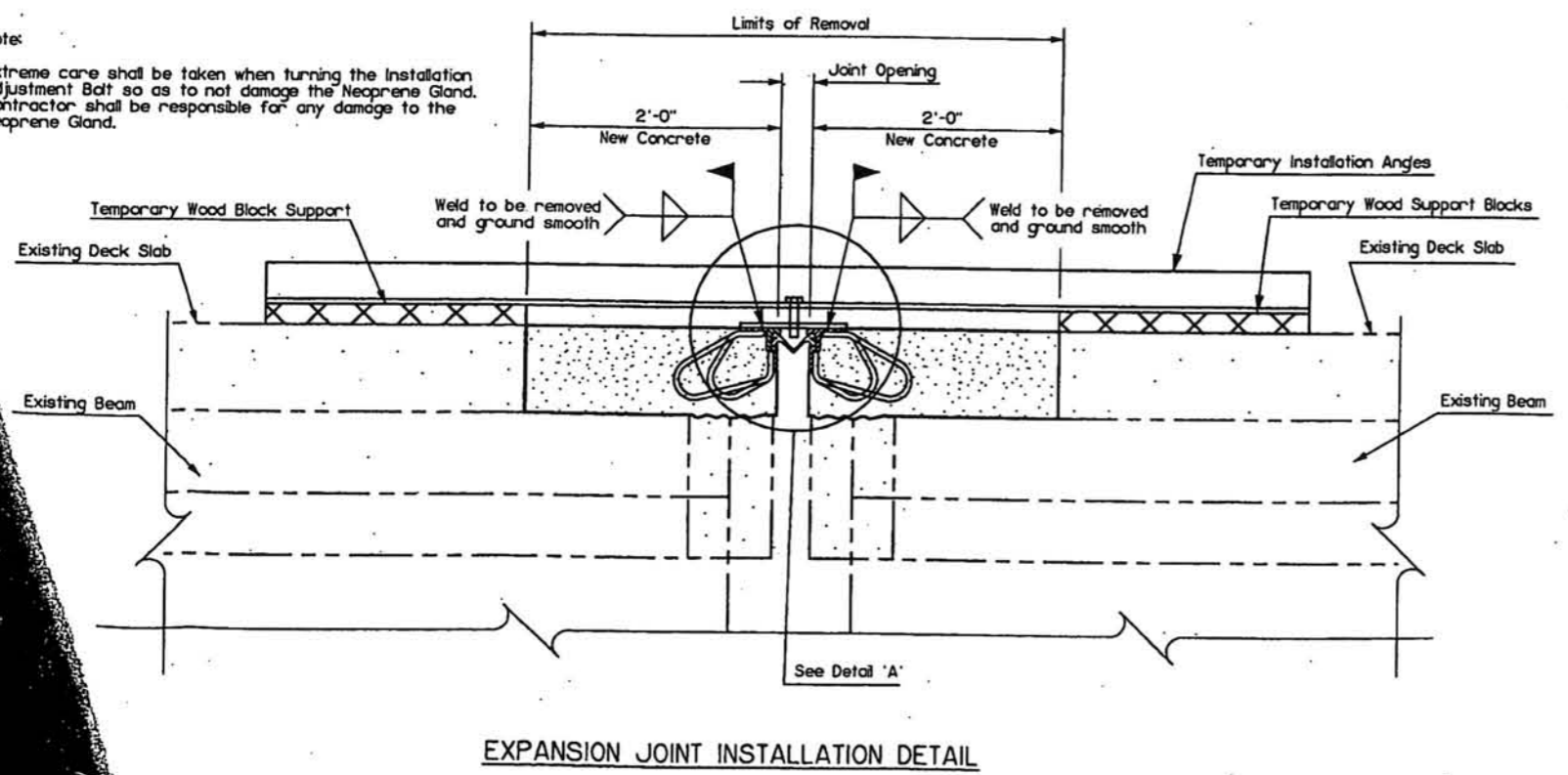
Silicone Joint Sealant at Existing Construction Joint Location

SECTION OF CONSTRUCTION JOINT AT CURB

NOTE: Place Silicone Joint Sealant across the top and along the traffic face of Bridge and 6" up the roadway face of the Parapet as shown.



Notes:
Extreme care shall be taken when turning the Installation Adjustment Bolt so as to not damage the Neoprene Gland. Contractor shall be responsible for any damage to the Neoprene Gland.



NOTE: Contractor shall match the existing opening

NOTE: Concrete over diaphragms shall be cut flush with top of beam. All exposed diaphragm bars shall be stripped clean to near white condition for banding purpose. Any damage to the end diaphragms during the repair of the joint shall be repaired at the Contractor expense to the satisfaction of the Engineer.

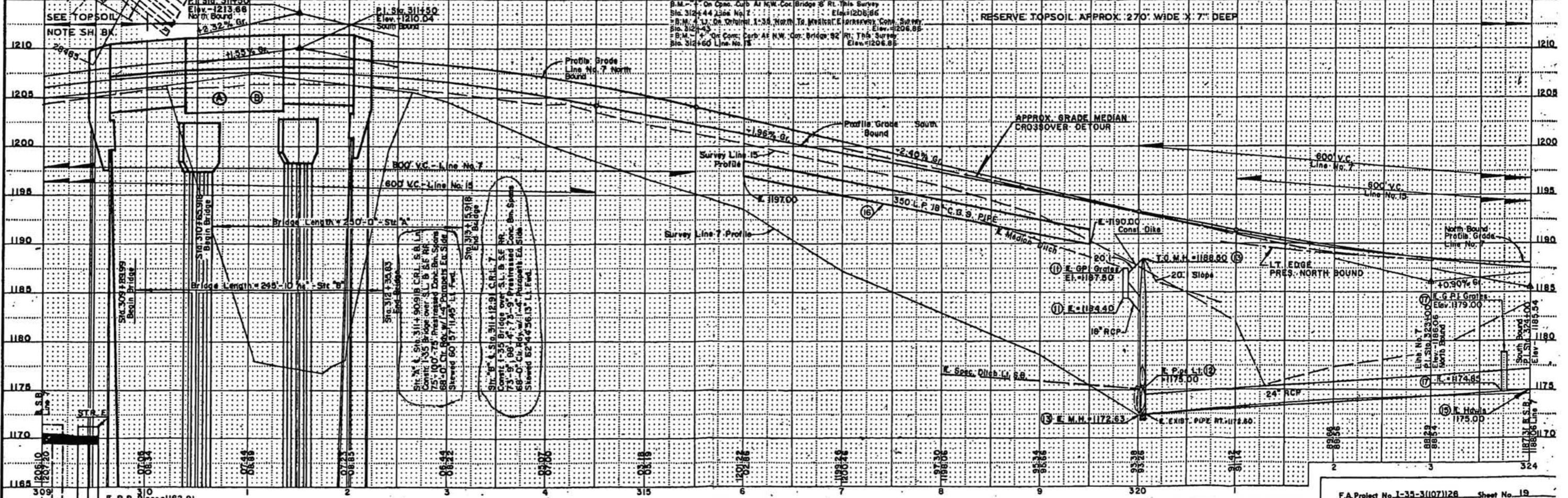
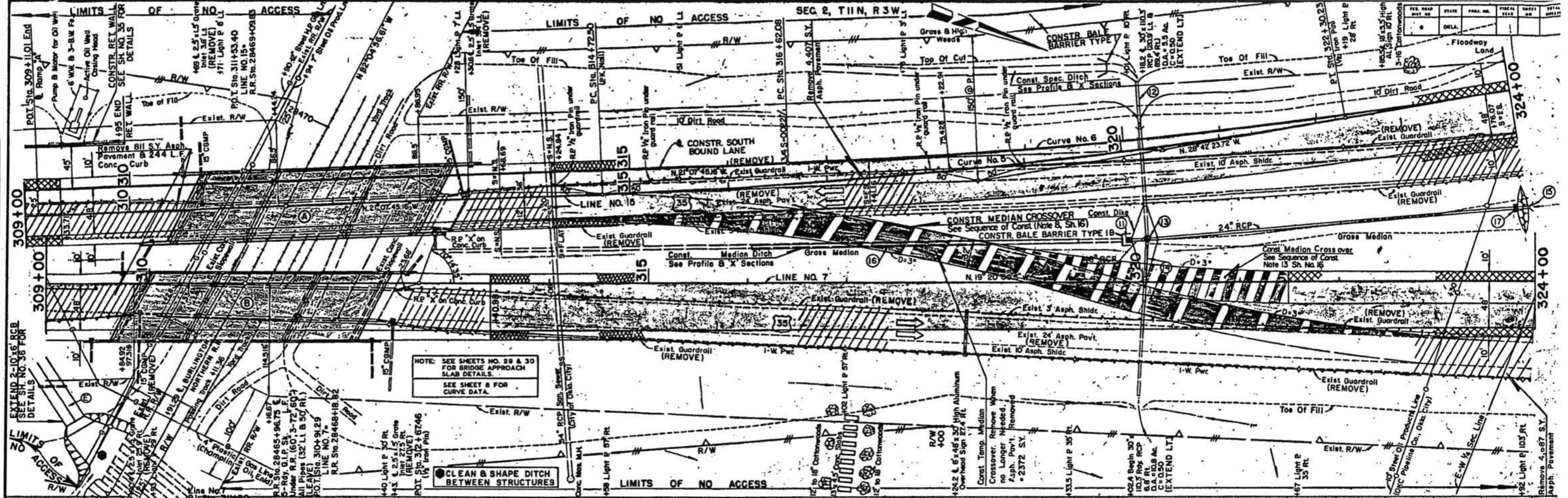
2005 Joint Repair Job.

| | | | | |
|-------------------------------------|-----------------|-------------|--------|-------|
| BRIDGE 'A' & 'B' | OKLAHOMA COUNTY | Design | N/A | N/A |
| DETAILS OF PARAPET AND JOINT REPAIR | | Detail | KDS | 02/05 |
| | | Check | LKC | 02/05 |
| STATE OF OKLAHOMA | | Sign | KELTON | |
| | | Eng. | JACOBI | |
| JOB FILE NO. 20869(04) | | SHEET NO. 6 | | |

| PER. ROAD DIST. NO. | DATE | PROJ. NO. | PHYSICAL SCALE | SHEET NO. | TOTAL SHEETS |
|---------------------|------|-----------|----------------|-----------|--------------|
| | | | | | |

| DATE | BY | REVISION |
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| DATE | BY | REVISION |
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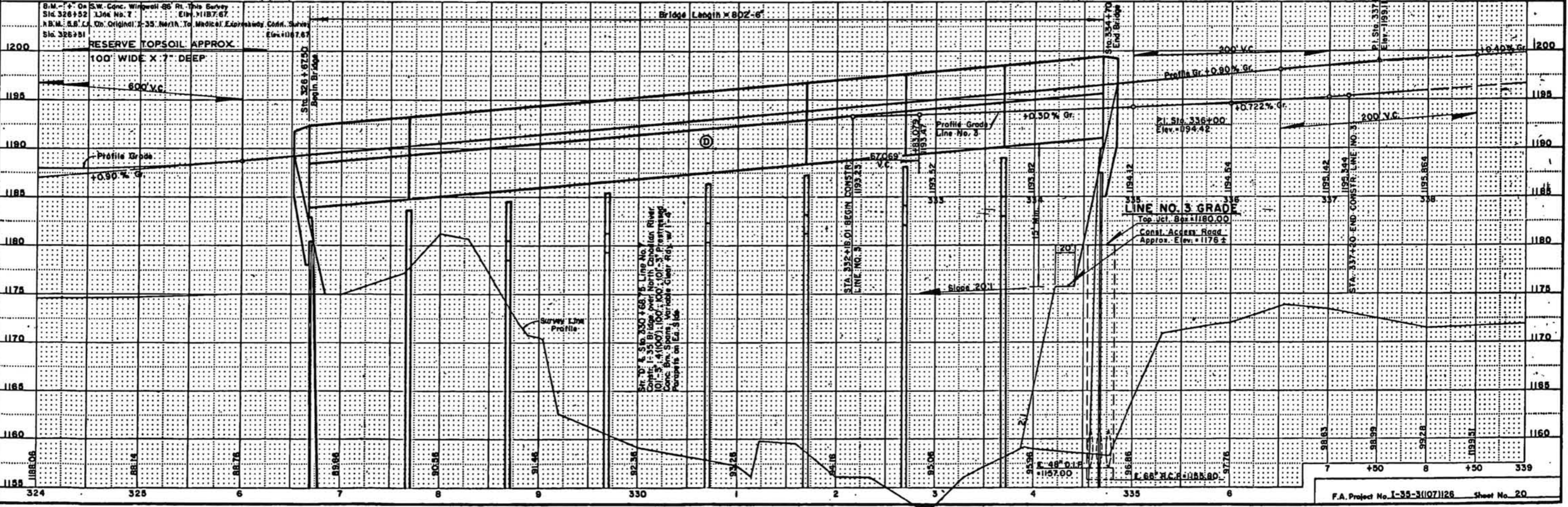
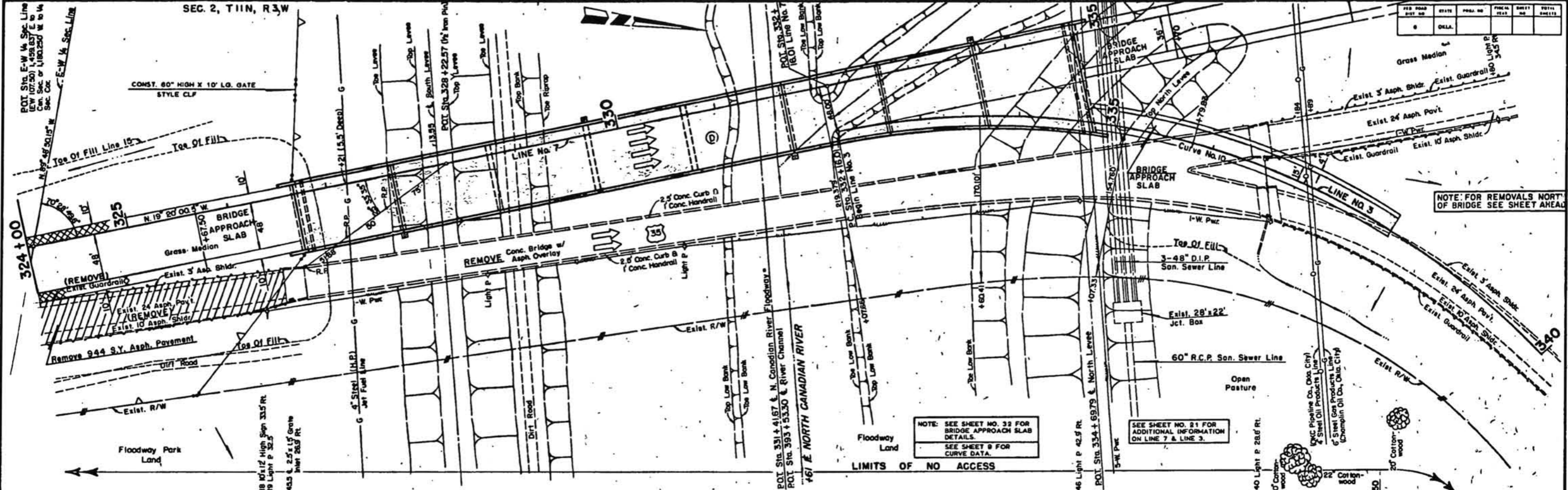
NOTE: SEE SHEETS NO. 29 & 30 FOR BRIDGE APPROACH SLAB DETAILS.
SEE SHEET 8 FOR CURVE DATA.

CLEAN & SHAPE DITCH BETWEEN STRUCTURES

RESERVE TOPSOIL APPROX. 270' WIDE X 7" DEEP

| | |
|-----------|--|
| DATE | |
| BY | |
| REVISIONS | |
| NO. | |
| DATE | |
| BY | |
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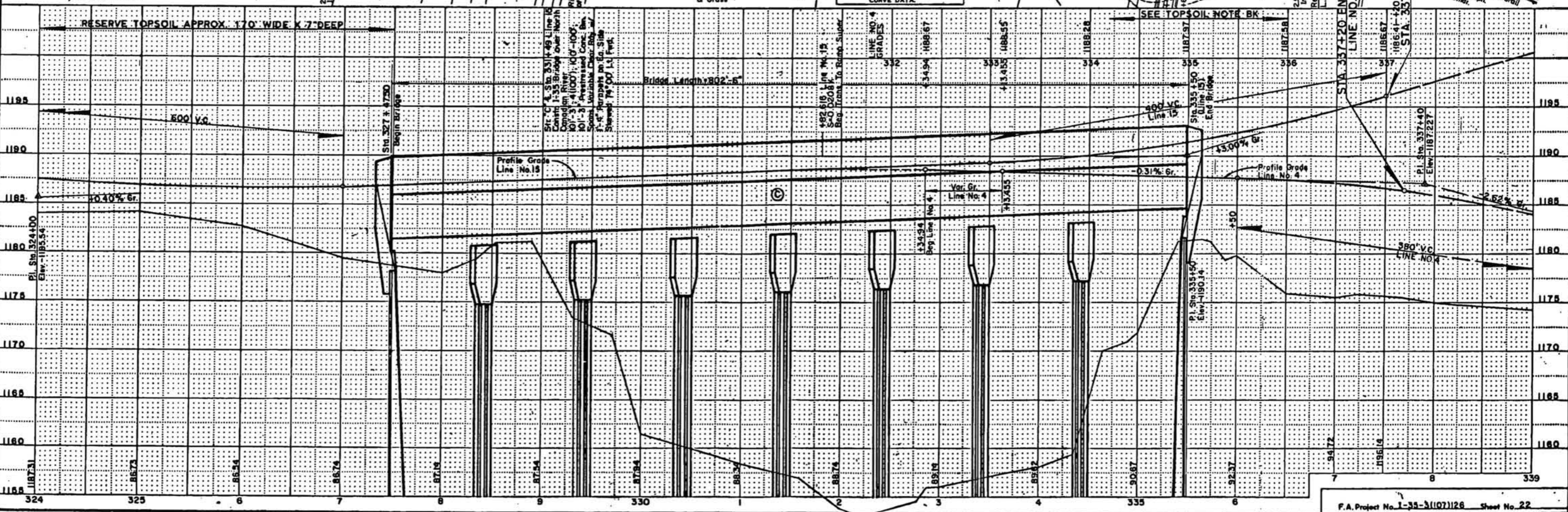
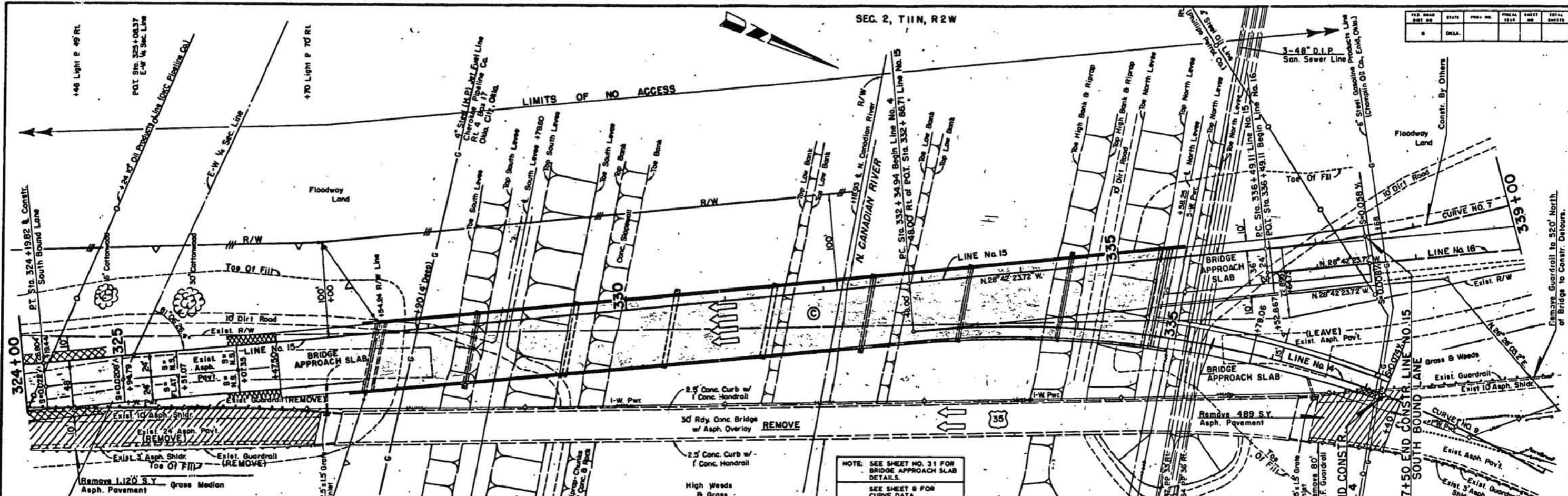
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| DATE | |
| BY | |
| REVISIONS | |
| NO. | |



F.A. Project No. I-35-3(107)126 Sheet No. 20

| DATE | |
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| BY | |
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| 47 | REVISED |
| 48 | REVISED |
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| 88 | REVISED |
| 89 | REVISED |
| 90 | REVISED |
| 91 | REVISED |
| 92 | REVISED |
| 93 | REVISED |
| 94 | REVISED |
| 95 | REVISED |
| 96 | REVISED |
| 97 | REVISED |
| 98 | REVISED |
| 99 | REVISED |
| 100 | REVISED |



| FED. ROAD DIST. NO. | 6 |
|---------------------|-------|
| STATE | DELA. |
| PROJECT NO. | |
| SCALE | |
| SHEET NO. | 22 |
| TOTAL SHEETS | 25 |

| DESIGN NO. | DATE | PROJECT NO. | SCALE | SHEET NO. | TOTAL SHEETS |
|------------|-------|-------------|-------|-----------|--------------|
| 1 | 04.14 | | | | |

DESIGN DATA

DESIGN: AASHTO SPECIFICATIONS, 1978 EDITION WITH SUPPLEMENTS

LIVE LOAD: HS 20-44

STRUCTURAL STEEL: 20,000 psi

REINFORCING STEEL: (GR. 40) 20,000 psi

REINFORCING STEEL: (GR. 60) 24,000 psi

CONCRETE:

CLASS "A" 1,000 psi

CLASS "AA" 1,200 psi

MAXIMUM FOUNDATION LOADS:

ABUTMENTS 43.2 Tons/Pile

PIERS

FRICITION CAPACITY 424.1 Tons/Shaft

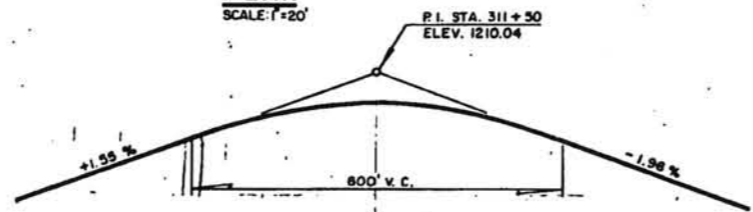
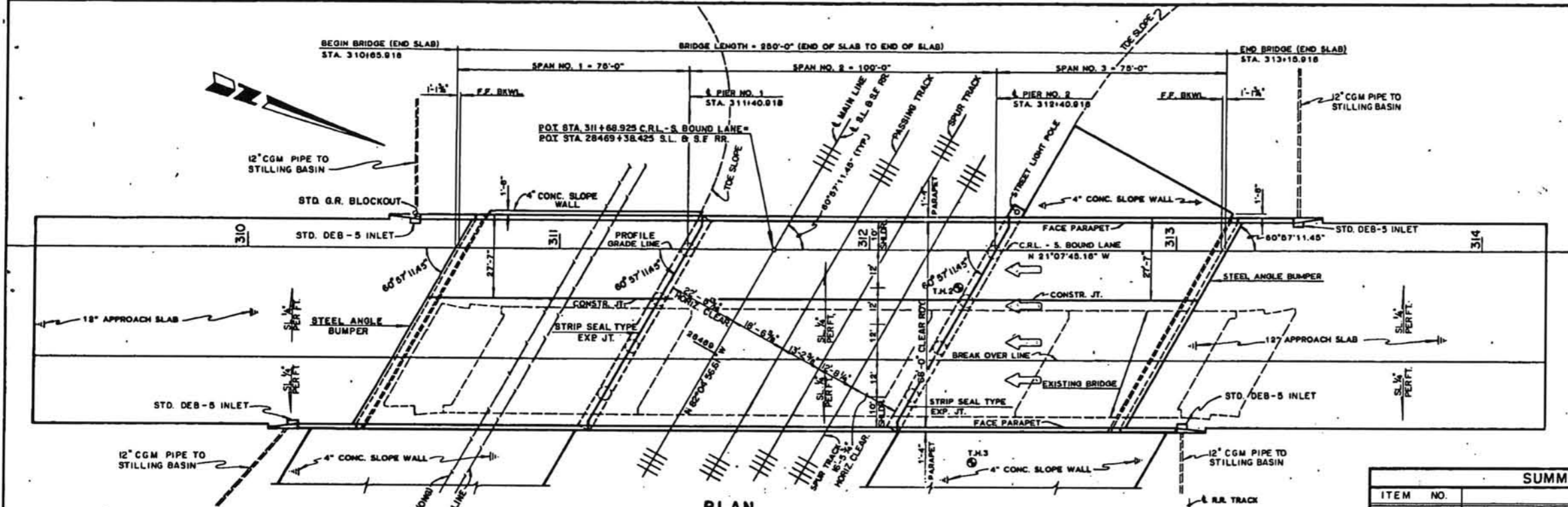
BEARING CAPACITY 117.8 Tons/Shaft

TOTAL CAPACITY 541.9 Tons/Shaft

MAX. SHAFT LOAD 415.0 Tons/Shaft

ALLOWABLE FRICTION LOAD 1.8 Tons/S.F.

ALLOWABLE POINT BEARING LOAD 60 Tons/S.F.



RAILROAD CLEARANCE DIAGRAM

NOTE: Falsework Clearance required by railway for operation during construction. Dimensions are at right angle to track.

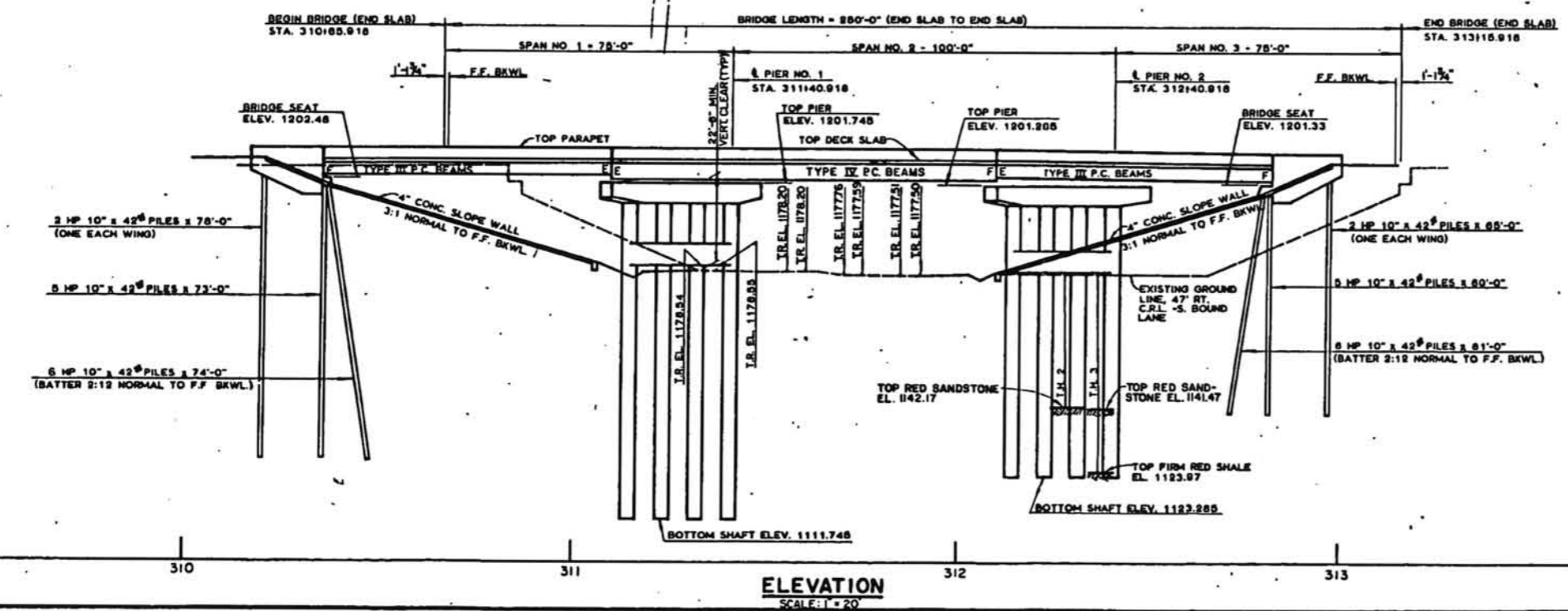
B.M. "X" ON CONC. CURB @ N.W. CORNER BRIDGE, 92' RT THIS SURVEY STA. 312+60 LINE NO. 15 ELEV. 1206.86

| SUMMARY OF QUANTITIES | | | | | | |
|-----------------------|---|-------|---------|---------|----------|----------|
| ITEM NO. | ITEM | UNIT | ABUTS. | PIERS | SUPSTR. | TOTAL |
| 202.06 (E) | Unclassified Borrow | C.Y. | 184. | | | 184. |
| 501.06 (B) | Substructure Excavation, Common | C.Y. | 146. | | | 146. |
| 503.06 | Prestressed Concrete Beam (Type III) | L.F. | | | 1,306.31 | 1,306.31 |
| 503.06 | Prestressed Concrete Beam (Type IV) | L.F. | | | 890.58 | 890.58 |
| 504.06 (A) | Class "AA" Concrete | C.Y. | | | 473.2 | 473.2 |
| 506.06 (A) | Structural Steel (A36) | LBS. | | 170. | 8,700. | 8,870. |
| 509.06 (B) | Class "A" Concrete | C.Y. | 138.6 | 288.8 | | 427.4 |
| 511.06 | Reinforcing Steel (Grade 60) | LBS. | | | 72,420. | 72,420. |
| 511A.06 | Epoxy Coated Reinforcing Steel (Grade 60) | LBS. | | | 55,700. | 55,700. |
| 511.06 | Reinforcing Steel (Grade 40) | LBS. | 17,070. | 20,000. | | 37,070. |
| 511A.06 | Epoxy Coated Reinforcing Steel (Grade 40) | LBS. | | 26,660. | | 26,660. |
| 514.06 (E) | Steel Piling (HP 12" x 42") | L.F. | 1,761. | | | 1,761. |
| 514.06 (F) | Steel Piling Splices (HP 10" x 42") | EA. | 26. | | | 26. |
| 515.06 | Penetrating Water Repellent Surface Treatment | S.Y. | 93.4 | 308.3 | 2,016.0 | 2,417.7 |
| 516A.06 (A) | Drilled Shafts (60" Diameter) | L.F. | | 476.0 | | 476.0 |
| 516A.06 (C) | Corr. Galv. Steel Pipe (60" Dia.) | L.F. | | 356.0 | | 356.0 |
| 516A.06 (D) | Obstructions | C.Y. | | | | 35. |
| Sp | Strip Seal Type Expansion Joint W/ Angles | L.F. | | | 159.6 | 159.6 |
| 519.06 (D) | Removal of Existing Bridge Structure | L.SUN | | | | 1. |
| 802.06 (A) | 1" Galv. Steel Elct. Conduit | L.F. | | 2.3 | | 2.3 |
| Sp | 4" Concrete Slope Wall | S.Y. | | | | 1,844. |
| 503.06 (B) | Concrete Parapet | L.F. | 72.0 | | 495.4 | 567.4 |

① A maximum of one pile splice will be allowed for each pile.

INDEX OF SHEETS

| SHEET NO. | TITLE |
|-----------|--|
| 49 | GENERAL PLAN AND ELEVATION AND SUMMARY OF QUANTITIES |
| 50 | BRIDGE STAKING DIAGRAM |
| 51 & 52 | ABUTMENT DETAILS |
| 53 & 54 | PIER DETAILS |
| 55 - 60 | SUPERSTRUCTURE DETAILS |
| | STD. P-1 |
| | STD. EXP-1 |
| | STD. DEB-5-3 |
| | STD. GRAU-3-2 |
| | STD. GRH-2-2 |
| | STD. SSIF-1-0 |
| | STD. SSG-1-5 |
| | STD. AS-SW-4 |
| | STD. SFP-1-0 |



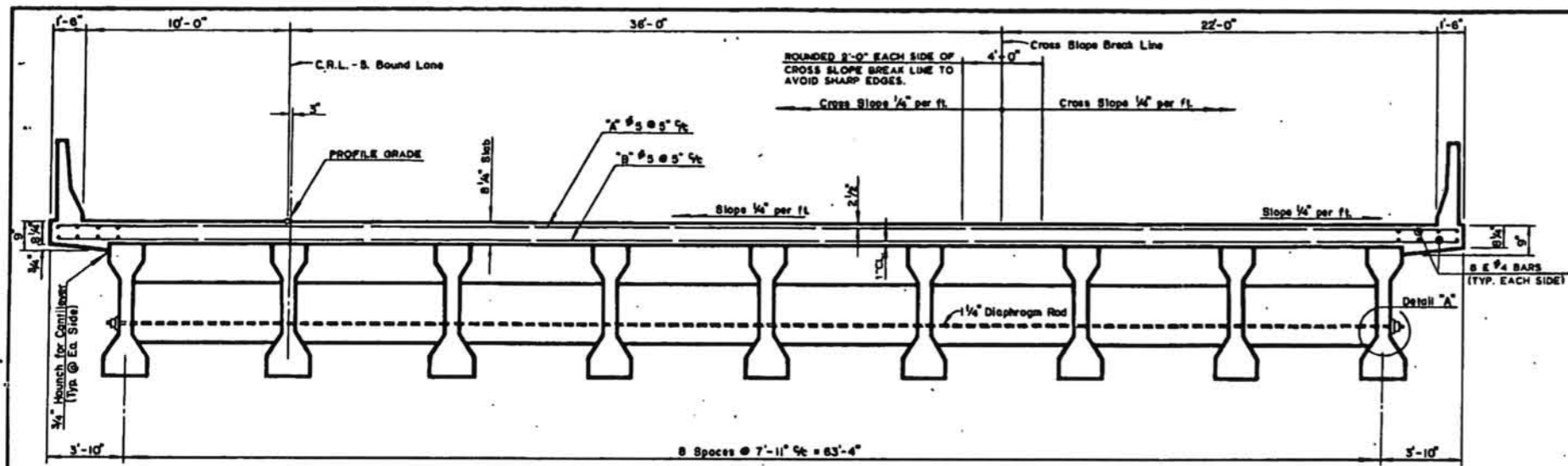
I-35 BRIDGE OVER S.L. & S.F. RAILROAD
 75'-100'-75' PRESTRESSED CONCRETE BEAM SPANS
 68'-0" CLEAR ROADWAY WITH 1'-4" PARAPETS ON EACH SIDE
 SKEWED 60° 57' 11.45" LT. FWD.

STRUCTURE "A" @ STA. 311+90.918
GENERAL PLAN AND ELEVATION AND SUMMARY OF QUANTITIES

F.A. Project No. I-35-3 (102)126 Sheet No. 49

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

| REV. NO. | DATE | BY | CHKD. | APPD. | TITLE |
|-------------|-------|----|-------|-------|-------|
| 1 | 08.14 | | | | |
| REVISIONS | | | | | |
| DESCRIPTION | DATE | | | | |



BRIDGE SECTION

SPECIAL NOTES:
CONCRETE SURFACE FINISH

NOTE "B"
These surfaces indicated in detail shall be finished in accordance with Section 509.04 of the Std. Specifications and the material used shall be approved by the Engineer prior to application. The Concrete Surface Finish for these areas shall have a Color No. 36586. Color chips shall be submitted to the Engineer for approval of color and texture prior to application of the material.

NOTE "C"
These surfaces indicated in detail shall be finished in accordance with Section 509.04 of the Std. Specifications and the material used shall be approved by the Engineer prior to application. The color for these areas shall be Color No. 33551, with shade of color to be selected by the Engineer after observing color chips for both colors. Color chips shall be submitted to the Engineer for approval of color and texture prior to the application of the material.

All cost of concrete finish shall be included in the price bid for P.C. Beams, Class AA Concrete, or Concrete Parapet and no additional compensation will be allowed for color or texture of the finish.

FLOOR: 8 1/4" uniform slab thickness, 60'-0" Clear Roadway with concrete parapet each side. Class "AA" Concrete, Maximum aggregate 1 1/2" #. All construction shall conform to the 1976 Oklahoma Standard Specifications. Surface of Roadway shall slope as shown. All exposed edges shall have a 3/4" chamfer unless otherwise noted.

Transverse reinforcing steel in the floor slab shall begin and end with "A" and "B" Bars a maximum of 2" from end of concrete panels.

All reinforcing steel bars in top of slab shall be supported on approved metal high chairs. The maximum spacing of the metal chairs shall be 1/2 beam spacing or a maximum of 4'-0" on center.

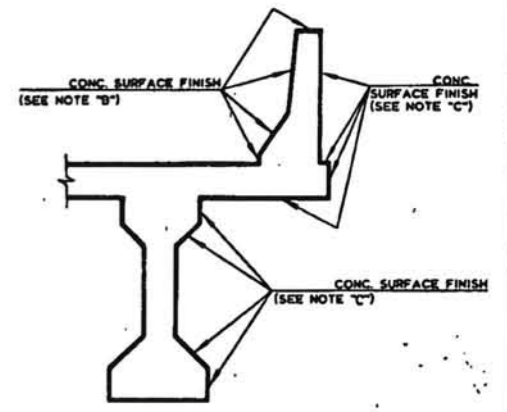
Bottom layer of reinforcing steel shall be supported on continuous steel slab spacers as shown.

Longitudinal bars shall begin and end 1" from ends of slab.

Minimum Compressive Strength for Class "AA" concrete shall be 3,000 psi at 28 days.

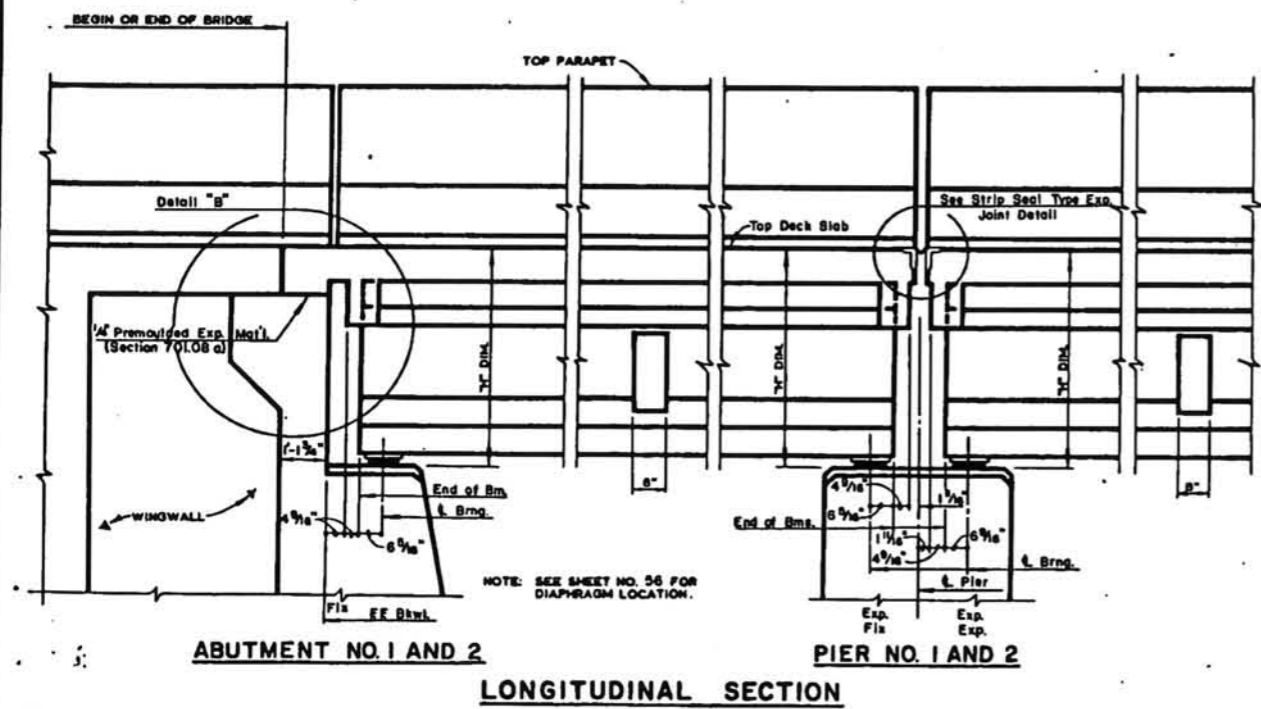
All reinforcing steel in Deck Slab, Diaphragms and Parapets shall be Grade 60.

Reference Bar List on Sheet No. 58 for Reinforcing Bars to be Epoxy Coated. Penetrating Water Repellent Surface Treatment shall be applied to the Deck Slab and Roadway Face of Parapets.



NOTE: Reference STD. SFP-1 for Parapet Details and Notes. Parapets shall be constructed without drain openings.

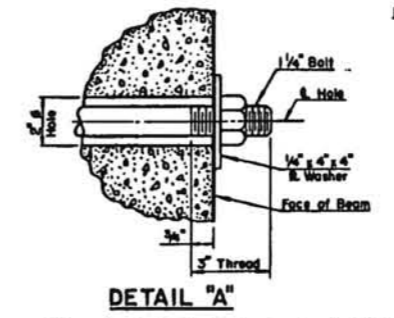
CONCRETE FINISHING DETAIL



ABUTMENT NO. 1 AND 2

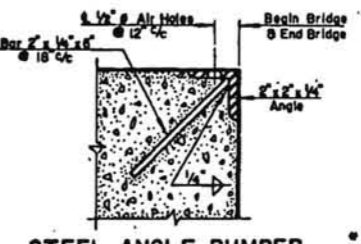
PIER NO. 1 AND 2

LONGITUDINAL SECTION



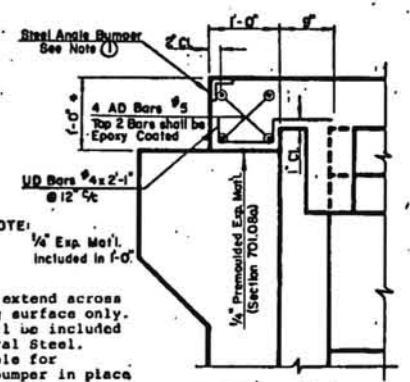
DETAIL "A"

All cost of 1 1/4" Bolts to be included in the unit price bid for Structural Steel. Paint Bolt End, Nut and Washer with two coats of zinc-rich paint.



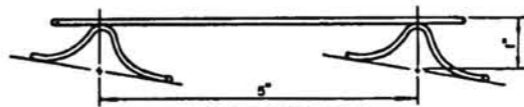
STEEL ANGLE BUMPER DETAIL

NOTE: The Steel Angle Bumper shall extend across the full width of the driving surface only. All cost of steel bumper shall be included in the price bid for Structural Steel. Contractor shall be responsible for adequately supporting steel bumper in place.



DETAIL "B"

Structural Steel Quantities shown include quantities for Steel Angle Bumpers, Diaphragm Rod Assemblies and P.C. Beam Bearing Assemblies. All Structural Steel shall be A36 Steel. All cost for Elastomeric Bearing Pads shall be included in the price bid for other items of work. Reference Sheet No. 55 for Concrete Finishing Notes.



SLAB SPACER

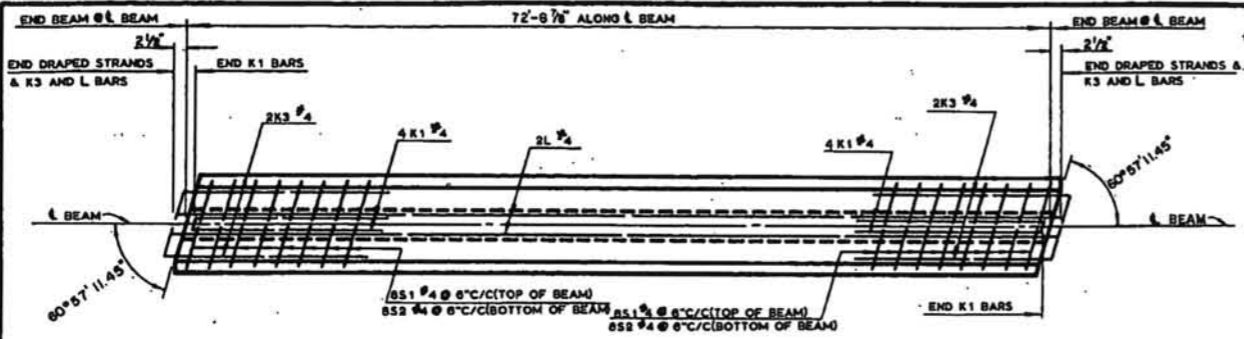
| ITEM | "H" DIM. @ |
|------------------------|------------|
| ABUTMENT NO. 1 | 4.582' |
| SPAN NO. 1, PIER NO. 1 | 4.583' |
| SPAN NO. 2, PIER NO. 1 | 5.344' |
| SPAN NO. 2, PIER NO. 2 | 5.344' |
| SPAN NO. 3, PIER NO. 2 | 4.582' |
| ABUTMENT NO. 2 | 4.583' |

① Dimension measured from Top of Slab to Bottom of Bearing Assembly.

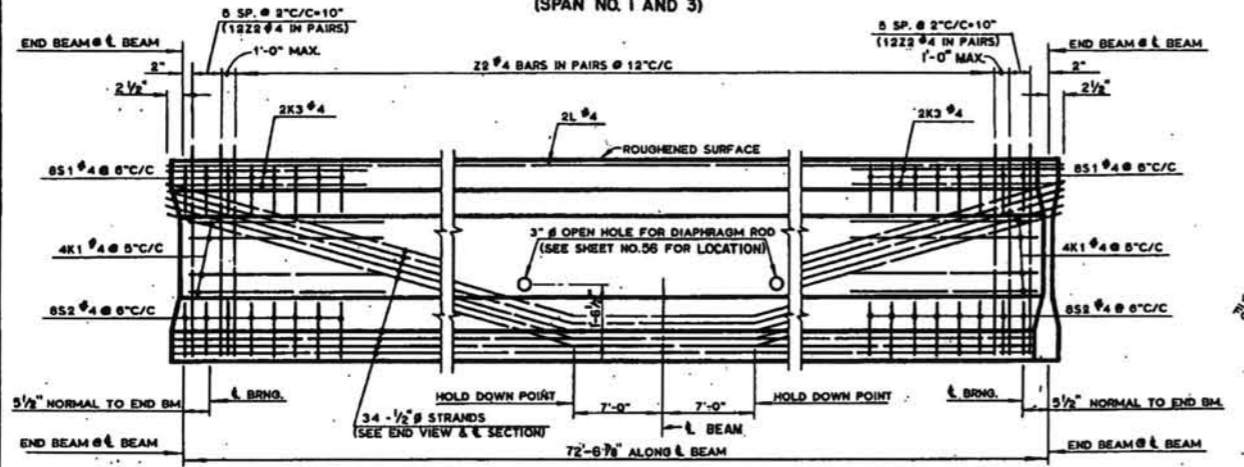
| QUANTITIES | | |
|---|------|----------|
| ITEM | UNIT | TOTAL |
| CLASS "AA" CONCRETE | C.Y. | 473.2 |
| REINFORCING STEEL (GRADE 60) | LBS. | 72,420 |
| STRUCTURAL STEEL (A36) | LBS. | 8,700 |
| P.C. BEAMS (TYPE III) | L.F. | 1,306.31 |
| P.C. BEAMS (TYPE IV) | L.F. | 890.58 |
| STRIP SEAL TYPE EXP. JT. w/ ANGLES | L.F. | 159.6 |
| EPOXY COATED REINFORCING STEEL (GRADE 60) | LBS. | 59,700 |
| PENET. WATER REPEL. SURFACE TREATMENT | S.Y. | 2,016 |
| CONCRETE PARAPET | L.F. | 495.4 |

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

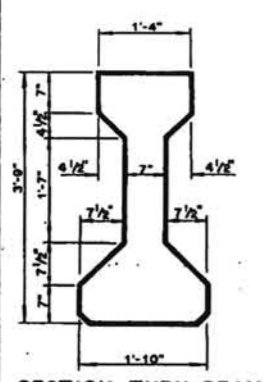
**STRUCTURE "A"
SUPERSTRUCTURE DETAILS**



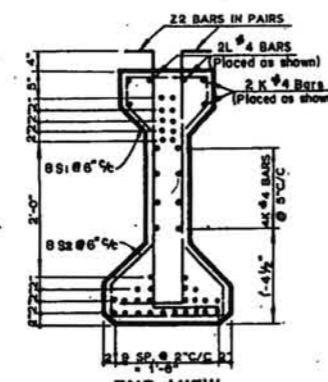
PLAN
(SPAN NO. 1 AND 3)



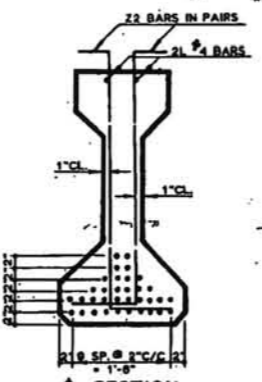
ELEVATION
(SPAN NO. 1 AND 3)



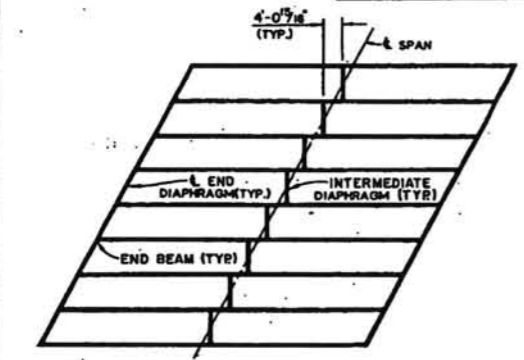
SECTION THRU BEAM
TYPE III PC. BM.



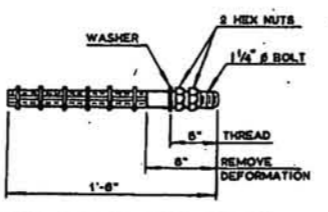
END VIEW
WITH REINFORCING



SECTION
(34 - 1/2 #8 STRANDS)



BEAM AND DIAPHRAGM LAYOUT

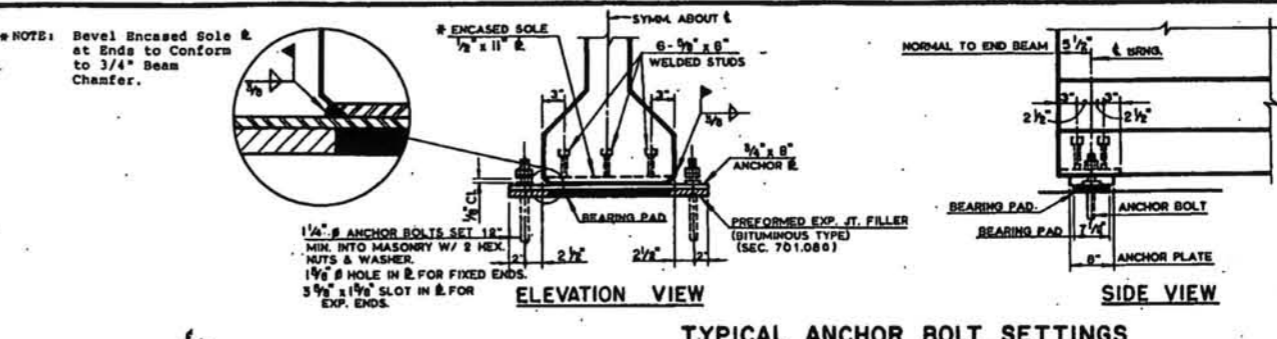


DETAIL OF ANCHOR BOLT

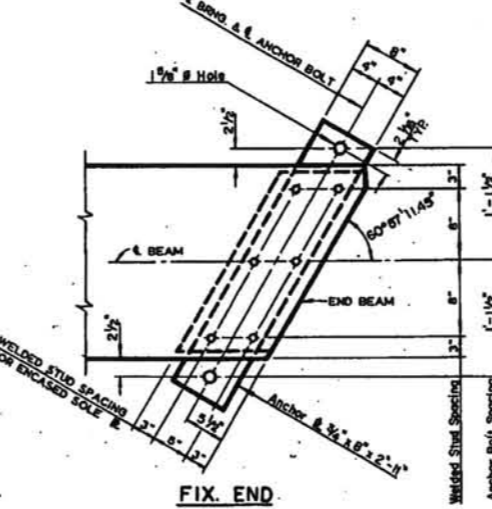
PRESTRESSED CONCRETE BEAMS

Chamfer all exposed edges of P.C. Beams 3/4" unless otherwise noted.
FORMS & PALLETS - All beams shall be cast in concrete floored pallets and metal forms.
FINISH - Top of beams to be rough floated. At approximately the time of initial set, entire top of beam shall be scrubbed transversely with coarse wire brush to remove all laitance and to produce a roughened surface for bonding slab.
CONCRETE - Concrete for beams shall have a min. 6,000 psi strength at 28 days. Where 6,000 psi concrete strength is specified for prestressed concrete beams, the contractor may increase the maximum cement requirements as specified in Section 501.02(a) of the Oklahoma Standard Specifications for Highway Construction to 8 sacks per cubic yard.
CEMENT - Type I or III Portland Cement may be used for the Prestressed Concrete Beams.
CYLINDER STRENGTH - At transfer of the tensioning load, the cylinder strength of the concrete shall be at least 5,000 psi for 6,000 psi concrete.
BEARING SURFACES - Concrete surfaces under all beam supports (bearing assembly) shall be ground with a carborundum brick before placement of bearing assembly to secure full bearing of assembly on concrete.
HANDLING - In the handling of the beams, they must be maintained in an upright position at all times and must be picked up from the lifting eye provided at the beam ends. Disregard of this requirement may lead to collapse of the member.
CABLE EXTENSIONS - All non-draped prestressing strands shall be cut off flush with the end of beam. All cut off strands that will be exposed are to be coated with two coats of an approved zinc rich paint (min. 6 mils).
STEEL - All strands shall be the size as shown on the plans. Initial load per strand shall be 70 percent of the breaking strength of strand.
SPECIFICATIONS FOR STEEL STRANDS - Type 270 K, 7-wire, uncoated, stress-relieved steel strand shall conform to the requirements of AASHTO Designation: M-203 (ASTM A-416).
SHOP DRAWINGS - The Contractor shall have his Prestressed Concrete Beam Fabricator furnish the Bridge Engineer, for his approval, two sets of checked shop drawings. One copy shall be returned to the fabricator approved or with any desired corrections indicated. The fabricator shall then furnish the Bridge Engineer with as many, generally five, corrected copies of the shop drawings as may be required for approval and distribution. The approval of the shop drawings in no way relieves the contractor or his fabricator of the responsibility for mistakes on the shop drawings. Shop drawings shall show the casting length center to center of bearing and the calculated prestressing shortening.
NOTE: Pretensioning force for all strands shall be 28,910 lbs. Maximum size of coarse aggregate in all beams shall be 1". All Bar Bends are out to out.

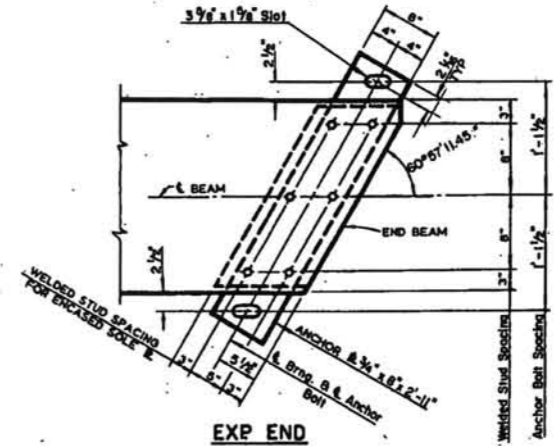
NOTE: Weight of bearing assembly (including plates, studs, anchor bolts and shop-welding on diaphragm bolt assembly for prestressed girder bridges is included in the structural steel quantities. All cost of material, labor, equipment and incidentals necessary to install diaphragm bolt and bearing assembly will be paid for at unit price bid for structural steel. All cost of Elastomeric Bearing Pads will be included in the price bid for other items of work.



TYPICAL ANCHOR BOLT SETTINGS



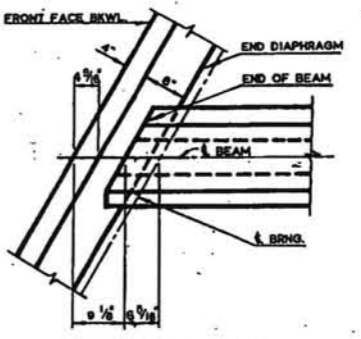
FIX. END



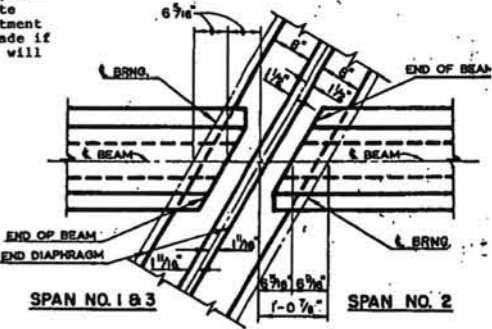
EXP. END

PLAN VIEW

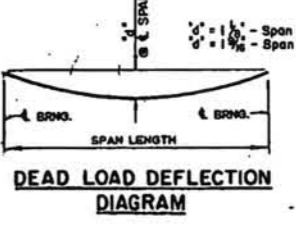
NOTE: 3/4" Anchor Plates for bearing assembly shall be match-marked, shipped loose, and field welded to the encased sole plates in order that minor horizontal adjustment of the bearing pad location may be made if necessary. Metal used in field weld will not be measured for payment.



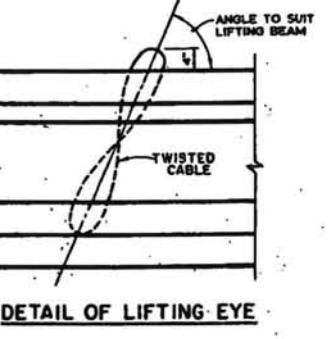
DETAIL AT ABUTMENT



DETAIL AT PIER



DEAD LOAD DEFLECTION DIAGRAM



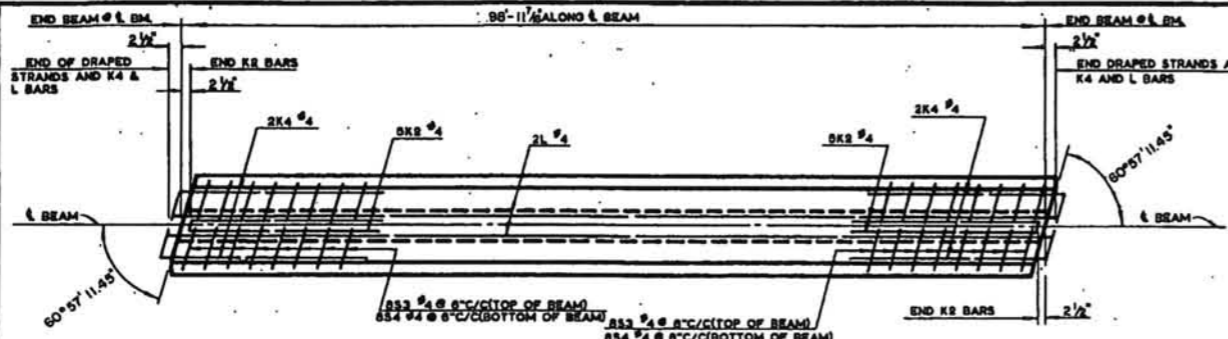
DETAIL OF LIFTING EYE

Reference Sheet No. 57 for Bearing Pad Sizes and Details.

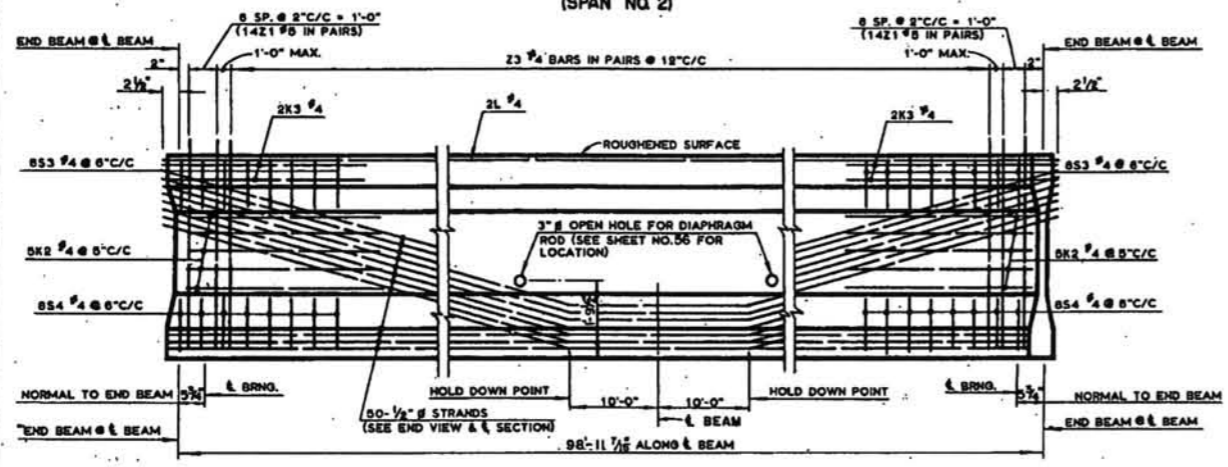
| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Equip | |

STRUCTURE "A"
SUPERSTRUCTURE DETAILS

| REV. NO. | DATE | BY | CHKD. | DESCR. | TOTAL SHEETS |
|----------|-------|----|-------|--------|--------------|
| 1 | 08.14 | | | | |

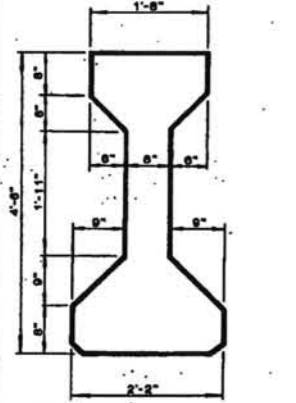


PLAN
(SPAN NO. 2)

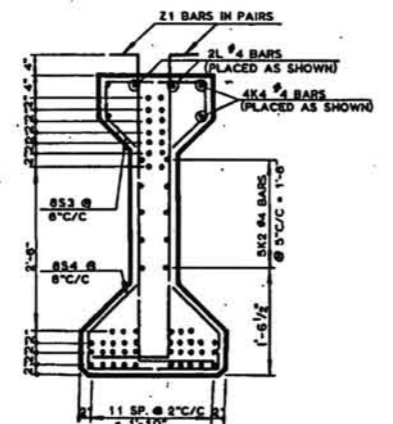


ELEVATION
(SPAN NO. 2)

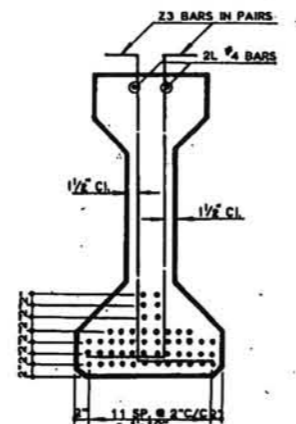
NOTE: Draped Strands, K4 and L Bars shall extend 2 1/2" beyond ends of beam. Straight strands shall be cut flush with ends of beam.



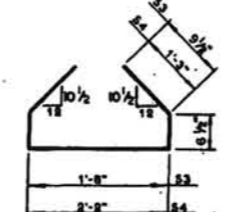
SECTION THRU BEAM
TYPE IV P.C. BEAM



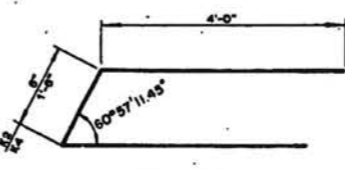
END VIEW
WITH REINFORCING



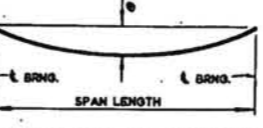
SECTION
(50-1/2" STRANDS)



S3 BARS #4 x 4'-4"
S4 BARS #4 x 5'-9"

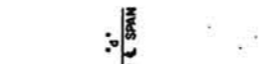


K2 #4 x 8'-6"
K4 #4 x 9'-6"

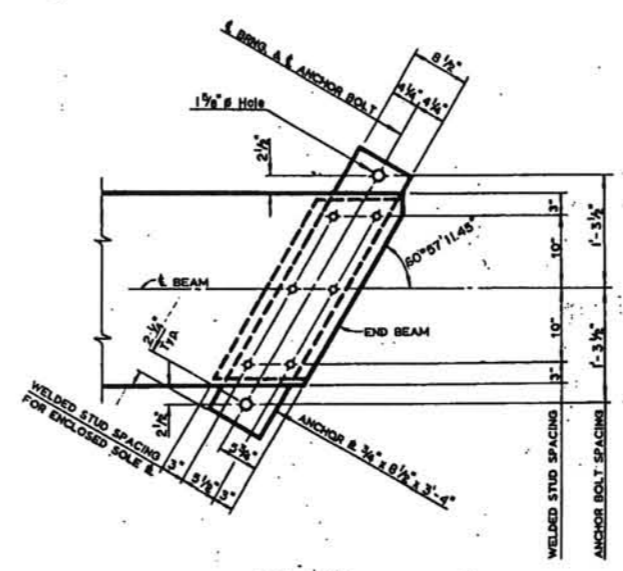


DEAD LOAD DEFLECTION
DIAGRAM

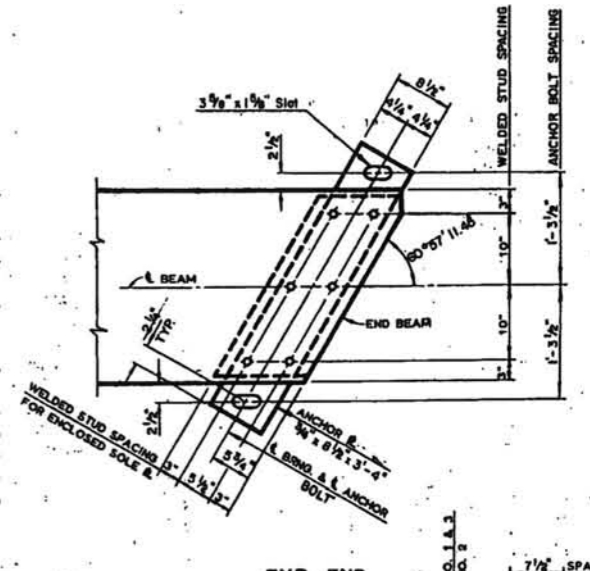
d' = 1 1/8" - SPAN NO. 1 & 3
d' = 1 3/8" - SPAN NO. 2



SPAN LENGTH



FIX. END



EXP. END

NOTE: 3/4" Anchor Plates for bearing assembly shall be match-marked, shipped loose, and field welded to the encased sole plate in order that minor horizontal adjustment of the bearing pad location may be made if necessary. Metal used in field weld will not be measured for payment.

NOTE: All Bearing Pads shall be centered between anchor bolts.

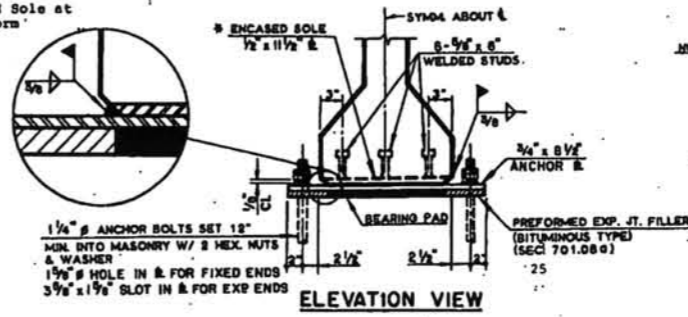
| ELASTOMERIC BEARING PAD SCHEDULE | | |
|---|------------------------|------------------|
| LOCATION | BEARING PAD SIZE | NO. OF LAMINATES |
| ABUTMENT NO. 1 & 2 | 7 1/2" x 1" (60 DURO.) | 1 |
| SPAN NO. 1, PIER NO. 1 & SPAN NO. 3, PIER NO. 2 | 7 1/2" x 1" (60 DURO.) | 1 |
| SPAN NO. 2, PIER NO. 2 & SPAN NO. 3, PIER NO. 1 | 8" x 1 1/2" (60 DURO.) | 1 |

Reference Sheet No. 58 for the following:
1. Detail of Lifting Eye.
2. Beam and Diaphragm Layout.
3. Detail of Anchor Bolt.
4. General Notes.
5. Detail at Pier.

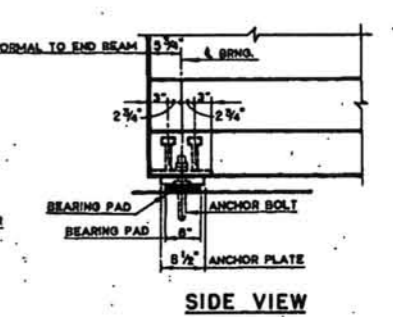
| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

STRUCTURE "A"
SUPERSTRUCTURE DETAILS

NOTE: Bevel Encased Sole at Ends to Conform to 3/4" Beam Chamfer.

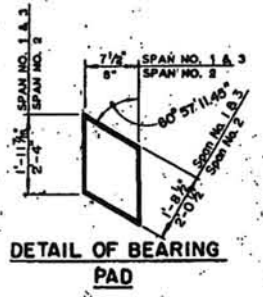


ELEVATION VIEW



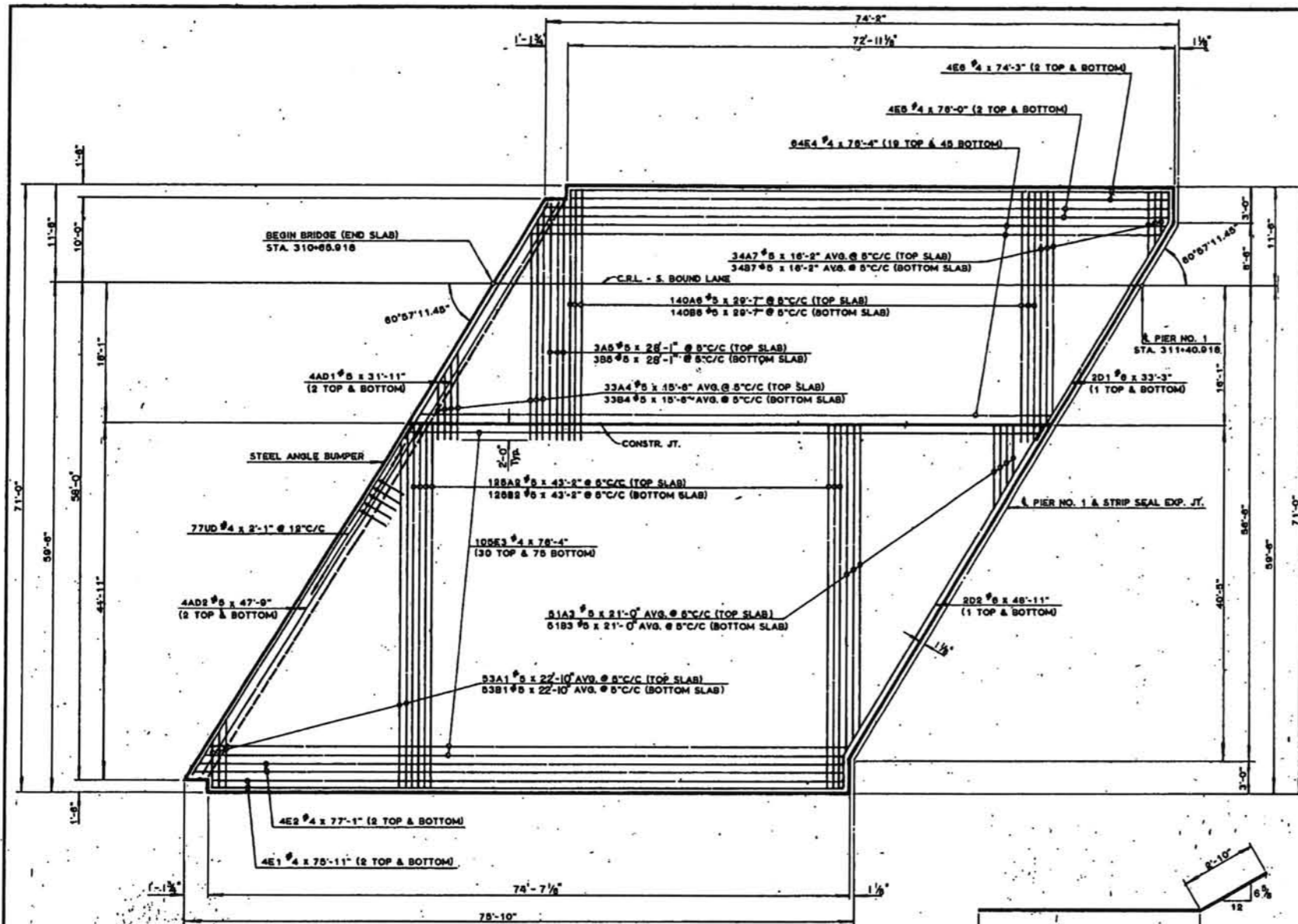
SIDE VIEW

TYPICAL ANCHOR BOLT SETTINGS

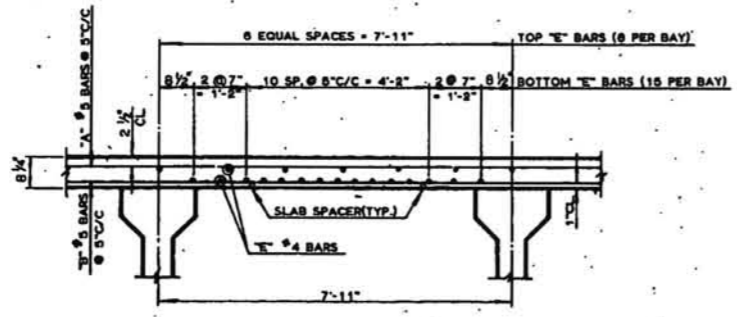


DETAIL OF BEARING
PAD

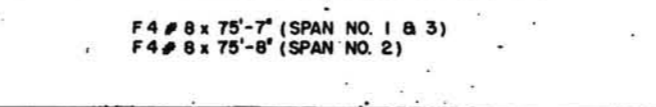
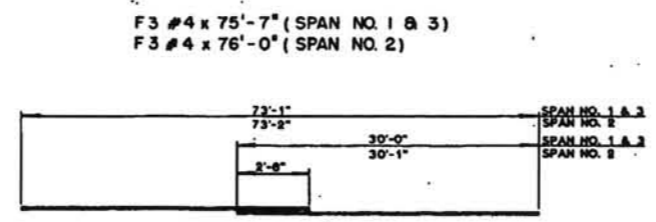
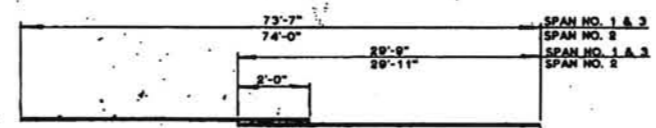
| REV. | DATE | BY | CHKD. | APP'D. | TITLE |
|------|------|----|-------|--------|--------------------------------------|
| 1 | | | | | STRUCTURE 'A' SUPERSTRUCTURE DETAILS |



SPAN NO. 1 - DECK REINFORCING PLAN



TYPICAL DECK REINFORCING



| BAR LIST (Grade 60) | | | | | |
|---------------------|-----|------|------|--------------|-----------|
| MARK | NO. | SIZE | FORM | LENGTH | |
| AD1 | 4 | # 5 | Str. | 32'-1" | |
| AD2 | 4 | # 5 | Str. | 47'-9" | |
| B1 | 53 | # 5 | Str. | 22'-10" Avg. | (4) |
| B2 | 125 | # 5 | Str. | 43'-2" | |
| B3 | 51 | # 5 | Str. | 21'-0" Avg. | (5) |
| B4 | 33 | # 5 | Str. | 15'-6" Avg. | (6) |
| B5 | 3 | # 5 | Str. | 28'-1" | |
| B6 | 140 | # 5 | Str. | 29'-7" | |
| B7 | 34 | # 5 | Str. | 16'-2" Avg. | (7) |
| B8 | 53 | # 5 | Str. | 23'-0" Avg. | (8) |
| B9 | 185 | # 5 | Str. | 43'-2" | |
| B10 | 51 | # 5 | Str. | 21'-0" Avg. | (9) |
| B11 | 32 | # 5 | Str. | 14'-7" Avg. | (10) |
| B12 | 205 | # 5 | Str. | 29'-7" | |
| B13 | 34 | # 5 | Str. | 16'-0" Avg. | (11) |
| B14 | 53 | # 5 | Str. | 23'-0" Avg. | (12) |
| B15 | 122 | # 5 | Str. | 43'-2" | |
| B16 | 3 | # 5 | Str. | 41'-9" | |
| B17 | 52 | # 5 | Str. | 21'-9" Avg. | (13) |
| B18 | 32 | # 5 | Str. | 14'-7" Avg. | (14) |
| B19 | 144 | # 5 | Str. | 29'-7" | |
| B20 | 35 | # 5 | Str. | 16'-0" Avg. | (15) |
| D1 | 4 | # 6 | Bnt. | 33'-3" | |
| D2 | 4 | # 6 | Bnt. | 48'-11" | |
| E1 | 2 | # 4 | Str. | 75'-11" | (2) (3) |
| E2 | 2 | # 4 | Str. | 77'-1" | (2) (3) |
| E3 | 75 | # 4 | Str. | 76'-4" | (2) (3) |
| E4 | 45 | # 4 | Str. | 76'-4" | (2) (3) |
| E5 | 2 | # 4 | Str. | 76'-0" | (2) (3) |
| E6 | 2 | # 4 | Str. | 74'-3" | (2) (3) |
| E7 | 79 | # 4 | Str. | 101'-2" | (2) (3) |
| E8 | 49 | # 4 | Str. | 101'-2" | (2) (3) |
| E9 | 2 | # 4 | Str. | 74'-3" | (2) (3) |
| E10 | 2 | # 4 | Str. | 76'-0" | (2) (3) |
| E11 | 75 | # 4 | Str. | 76'-4" | (2) (3) |
| E12 | 45 | # 4 | Str. | 76'-4" | (2) (3) |
| E13 | 2 | # 4 | Str. | 77'-1" | (2) (3) |
| E14 | 2 | # 4 | Str. | 75'-11" | (2) (3) |
| F1 | 32 | # 8 | Bnt. | 9'-10" | (17) (21) |
| F2 | 16 | # 8 | Bnt. | 9'-11" | (17) (22) |
| F3 | 32 | # 4 | Str. | 75'-7" | (23) |
| F4 | 2 | # 4 | Str. | 76'-0" | (23) (24) |
| F5 | 64 | # 4 | Str. | 75'-7" | (23) (24) |
| F6 | 2 | # 4 | Str. | 75'-8" | (23) (24) |
| F7 | 64 | # 4 | Str. | 61'-9" | (23) |
| F8 | 32 | # 4 | Str. | 61'-9" | (24) |
| U1 | 112 | # 4 | Bnt. | 3'-10" | (16) (17) |
| U2 | 56 | # 4 | Bnt. | 4'-6" | (17) (18) |
| U3 | 288 | # 4 | Bnt. | 3'-1" | (17) (19) |
| U4 | 144 | # 4 | Bnt. | 3'-6" | (17) (20) |
| UD | 154 | # 4 | Bnt. | 2'-1" | (1) |

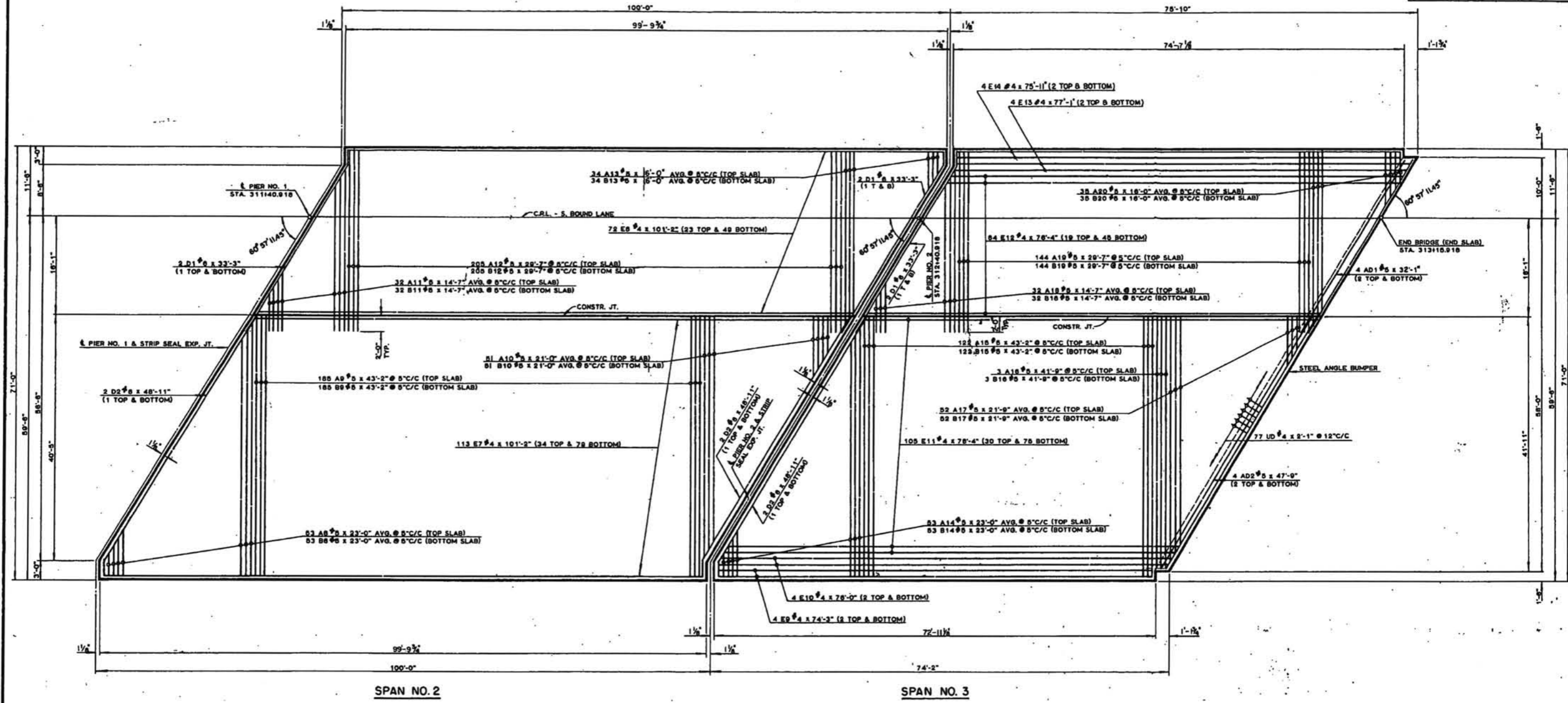
| BAR LIST (Grade 60, Epoxy Coated) | | | | | |
|-----------------------------------|-----|------|------|--------------|---------|
| MARK | NO. | SIZE | FORM | LENGTH | |
| A1 | 53 | # 5 | Str. | 22'-10" Avg. | (4) |
| A2 | 125 | # 5 | Str. | 43'-2" | |
| A3 | 51 | # 5 | Str. | 21'-0" Avg. | (5) |
| A4 | 33 | # 5 | Str. | 15'-6" Avg. | (6) |
| A5 | 3 | # 5 | Str. | 28'-1" | |
| A6 | 140 | # 5 | Str. | 29'-7" | |
| A7 | 34 | # 5 | Str. | 16'-2" Avg. | (7) |
| A8 | 53 | # 5 | Str. | 23'-0" Avg. | (8) |
| A9 | 185 | # 5 | Str. | 43'-2" | |
| A10 | 51 | # 5 | Str. | 21'-0" Avg. | (9) |
| A11 | 32 | # 5 | Str. | 14'-7" Avg. | (10) |
| A12 | 205 | # 5 | Str. | 29'-7" | |
| A13 | 34 | # 5 | Str. | 16'-0" Avg. | (11) |
| A14 | 53 | # 5 | Str. | 23'-0" Avg. | (12) |
| A15 | 122 | # 5 | Str. | 43'-2" | |
| A16 | 3 | # 5 | Str. | 41'-9" | |
| A17 | 52 | # 5 | Str. | 21'-9" Avg. | (13) |
| A18 | 32 | # 5 | Str. | 14'-7" Avg. | (14) |
| A19 | 144 | # 5 | Str. | 29'-7" | |
| A20 | 35 | # 5 | Str. | 16'-0" Avg. | (15) |
| AD1 | 4 | # 5 | Str. | 32'-1" | |
| AD2 | 4 | # 5 | Str. | 47'-9" | |
| D1 | 4 | # 6 | Bnt. | 33'-3" | |
| D2 | 4 | # 6 | Bnt. | 48'-11" | |
| E1 | 2 | # 4 | Str. | 75'-11" | (2) (3) |
| E2 | 2 | # 4 | Str. | 77'-1" | (2) (3) |
| E3 | 30 | # 4 | Str. | 76'-4" | (2) (3) |
| E4 | 19 | # 4 | Str. | 76'-4" | (2) (3) |
| E5 | 2 | # 4 | Str. | 76'-0" | (2) (3) |
| E6 | 2 | # 4 | Str. | 74'-3" | (2) (3) |
| E7 | 34 | # 4 | Str. | 101'-2" | (2) (3) |
| E8 | 23 | # 4 | Str. | 101'-2" | (2) (3) |
| E9 | 2 | # 4 | Str. | 74'-3" | (2) (3) |
| E10 | 2 | # 4 | Str. | 76'-0" | (2) (3) |
| E11 | 30 | # 4 | Str. | 76'-4" | (2) (3) |
| E12 | 19 | # 4 | Str. | 76'-4" | (2) (3) |
| E13 | 2 | # 4 | Str. | 77'-1" | (2) (3) |
| E14 | 2 | # 4 | Str. | 75'-11" | (2) (3) |
| EP | 498 | # 6 | Bnt. | 6'-6" | (A) |

- (1) Reference Sheet No. 55 for Bar Bend Detail.
- (2) Includes 1'-6" Lap, Minimum.
- (3) Stagger Splices.
- (4) 3'-4" to 42'-4"
- (5) 2'-6" to 39'-6"
- (6) 3'-6" to 27'-6"
- (7) 3'-8" to 28'-8"
- (8) 3'-6" to 42'-6"
- (9) 2'-6" to 39'-6"
- (10) 3'-0" to 26'-2"
- (11) 3'-8" to 28'-4"
- (12) 3'-6" to 42'-6"
- (13) 2'-6" to 41'-0"
- (14) 3'-0" to 26'-2"
- (15) 3'-4" to 28'-8"
- (16) Vertical in Intermediate Diaphragms (Span No. 1 & 3).
- (17) Reference Sheet No. 60 for Bar Bend Details.
- (18) Vertical in Intermediate Diaphragms (Span No. 2).
- (19) Vertical in End Diaphragms (Span No. 1 & 3).
- (20) Vertical in End Diaphragms (Span No. 2).
- (21) Span No. 1 & 3 End Diaphragms.
- (22) Span No. 2 End Diaphragms.
- (23) Horizontal in Intermediate Diaphragms (Span No. 1 & 3).
- (24) Horizontal in Intermediate Diaphragms (Span No. 2).
- (25) See Bar Detail, this Sheet.

(A) Reference Std. SPP-1 for bar bend details and notes.

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

| NO. | DATE | BY | REVISION |
|-----|------|----|----------|
| | | | |
| | | | |
| | | | |

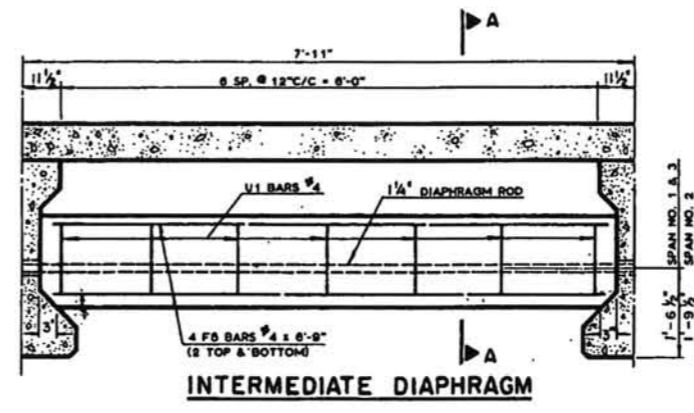


DECK REINFORCING PLAN

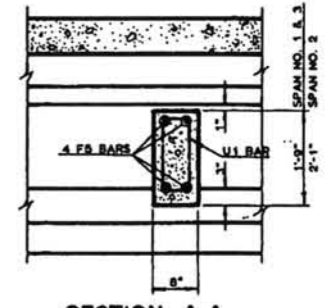
| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

STRUCTURE "A"
SUPERSTRUCTURE DETAILS
 F.A. Project No. 1-35-3(107) 126 Sheet No. 59

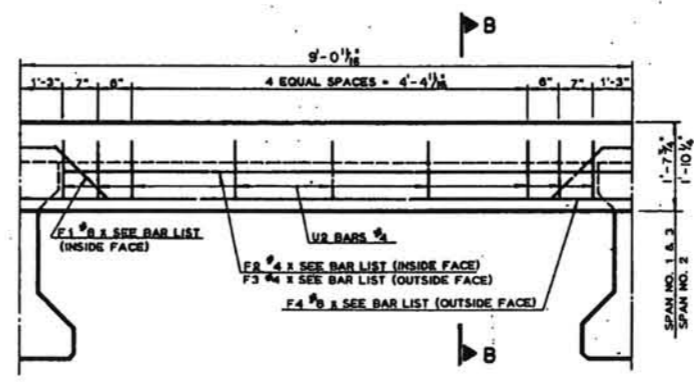
| NO. | DATE | BY | CHKD. | REVISIONS |
|-----|-------|----|-------|-----------|
| 1 | 08/14 | | | |



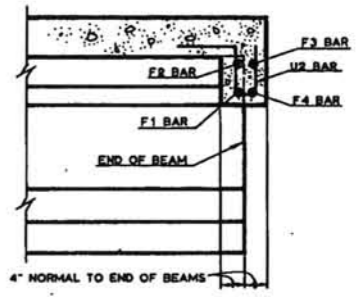
INTERMEDIATE DIAPHRAGM



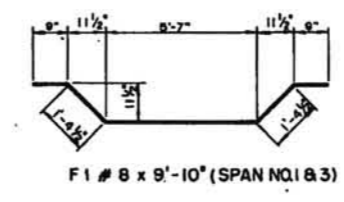
SECTION A-A



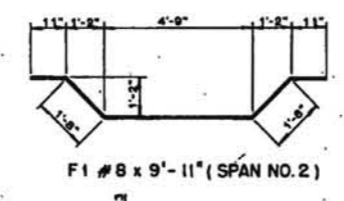
END DIAPHRAGM



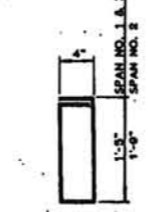
SECTION B-B



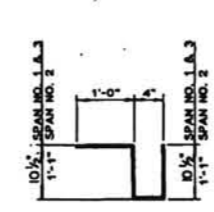
F1 #8 x 9'-10" (SPAN NO. 1 & 3)



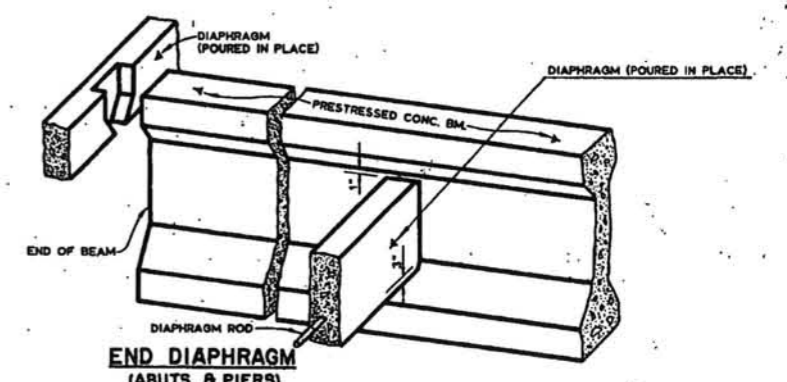
F1 #8 x 9'-11" (SPAN NO. 2)



U1 #4 x 3'-10" (SPAN NO. 1 & 3)



U2 #4 x 3'-6" (SPAN NO. 2)



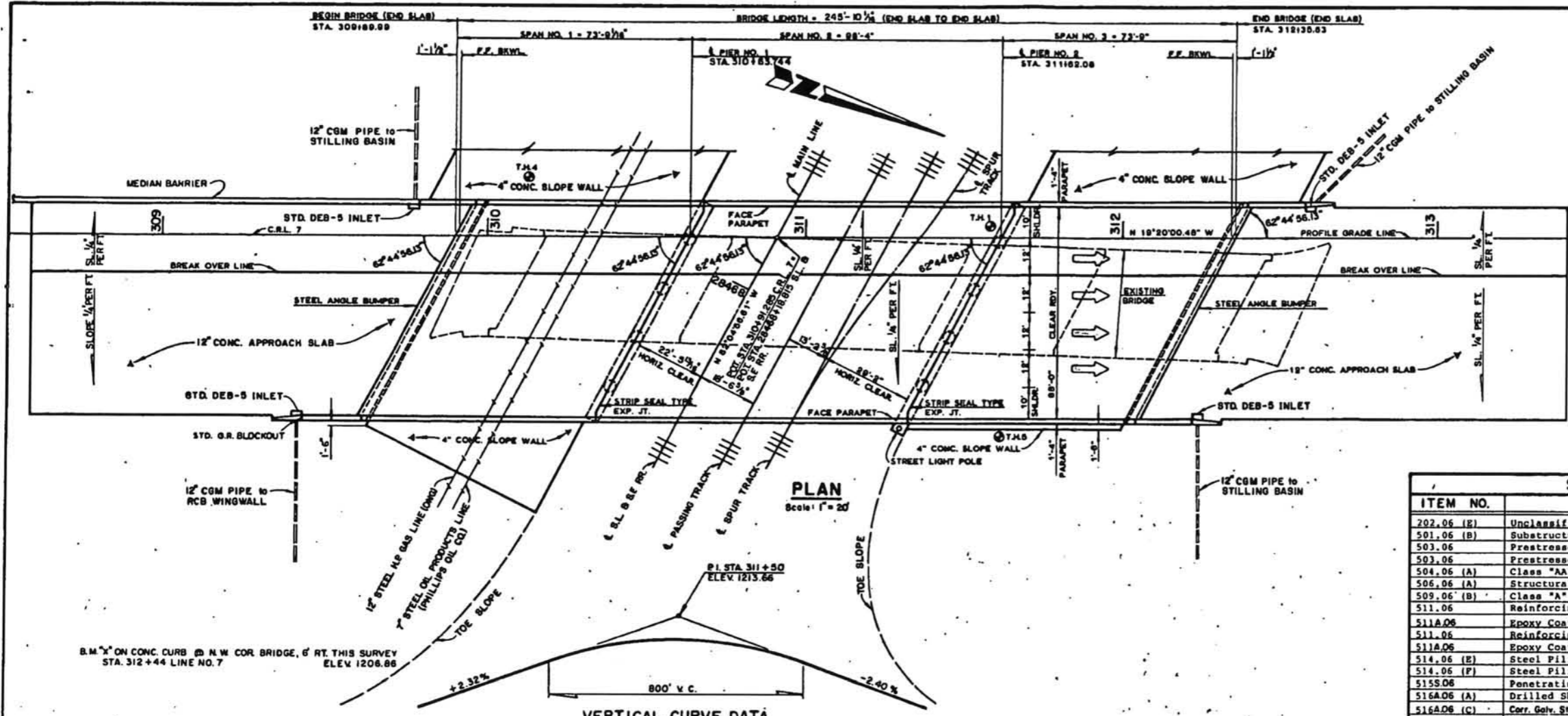
END DIAPHRAGM (ABUTS. & PIERS)
INTERMEDIATE DIAPHRAGM
PICTORIAL VIEW INTERIOR BEAM SHOWN

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

STRUCTURE "A"
SUPERSTRUCTURE DETAILS

NOTE: ALL BAR BENDS ARE OUT TO OUT.

| NO. | DATE | BY | REVISION |
|-----|-------|----|----------|
| 1 | 06.14 | | |



DESIGN DATA

DESIGN: AASHTO SPECIFICATIONS, 1976 EDITION WITH SUPPLEMENTS

LIVE LOAD: HS 20-44

STRUCTURAL STEEL: 20,000 psi

REINFORCING STEEL: (GR. 40) 20,000 psi

REINFORCING STEEL: (GR. 60) 24,000 psi

CONCRETE:

CLASS "A" 1,000 psi

CLASS "AA" 1,200 psi

MAXIMUM FOUNDATION LOADS:

ABUTMENTS 43.0 TONS/PILE

PIERS

FRICITION CAPACITY 424.1 TONS/SHAFT

BEARING CAPACITY 117.8 TONS/SHAFT

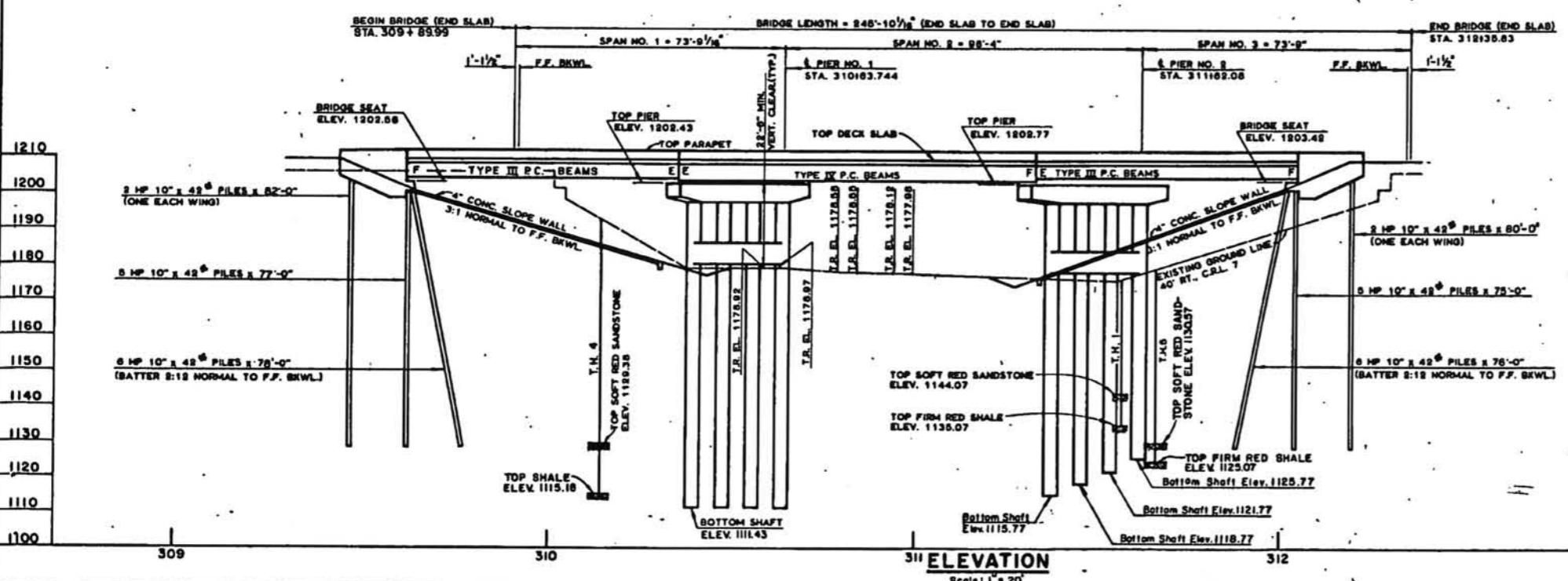
TOTAL CAPACITY 541.9 TONS/SHAFT

MAX. SHAFT LOAD 414.0 TONS/SHAFT

ALLOWABLE FRICTION LOAD 1.8 TONS/S.F.

ALLOWABLE POINT BEARING LOAD 5.0 TONS/S.F.

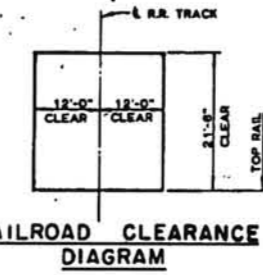
VERTICAL CURVE DATA



SUMMARY OF QUANTITIES

| ITEM NO. | ITEM | UNIT | ABUTS. | PIERS | SUPSTR. | TOTAL |
|-------------|---|-------|--------|--------|----------|----------|
| 202.06 (E) | Unclassified Borrow | C.Y. | 176 | | | 176 |
| 501.06 (B) | Substructure Excavation, Common | C.Y. | 146 | | | 146 |
| 503.06 | Prestressed Concrete Beam (Type III) | L.F. | | | 1,284.56 | 1,284.56 |
| 503.06 | Prestressed Concrete Beam (Type IV) | L.F. | | | 875.72 | 875.72 |
| 504.06 (A) | Class "AA" Concrete | C.Y. | | 471.7 | | 471.7 |
| 506.06 (A) | Structural Steel (A36) | LBS. | | 471.7 | 8,600 | 8,770 |
| 509.06 (B) | Class "A" Concrete | C.Y. | 139.9 | 290.1 | | 429.6 |
| 511.06 | Reinforcing Steel (Grade 60) | LBS. | | | 72,115 | 72,115 |
| 511A.06 | Epoxy Coated Reinforcing Steel (Grade 60) | LBS. | | | 58,035 | 58,035 |
| 511.06 | Reinforcing Steel (Grade 40) | LBS. | 16,980 | 20,520 | | 37,500 |
| 511B.06 | Epoxy Coated Reinforcing Steel (Grade 40) | LBS. | | 25,740 | | 25,740 |
| 514.06 (E) | Steel Piling (HP 10 x 42) | L.F. | 2,008 | | | 2,008 |
| 514.06 (F) | Steel Piling Splices (HP 10 x 42) (1) | EA. | 26 | | | 26 |
| 515S.06 | Penetrating Water Repellent Surface Treatment | S.Y. | 91.8 | 356.1 | 1,982.5 | 2,430.2 |
| 516A.06 (A) | Drilled Shafts (60" Diameter) | L.F. | | 487.0 | | 487.0 |
| 516A.06 (C) | Corr. Galv. Steel Pipe (60" Dia.) | L.F. | | 367.0 | | 367.0 |
| 516A.06 (D) | Obstructions | C.Y. | | | | 34 |
| Sp. | Strip Seal Type Expansion Joint w/ Angles | L.F. | | | 157.2 | 157.2 |
| 619.06 (D) | Removal of Existing Bridge Structure | L.SUM | | | | 1 |
| 802.06 (A) | 1" Galv. Steel Elcut. Conduit | L.F. | | 2.3 | | 2.3 |
| Sp. | 4" Concrete Slope Wall | S.Y. | | | | 1,796 |
| 505.06 (B) | Concrete Parapet | L.F. | 72.0 | | 487.18 | 559.18 |

① A maximum of one pile splice will be allowed for each pile.



RAILROAD CLEARANCE DIAGRAM

NOTE: Falsework Clearance required by Railway for operation during construction. Dimensions are at right angle to track.

INDEX OF SHEETS

| SHEET NO. | TITLE |
|-----------|--|
| 61 | GENERAL PLAN AND ELEVATION AND SUMMARY OF QUANTITIES |
| 62 | BRIDGE STAKING DIAGRAM |
| 63 & 64 | ABUTMENT DETAILS |
| 65 & 66 | PIER DETAILS |
| 67 - 72 | SUPERSTRUCTURE DETAILS |
| | STD. P-1 |
| | STD. EXP-1 |
| | STD. DEB-5-3 |
| | STD. GRAU-3-2 |
| | STD. GRH-2-2 |
| | STD. SSIF-1-0 |
| | STD. SSG-1-5 |
| | STD. AS-SW-4 |
| | STD. SFP-1-0 |

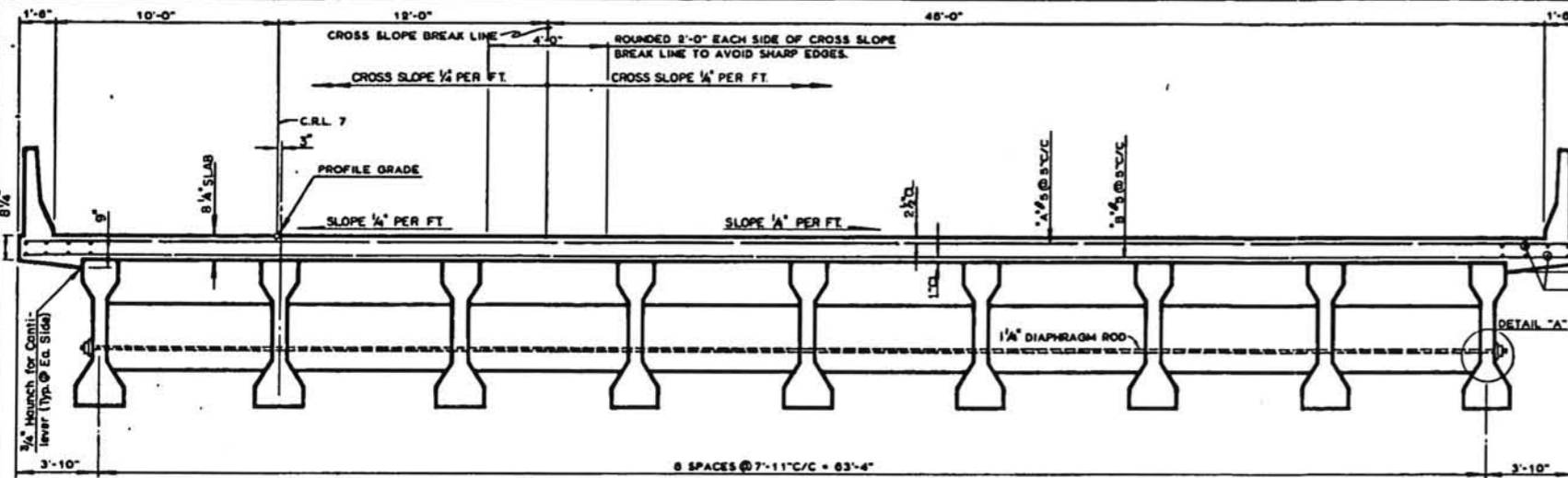
1-35 BRIDGE OVER S.L. & S.F. RAILROAD
 73'-9", 98'-4", 73'-9" PRESTRESSED CONCRETE BEAM SPANS
 68'-0" CLEAR ROADWAY WITH 1'-4" PARAPETS ON EACH SIDE
 SKEWED 62° 44' 56.13" LT. FWD.

STRUCTURE 'B' & STA. 311+12.91
GENERAL PLAN AND ELEVATION AND SUMMARY OF QUANTITIES

Design _____
 Drawn _____
 Checked _____
 Approved _____
 Squad _____

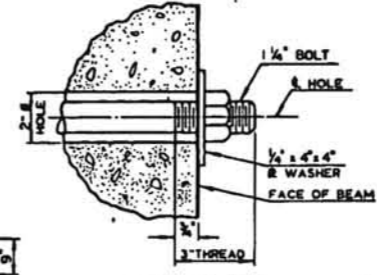
F.A. Project No. 1-35-3 (107)126 Sheet No. 61

| REV. NO. | DATE | BY | CHKD. | REVISIONS |
|----------|------|----|-------|-----------|
| | | | | |



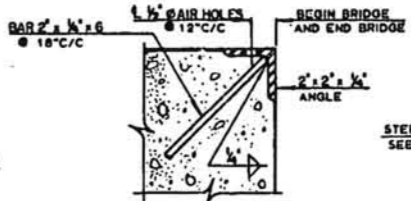
BRIDGE SECTION

FLOOR: 8 1/4" uniform slab thickness, 68'-0" Clear Roadway with concrete parapet each side. Class "AA" Concrete, Maximum aggregate 1 1/2". All construction shall conform to the 1976 Oklahoma Standard Specifications. Surface of Roadway shall slope as shown. All exposed edges shall have a 3/4" chamfer unless otherwise noted. Transverse reinforcing steel in the floor slab shall begin and end with "A" and "B" Bars a maximum of 2' from end of concrete panels. All reinforcing steel bars in top of slab shall be supported on approved metal high chairs. The maximum spacing of the metal chairs shall be 1/2 beam spacing or a maximum of 4'-0" on center. Bottom layer of reinforcing steel shall be supported on continuous steel slab spacers as shown. Longitudinal bars shall begin and end 1' from ends of slab. Minimum Compressive Strength for Class "AA" Concrete shall be 3,000 psi at 28 days. All reinforcing steel in Deck Slab, Diaphragms and Parapets shall be Grade 60. Reference Bar List on Sheet No. 70 for Reinforcing Bars to be Epoxy Coated. Penetrating Water Repellent Surface Treatment shall be applied to the Deck Slab and Roadway Face of Parapets.



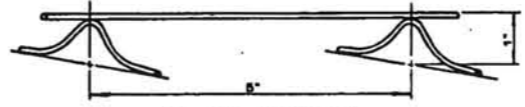
DETAIL "A"

NOTES
All cost of 1 1/4" Bolts to be included in the unit price bid for Structural Steel. Paint Bolt End, Nut and Washer with two coats of zinc-rich paint.



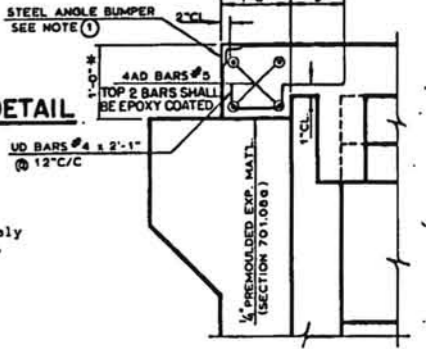
STEEL ANGLE BUMPER DETAIL

NOTE ①: The Steel Angle Bumper shall extend across the full width of the driving surface only. All cost of steel bumper shall be included in the price bid for Structural Steel. Contractor shall be responsible for adequately supporting steel bumper in place.



SLAB SPACER

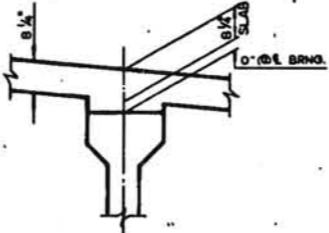
MAX. SPACING 4'-0" CTR. TO CTR.



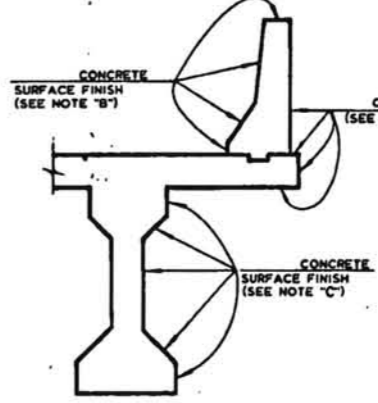
DETAIL "B"

NOTE * 1/2" EXP. MAT. INCLUDED IN 1'-0"

UD # 4 x 2'-1"



HAUNCH DETAIL

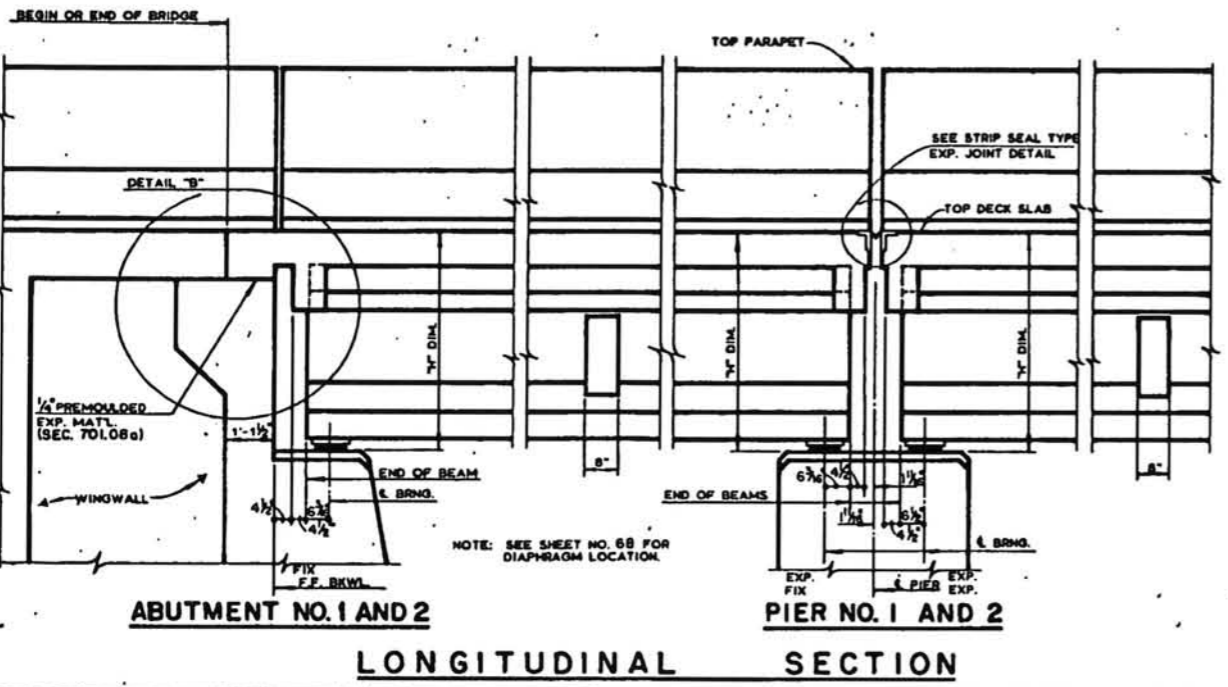


CONCRETE FINISHING DETAIL

| "H" DIMENSIONS | |
|------------------------|--------------|
| LOCATION | "H" DIM. (①) |
| ABUTMENT NO. 1 | 4.583' |
| SPAN NO. 1, PIER NO. 1 | 4.583' |
| SPAN NO. 2, PIER NO. 1 | 5.344' |
| SPAN NO. 2, PIER NO. 2 | 5.344' |
| SPAN NO. 3, PIER NO. 2 | 4.583' |
| ABUTMENT NO. 2 | 4.583' |

① Dimension measured from Top of Slab to Bottom of Bearing Assembly

Structural Steel Quantities shown include quantities for Steel Angle Bumpers, Diaphragm Rod Assemblies and P.C. Beam Bearing Assemblies. All Structural Steel shall be A36 Steel. All cost for Elastomeric Bearing Pads shall be included in the price bid for other items of work. Reference Sheet No. 55 for Concrete Finishing Notes.



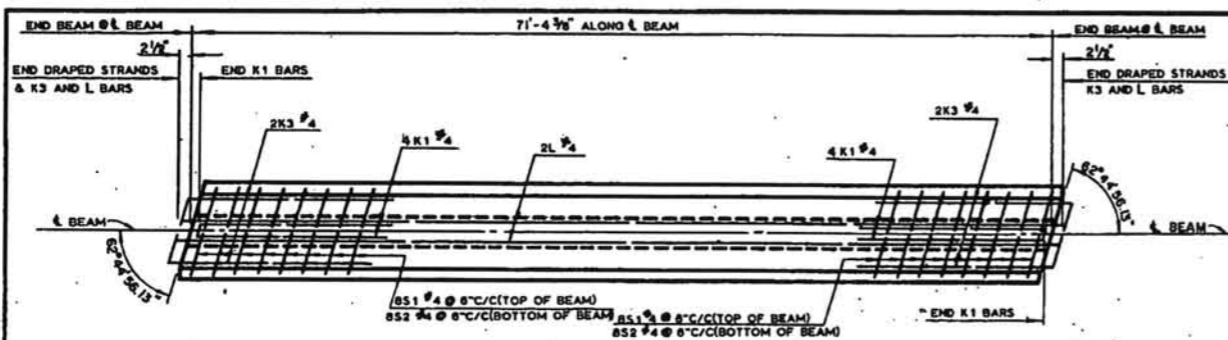
LONGITUDINAL SECTION

NOTE: Reference STD. 6FP-1 for Parapet Details and Notes. Parapets shall be constructed without drain openings.

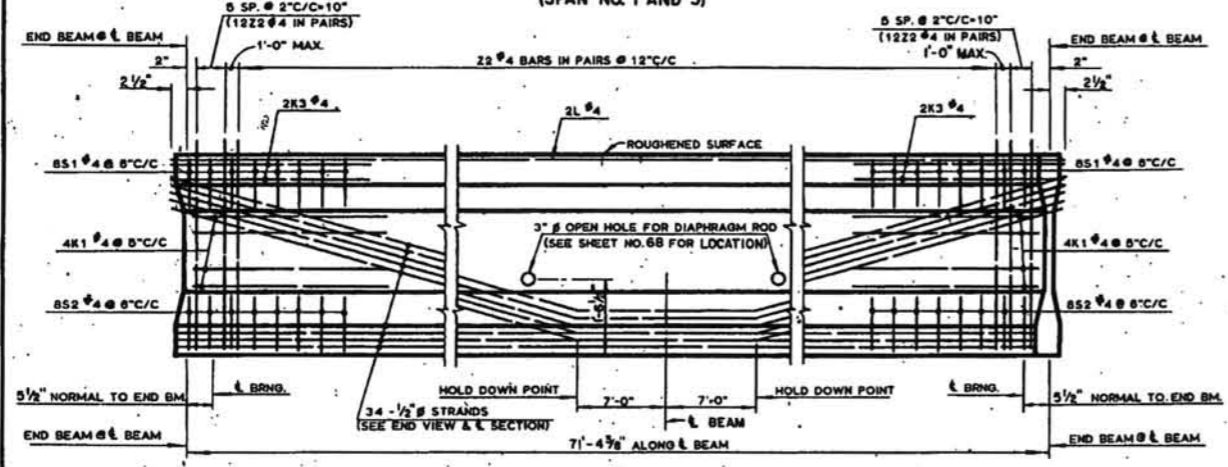
| QUANTITIES | | |
|---|------|----------|
| ITEM | UNIT | TOTAL |
| CLASS "AA" CONCRETE | C.Y. | 471.7 |
| REINFORCING STEEL (GRADE 60) | L.B. | 72,115. |
| STRUCTURAL STEEL (A36) | L.B. | 8,600. |
| P.C. BEAMS (TYPE III) | L.F. | 1,284.58 |
| P.C. BEAMS (TYPE IV) | L.F. | 875.72 |
| STRIP SEAL TYPE EXP JOINT w/ ANGLES | L.F. | 157.2 |
| EPOXY COATED REINFORCING STEEL (GRADE 60) | L.B. | 58,035. |
| PENET. WATER REPEL. SURE TREATMENT | S.Y. | 1,982.5 |
| CONCRETE PARAPET | L.F. | 487.18 |

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| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

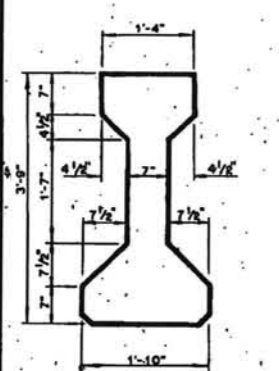
STRUCTURE "B" SUPERSTRUCTURE DETAILS



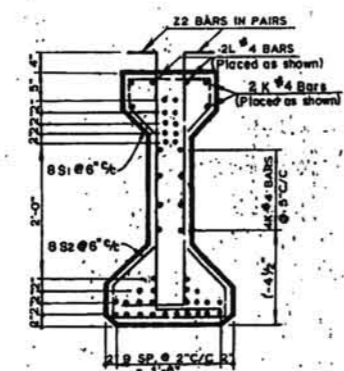
PLAN
(SPAN NO. 1 AND 3)



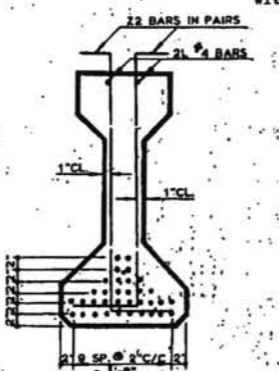
ELEVATION
(SPAN NO. 1 AND 3)



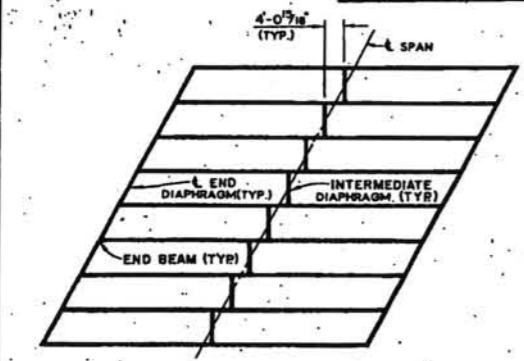
SECTION THRU BEAM
TYPE III RC. BM.



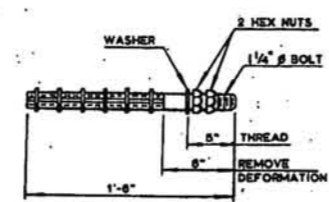
END VIEW
WITH REINFORCING
SPAN NO. 1 AND 3



SECTION
134-1/2 #5 STRANDS



BEAM AND DIAPHRAGM LAYOUT



DETAIL OF ANCHOR BOLT

PRESTRESSED CONCRETE BEAMS

Chamfer all exposed edges of P.C. Beams 3/4" unless otherwise noted.

FORMS & PALLETS - All beams shall be cast in concrete floored pallets and metal forms.

FINISH - Top of beams to be rough floated. At approximately the time of initial set, entire top of beam shall be scrubbed transversely with coarse wire brush to remove all laitance and to produce a roughened surface for bonding slab.

CONCRETE - Concrete for beams shall have a min. 6,000 psi strength at 28 days. Where 6,000 psi concrete strength is specified for prestressed concrete beams, the contractor may increase the maximum cement requirements as specified in Section 503.02(a) of the Oklahoma Standard Specifications for Highway Construction to 9 sacks per cubic yard.

CEMENT - Type I or III Portland Cement may be used for the Prestressed Concrete Beams.

CYLINDER STRENGTH - At transfer of the tensioning load, the cylinder strength of the concrete shall be at least 5,000 psi for 6,000 psi concrete.

BEARING SURFACES - Concrete surfaces under all beam supports (bearing assembly) shall be ground with a carborundum brick before placement of bearing assembly to secure full bearing of assembly on concrete.

HANDLING - In the handling of the beams, they must be maintained in an upright position at all times and must be picked up from the lifting eye provided at the beam ends. Disregard of this requirement may lead to collapse of the member.

CABLE EXTENSIONS - All non-draped prestressing strands shall be cut off flush with the end of beam.

will be exposed are to be coated with two coats of an approved zinc rich paint (min. 6 mils).

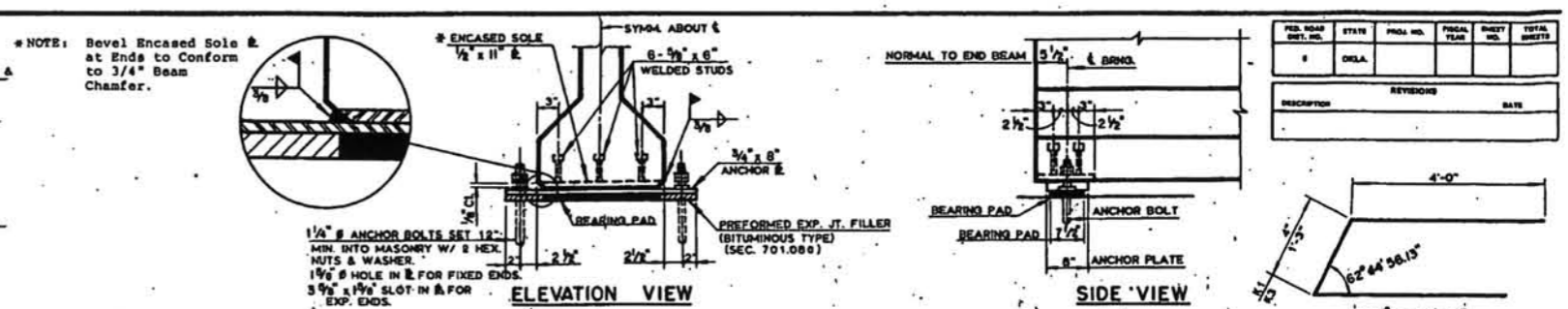
STEEL - All strands shall be the size as shown on the plans. Initial load per strand shall be 70 percent of the breaking strength of strand.

SPECIFICATIONS FOR STEEL STRANDS - Type 270 K, 7-wire, uncoated, stress-relieved steel strand shall conform to the requirements of AASHTO Designation: M-203 (ASTM A-416).

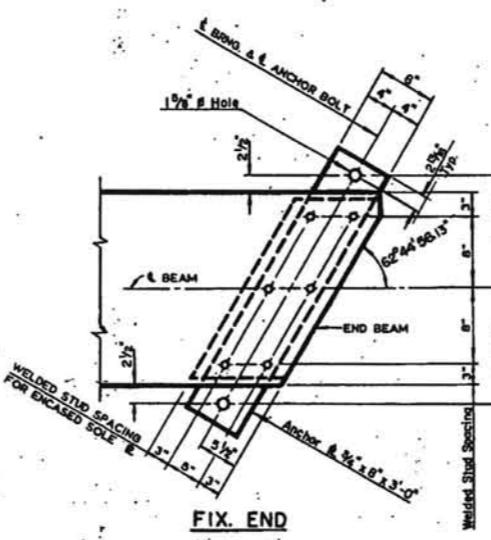
SHOP DRAWINGS - The Contractor shall have his Prestressed Concrete Beam Fabricator furnish the Bridge Engineer, for his approval, two sets of checked shop drawings. One copy shall be returned to the fabricator approved of with any desired corrections indicated. The fabricator shall then furnish the Bridge Engineer with as many, generally five, corrected copies of the shop drawings as may be required for approval and distribution. The approval of the shop drawings in no way relieves the contractor or his fabricator of the responsibility for mistakes on the shop drawings. Shop drawings shall show the casting length center to center of bearing and the calculated prestressing shortening.

NOTE: Pretensioning force for all strands shall be 28,910 lbs.
Maximum size of coarse aggregate in all beams shall be 1".
All Bar Bends are out to out.

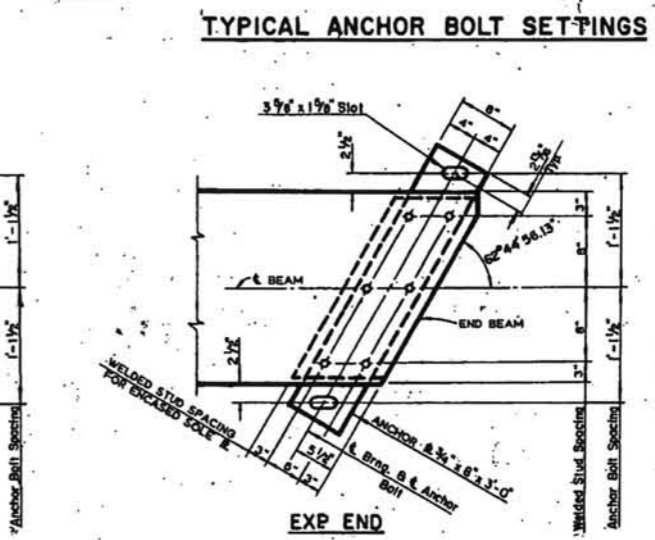
NOTE: Weight of bearing assembly (including plates, studs, anchor bolts and shop welding) and diaphragm bolt assembly for prestressed girder bridges is included in the structural steel quantities. All cost of material, labor, equipment and incidentals necessary to install diaphragm bolt and bearing assembly will be paid for at unit price bid for structural steel.
All cost of Elastomeric Bearing Pads will be included in the price bid for other items of work.



TYPICAL ANCHOR BOLT SETTINGS

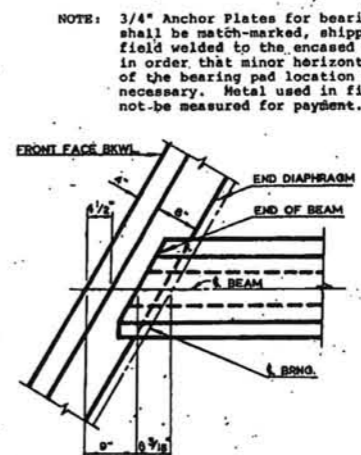


FIX. END

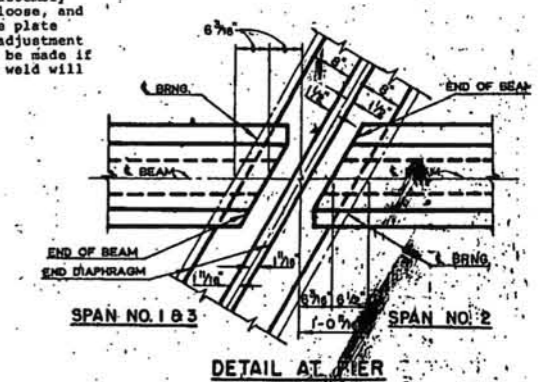


EXP. END

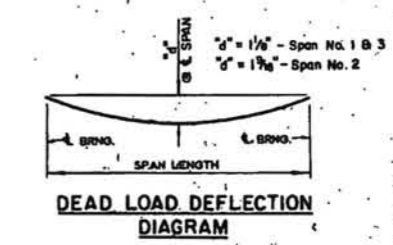
PLAN VIEW



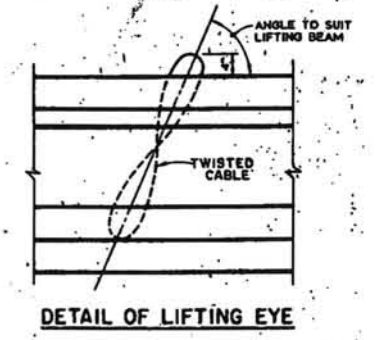
DETAIL AT ABUTMENT



DETAIL AT PIER



DEAD LOAD DEFLECTION DIAGRAM

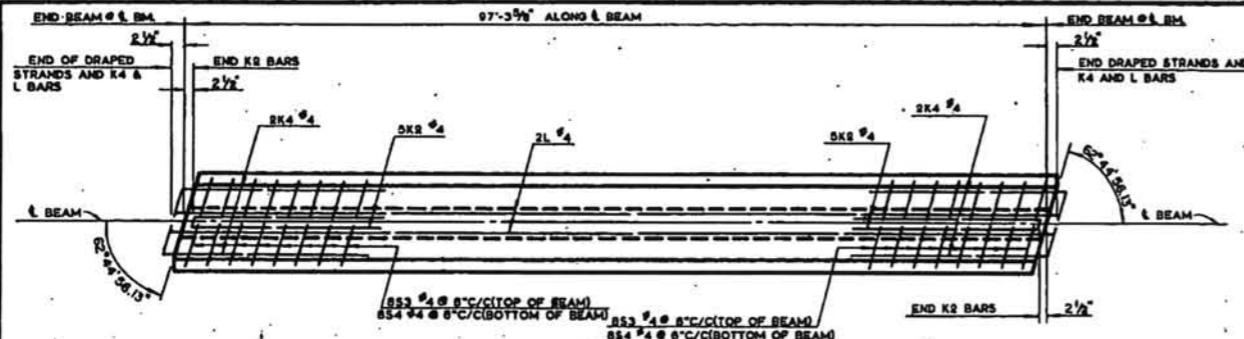


DETAIL OF LIFTING EYE

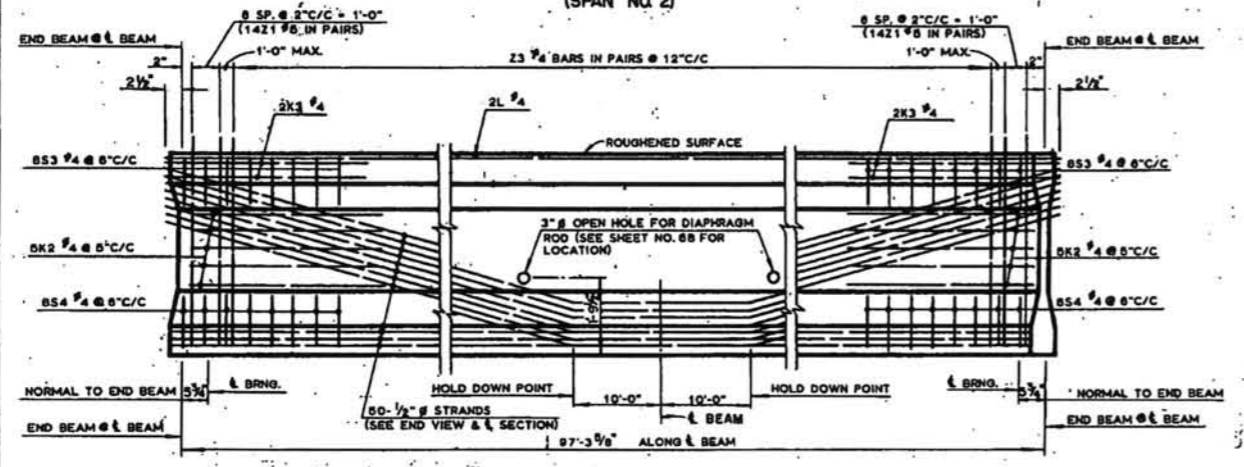
| REV. NO. | DATE | BY | CHKD. | APP'D. | DESCRIPTION |
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| Checked | |
| Approved | |
| Squad | |

STRUCTURE "B"
SUPERSTRUCTURE DETAILS

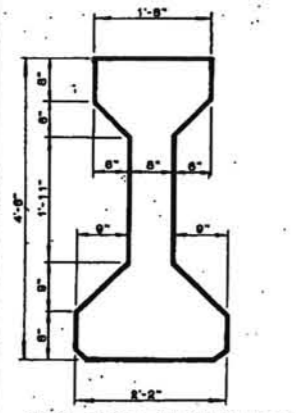


PLAN
(SPAN NO. 2)

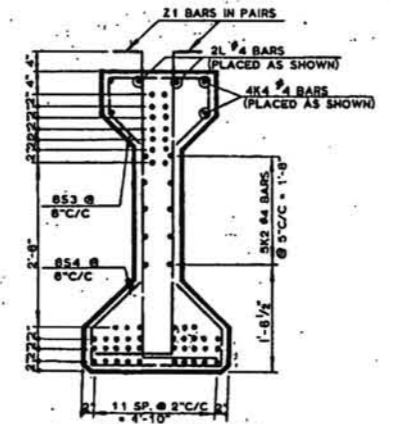


ELEVATION
(SPAN NO. 2)

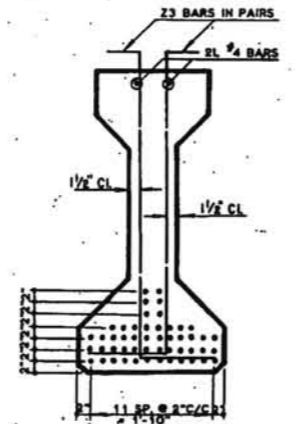
NOTE: Draped Strands, K4 and L Bars shall extend 2 1/2' beyond ends of beam. Straight strands shall be cut flush with ends of beam.



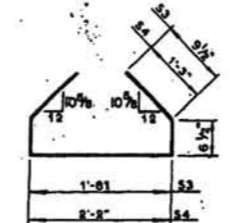
SECTION THRU BEAM
TYPE IV PC BEAM



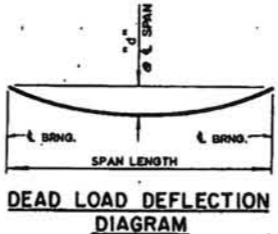
END VIEW
WITH REINFORCING



SECTION
(50-1/2\"/>

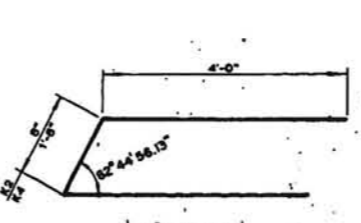


S3 BARS #4 x 4'-4"
S4 BARS #4 x 5'-9"

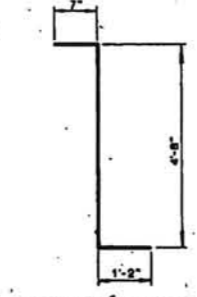


DEAD LOAD DEFLECTION
DIAGRAM

9' = 1 1/2" - SPAN NO. 1 & 3
9' = 1 1/2" - SPAN NO. 2



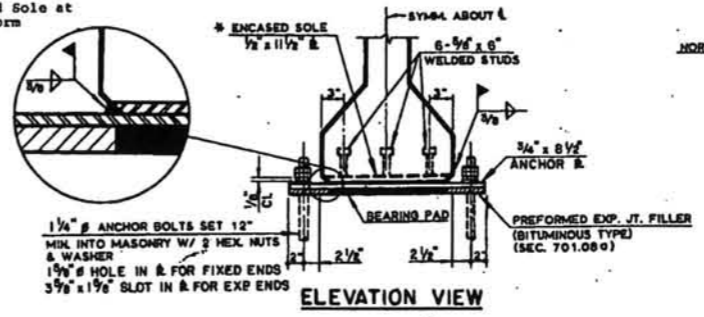
K2 #4 x 8'-6"
K4 #4 x 9'-6"



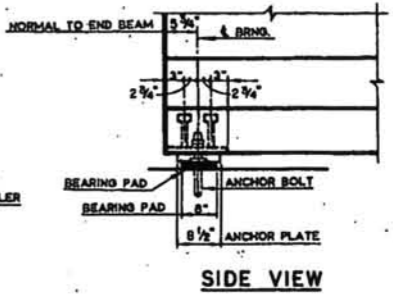
Z1 BARS #5 x 6'-5"
Z3 BARS #4 x 6'-5"

NOTE: All "2" Bars shall be epoxy coated reinforcing.

NOTE: Bevel Encased Sole at Ends to Conform to 3/4" Beam Chamfer.

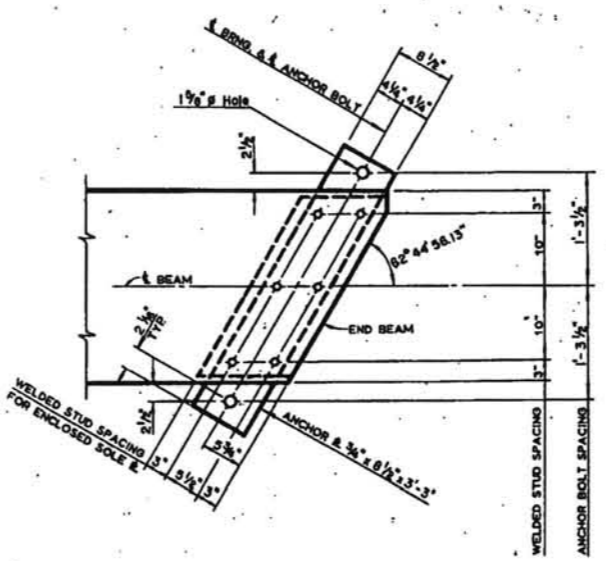


ELEVATION VIEW

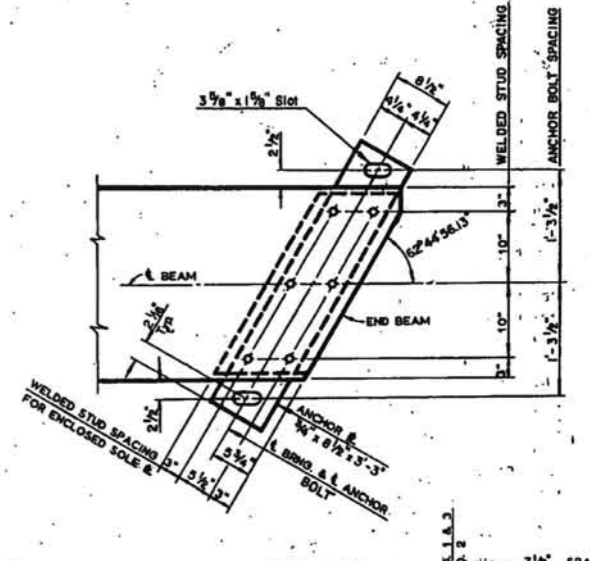


SIDE VIEW

TYPICAL ANCHOR BOLT SETTINGS



FIX. END



EXP. END

PLAN VIEW

NOTE: 3/4" Anchor Plates for bearing assembly shall be match-marked, shipped loose, and field welded to the encased sole plate in order that minor horizontal adjustment of the bearing pad location may be made if necessary. Metal used in field weld will not be measured for payment.

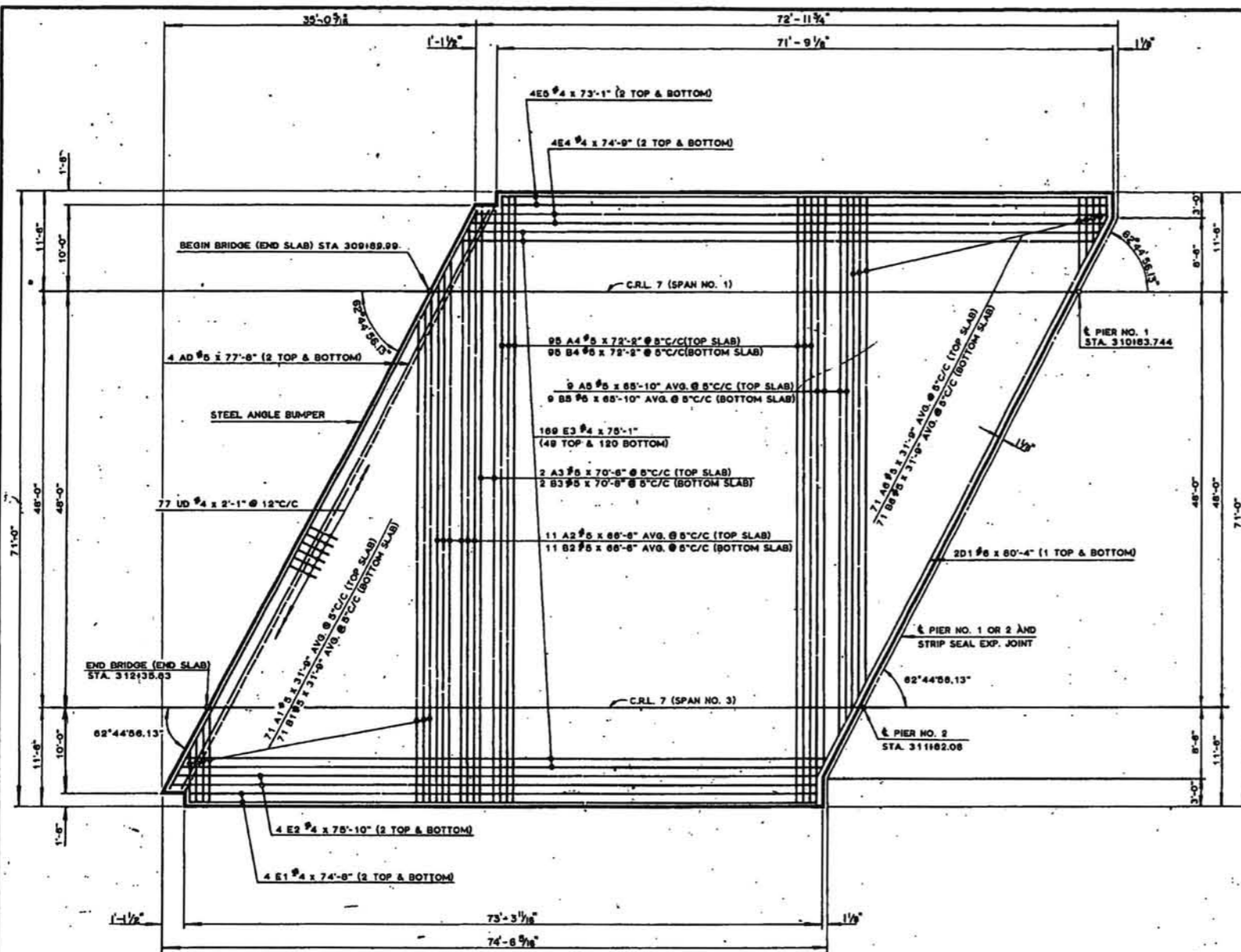
NOTE: All Bearing Pads shall be centered between anchor bolts.

| ELASTOMERIC BEARING PAD SCHEDULE | | |
|---|------------------------|------------------|
| LOCATION | BEARING PAD SIZE | NO. OF LAMINATES |
| ABUTMENT NO. 1 & 2 | 7 1/2" x 1" (60 DURO.) | 1 |
| SPAN NO. 1, PIER NO. 1 & SPAN NO. 3, PIER NO. 2 | 7 1/2" x 1" (60 DURO.) | 1 |
| SPAN NO. 2, PIER NO. 2 & SPAN NO. 2, PIER NO. 1 | 8" x 1 1/2" (60 DURO.) | 1 |

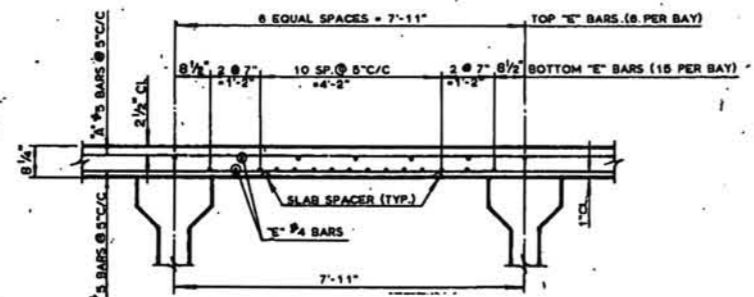
Reference Sheet No. 88 for the following:
1. Detail of Lifting Eye.
2. Beam and Diaphragm Layout.
3. Detail of Anchor Bolt.
4. General Notes.
5. Detail at Pier.

| | |
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| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

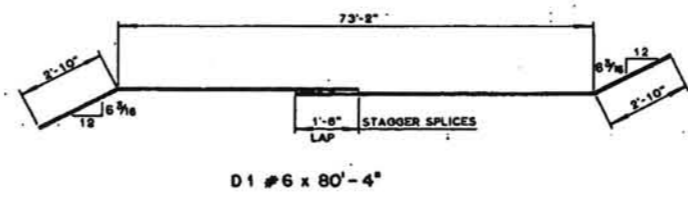
STRUCTURE "B"
SUPERSTRUCTURE DETAILS



SPAN NO. 1 DECK REINFORCING PLAN
(SPAN NO. 3 BY 180° ROTATION)



TYPICAL DECK REINFORCING



D1 #6 x 80'-4"

| PRIOR SHEET NO. | DATE | BY | CHKD. | APP'D. | REVISION |
|-----------------|------|----|-------|--------|----------|
| | | | | | |

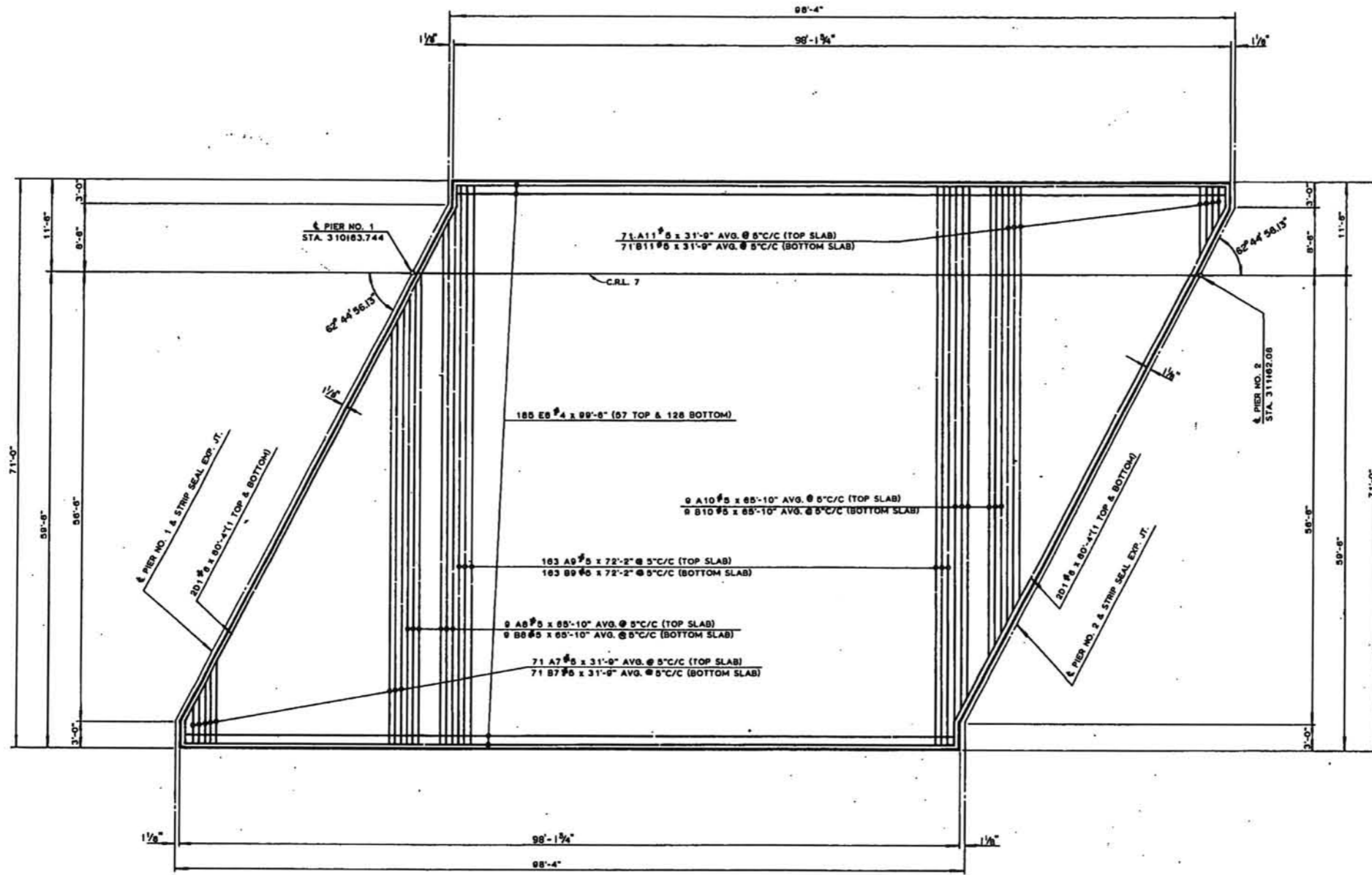
| BAR LIST (GR 60 EPOXY COATED) | | | | | BAR LIST (GRADE 60) | | | | |
|-------------------------------|-----|------|------|------------------|---------------------|-----|------|------|------------------|
| MARK | NO. | SIZE | FORM | LENGTH | MARK | NO. | SIZE | FORM | LENGTH |
| A1 | 142 | #5 | Str. | 31'-9" Avg. ① | AD | 4 | #5 | Str. | 77'-8" ①② |
| A2 | 22 | #5 | Str. | 66'-6" Avg. ①②⑤ | B1 | 142 | #5 | Str. | 31'-9" Avg. ① |
| A3 | 4 | #5 | Str. | 70'-8" ①② | B2 | 22 | #5 | Str. | 66'-6" Avg. ①②⑤ |
| A4 | 190 | #5 | Str. | 72'-2" ①② | B3 | 4 | #5 | Str. | 70'-8" ①② |
| A5 | 18 | #5 | Str. | 65'-10" Avg. ①②⑥ | B4 | 190 | #5 | Str. | 72'-2" ①② |
| A6 | 142 | #5 | Str. | 31'-9" Avg. ⑦ | B5 | 18 | #5 | Str. | 65'-10" Avg. ①②⑥ |
| A7 | 71 | #5 | Str. | 31'-9" Avg. ⑧ | B6 | 142 | #5 | Str. | 31'-9" Avg. ⑦ |
| A8 | 9 | #5 | Str. | 65'-10" Avg. ①②⑨ | B7 | 71 | #5 | Str. | 31'-9" Avg. ⑧ |
| A9 | 163 | #5 | Str. | 72'-2" ①② | B8 | 9 | #5 | Str. | 65'-10" Avg. ①②⑨ |
| A10 | 9 | #5 | Str. | 65'-10" Avg. ⑩①② | B9 | 163 | #5 | Str. | 72'-2" ①② |
| A11 | 71 | #5 | Str. | 31'-9" Avg. ⑪ | B10 | 9 | #5 | Str. | 65'-10" Avg. ⑩①② |
| AD | 4 | #5 | Str. | 77'-8" ①② | B11 | 71 | #5 | Str. | 31'-9" Avg. ⑪ |
| D1 | 4 | #6 | Bnt. | 80'-4" ①② | D1 | 4 | #6 | Bnt. | 80'-4" ①② |
| E1 | 4 | #4 | Str. | 74'-8" ①② | E1 | 4 | #4 | Str. | 74'-8" ①② |
| E2 | 4 | #4 | Str. | 75'-10" ①② | E2 | 4 | #4 | Str. | 75'-10" ①② |
| E3 | 98 | #4 | Str. | 75'-1" ①② | E3 | 240 | #4 | Str. | 75'-1" ①② |
| E4 | 4 | #4 | Str. | 74'-9" ①② | E4 | 4 | #4 | Str. | 74'-9" ①② |
| E5 | 4 | #4 | Str. | 73'-1" ①② | E5 | 4 | #4 | Str. | 73'-1" ①② |
| E6 | 57 | #4 | Str. | 99'-6" ①② | E6 | 128 | #4 | Str. | 99'-6" ①② |
| EP | 488 | #5 | Bnt. | 5'-5" ① | F1 | 32 | #8 | Bnt. | 9'-9" ①③④ |
| | | | | | F2 | 16 | #4 | Str. | 7'-0" ② |
| | | | | | F2 | 16 | #4 | Str. | 6'-6" ② |
| | | | | | F3 | 4 | #4 | Str. | 74'-5" ②③④ |
| | | | | | F3 | 2 | #4 | Str. | 74'-9" ②③④ |
| | | | | | F4 | 4 | #8 | Str. | 74'-5" ②③④ |
| | | | | | F4 | 2 | #8 | Str. | 74'-6" ②③④ |
| | | | | | F5 | 64 | #4 | Str. | 6'-9" ②③ |
| | | | | | F5 | 32 | #4 | Str. | 6'-9" ②③ |
| | | | | | U1 | 112 | #4 | Bnt. | 3'-10" ①③④ |
| | | | | | U1 | 56 | #4 | Bnt. | 4'-6" ①③④ |
| | | | | | U2 | 288 | #4 | Bnt. | 3'-1" ①③④ |
| | | | | | U2 | 144 | #4 | Bnt. | 3'-6" ①③④ |
| | | | | | UD | 154 | #4 | Bnt. | 2'-1" ① |

- ① Includes 1'-6" Lap, Minimum
- ② Stagger Splices
- ③ Reference Sheet No. 67 for Bar Bend Detail
- ④ 3'-6" to 60'-0"
- ⑤ 62'-5" to 70'-7"
- ⑥ 62'-7" to 69'-1"
- ⑦ 3'-6" to 60'-0"
- ⑧ 3'-6" to 60'-0"
- ⑨ 62'-7" to 69'-1"
- ⑩ Vertical in Intermediate Diaphragms (Span No. 1 & 3)
- ⑪ Reference Sheet No. 72 for Bar Bend Detail
- ⑫ Vertical in Intermediate Diaphragms (Span No. 2)
- ⑬ Vertical in End Diaphragms (Span No. 1 & 3)
- ⑭ Span No. 1 & 3 End Diaphragms
- ⑮ Span No. 2 End Diaphragms
- ⑯ Includes 2'-6" Lap
- ⑰ Includes 2'-0" Lap
- ⑱ Horizontal in Intermediate Diaphragms (Span No. 1 & 3)
- ⑲ Horizontal in Intermediate Diaphragms (Span No. 2)
- ⑳ Reference STD. SFP-1 for bar bend details and notes.

| | |
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| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

STRUCTURE "B"
SUPERSTRUCTURE DETAILS

| PROJ. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|----------------------|-------|-----------|-------------|-----------|--------------|
| 8 | DELA. | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |



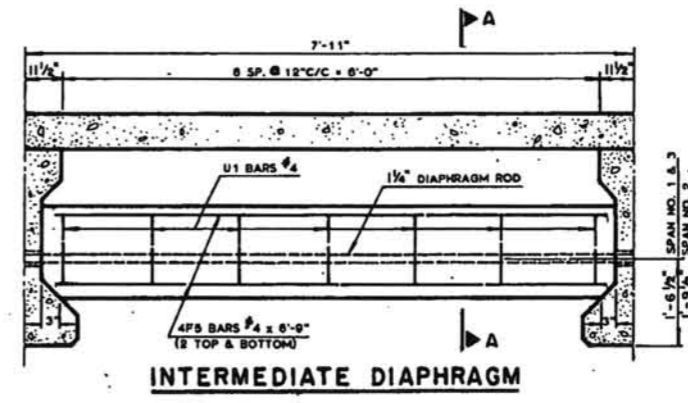
SPAN NO. 2 - DECK REINFORCING PLAN

| | |
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| Design | |
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| Checked | |
| Approved | |
| Squad | |

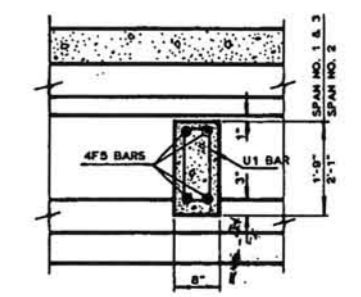
**STRUCTURE "B"
SUPERSTRUCTURE DETAILS**

F. A. Project No. I-35-3(107)128 Sheet No. 71

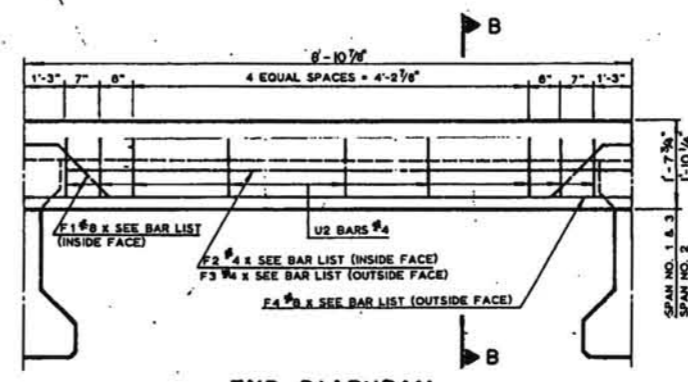
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|-------------|-------|-----------|-------------|-----------|--------------|
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| DESCRIPTION | | REVISIONS | | DATE | |
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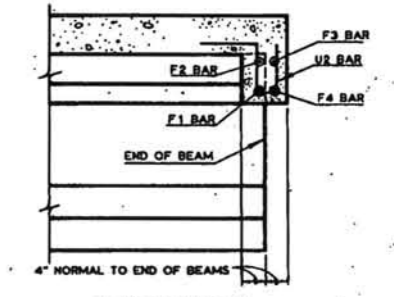
INTERMEDIATE DIAPHRAGM



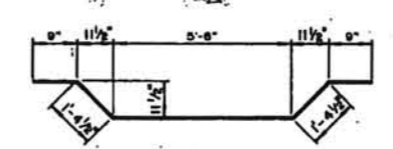
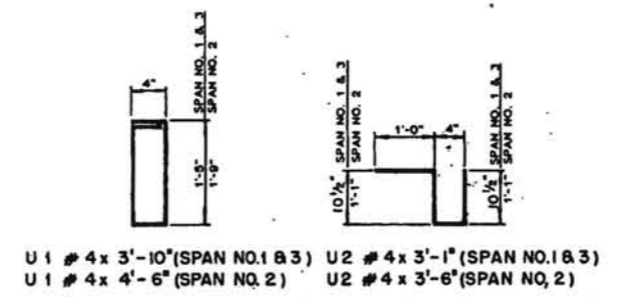
SECTION A-A



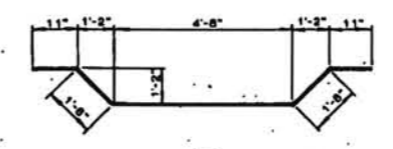
END DIAPHRAGM



SECTION B-B

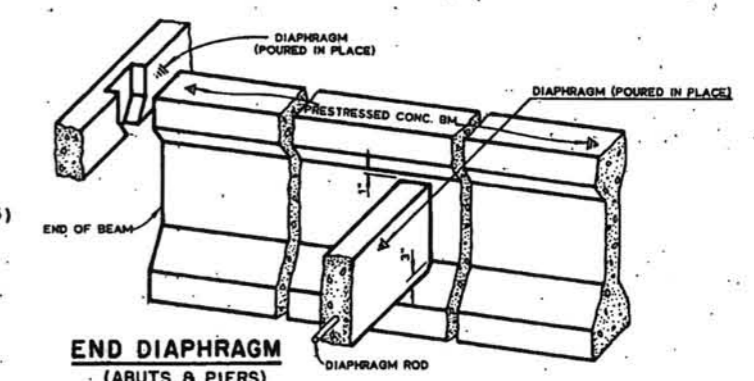


F1 #8 x 9'-9" (SPAN NO. 1 & 3)



F1 #8 x 9'-10" (SPAN NO. 2)

NOTE: ALL BAR BENDS ARE OUT TO OUT.



END DIAPHRAGM (ABUTS. & PIERS)
INTERMEDIATE DIAPHRAGM
PICTORIAL VIEW
INTERIOR BEAM SHOWN

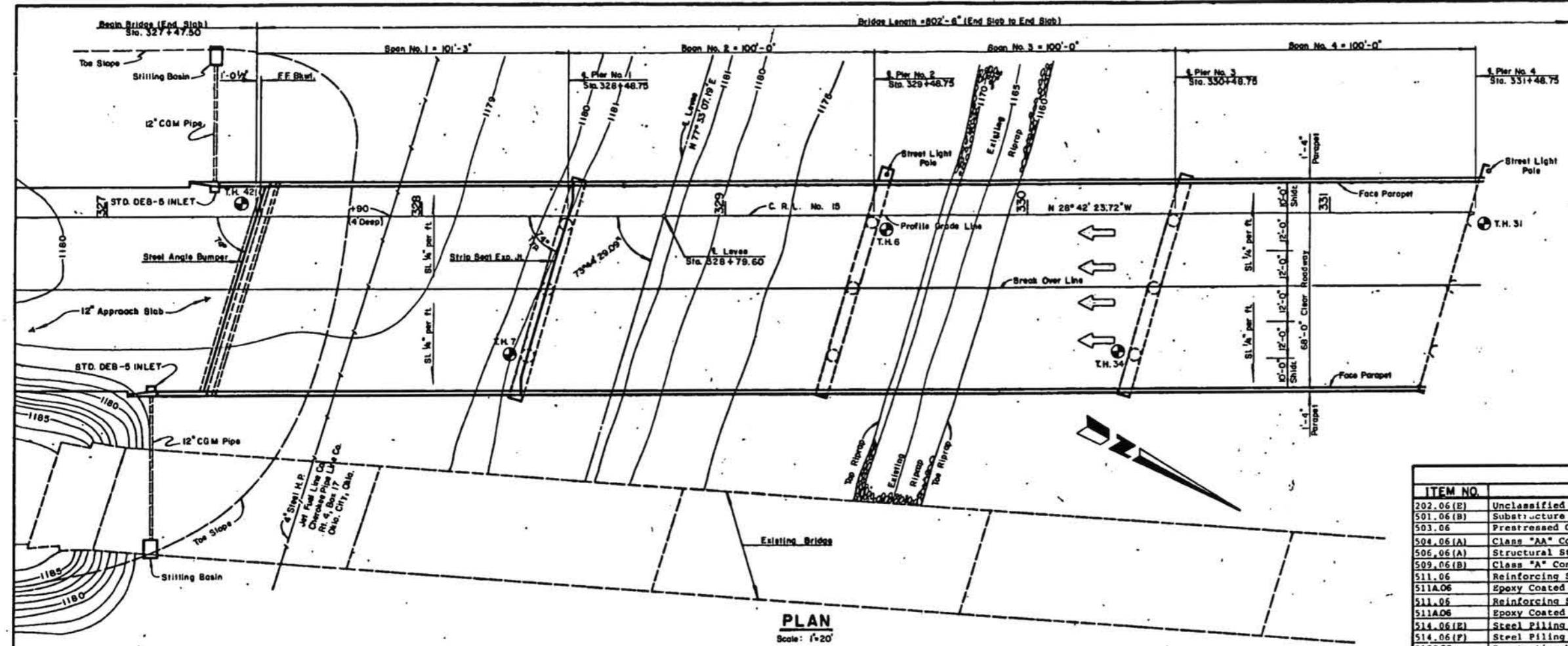
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| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

STRUCTURE "B"
SUPERSTRUCTURE DETAILS
FA, Project No. 1-35-3(107)128 Sheet No. 72

| NO. | DATE | BY | CHKD. | APPD. | REVISION |
|-----|-------|----|-------|-------|----------|
| 1 | 08.14 | | | | |

DESIGN DATA

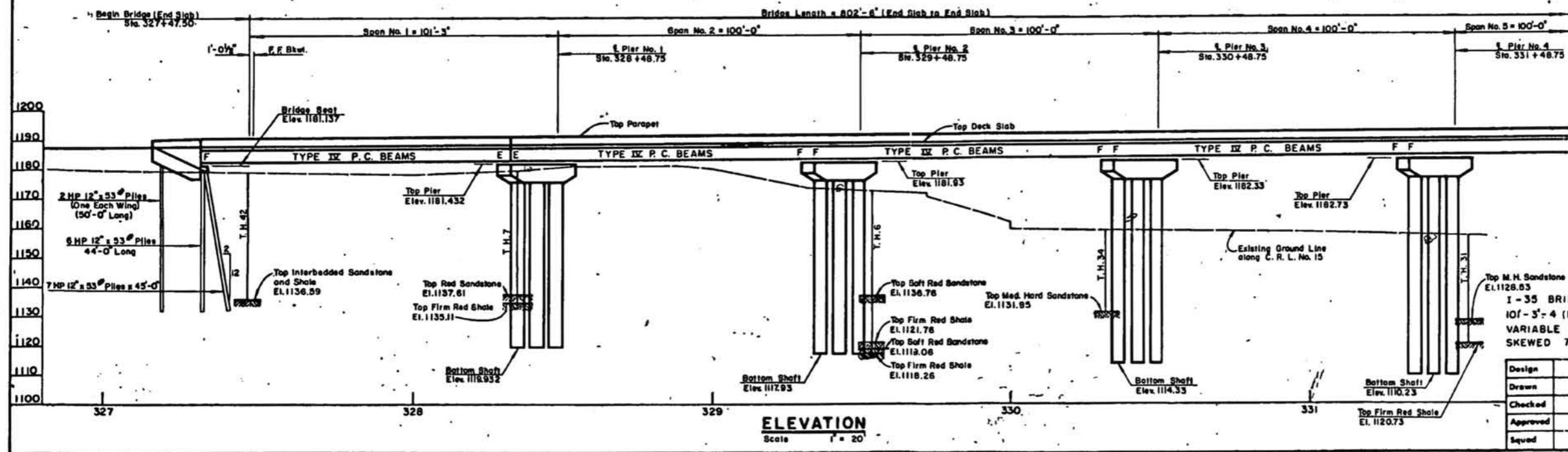
DESIGN: AASHTO Specifications, 1976 Edition with Supplements
 LIVE LOAD: H S 20-44
 STRUCTURAL STEEL: 20,000 psi
 REINFORCING STEEL: (Gr. 40) 20,000 psi
 REINFORCING STEEL: (Gr. 60) 24,000 psi
 CONCRETE:
 CLASS 'A' 1,000 psi
 CLASS 'AA' 1,200 psi
 MAXIMUM FOUNDATION LOADS:
 ABUTMENTS 51.4 Tons /Pile
 PIERS
 FRICTION CAPACITY 400.6 Tons /Shaft
 BEARING CAPACITY 196.3 Tons /Shaft
 TOTAL CAPACITY 597.0 Tons /Shaft
 MAX. SHAFT LOAD 514.0 Tons /Shaft
 ALLOWABLE FRICTION LOAD 3.3 Tons /S.F.
 ALLOWABLE POINT BEARING LOAD 110 Tons/S.F.
 HYDRAULIC DATA:
 DRAINAGE AREA 13,354 Sq. Miles
 # Q50=99,000 cfs, High Water El. 1181.90 v = 7.3 fps
 # Q100=47,500 cfs, High Water El. 1174.40 v = 6.6 fps
 # Q50=33,100 cfs, High Water El. 1171.40 v = 5.9 fps
 # Denotes Data From Corp of Engineers.



PLAN
Scale: 1"=20'

| SUMMARY OF QUANTITIES | | | | | | |
|-----------------------|--|-------|---------|---------|----------|----------|
| ITEM NO. | ITEM | UNIT | ABUTS. | PIERS | SUPSTR. | TOTAL |
| 202.06(E) | Unclassified Borrow | C.Y. | 414. | | | 414. |
| 501.06(B) | Substructure Excavation, Common | C.Y. | 154. | 120. | | 274. |
| 503.06 | Prestressed Concrete Beam (Type IV) | L.F. | | | 7,514.39 | 7,514.39 |
| 504.06(A) | Class "AA" Concrete | C.Y. | | | 1,659.3 | 1,659.3 |
| 506.06(A) | Structural Steel (A36) | LBS. | | 510. | 27,660. | 28,170. |
| 509.06(B) | Class "A" Concrete | C.Y. | 180.9 | 590.2 | | 771.1 |
| 511.06 | Reinforcing Steel (Gr. 60) | LBS. | | | 249,705. | 249,705. |
| 511A.06 | Epoxy Coated Reinforcing Steel (Gr. 60) | LBS. | | | 221,680. | 221,680. |
| 511.06 | Reinforcing Steel (Gr. 40) | LBS. | 21,520. | 29,975. | | 51,495. |
| 511A.06 | Epoxy Coated Reinforcing Steel (Gr. 40) | LBS. | | 98,135. | | 98,135. |
| 514.06(E) | Steel Piling (HP 12 x 53) | L.F. | 2,112. | | | 2,112. |
| 514.06(F) | Steel Piling Splices (HP 12 x 53) | EA. | 36. | | | 36. |
| 515S.06 | Penetrating Water Repellent Surface Treat. | S.Y. | 126.2 | 997.7 | 6,885.0 | 8,008.9 |
| 516A.06(A) | Drilled Shafts (60" Diameter) | L.F. | | 1,145.0 | | 1,145.0 |
| 516A.06(C) | Corr. Galv. Steel Pipe (60" Diameter) | L.P. | | 815.0 | | 815.0 |
| 516A.06(D) | Obstructions | C.Y. | | | | 80. |
| Sp. | Strip Seal Type Expansion Joint W/ Angles | L.F. | | | 303.7 | 303.7 |
| 619.06(D) | Removal of Existing Bridge Structure | L.SUM | | | | 1. |
| 802.06(A) | 1" Galv. Steel Elect. Conduit | L.F. | | 6.9 | | 6.9 |
| 505.06(B) | Concrete Parapet | L.F. | 77.0 | | 1,593.0 | 1,670.0 |

① A Maximum of One Pile Splice will be allowed for each Pile.



ELEVATION
Scale: 1"=20'

INDEX OF SHEETS

| SHEET NO. | TITLE |
|-----------|--|
| 73 & 74 | GENERAL PLAN AND ELEVATION AND SUMMARY OF QUANTITIES |
| 75 | BRIDGE STAKING DIAGRAM |
| 76-78 | ABUTMENT DETAILS |
| 79 & 82 | PIER DETAILS |
| 83-92 | SUPERSTRUCTURE DETAILS |
| | STD. P-1 |
| | STD. EXP-1 |
| | STD. DEB-3-3 |
| | STD. GRAU-3-2 |
| | STD. GRH-2-2 |
| | STD. SSIF-1-0 |
| | STD. B50-1-3 |
| | STD. SFP-1-0 |

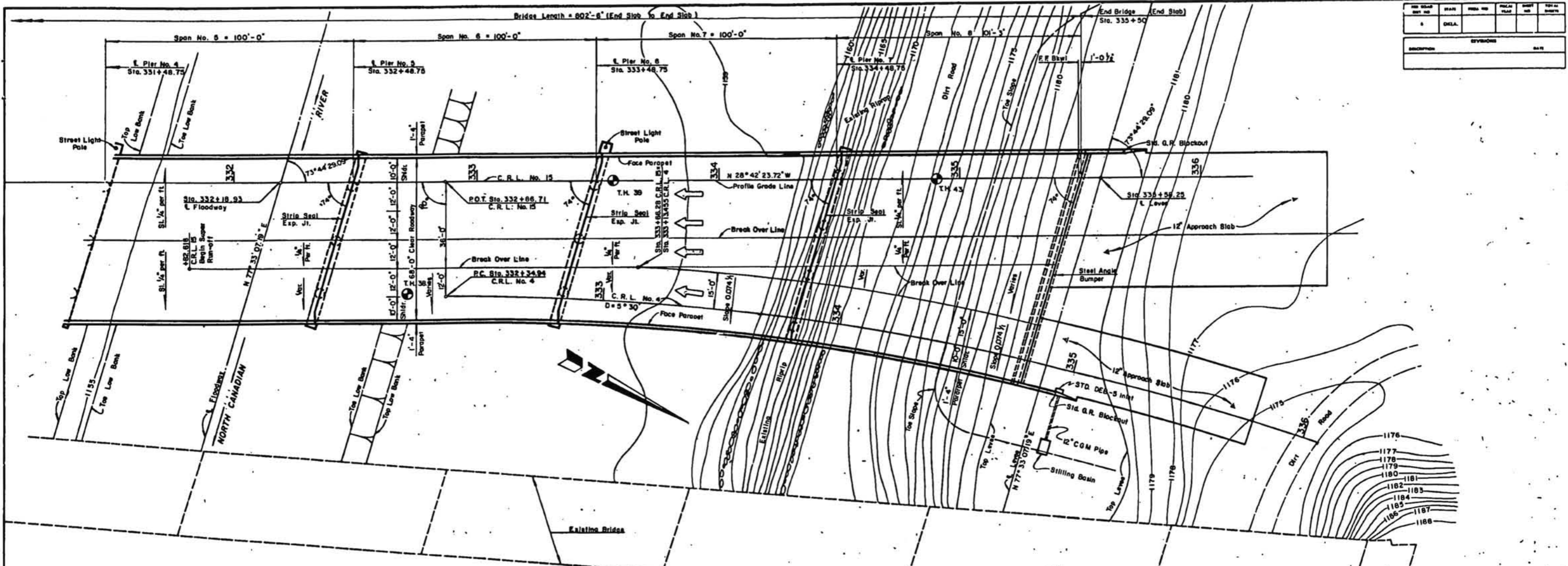
I-35 BRIDGE OVER NORTH-CANADIAN RIVER
 101'-3"-4 (100')-100'-100'-101'-3" PRESTRESSED CONCRETE BEAM SPANS
 VARIABLE CLEAR ROADWAY WITH 1'-4" PARAPETS ON EACH SIDE
 SKEWED 74° 00' LT. FWD.

STRUCTURE "C" @ STA. 331 + 49
GENERAL PLAN AND ELEVATION AND SUMMARY OF QUANTITIES

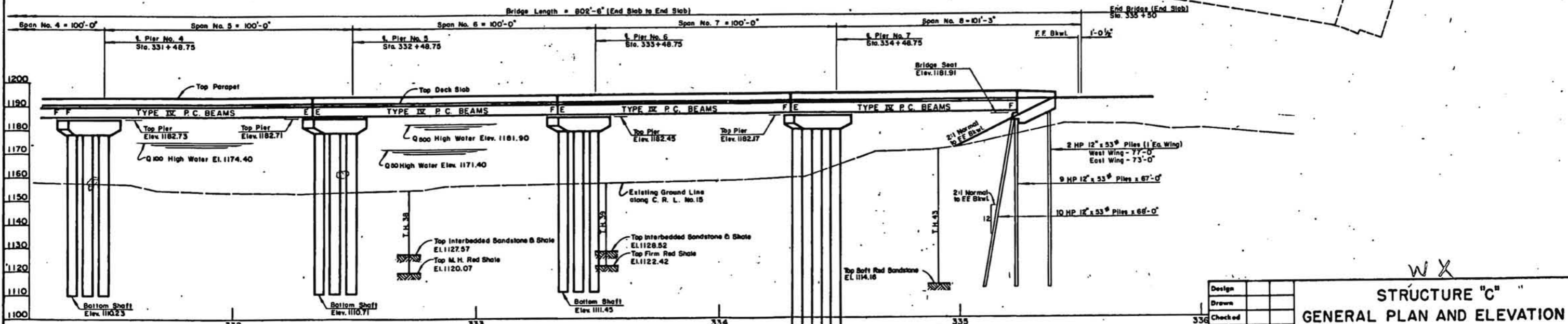
| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

E.A. Project No. 1-35-3(107)126 Sheet No. 73

| | | | | |
|------------|------|----|---------|----------|
| DESIGN NO. | DATE | BY | CHECKED | APPROVED |
| | | | | |



PLAN
Scale: 1" = 20'



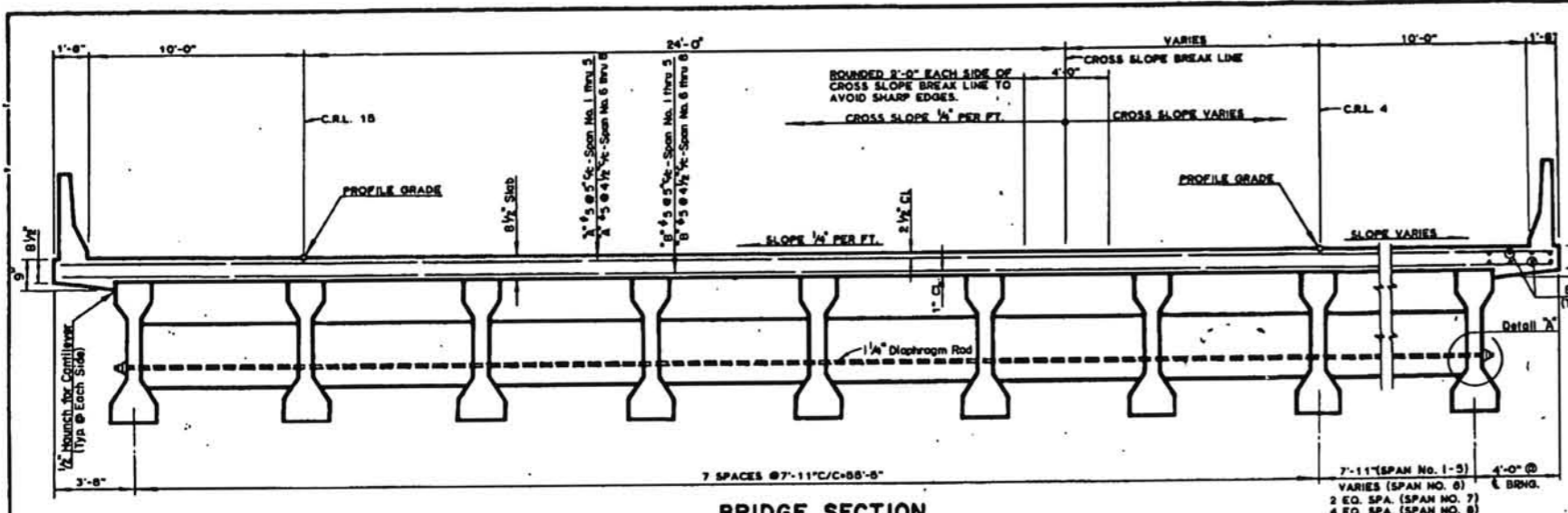
ELEVATION
Scale: 1" = 20'

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

W X

STRUCTURE "C"
GENERAL PLAN AND ELEVATION

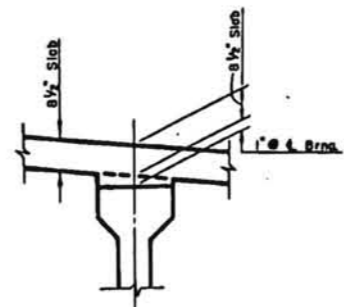
E. A. Project No. 1-35-3 (107) 126 Sheet No. 74



BRIDGE SECTION

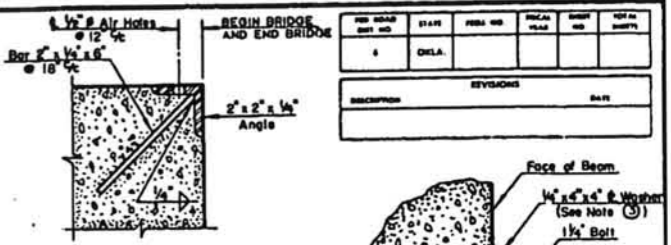
FLOOR: 8 1/2" uniform slab thickness, Variable Clear Roadway with concrete parapet each side. Class "AA" Concrete, Maximum Aggregate 1 1/2". All construction shall conform to the 1976 Oklahoma Standard Specifications. Surface of Roadway shall slope as shown. All exposed edges shall have a 3/4" chamfer unless otherwise noted. Transverse reinforcing steel in the floor slab shall begin and end with "A" and "B" Bars a maximum of 2' from end of concrete panels. All reinforcing steel bars in top of slab shall be supported on approved metal high chairs. The maximum spacing of the metal chairs shall be 1/2 beam spacing or a maximum of 4'-0" on center.

Bottom layer of reinforcing steel shall be supported on continuous steel slab spacers as shown. Longitudinal Bars shall begin and end 1" from ends of slab. Minimum Compressive Strength for Class "AA" Concrete shall be 3,000 psi at 28 days. All reinforcing steel in Deck Slab, Diaphragms and Parapets shall be Grade 60. Reference Bar List on Sheet No. 88 for Reinforcing Bars to be Epoxy Coated. Penetrating Water Repellent Surface Treatment shall be applied to the Deck Slab and Roadway Face of Parapets.



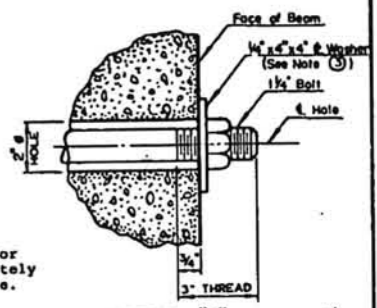
HAUNCH DETAIL

NOTE: Plan quantity for Class "AA" Concrete includes 12.50 C.Y. for haunches over P.C. Beams. The forming of the haunches will be varied to bring profile of slab to desired grade, but quantity for haunches will be as shown above.



STEEL ANGLE BUMPER DETAIL

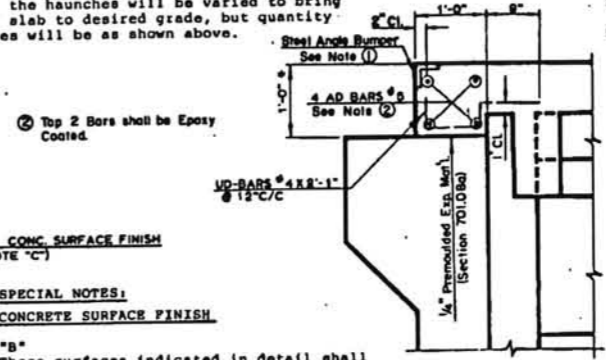
NOTE 1: The Steel Angle Bumper shall extend across the full width of the driving surface only. All cost of steel bumper shall be included in the price bid for Structural Steel. Contractor shall be responsible for adequately supporting steel bumper in place.



DETAIL "A"

All cost of 1 1/2" Bolts to be included in the unit price bid for Structural Steel. Paint Bolt End, Nut and Washer with two coats of zinc-rich paint.

NOTE 3: Span No. 7 & 8 Washers shall be beveled when diaphragms are skewed to the beam. Minimum washer thickness shall be 1/4".



DETAIL "B"

SPECIAL NOTES:
CONCRETE SURFACE FINISH

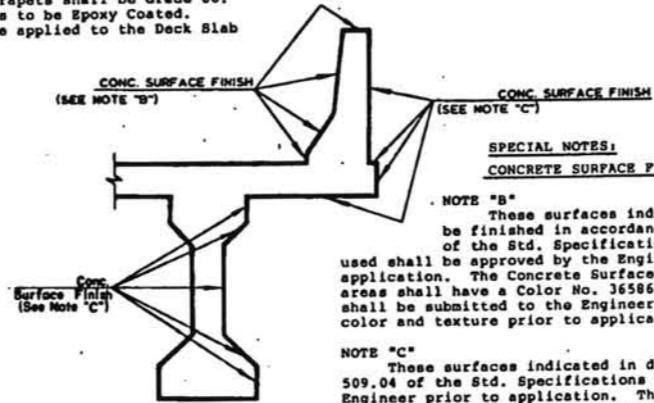
NOTE "B": These surfaces indicated in detail shall be finished in accordance with Section 509.04 of the Std. Specifications and the material used shall be approved by the Engineer prior to application. The Concrete Surface Finish for these areas shall have a Color No. 36586. Color chips shall be submitted to the Engineer for approval of color and texture prior to application of the material.

NOTE "C": These surfaces indicated in detail shall be finished in accordance with Section 509.04 of the Std. Specifications and the material used shall be approved by the Engineer prior to application. The color for these areas shall be Color No. 33531, with shade of color to be selected by the Engineer after observing color chips for both colors. Color chips shall be submitted to the Engineer for approval of color and texture prior to the application of the material.

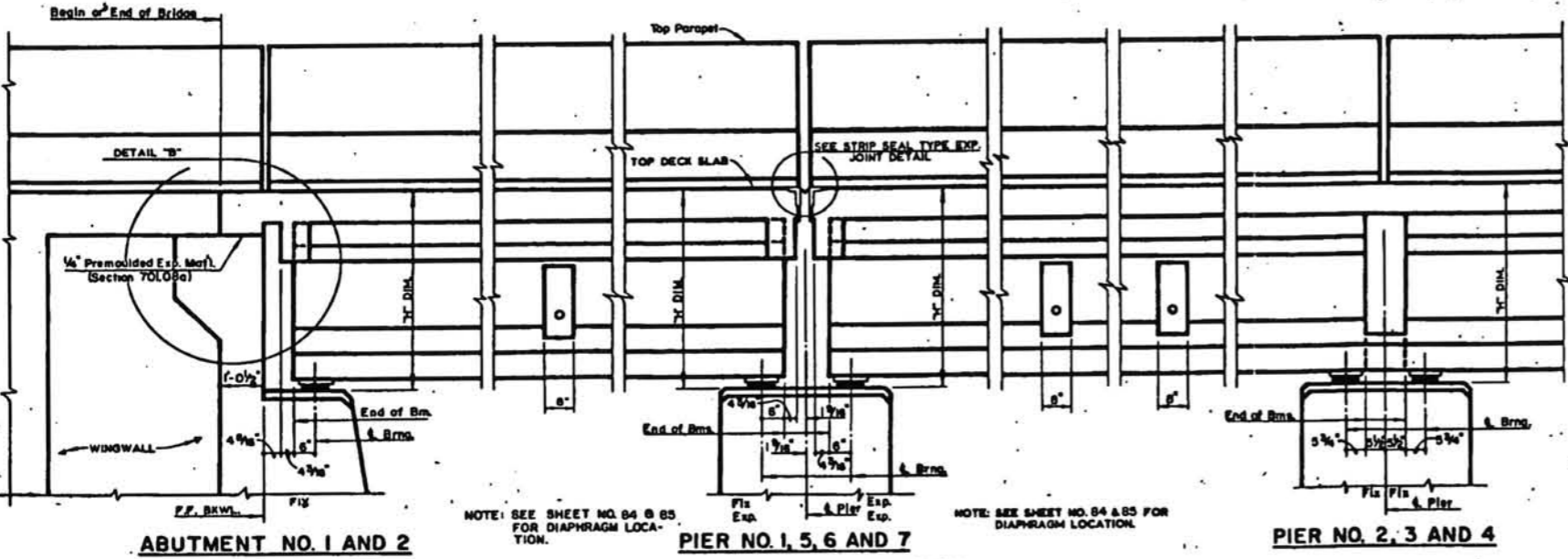
All cost of concrete surface finish shall be included in the price bid for P.C. Beams, Class AA Concrete, or Concrete Parapet and no additional compensation will be allowed for color or texture of the finish.

NOTE: Reference STD. SFP-1 for Parapet Details and Notes. Span No. 1 and 8 shall be constructed without drain openings. Span No. 2 thru 7 shall be constructed with drain openings.

Structural Steel Quantities shown include quantities for Steel Angle Bumpers, Diaphragm Rod Assemblies and P.C. Beam Bearing Assemblies. All Structural Steel shall be A36 Steel. All cost for Elastomeric Bearing Pads shall be included in the price bid for other items of work.



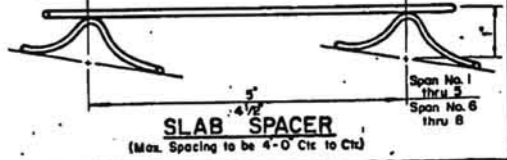
CONCRETE FINISHING DETAIL



LONGITUDINAL SECTION

| "H" DIMENSIONS | |
|------------------------|--------------|
| LOCATION | "H" DIM. (1) |
| ABUTMENT NO. 1 | 5.417 |
| SPAN NO. 1, PIER NO. 1 | 5.448 |
| SPAN NO. 2, PIER NO. 1 | 5.521 |
| PIER NO. 2 | 5.417 |
| PIER NO. 3 | 5.417 |
| PIER NO. 4 | 5.417 |
| SPAN NO. 5, PIER NO. 5 | 5.521 |
| SPAN NO. 6, PIER NO. 6 | 5.448 |
| SPAN NO. 6, PIER NO. 6 | 5.417 |
| SPAN NO. 7, PIER NO. 6 | 5.448 |
| SPAN NO. 7, PIER NO. 7 | 5.417 |
| SPAN NO. 8, PIER NO. 7 | 5.448 |
| ABUTMENT NO. 2 | 5.417 |

(1) DIMENSION MEASURED FROM TOP OF SLAB TO BOTTOM OF BEARING ASSEMBLY.



| QUANTITIES | | |
|---|------|----------|
| ITEM | UNIT | TOTAL |
| CLASS "AA" CONCRETE | C.Y. | 1,659.3 |
| REINFORCING STEEL (GR. 60) | L.B. | 249,705 |
| STRUCTURAL STEEL (A36) | L.B. | 27,660 |
| P.C. BEAMS (TYPE IV) | L.F. | 7,514.39 |
| STRIP SEAL TYPE EXP. JT. W/ANGLES | L.F. | 303.7 |
| EPOXY COATED REINFORCING STEEL (GR. 60) | L.B. | 221,680 |
| PENET. WATER REPEL. SURFACE TREATMENT | S.Y. | 6,885.0 |
| CONCRETE PARAPET | L.F. | 1,593.0 |

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

**STRUCTURE "C"
SUPERSTRUCTURE DETAILS**

| REV | DATE | BY | CHKD | APPD |
|-----|-------|----|------|------|
| 1 | 08/14 | | | |

DESIGN DATA

Design: AASHTO Specifications, 1976 Edition with Supplements
 Live Load: HS 20-44
 Structural Steel: 20,000 psi
 Reinforcing Steel: (Gr. 40) 20,000 psi
 Reinforcing Steel: (Gr. 60) 24,000 psi
 CONCRETE:
 Class "A" 1,000 psi
 Class "AA" 1,200 psi
 MAXIMUM FOUNDATION LOADS:
 ABUTMENTS 51.4 Tons/Pile
 PIERS
 Friction Capacity 400.6 Tons/Shaft
 Bearing Capacity 218.0 Tons/Shaft
 Total Capacity 618.6 Tons/Shaft
 Max. Shaft Load 518.0 Tons/Shaft
 Allowable Friction Load 3.3 Tons/S.F.
 Allowable Point Bearing Load 11.0 Tons/S.F.
 HYDRAULIC DATA:
 DRAINAGE AREA 13,354 Sq Miles
 #0500=99,000 cfs, High Water El. 1181.90 v = 7.3 fps
 #0100=47,500 cfs, High Water El. 1174.40 v = 6.6 fps
 #050=33,100 cfs, High Water El. 1171.40 v = 5.9 fps
 # Denotes Data From Corp of Engineers.

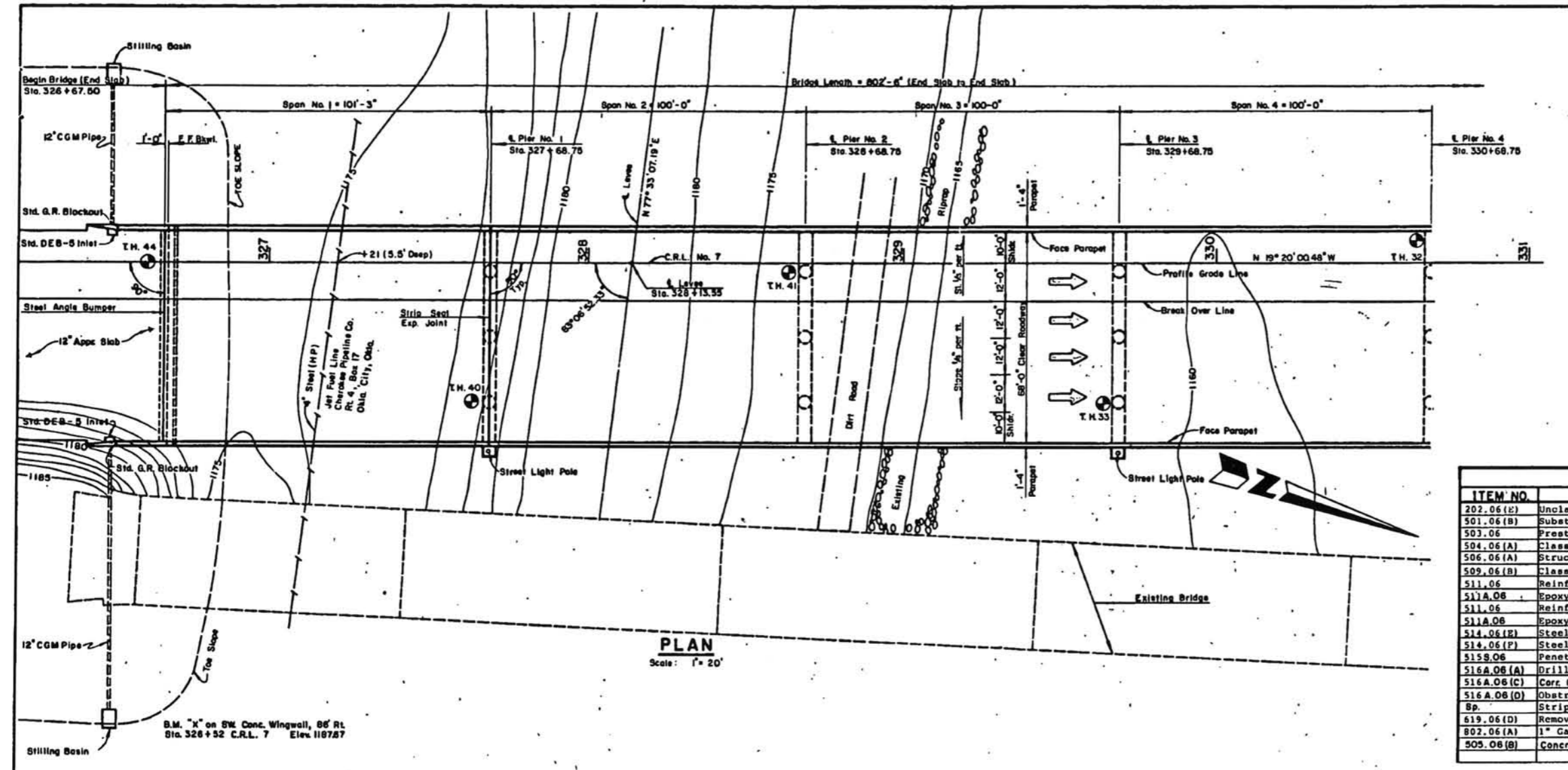
SUMMARY OF QUANTITIES

| ITEM NO. | ITEM | UNIT | ABUTS. | PIERS | SUPSTR. | TOTAL |
|-------------|--|--------|---------|---------|----------|----------|
| 202.06 (E) | Unclassified Borrow | C.Y. | 401. | | | 401. |
| 501.06 (B) | Substructure Excavation, Common | C.Y. | 161. | 35. | | 196. |
| 503.06 | Prestressed Concrete Beam (Type IV) | L.F. | | | 7,643.62 | 7,643.62 |
| 504.06 (A) | Class "A" Concrete | C.Y. | | | 1,691.6 | 1,691.6 |
| 506.06 (A) | Structural Steel (A36) | LB. | 680. | 26,590. | | 27,270. |
| 509.06 (B) | Class "A" Concrete | C.Y. | 178.7 | 606.4 | | 785.1 |
| 511.06 | Reinforcing Steel (Gr. 60) | LB. | | | 264,310. | 264,310. |
| 511A.06 | Epoxy Coated Reinforcing Steel (Gr. 60) | LB. | | | 221,910. | 221,910. |
| 511.06 | Reinforcing Steel (Gr. 40) | LB. | 21,790. | 34,220. | | 56,010. |
| 511A.06 | Epoxy Coated Reinforcing Steel (Gr. 40) | LB. | | 92,275. | | 92,275. |
| 514.06 (E) | Steel Piling (HP 12 x 53) | L.F. | 1,993.5 | | | 1,993.5 |
| 514.06 (F) | Steel Piling Solices (HP 12 x 53) @ | EA. | 38. | | | 38. |
| 5159.06 | Penetrating Water Repellent Surface Treat. | S.Y. | 127. | 996.3 | 7,000.0 | 8,123.3 |
| 516A.06 (A) | Drilled Shafts (60" Diameter) | L.F. | | 1,110.5 | | 1,110.5 |
| 516A.06 (C) | Corr. Galv. Steel Pipe (60" Dia.) | L.F. | | 780.5 | | 780.5 |
| 516A.06 (D) | Obstructions | C.Y. | | | | 72. |
| Bp. | Strip Seal Type Expansion Joint W/ Angles | L.F. | | | 299.3 | 299.3 |
| 619.06 (D) | Removal of Existing Bridge Structure | L.S.U. | | | | 1. |
| 802.06 (A) | 1" Galv. Steel Elect. Conduit | L.F. | | 9.2 | | 9.2 |
| 505.06 (B) | Concrete Parapet | L.F. | 74.6 | | 1,604.1 | 1,678.7 |

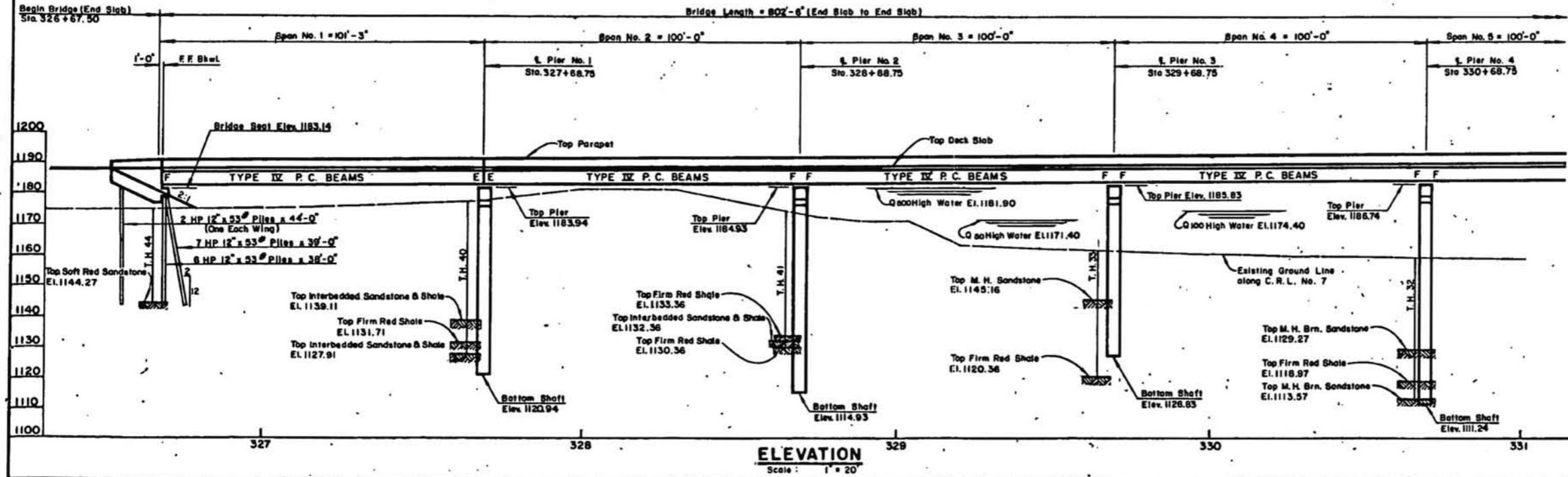
ⓐ A Maximum of One Pile Splice will be allowed for each Pile.

INDEX OF SHEETS

| SHEET NO. | TITLE |
|-----------|--|
| 93 & 94 | GENERAL PLAN AND ELEVATION AND SUMMARY OF QUANTITIES |
| 95 | BRIDGE STAKING DIAGRAM |
| 96-98 | ABUTMENT DETAILS |
| 99-102 | PIER DETAILS |
| 103-111 | SUPERSTRUCTURE DETAILS |
| | STD. P-1 |
| | STD. EXP-1 |
| | STD. DEB-3-3 |
| | STD. GRAU-3-2 |
| | STD. GRN-2-2 |
| | STD. SSIF-1-0 |
| | STD. SSO-1-5 |
| | STD. SFP-1-0 |



PLAN
Scale: 1" = 20'



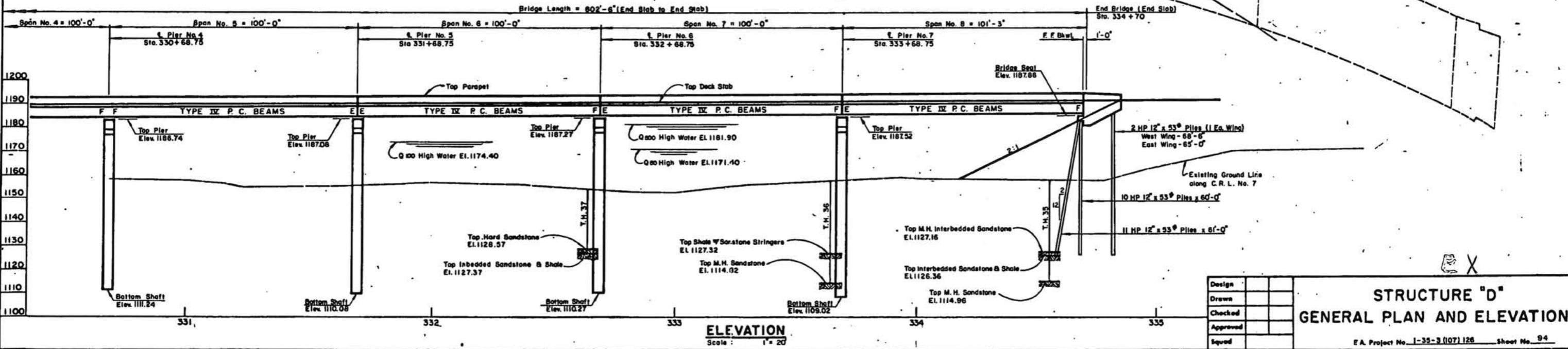
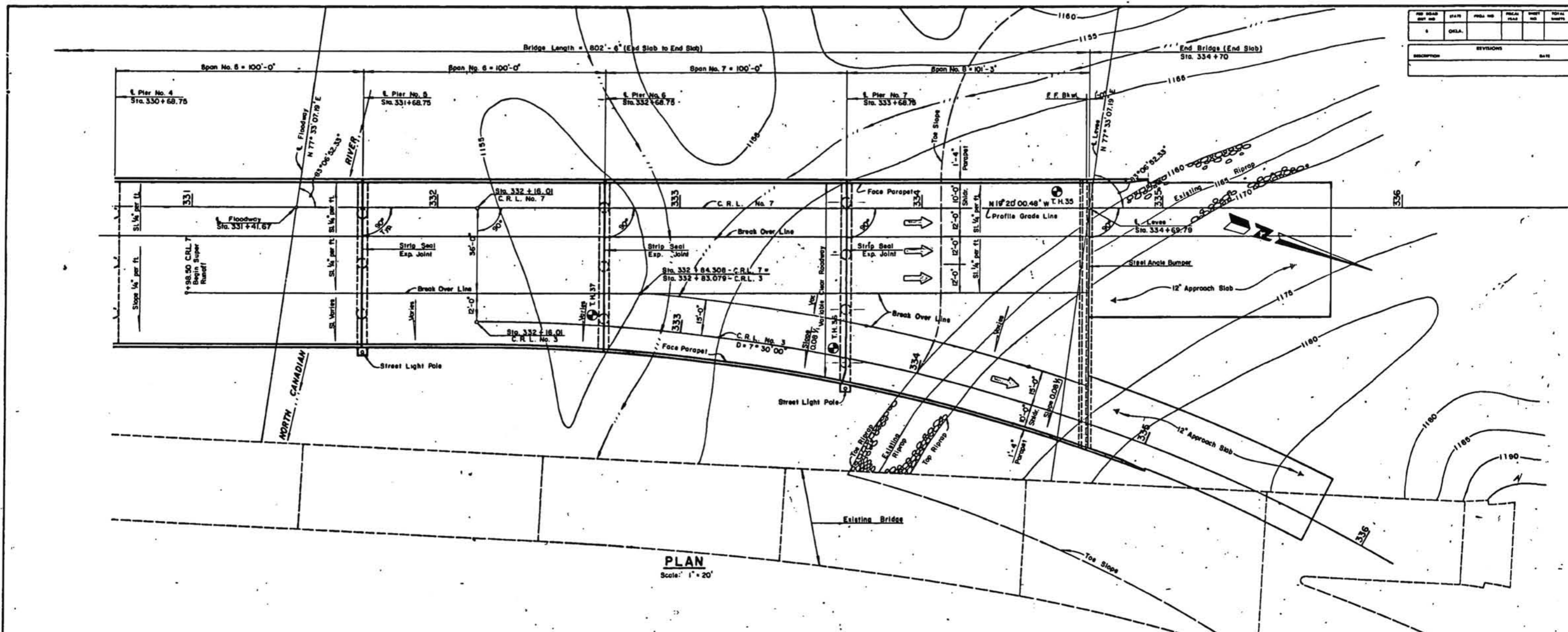
ELEVATION
Scale: 1" = 20'

I-35 BRIDGE OVER NORTH CANADIAN RIVER
 101'-3" - 4(100') - 100' - 100' - 101'-3" PRESTRESSED CONCRETE BEAM SPANS
 VARIABLE CLEAR ROADWAY WITH 1'-4" PARAPETS ON EACH SIDE

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

STRUCTURE "D" & STA. 330 + 68.75
GENERAL PLAN AND ELEVATION
AND SUMMARY OF QUANTITIES
 E.A. Project No. I-35-3(107)126 Sheet No. 93

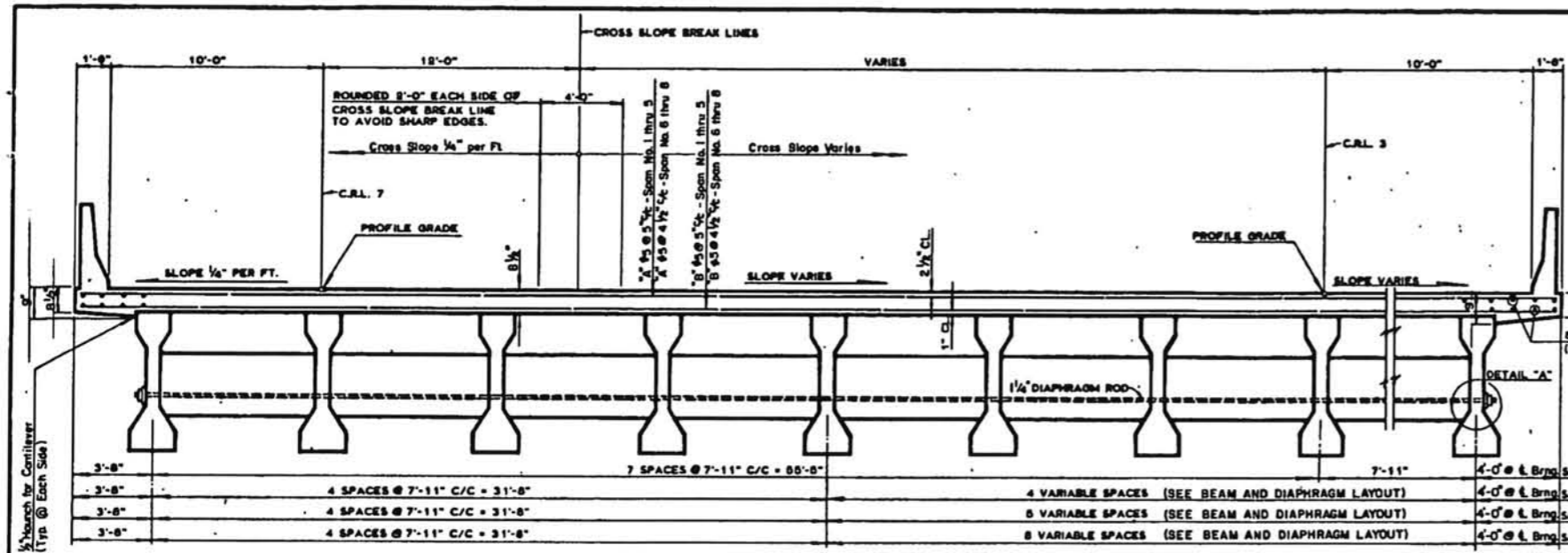
| NO. | DATE | BY | CHKD. | APP'D. | REVISIONS |
|-----|------|----|-------|--------|-----------|
| 1 | | | | | |



| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

STRUCTURE "D"
GENERAL PLAN AND ELEVATION

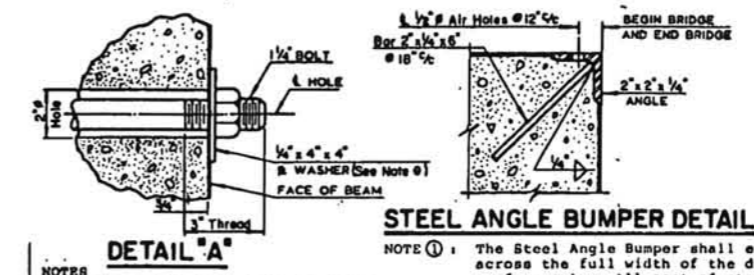
E.A. Project No. 1-35-3 (07) 128 Sheet No. 94



BRIDGE SECTION

FLOOR: 8 1/2" uniform slab thickness, Variable Clear Roadway with concrete parapet each side. Class "AA" Concrete, Maximum aggregate 1 1/2". All construction shall conform to the 1976 Oklahoma Standard Specifications. Surface of Roadway shall slope as shown. All exposed edges shall have a 1/4" chamfer unless otherwise noted. Transverse reinforcing steel in the floor slab shall begin and end with "A" and "B" Bars a maximum of 2" from end of concrete panels. All reinforcing steel bars in top of slab shall be supported on approved metal high chairs. The maximum spacing of the metal chairs shall be 1/2 beam spacing or a maximum of 4'-0" on center. Bottom layer of reinforcing steel shall be supported on continuous steel slab spacers as shown. Longitudinal bars shall begin and end 1" from ends of slab. Minimum Compressive Strength for Class "AA" Concrete shall be 3,000 psi at 28 days. All reinforcing steel in Deck Slab, Diaphragms and Parapets shall be Grade 60. Reference Bar List on Sheet No. 107 for Reinforcing Bars to be Epoxy Coated. Penetrating Water Repellent Surface Treatment shall be applied to the Deck Slab and Roadway Face of Parapets.

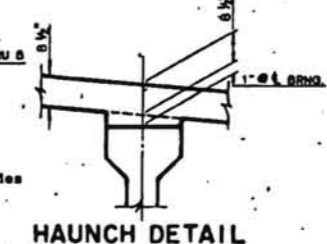
NOTE: Reference STD. SPP-1 for parapet Details and Notes. Span No. 1 and 8 shall be constructed without drain openings. Span No. 2 thru 7 shall be constructed with drain openings.



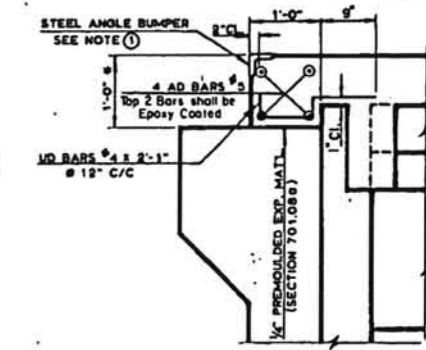
STEEL ANGLE BUMPER DETAIL

NOTE 1: The Steel Angle Bumper shall extend across the full width of the driving surface only. All cost of steel bumper shall be included in the price bid for Structural Steel. Contractor shall be responsible for adequately supporting steel bumper in place.

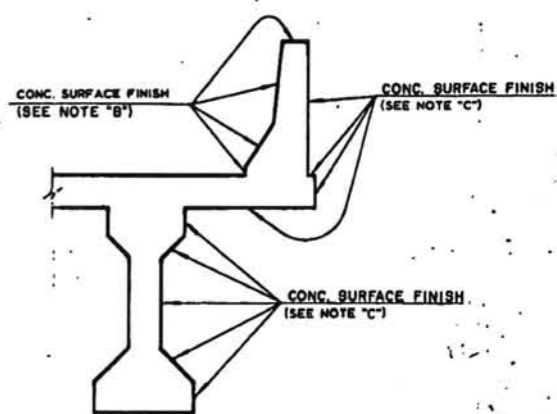
NOTE 2: Span No. 7 & 8 Washers shall be beveled when diaphragms are skewed to the beam. Minimum washer thickness shall be 1/4".



HAUNCH DETAIL

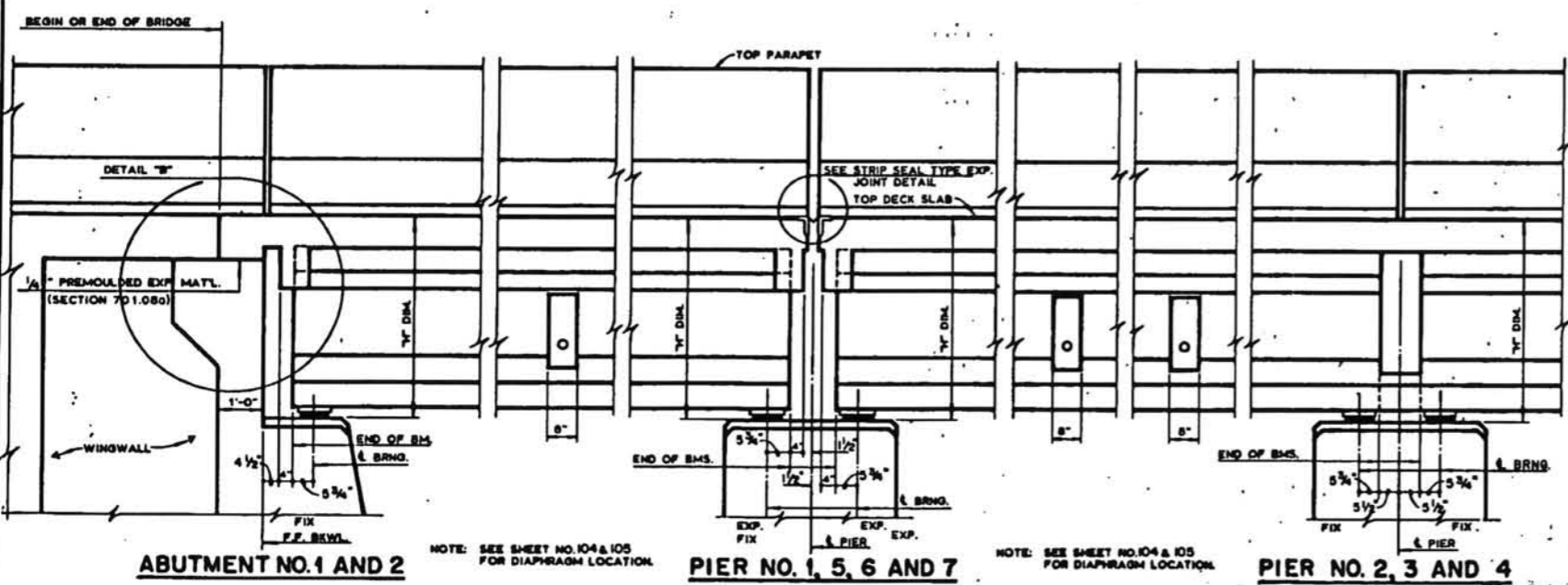


DETAIL "B"

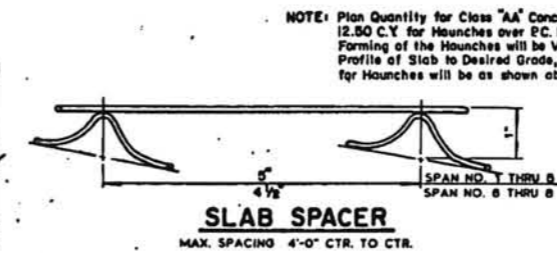


CONCRETE FINISHING DETAIL

REFERENCE SHEET No. 83 for CONCRETE SURFACE FINISH NOTE "B" & "C".



LONGITUDINAL SECTION



SLAB SPACER

MAX. SPACING 4'-0" CTR. TO CTR.

| "H" DIMENSIONS | |
|------------------------|--------------|
| LOCATION | "H" DIM. (1) |
| ABUTMENT NO. 1 | 5.417' |
| SPAN NO. 1, PIER NO. 1 | 5.448' |
| SPAN NO. 8, PIER NO. 1 | 5.521' |
| PIER NO. 2 | 5.417' |
| PIER NO. 3 | 5.417' |
| PIER NO. 4 | 5.417' |
| SPAN NO. 5, PIER NO. 5 | 5.521' |
| SPAN NO. 6, PIER NO. 6 | 5.448' |
| SPAN NO. 6, PIER NO. 6 | 5.417' |
| SPAN NO. 7, PIER NO. 6 | 5.448' |
| SPAN NO. 7, PIER NO. 7 | 5.417' |
| SPAN NO. 8, PIER NO. 7 | 5.448' |
| ABUTMENT NO. 2 | 5.417' |

(1) Dimension measured from Top of Slab to Bottom of Bearing Assembly.

NOTE: Plan Quantity for Class "AA" Concrete includes 12.50 C.Y. for Haunches over P.C. Beams. The Forming of the Haunches will be Varied to bring Profile of Slab to Desired Grade, but Quantity for Haunches will be as shown above.

Structural Steel Quantities shown include quantities for Steel Angle Bumpers, Diaphragm Rod Assemblies and P.C. Beam Bearing Assemblies. All Structural Steel shall be A36 Steel. All cost for Elastomeric Bearing Pads shall be included in the price bid for other items of work. Reference Sheet No. 83 for Concrete Finishing Detail and Notes.

| QUANTITIES | | |
|---|------|----------|
| ITEM | UNIT | TOTAL |
| CLASS "AA" CONCRETE | C.Y. | 1,691.6 |
| REINFORCING STEEL (GR. 60) | LBS. | 264,310. |
| STRUCTURAL STEEL (A36) | LBS. | 26,590. |
| P.C. BEAMS (TYPE IV) | L.F. | 7,643.62 |
| STRIP SEAL TYPE EXP. JOINT | L.F. | 299.3 |
| EPOXY COATED REINFORCING STEEL (GR. 60) | LBS. | 221,910. |
| PENET. WATER REPEL SURFACE TREATMENT | S.Y. | 7,000. |
| CONCRETE PARAPET | L.F. | 1,604.1 |

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

STRUCTURE "D"
SUPERSTRUCTURE DETAILS

Sub. 6-28-85

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED
STATE HIGHWAY

F.A.P. NO. I-235-1(019) 000

OKLAHOMA COUNTY

CONTROL SECTION NO. 235-55-42

STATE JOB NO. 00302 (04)

| FED. ROAD DIST. NO. | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|------------------|-----------|--------------|
| 6 | FAP I-235-1(019) | 1 | 778 |

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|--------|--|
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| | |
|----------|----------|
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| TCO-4-3 | SAAB-2-3 |
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| TCO-6-2 | PUD-1-17 |
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| | |
|-----|--|
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| 25a | PLAN SHEET - DETOUR 2 PHASE II |
| 28a | PROFILE SHEET - DETOUR 5 & DETOUR 2 PHASE II |
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| 72b | DETAILS OF ABUTMENTS - BRIDGE "T" |
| 72c | R. C. BEAMS - BRIDGE "T" |
| 72d | MISCELLANEOUS DETAILS - BRIDGE "T" |

THE BEARINGS SHOWN ON THESE PLANS ARE GRID BEARINGS BASED ON AND DERIVED FROM THE USC & G.S. OKLAHOMA HIGHWAY DEPARTMENT PLANE COORDINATE SYSTEM, LAMBERT PROJECTION (NORTH ZONE) AND ARE NOT ASTRONOMICAL.

DESIGN DATA

| | |
|----------------------|-------------|
| ADT - 1983 | = 84,000 |
| ADT - 2003 | = 110,000 |
| DHV (ONE WAY) | = 7,018 |
| D | = 55 % |
| T (ADT) | = 12 % |
| T (DHV) | = 8 % |
| T ₃ (ADT) | = 9 % |
| V | = 60 M.P.H. |

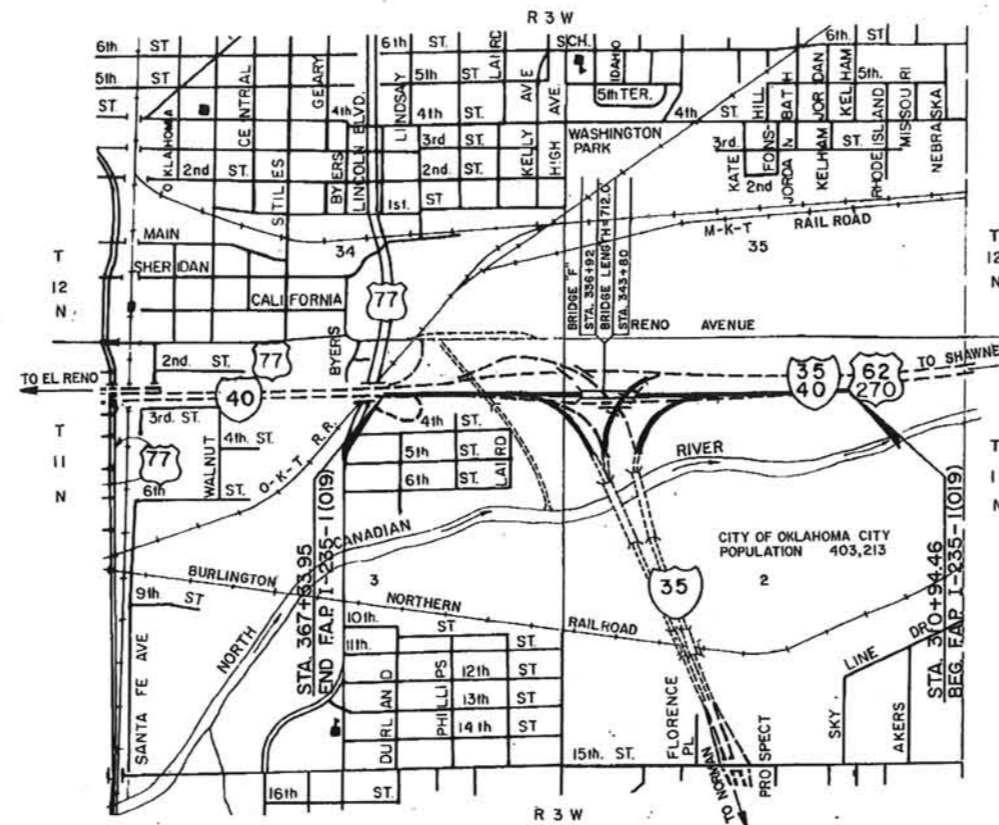
SCALES

| | |
|--|------------|
| PLAN | 1" = 50' |
| PROFILE HOR. | 1" = 50' |
| VER. | 1" = 10' |
| LAYOUT MAP | 1" = 1000' |
| LEVEL DATUM IS MEAN SEA LEVEL (USC & GS) | |

CONVENTIONAL SIGNS

| | |
|--|---------------------------------------|
| | PROPOSED ROAD |
| | RAILROADS |
| | RANGE & TOWNSHIP |
| | SECTION LINES |
| | QUARTER SECTION LINES |
| | FENCES |
| | GROUND LINE |
| | EXISTING ROADS |
| | BASE LINE |
| | GRADE LINES |
| | TELEPHONE & TELEGRAPH |
| | POWER LINES |
| | OIL WELLS |
| | BUILDINGS |
| | DRAINAGE STRUCTURES - IN PLACE |
| | DRAINAGE STRUCTURES - NEW |
| | RIGHT-OF-WAY LINES - EXISTING |
| | RIGHT-OF-WAY LINES - NEW |
| | RIGHT-OF-WAY MARKERS - IN PLACE |
| | RIGHT-OF-WAY MARKERS - REMOVE & RESET |
| | RIGHT-OF-WAY MARKERS - NEW |
| | CONTROLLED ACCESS |
| | RIGHT-OF-WAY FENCE |

1976 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION GOVERN, APPROVED BY THE DEPARTMENT OF TRANSPORTATION AND FEDERAL HIGHWAY ADMINISTRATION, MAY 4, 1976.
SUPPLEMENTAL SPECIFICATIONS TO 1976 STANDARD SPECIFICATIONS GOVERN OVER THE STANDARD SPECIFICATIONS.
SPECIAL PROVISIONS GOVERN OVER STANDARD SPECIFICATIONS AND SUPPLEMENTAL SPECIFICATIONS.



| | | | | |
|----------------|---------|------|--------|-----|
| ROADWAY LENGTH | 4927.49 | FT. | 0.9332 | MI. |
| BRIDGE LENGTH | 712.0 | FT. | 0.1349 | MI. |
| PROJECT LENGTH | | | 1.0681 | MI. |
| EQUATIONS | | NONE | | |
| EXCEPTIONS | | NONE | | |

| F.A.P. I-235-1(019)000 | |
|------------------------|---------------------------------|
| GRADING & SURFACING | BRIDGES |
| 1-11, 13-50 73-111 | 1, 12, 15, 16, 20, 36, 51-72 |

Under the rules and regulations of the Federal Highway Administration, any changes to the design, including entrances to through lanes or location of median openings shall first have written approval from the Oklahoma Department of Transportation and the Federal Highway Administration before executing any work.

PREPARED AND CHECKED BY
THE ENGINEERING GROUP
CONSULTING ENGINEER
OKLAHOMA CITY, OKLA.
P.E. No. 1353

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION |
|---------------------------------------|---|
| DATE APPROVED _____ | DATE APPROVED _____ |
| BY _____ | BY _____ |
| CHIEF ENGINEER | DIVISION ADMINISTRATOR |
| SWO. 2969 (1) | F.A. Project No. I-235-1(019)000 Sheet No. 1 |

NBT 21722

| DATE | BY | REVISION |
|-------|----|----------|
| 11/13 | | |

SEC. 2, T111, R3W.
FLOODWAY PARK LAND

CURVE DATA
Curve No. 5
P.I. Sta. 343+07.56 LINE 3
X=2,151,430.922
Y=168,620.582
Δ=110°01'33.85" RT
D=7°30'00"
T=1091.55'
L=1467.01'
R=763.9437'
E=568.38'
V=50 M.P.H.
e=0.08%
s=0.08%

CURVE DATA
Curve No. 20
P.I. Sta. 354+59.64 Line 4
X=2,151,585.179
Y=168,768.939
Δ=48°26'10.19" RT
D=8°00'00"
T=322.14'
L=605.45'
R=716.20'
E=69.11'
V=50 M.P.H.
e=0.08%
s=0.08%

MATCH SHEET NO. 14
STA. 329+00

STA. 344+00
MATCH SHEET NO. 16

NOTE:
For Section C-C see Sheet No. 3.
For Section D-D see Sheet No. 4.
For Section F-F see Sheet No. 5.
For Section O-O see Sheet No. 4.
For Section P-P see Sheet No. 4.

FOR PROFILE OF LINE 1 SEE SHEET NO. 20
FOR PROFILE OF LINE 3 SEE SHEET NO. 21
FOR PROFILE OF LINE 4 SEE SHEET NO. 22

DAUGHERTY'S FACTORY
ADDITION

RENO AVE
ADDITION

LINE 1, 3&4
PHASE I

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

FA. Project No. J-235-1(019) Sheet No. 15

| | | | | | |
|---------------------|-------|-----------|----------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FILE NO. | SHEET NO. | TOTAL SHEETS |
| 6 | OKLA. | 1-235-119 | | 45 | |
| DESCRIPTION | | | | | REV'S/DNS |
| | | | | | DATE |

DESIGN DATA

LOADING HS20+20 psf FUT. W.S. OKLA. O. TRUCK

DESIGN LFD AASHTO

CONCRETE
 CL AA f'c = 3000 PSI
 CL A f'c = 3000 PSI

REINFORCING STEEL
 GRADE 60 fy = 60,000 PSI
 GRADE 40 fy = 47,000 PSI

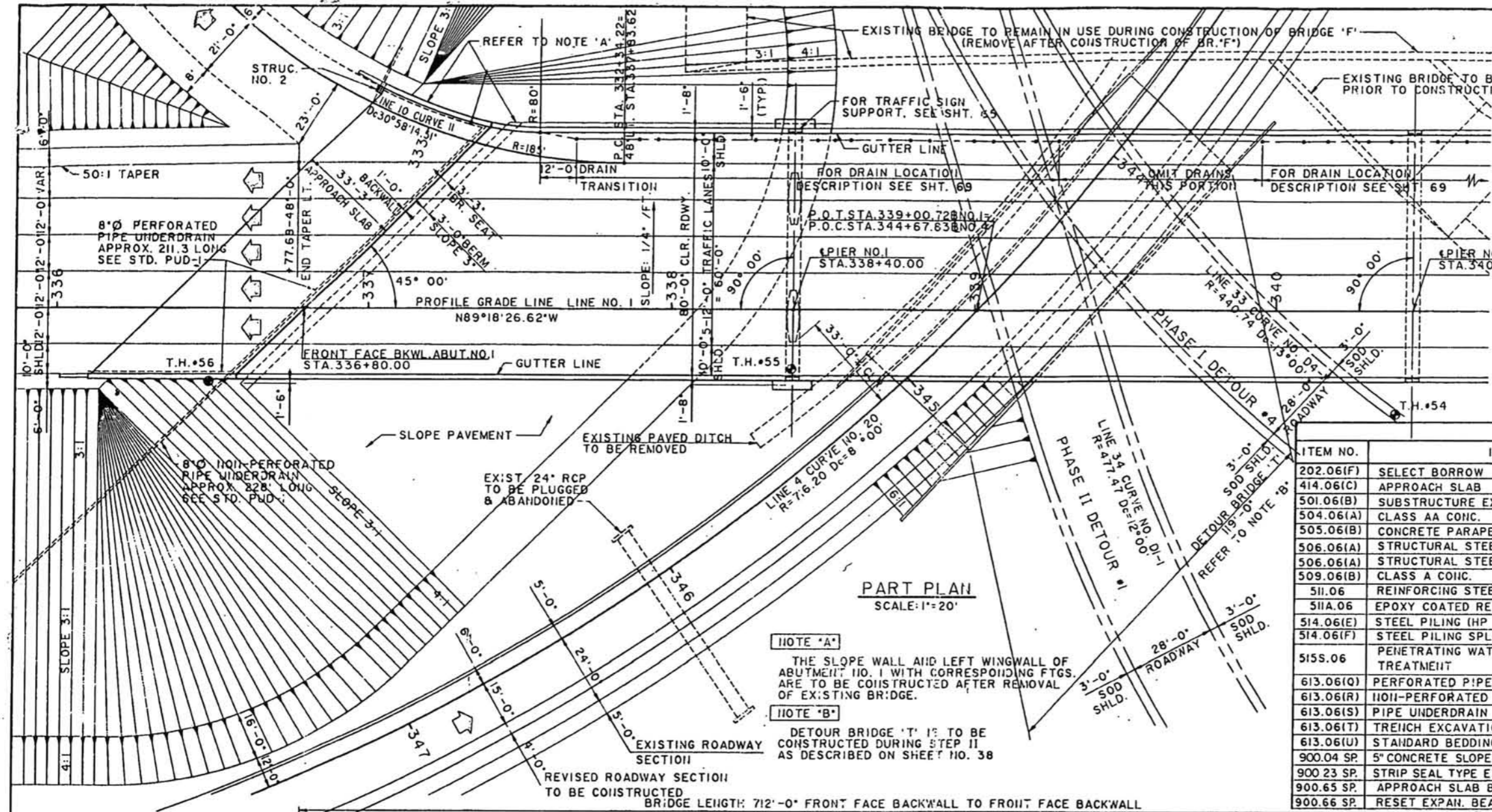
STRUCTURAL STEEL
 A-588 fy = 50,000 PSI
 A-36 fy = 36,000 PSI

DESIGN SPEC: AASHTO SPECIFICATIONS 1977 EDITION AND INTERIM SPECIFICATIONS

FOUNDATION PRESSURE
 ABUTMENT (DIRECT) 55.7 TONS/PILE
 PIER (DIRECT) 61.2 TONS/PILE

SUMMARY OF QUANTITIES

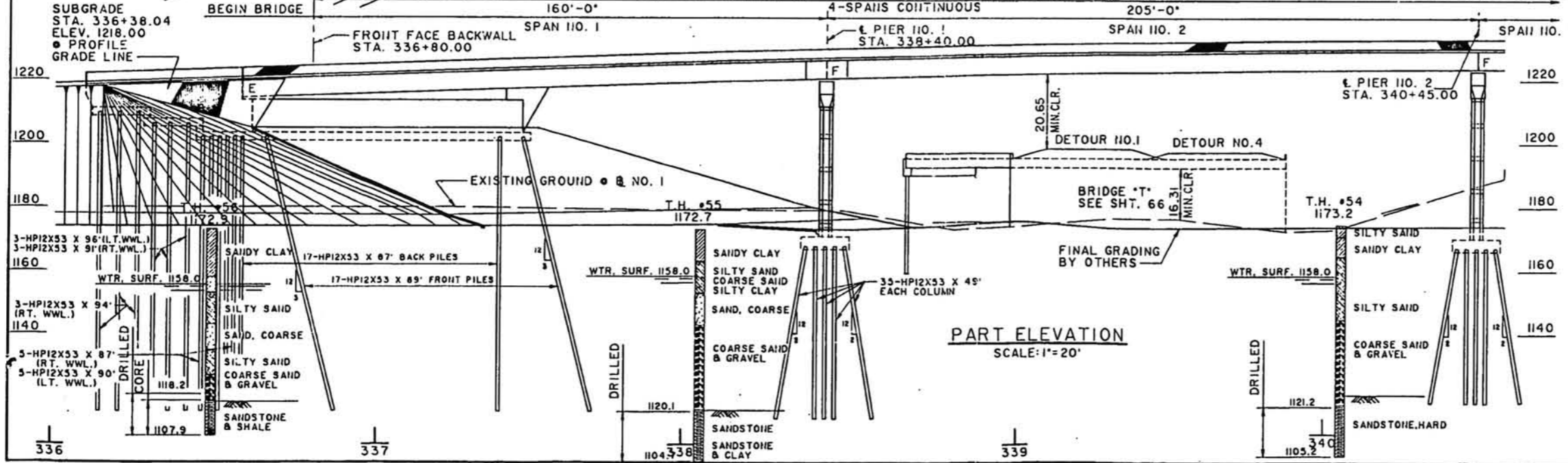
| ITEM NO. | ITEM | UNIT | ABUTS. | PIERS | SUPER | TOTAL |
|-----------|---|------|---------|----------|------------|------------|
| 202.06(F) | SELECT BORROW | C.Y. | 1,415. | | | 1,415. |
| 414.06(C) | APPROACH SLAB | S.Y. | 763.7 | | | 763.7 |
| 501.06(B) | SUBSTRUCTURE EXC. COMMON | C.Y. | 1,200. | 970. | | 2,170. |
| 504.06(A) | CLASS AA CONC. | C.Y. | | | 1,664.8 | 1,664.8 |
| 505.06(B) | CONCRETE PARAPET | L.F. | 153.6 | | 1,385.9 | 1,539.5 |
| 506.06(A) | STRUCTURAL STEEL A-36 | L.B. | | | 1,477,370. | 1,477,370. |
| 506.06(A) | STRUCTURAL STEEL A-588 | L.B. | | | 704,760. | 704,760. |
| 509.06(B) | CLASS A CONC. | C.Y. | 780.5 | 1,068.5 | | 1,849. |
| 511.06 | REINFORCING STEEL (GR. 40) | L.B. | 61,200. | 180,860. | | 242,060. |
| 511A.06 | EPOXY COATED REINF. STEEL (GR. 60) | L.B. | | | 358,360. | 358,360. |
| 514.06(E) | STEEL PILING (HP 12X53) | L.F. | 8,035. | 10,010. | | 18,045. |
| 514.06(F) | STEEL PILING SPLICES (HP 12X53) | EA. | 92. | 210. | | 302. |
| 515S.06 | PENETRATING WATER REPELLENT SURFACE TREATMENT | S.Y. | 496.4 | 535.8 | 6,721.5 | 7,753.7 |
| 613.06(I) | PERFORATED PIPE UNDERDRAIN | L.F. | 359.8 | | | 359.8 |
| 613.06(R) | HOH-PERFORATED PIPE UNDERDRAIN | L.F. | 478. | | | 478. |
| 613.06(S) | PIPE UNDERDRAIN COVER MATERIAL | C.Y. | 167.5 | | | 167.5 |
| 613.06(T) | TRENCH EXCAVATION | C.Y. | | | | 88.5 |
| 613.06(U) | STANDARD BEDDING MATERIAL | C.Y. | | | | 82.3 |
| 900.04 SP | 5" CONCRETE SLOPE WALL | S.Y. | 3125.5 | | | 3125.5 |
| 900.23 SP | STRIP SEAL TYPE EXP. JT. W/ANGLES | L.F. | | | 200.8 | 200.8 |
| 900.65 SP | APPROACH SLAB BASE | S.Y. | 763.7 | | | 763.7 |
| 900.66 SP | RESET EXPAN. BEARINGS | EA. | | | 20. | 20. |



NOTE 'A'
THE SLOPE WALL AND LEFT WINGWALL OF ABUTMENT NO. 1 WITH CORRESPONDING FTGS. ARE TO BE CONSTRUCTED AFTER REMOVAL OF EXISTING BRIDGE.

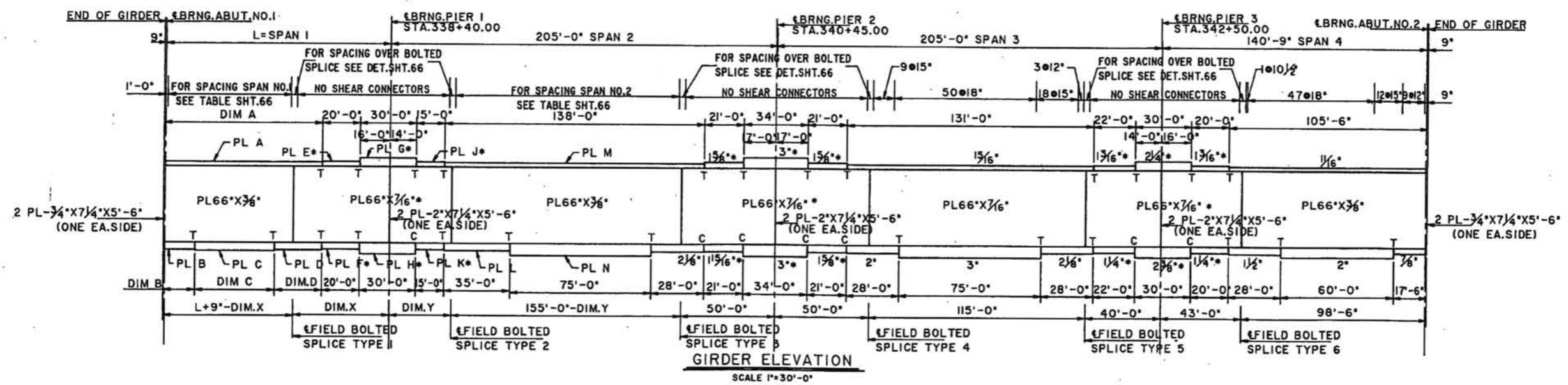
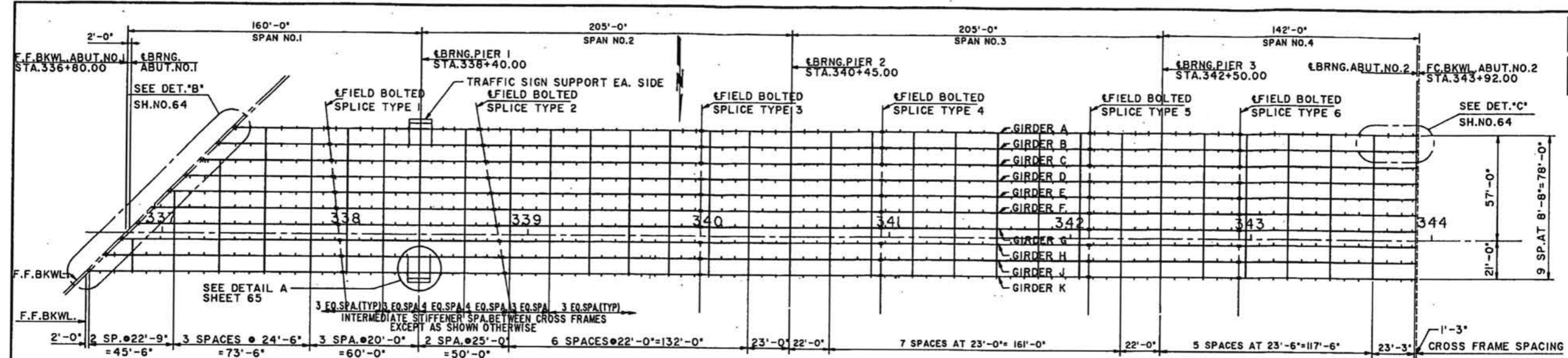
NOTE 'B'
DETOUR BRIDGE 'T' IS TO BE CONSTRUCTED DURING STEP II AS DESCRIBED ON SHEET NO. 38

NOTE: WATER SURFACE ELEVATIONS SHOWN ON THE BORING LOGS WERE DETERMINED DURING DRILLING OPERATIONS OF THE SUBSURFACE INVESTIGATION PERFORMED DURING THE MONTHS OF MAY AND JUNE, 1983. FLUCTUATIONS OF THE WATER TABLE CAN BE EXPECTED WITH SEASONAL VARIATIONS IN THE AMOUNT OF RAINFALL, RUNOFF, EVAPORATION, AND OTHER CONDITIONS NOT EVIDENT AT THE TIME THE BORINGS WERE PERFORMED.



| | | |
|----------|-----|---|
| Design | JFB | BRIDGE F GENERAL PLAN AND ELEVATION 160'-205'-205'-142' CONT. PL. GIRDERS 80'-0" CLEAR ROADWAY WITH PARAPETS F.A. Project No. 1-235-119(19)000 Sheet No. 52 |
| Drawn | JCS | |
| Checked | LRB | |
| Approved | | |
| Squad | | |

| | | | | | |
|-------------|-------|-----------|-------------|-----------|--------------|
| PROJ. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| 6 | OKLA. | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |



ALL FLANGE PLATES ARE 16" WIDE.

DIMENSION AND FLANGE PLATE THICKNESS SCHEDULE

NOTES: (1) ALL STRUCTURAL STEEL, EXCEPT NOTED THUS (*), SHALL BE A-36 (M-183)
(2) ALL STEEL NOTED THUS (*) SHALL BE A-588 (M-222)

| GIRDER | SPAN LENGTH L | DIM. A | PL A | DIM. B | PL B | DIM. C | PL C | DIM. D | PL D | PL E | PL F | PL G | PL H | PL J | PL K | PL L | PL M | PL N | DIM. X | DIM. Y |
|--------|---------------|---------|---------|--------|---------|--------|---------|--------|----------|---------|---------|----------|--------|---------|---------|--------|---------|--------|--------|--------|
| A | 101'-0" | 65'-9" | 11/16" | 15'-9" | 1/2" | 30'-0" | 15/16" | 20'-0" | 15/16" | 7/8" | 1 1/16" | 1 13/16" | 2" | 13/16" | 15/16" | 2 5/8" | 1 3/16" | 3" | 51'-0" | 33'-0" |
| B | 109'-8" | 74'-5" | 11/16" | 16'-5" | 1/2" | 36'-0" | 15/16" | 22'-0" | 15/16" | 7/8" | 1 1/16" | 1 13/16" | 2" | 13/16" | 15/16" | 2 5/8" | 1 3/16" | 3" | 50'-0" | 34'-6" |
| C | 118'-4" | 83'-1" | 11/16" | 17'-1" | 1/2" | 41'-0" | 1 1/16" | 25'-0" | 1 1/16" | 15/16" | 1 1/8" | 1 7/8" | 2 1/8" | 7/8" | 1" | 2 5/8" | 1 3/16" | 3" | 49'-0" | 36'-3" |
| D | 127'-0" | 91'-9" | 11/16" | 17'-9" | 3/4" | 47'-0" | 1 3/8" | 27'-0" | 1 1/4" | 1 1/16" | 1 1/8" | 2" | 2 1/8" | 7/8" | 1" | 2 5/8" | 1 3/16" | 3" | 48'-0" | 37'-9" |
| E | 135'-8" | 100'-5" | 11/16" | 20'-5" | 13/16" | 50'-0" | 1 1/16" | 30'-0" | 1 7/16" | 1 1/16" | 1 1/8" | 2 1/8" | 2 1/8" | 1 1/16" | 1 1/16" | 2 1/4" | 1" | 3" | 47'-0" | 39'-3" |
| F | 144'-4" | 109'-1" | 5/8" | 23'-1" | 1 1/16" | 54'-0" | 2" | 32'-0" | 1 5/8" | 1 1/4" | 1 5/16" | 2 3/8" | 2 1/2" | 1 5/16" | 1 5/16" | 2 1/4" | 1" | 2 7/8" | 46'-0" | 41'-0" |
| G | 153'-0" | 117'-9" | 5/8" | 23'-9" | 1 1/4" | 60'-0" | 2 3/8" | 34'-0" | 1 15/16" | 1 5/16" | 1 5/16" | 2 1/2" | 2 1/2" | 1 5/16" | 1 3/8" | 2 1/4" | 1" | 2 7/8" | 45'-0" | 42'-6" |
| H | 161'-8" | 126'-5" | 13/16" | 26'-5" | 1 1/2" | 63'-0" | 2 5/8" | 37'-0" | 2 1/4" | 1 3/16" | 1 1/4" | 2 1/2" | 2 3/4" | 1 7/16" | 1 7/16" | 2 1/4" | 1" | 2 3/4" | 44'-0" | 44'-0" |
| J | 170'-4" | 135'-1" | 1 1/8" | 29'-1" | 1 3/4" | 67'-0" | 3" | 39'-0" | 2 5/8" | 1 3/8" | 1 1/2" | 2 5/8" | 2 3/4" | 1 1/2" | 1 9/16" | 2 1/4" | 1" | 2 3/4" | 43'-0" | 45'-9" |
| K | 179'-0" | 143'-9" | 1 3/16" | 31'-9" | 1 3/4" | 70'-0" | 3" | 42'-0" | 2 5/8" | 1 3/8" | 1 1/2" | 2 5/8" | 2 3/4" | 1 1/2" | 1 9/16" | 2 1/4" | 1" | 2 3/4" | 42'-0" | 47'-3" |

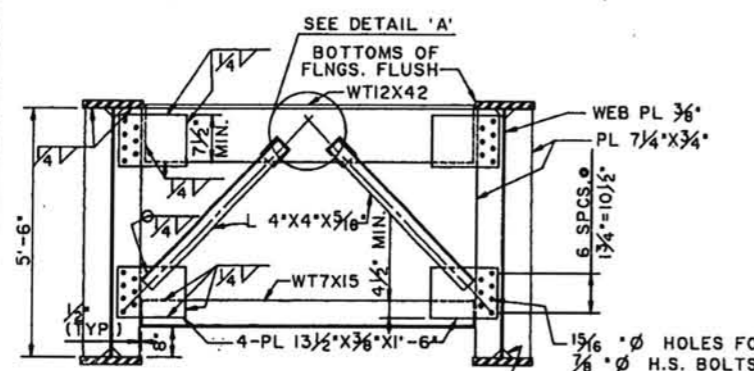
NOTES:

1. ALL FLANGE PLATES SHALL CONFORM TO THE CHARPY V-NOTCH REQUIREMENTS FOR CLIMATIC ZONE 2.
2. ALL WEB PLATES SHALL CONFORM TO THE CHARPY V-NOTCH REQUIREMENTS FOR CLIMATIC ZONE 2.
3. ALL GIRDER SPLICE PLATES SHALL CONFORM TO THE CHARPY V-NOTCH REQUIREMENTS FOR CLIMATIC ZONE 2.
4. GIRDERS SHOWN ON THIS SHEET ARE SHOWN AND DIMENSIONED WITH THE TOP FLANGE IN HORIZONTAL POSITION. VERTICAL CURVATURE AND EFFECTS OF DEAD LOAD DEFLECTION HAVE NOT BEEN TAKEN INTO CONSIDERATION. SHOP DRAWING SHALL INCLUDE A BLOCKING DIAGRAM FOR EACH GIRDER SHOWING ALL ADJUSTMENTS NECESSARY FOR DEADLOAD DEFLECTION AND VERTICAL CURVATURE. THE WEB PLATE OF THE GIRDERS SHALL BE FABRICATED WITH CAMBER FOR THE DEAD LOAD DEFLECTION AND VERTICAL CURVATURE.

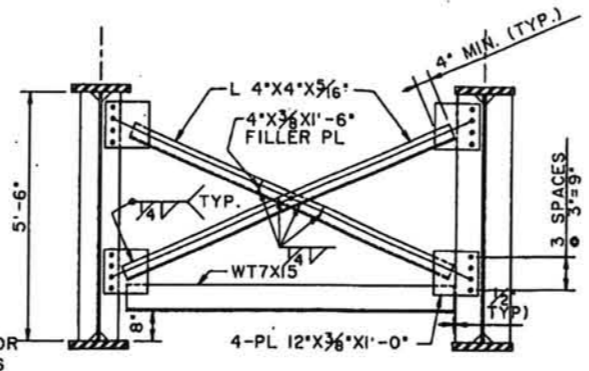
| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE F
DETAILS OF SUPERSTRUCTURE
(SHT. 1 OF 7)
F.A. Project No. I-235-10191000 Sheet No. 63

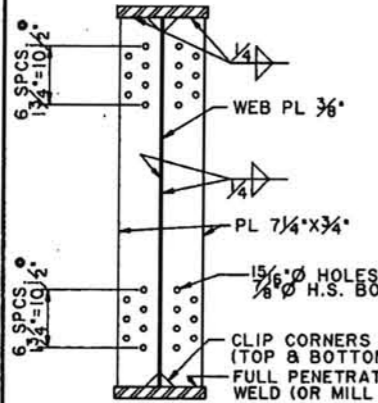
| REV. | DATE | BY | CHKD. | DESCR. | TOTAL SHEETS |
|------|-------|----|-------|--------|--------------|
| 0 | OKLA. | | | | |



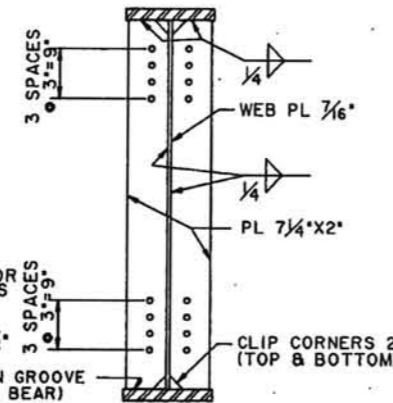
END FRAME ELEVATION AT ABUTMENT
SCALE: 1/2"=1'-0"



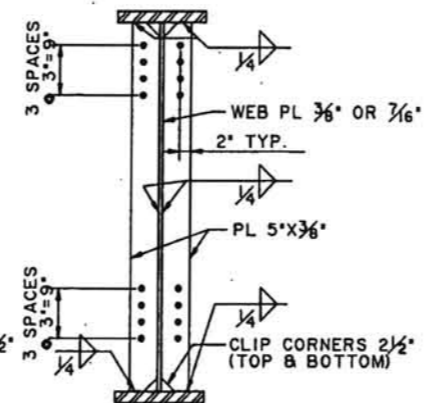
INTERMEDIATE FRAME ELEVATION
SCALE: 1/2"=1'-0"



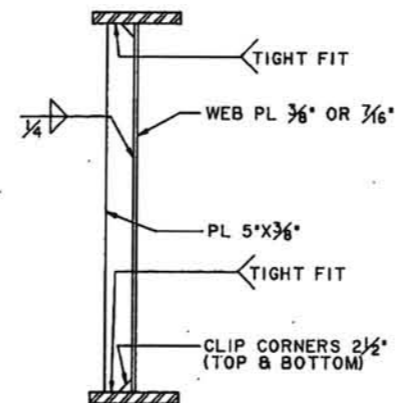
BEARING STIFFENERS AT ABUTMENTS
SCALE: 3/4"=1'-0"



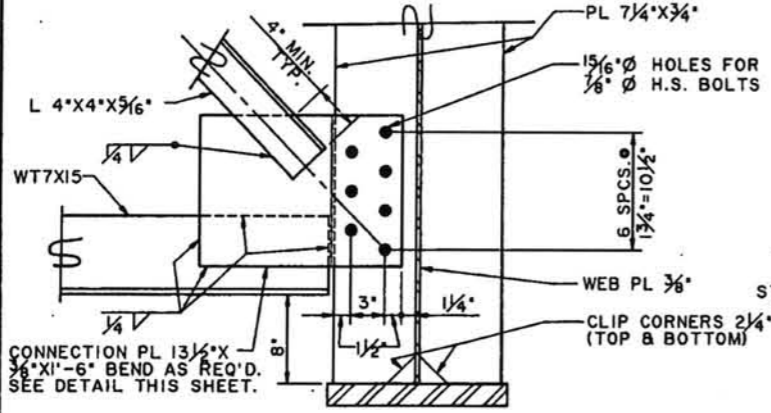
BEARING STIFFENERS AT PIERS
SCALE: 3/4"=1'-0"



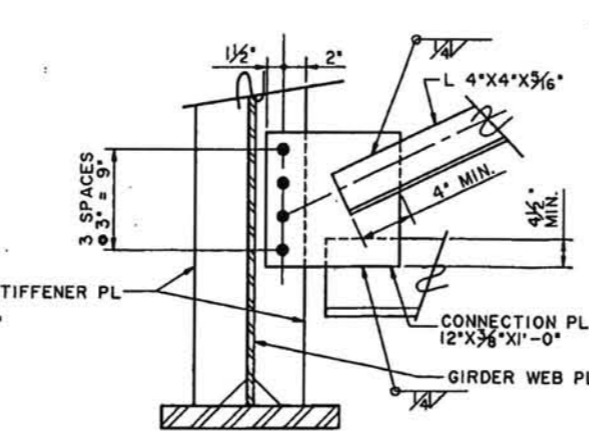
INTERMEDIATE STIFFENERS AT CROSS FRAME
SCALE: 3/4"=1'-0"



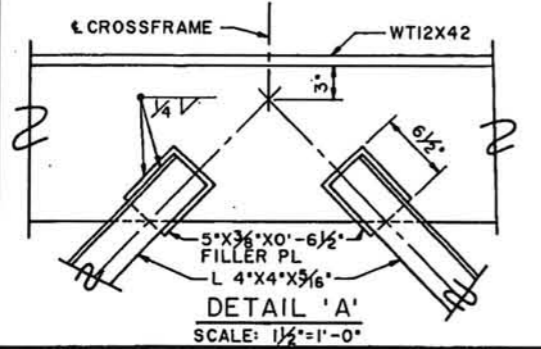
INTERMEDIATE STIFFENERS
SCALE: 3/4"=1'-0"



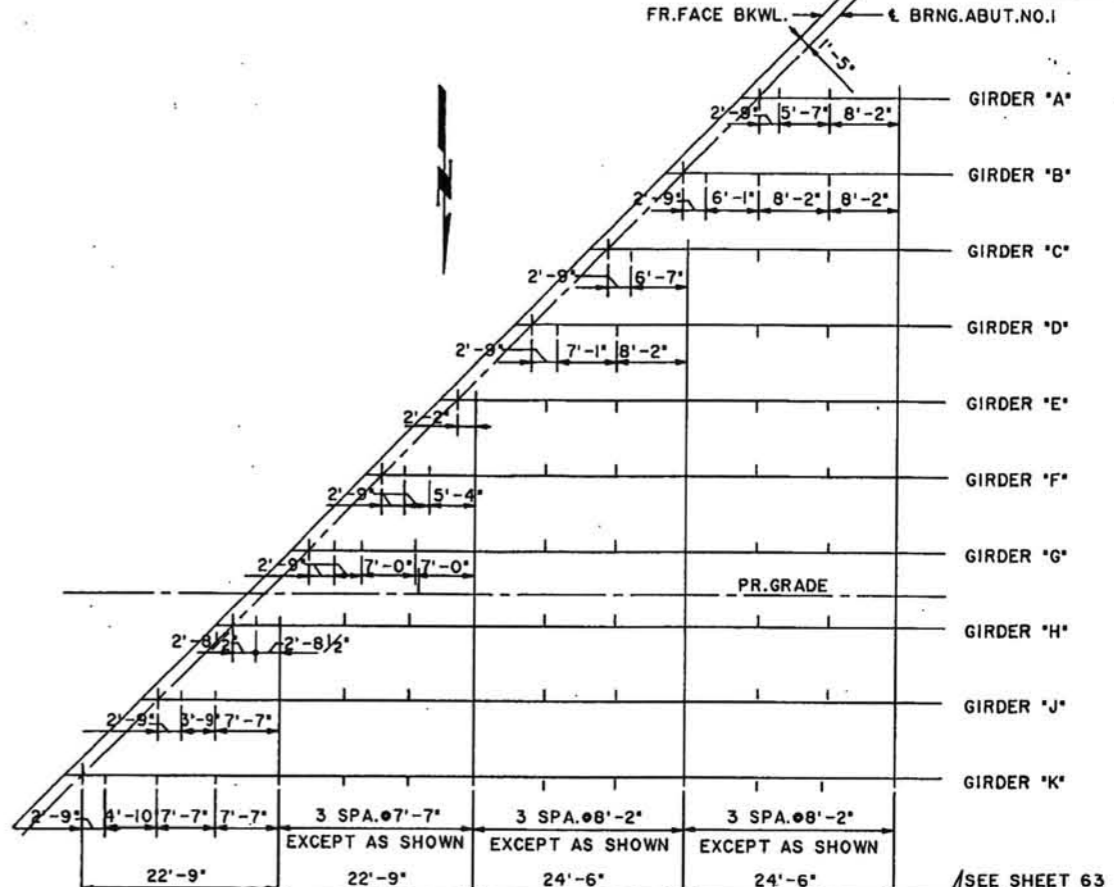
DETAIL OF LOWER CONNECTION OF ABUTMENT CROSS-FRAME
SCALE: 1 1/2"=1'-0"



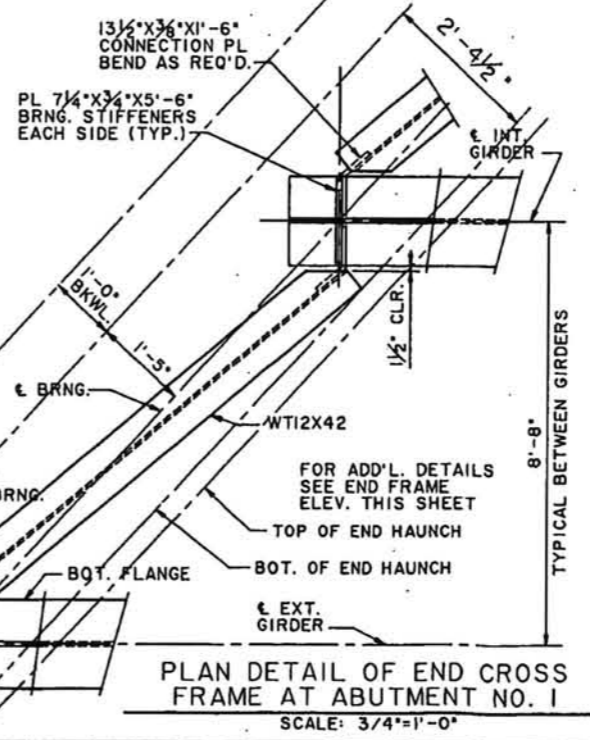
DETAIL OF LOWER CONNECTION OF INTERMEDIATE CROSS-FRAME
SCALE: 1 1/2"=1'-0"



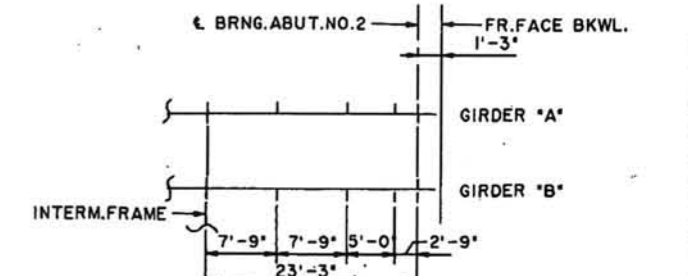
DETAIL 'A'
SCALE: 1 1/2"=1'-0"



DETAIL 'B'
SEE SHEET 63



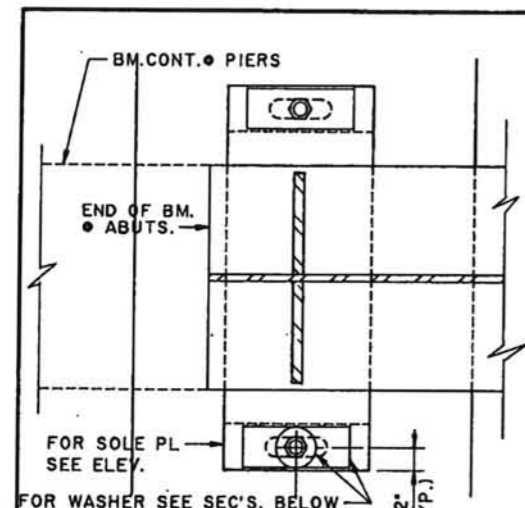
PLAN DETAIL OF END CROSS FRAME AT ABUTMENT NO. 1
SCALE: 3/4"=1'-0"



DETAIL 'C'
SEE SHEET 63

| | |
|----------|-----|
| Design | JTB |
| Drawn | JRS |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE F
DETAILS OF SUPERSTRUCTURE
(SHT. 2 OF 7)
F.A. Project No. 1-235-1(09)000 Sheet No. 64



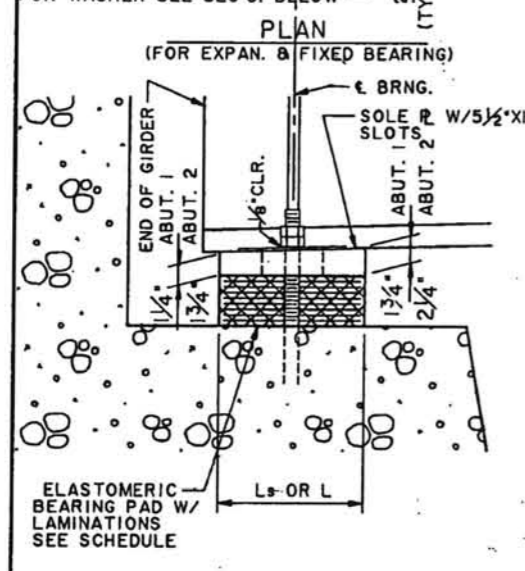
NOTES:

THE ANCHOR BOLTS AT EXPANSION BEARINGS SHALL CENTER THE SLOTS OF THE ANCHOR PLATE AT THE MEAN TEMPERATURE OF 60 DEGREES F. FOR EACH 100' OF EXPANSION TO THE BEARING ASSEMBLY, THE ANCHOR BOLT SHALL BE SET 1/8" FROM CENTERLINE SLOT IN THE DIRECTION OF THE EXPANSION FOR EACH 15 DEGREES THE TEMPERATURE IS ABOVE 60 DEGREES F. OR SHALL BE SET 1/8" FROM CENTERLINE SLOT IN THE DIRECTION OF THE CONTRACTION FOR EACH 15 DEGREES THE TEMPERATURE IS BELOW 60 DEGREES F.

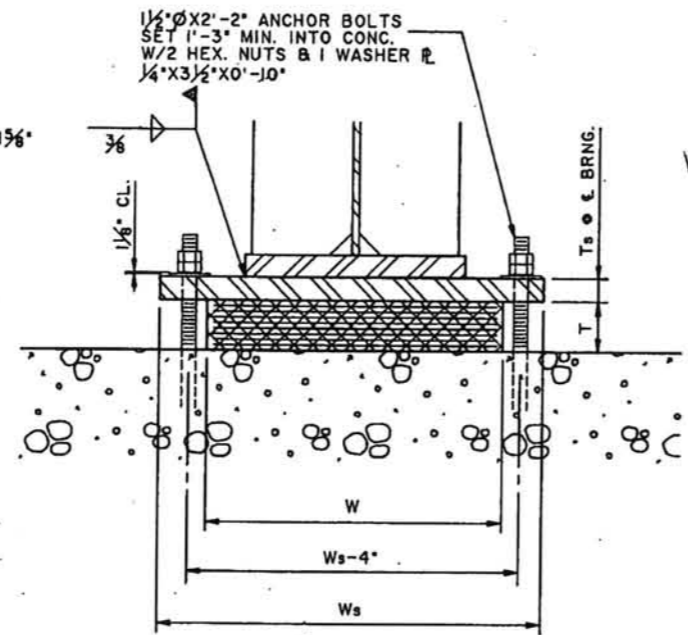
SETTING OF EXPANSION BEARINGS AT TEMPERATURES BELOW 40°F OR ABOVE 80°F WILL BE ALLOWED PROVIDED THAT THE CONTRACTOR JACKS THE GIRDERS AT THE ABUTMENTS AND RESETS THE BEARINGS WHEN THE TEMPERATURE FALLS BETWEEN 40°F AND 80°F. IT IS INTENDED THAT THE BRG. PADS SHALL BE VERTICAL BETWEEN 40°F AND 80°F. IF BRG. PADS ARE NOT VERTICAL, WITHIN THIS TEMPERATURE RANGE, THEN THEY SHALL BE RESET WITH ALL LOADS REMOVED FROM BRG. PADS. RESETTING OF THE EXPANSION BEARINGS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "RESET EXP. BEARINGS". THIS ITEM WILL BE DELETED IF RESETTING IS NOT REQUIRED. BEARING PAD AND SOLE PLATE AT ABUTMENTS SHALL BE BONDED TOGETHER IN SHOP. FOR ANCHOR BOLT DETAILS SEE SHEET 55.

NOTE:

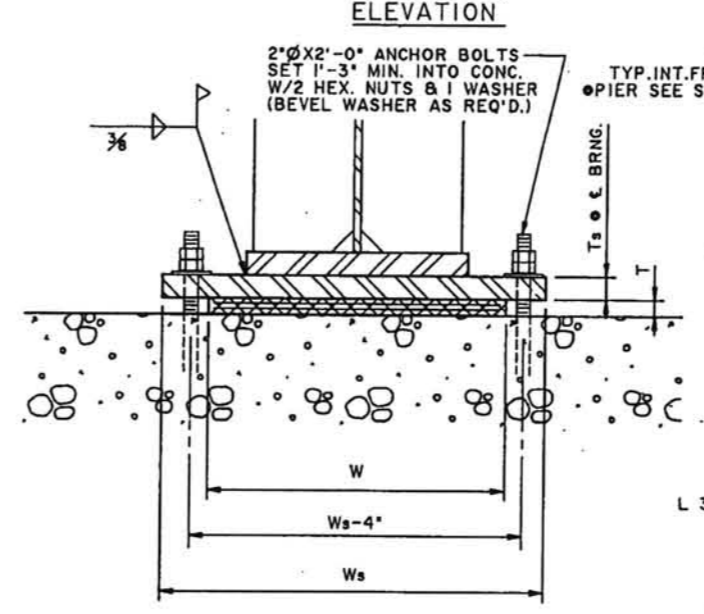
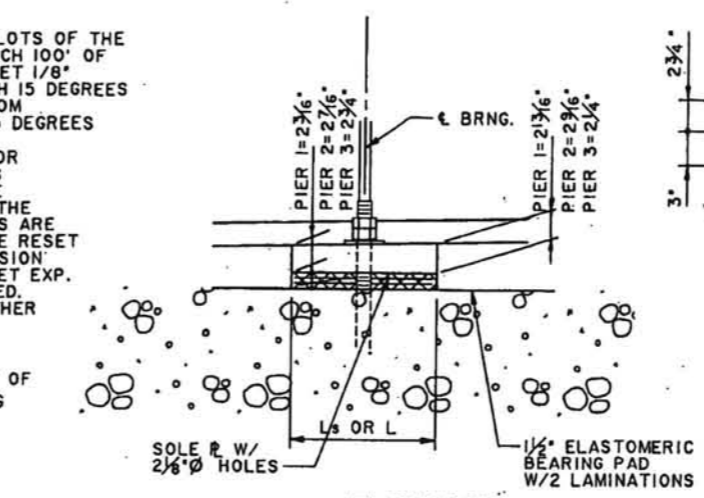
THE MAXIMUM ALLOWABLE DISPLACEMENT OF SOLE PLATE FROM CENTERLINE OF BEARING STIFFENERS AT PIERS SHALL BE 2".



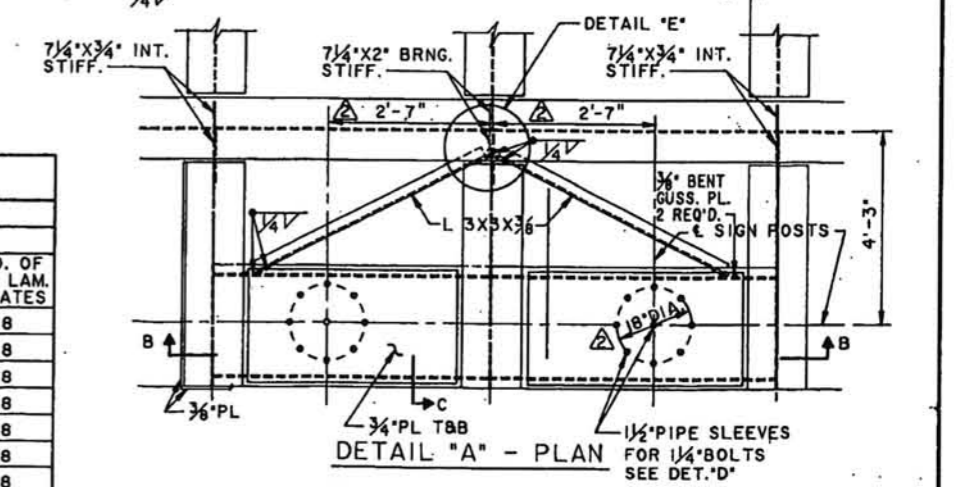
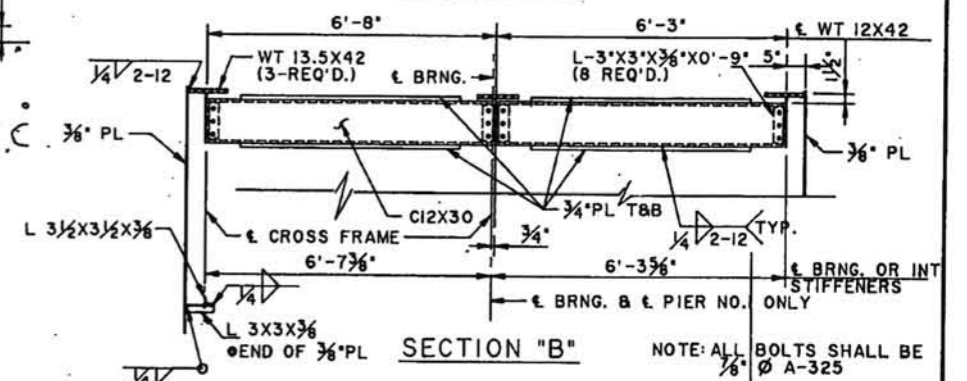
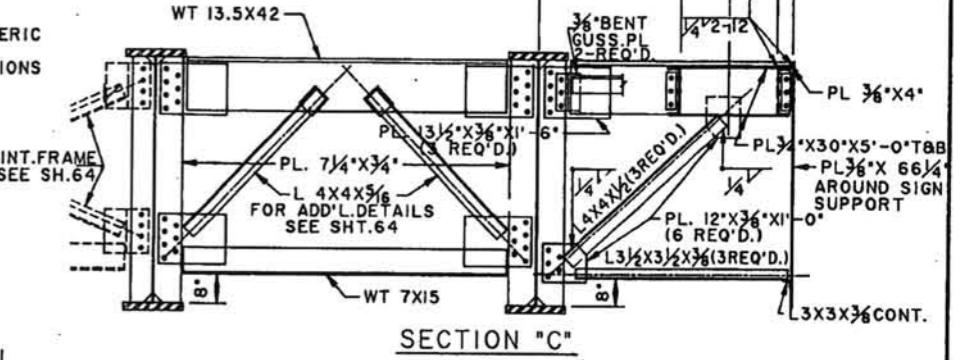
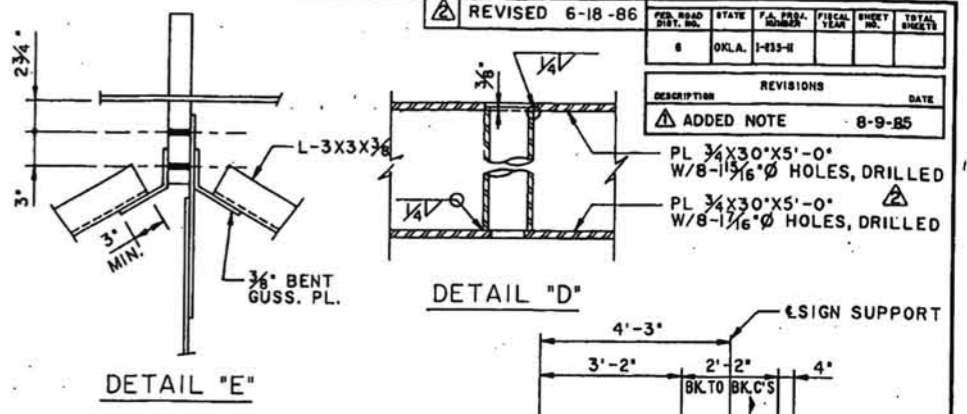
ELEVATION
ABUT. NO. 1 SHOWN, REVERSE FOR ABUT. NO. 2.



SECTION
TYPICAL EXPANSION BEARING



SECTION
TYPICAL FIXED BEARING



TRAFFIC SIGN SUPPORT

| GIRDER | BEARING SCHEDULE | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--------|------------------|-----|--------|-----|-----|----|-----------|-----|-----|--------|-----|-----|-----------|----|-----|-----|--------|-----|-----------|--------|-----|-----|-----|--------|---------------|-----|--------|---|-----|-----|----|-----|-----|--------|---|
| | ABUTMENT NO.1 | | | | | | PIER NO.1 | | | | | | PIER NO.2 | | | | | | PIER NO.3 | | | | | | ABUTMENT NO.2 | | | | | | | | | | |
| | EXPANSION | | | FIX | | | EXPANSION | | | FIX | | | EXPANSION | | | FIX | | | EXPANSION | | | FIX | | | | | | | | | | | | | |
| | Ws | Ls | Ts* | W | L | T | Ws | Ls | Ts* | W | L | T | Ws | Ls | Ts* | W | L | T | Ws | Ls | Ts* | W | L | T | Ws | Ls | Ts* | W | L | T | | | | | |
| A | 25' | 14' | 1 1/2" | 8' | 14' | 5' | 8 | 32' | 20' | 2 1/2" | 24' | 20' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 34' | 20' | 2 1/2" | 26' | 20' | 1 1/2" | 2 | 25' | 14' | 2" | 10' | 14' | 4 1/2" | 8 |
| B | 25' | 14' | 1 1/2" | 10' | 14' | 5' | 8 | 34' | 22' | 2 1/2" | 26' | 22' | 1 1/2" | 2 | 38' | 24' | 2 1/2" | 30' | 24' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 25' | 14' | 2" | 12' | 14' | 4 1/2" | 8 |
| C | 25' | 14' | 1 1/2" | 10' | 14' | 5' | 8 | 34' | 22' | 2 1/2" | 26' | 22' | 1 1/2" | 2 | 38' | 24' | 2 1/2" | 30' | 24' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 25' | 14' | 2" | 12' | 14' | 4 1/2" | 8 |
| D | 25' | 14' | 1 1/2" | 12' | 14' | 5' | 8 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 38' | 24' | 2 1/2" | 30' | 24' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 25' | 14' | 2" | 12' | 14' | 4 1/2" | 8 |
| E | 25' | 14' | 1 1/2" | 12' | 14' | 5' | 8 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 38' | 24' | 2 1/2" | 30' | 24' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 25' | 14' | 2" | 12' | 14' | 4 1/2" | 8 |
| F | 25' | 14' | 1 1/2" | 12' | 14' | 5' | 8 | 38' | 22' | 2 1/2" | 30' | 22' | 1 1/2" | 2 | 38' | 24' | 2 1/2" | 30' | 24' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 25' | 14' | 2" | 12' | 14' | 4 1/2" | 8 |
| G | 25' | 14' | 1 1/2" | 14' | 14' | 5' | 8 | 38' | 22' | 2 1/2" | 30' | 22' | 1 1/2" | 2 | 38' | 24' | 2 1/2" | 30' | 24' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 25' | 14' | 2" | 12' | 14' | 4 1/2" | 8 |
| H | 25' | 14' | 1 1/2" | 14' | 14' | 5' | 8 | 38' | 22' | 2 1/2" | 30' | 22' | 1 1/2" | 2 | 38' | 24' | 2 1/2" | 30' | 24' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 25' | 14' | 2" | 12' | 14' | 4 1/2" | 8 |
| J | 25' | 14' | 1 1/2" | 16' | 14' | 5' | 8 | 38' | 22' | 2 1/2" | 30' | 22' | 1 1/2" | 2 | 38' | 24' | 2 1/2" | 30' | 24' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 25' | 14' | 2" | 12' | 14' | 4 1/2" | 8 |
| K | 25' | 14' | 1 1/2" | 14' | 14' | 5' | 8 | 34' | 22' | 2 1/2" | 26' | 22' | 1 1/2" | 2 | 36' | 22' | 2 1/2" | 28' | 22' | 1 1/2" | 2 | 34' | 20' | 2 1/2" | 26' | 20' | 1 1/2" | 2 | 25' | 14' | 2" | 10' | 14' | 4 1/2" | 8 |

* R THICKNESS SHOWN IS AT E BEARING

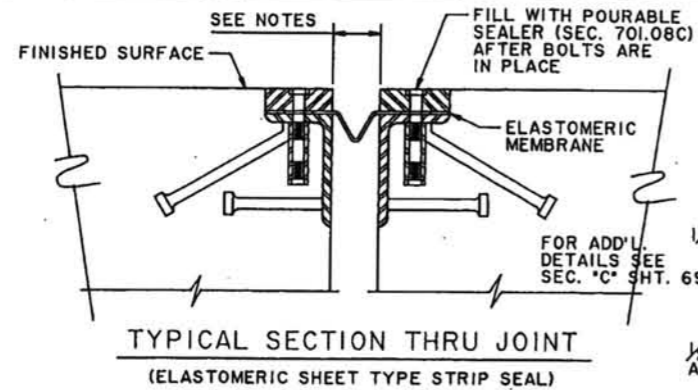
ALL ELASTOMERIC BRG. PADS SHALL BE DURO 60.

CHANGE - IN - PLANS

| | |
|----------|-----|
| Design | LRS |
| Drawn | RCC |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE F
DETAILS OF SUPERSTRUCTURE
(SHT. 3 OF 7)

| FED. ROAD DIST. NO. | STATE | F.A. PROJ. NUMBER | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|-------------------|-------------|-----------|--------------|
| 6 | OKLA. | I-235-H | | | |
| DESCRIPTION | | REVISIONS | DATE | | |
| | | | | | |



STRIP SEAL EXPANSION JOINT NOTES

THE STRIP-SEAL EXPANSION JOINT SHALL HAVE A TOTAL MOVEMENT RANGE OF UP TO 4" AND SEAL THE DECK TO PREVENT MOISTURE OR OTHER CONTAMINANTS FROM DESCENDING ONTO THE LOWER STRUCTURE COMPONENTS. THE MAXIMUM EXPANSION LENGTH TO THE JOINT SHALL BE 400' FOR STEEL BEAM SPANS. OTHER STRIP-SEAL JOINTS MAY BE USED WHEN APPROVED BY THE BRIDGE ENGINEER. THE TYPE OF STRIP-SEAL SELECTED FOR THE BRIDGE MUST BE USED FOR ALL STRIP-SEAL JOINTS ON THAT BRIDGE, EXCEPT AS SHOWN ON DRAWINGS.

ELASTOMER: THE ELASTOMER SHEET SHALL BE 1/8" THICK 100% VIRGIN POLYISOPRENE OR 100% VIRGIN CHLOROPRENE REINFORCED WITH A NON-WICKING SYNTHETIC FABRIC, MEETING THE PROPERTIES SPECIFIED IN SECTION 733.05, AND HAVING A DUROMETER HARDNESS FALLING WITHIN THE RANGE OF 55-80. THE WIDTH OF SHEET SHALL BE 10 1/2". THE SHEET SHALL BE COMPLETELY SEATED ON THE EMBEDDED ANGLES AND SHALL BE CONTINUOUS FOR THE FULL LENGTH OF THE JOINT INCLUDING PARAPET FACE. HOLES IN THE ELASTOMER SHEET SHALL BE SHOP PUNCHED 1/4" DIAMETER TO ACCEPT THE 1/4" X 3/32" WASHER. ALL LOCATIONS WHERE MATERIALS ARE SHOWN TO BE MITERED TO ACCOMMODATE SHARP SKEWS, THE MATERIAL SHALL BE SCARFED AND HEAT VULCANIZED IN AN APPROVED MANNER. SPECIAL SHAPE GLANDS MAY ALSO BE USED FOR SHARP SKEWS.

STRUCTURAL STEEL: ALL STRUCTURAL STEEL SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M-183 OR ASTM A-36. 1/2" X 2" BOLTS SHALL BE TYPE 304 STAINLESS STEEL.

PAINT: THE ENTIRE STEEL ASSEMBLY SHALL RECEIVE A SHOP COAT OF INORGANIC ZINC PRIMER.

FABRICATION OF JOINT: SHOP DRAWINGS SHOWING DIMENSIONS AND DETAILS OF THE COMPLETE EXPANSION JOINT SHALL BE SUBMITTED TO THE BRIDGE ENGINEER FOR APPROVAL. WELDED STUDS, OR THREADED INSERTS SHALL BE ELECTRIC ARC AND WELDED TO THE ANGLES WITH COMPLETE FUSION.

THE ALIGNMENT OF THE ASSEMBLED DEVICE AND THE PROTECTION REQUIRED FOR THE THREADED SLEEVE NUT TO INSURE THE RETAINING BOLTS WILL GO TO FULL DEPTH ARE CRITICAL TO THE INSTALLATION AND FUNCTION OF THE EXPANSION DEVICE. THE CONTRACTOR AND HIS FABRICATOR WILL BE REQUIRED TO TAKE ALL STEPS NECESSARY TO ASSURE THE SATISFACTORY INSTALLATION. COMPLETE PRE-ASSEMBLY, TEMPORARY SEALS FOR SLEEVE NUTS, AND OTHER STEPS MAY BE REQUIRED. THE SHOP DRAWINGS SHALL ADDRESS THE PROPOSED METHOD OF PROTECTION AND ASSEMBLY.

ALL THREADED SLEEVE NUTS SHALL BE BLOWN CLEAN WITH AN "AIR-NEEDLE" BEFORE THE STAINLESS STEEL BOLTS ARE INSERTED.

INSTALLATION OF JOINT: ELASTOMERIC SEAL AND CONTACTING STEEL SURFACES SHALL BE CLEANED PRIOR TO ASSEMBLY. THE JOINT SHALL BE SET TO ITS PROPER LINE, GRADE, AND OPENING AND SHALL BE ADEQUATELY BRACED IN A MANNER APPROVED BY THE ENGINEER. THE JOINT SHALL BE SET WITH AN OPENING OF 2 1/4" MEASURED PARALLEL TO THE ROADWAY BASED ON A TEMPERATURE OF 60° FAHRENHEIT. FOR EACH 100' OF EXPANSION TO THE JOINT TO BE PROVIDED FOR, THE SETTING OF THE OPENING SHALL BE INCREASED 1/8" FOR EACH 15° FAHRENHEIT THE TEMPERATURE IS BELOW 60° FAHRENHEIT AND DECREASED 1/8" FOR EACH 15° FAHRENHEIT THE TEMPERATURE IS ABOVE 60° FAHRENHEIT.

THE PARAPET OPENING AT THE JOINT SHALL BE THE SAME AS THE JOINT OPENING.

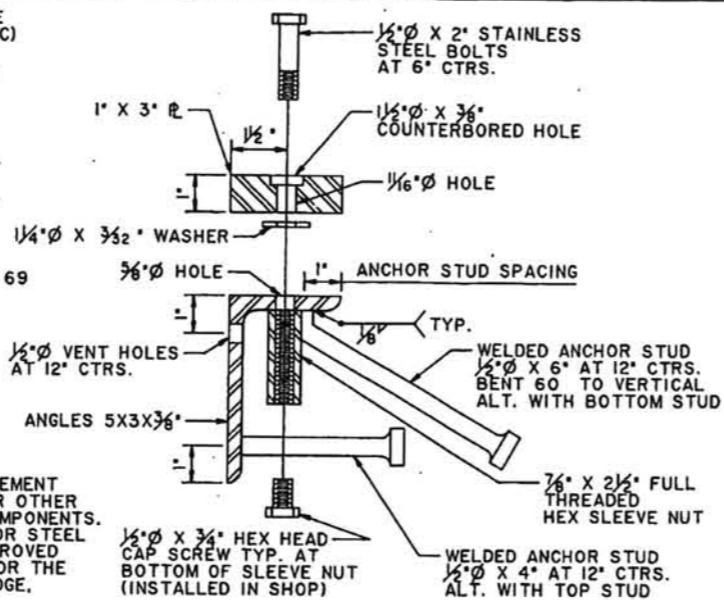
BOLTS SHALL BE TIGHTENED TO A MINIMUM TORQUE OF 100 FOOT-POUNDS. AFTER SEVERAL BOLTS HAVE BEEN TIGHTENED, BOLTS PREVIOUSLY TIGHTENED SHALL BE CHECKED AGAIN UNTIL ALL ARE TIGHTENED TO THE PRESCRIBED AMOUNT. ALL BOLTS WILL AGAIN BE TIGHTENED TO 100 FOOT-POUNDS 24 HOURS AFTER ORIGINAL TIGHTENING.

METHOD OF MEASUREMENT & BASIS OF PAYMENT: THE STRIP-SEAL TYPE EXPANSION JOINT SHALL BE MEASURED BY THE LINEAR FOOT ALONG THE CENTERLINE OF JOINT FROM END TO END OF JOINT AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID FOR:

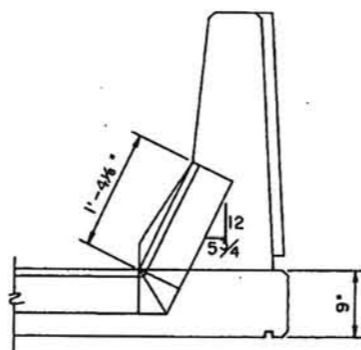
SP. STRIP SEAL EXP. JOINT W/ANGLES LIN. FT.

THE PRICE BID SHALL BE FULL COMPENSATION FOR FURNISHING ALL MATERIALS IN THE COMPLETED JOINT INCLUDING ELASTOMER SHEET, STRUCTURAL STEEL, ANCHORS, WELDING, EQUIPMENT, LABOR, AND INCIDENTALS NECESSARY TO INSTALL THE JOINT COMPLETE IN PLACE.

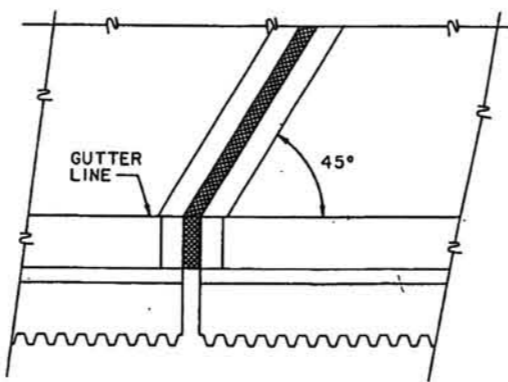
STRIP SEAL EXPANSION JOINT



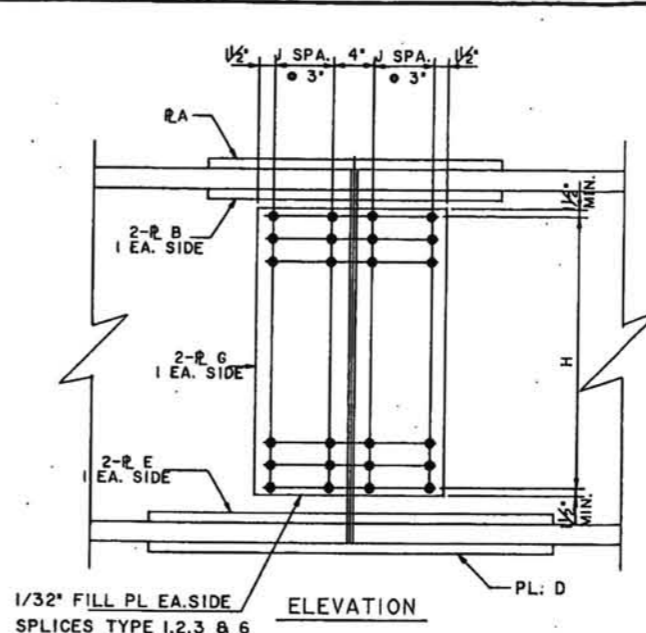
DETAIL OF JOINT STRUCTURAL STEEL



EXP. JT. DETAIL AT PARAPET

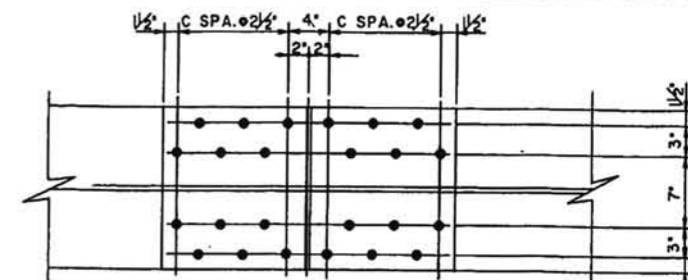


PLAN DETAIL AT GUTTER

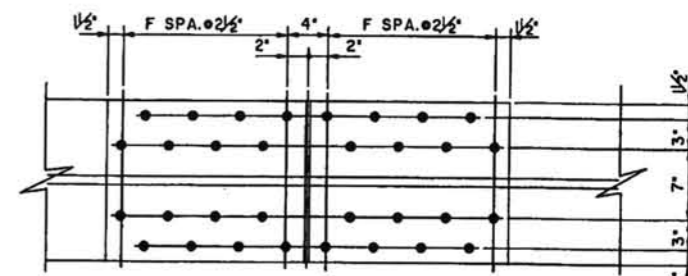


ELEVATION

NOTE: ALL BOLTS SHALL BE 7/8" Ø A-325 BOLTS. PLACE BOLT HEADS ON EXPOSED SIDE OF EXTERIOR GIRDERS.



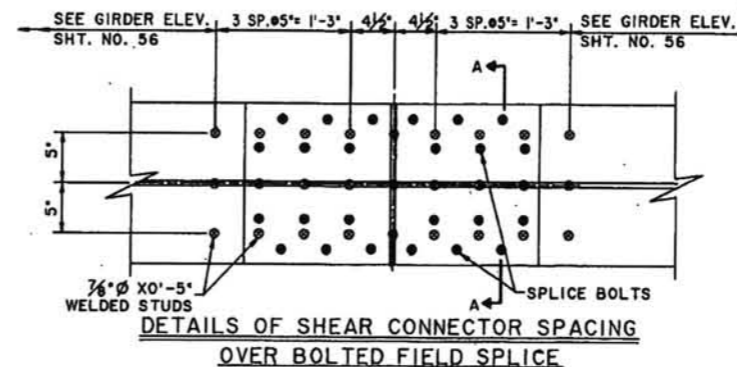
PLAN PLATE A



PLAN PLATE D

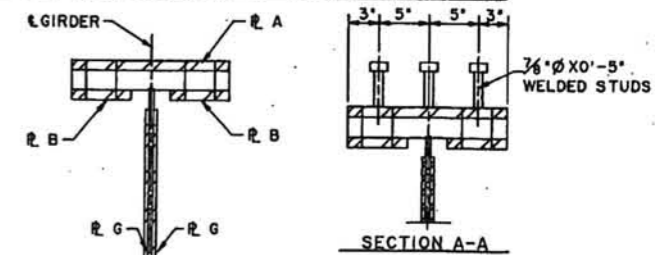
DETAILS OF BOLTED SPLICE

| TYPE | PL. A | PL. B | C | PL. D | PL. E | F | PL. G | H | J |
|------|----------------|---------------|---|------------------|-----------------|----|--------------------|------------------|---|
| 1 | 5/8"X16"X3'-6" | 5/8"X6"X3'-6" | 7 | 1 1/8"X16"X5'-2" | 1 1/4"X6"X5'-2" | 11 | 3/8"X19"X5'-1 1/2" | 13 SPA. @ 4 1/2" | 2 |
| 2 | 5/8"X16"X3'-6" | 5/8"X6"X3'-6" | 7 | 1 1/8"X16"X4'-4" | 1 1/4"X6"X4'-4" | 9 | 3/8"X13"X5'-1 1/2" | 13 SPA. @ 4 1/2" | 1 |
| 3 | 5/8"X16"X4'-4" | 3/4"X6"X4'-4" | 9 | 1"X16"X5'-2" | 1"X6"X5'-2" | 11 | 3/8"X13"X5'-1 1/2" | 15 SPA. @ 3 7/8" | 1 |
| 4 | 5/8"X16"X4'-4" | 5/8"X6"X4'-4" | 9 | 7/8"X16"X5'-2" | 1"X6"X5'-2" | 11 | 3/8"X13"X5'-1 1/2" | 15 SPA. @ 3 3/8" | 1 |
| 5 | 1/2"X16"X3'-6" | 1/2"X6"X3'-6" | 7 | 1"X16"X4'-4" | 1"X6"X4'-4" | 9 | 3/8"X13"X5'-1 1/2" | 13 SPA. @ 4 1/2" | 1 |
| 6 | 3/8"X16"X2'-8" | 1/2"X6"X2'-8" | 5 | 5/8"X16"X3'-6" | 3/4"X6"X3'-6" | 7 | 3/8"X13"X5'-3" | 15 SPA. @ 4" | 1 |

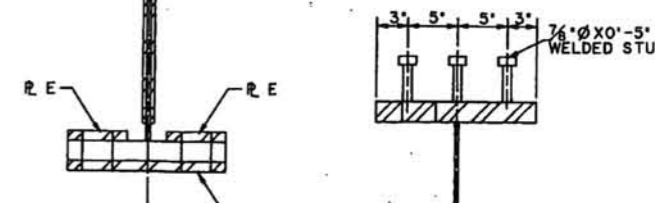


DETAILS OF SHEAR CONNECTOR SPACING OVER BOLTED FIELD SPLICE

| GIRDER | SPAN NO.1 | STARTING AT ABUT. NO.1 | GIRDER | SPAN NO.2 | STARTING AT SPLICE NO.2 |
|--------|-----------------------------------|------------------------|--------|-------------------------------|-------------------------|
| A | 2012, 14 015, 18 018, 1 010 1/2 | | A | 2012, 20 015, 52 018, 11 015 | |
| B | 12 012, 14 015, 18 018, 1 06 1/2 | | B | 2012, 20 015, 51 018, 11 015 | |
| C | 14 012, 10 015, 26 018, 1 014 1/2 | | C | 3 012, 18 015, 50 018, 12 015 | |
| D | 10 012, 10 015, 35 018, 1 016 1/2 | | D | 3 012, 18 015, 49 018, 12 015 | |
| E | 11 012, 11 015, 40 018, 1 015 1/2 | | E | 3 012, 18 015, 48 018, 12 015 | |
| F | 13 012, 17 015, 40 018, 1 017 1/2 | | F | 3 012, 18 015, 46 018, 13 015 | |
| G | 14 012, 12 015, 50 018, 1 016 1/2 | | G | 3 012, 18 015, 45 018, 13 015 | |
| H | 15 012, 13 015, 55 018, 1 015 1/2 | | H | 3 012, 18 015, 44 018, 13 015 | |
| J | 17 012, 17 015, 57 018, 1 011 1/2 | | J | 3 012, 18 015, 42 018, 14 015 | |
| K | 11 012, 15 015, 69 018, 1 013 1/2 | | K | 2 012, 20 015, 40 018, 14 015 | |



SECTION A-A



TYPICAL STUD SHEAR CONNECTOR DETAIL

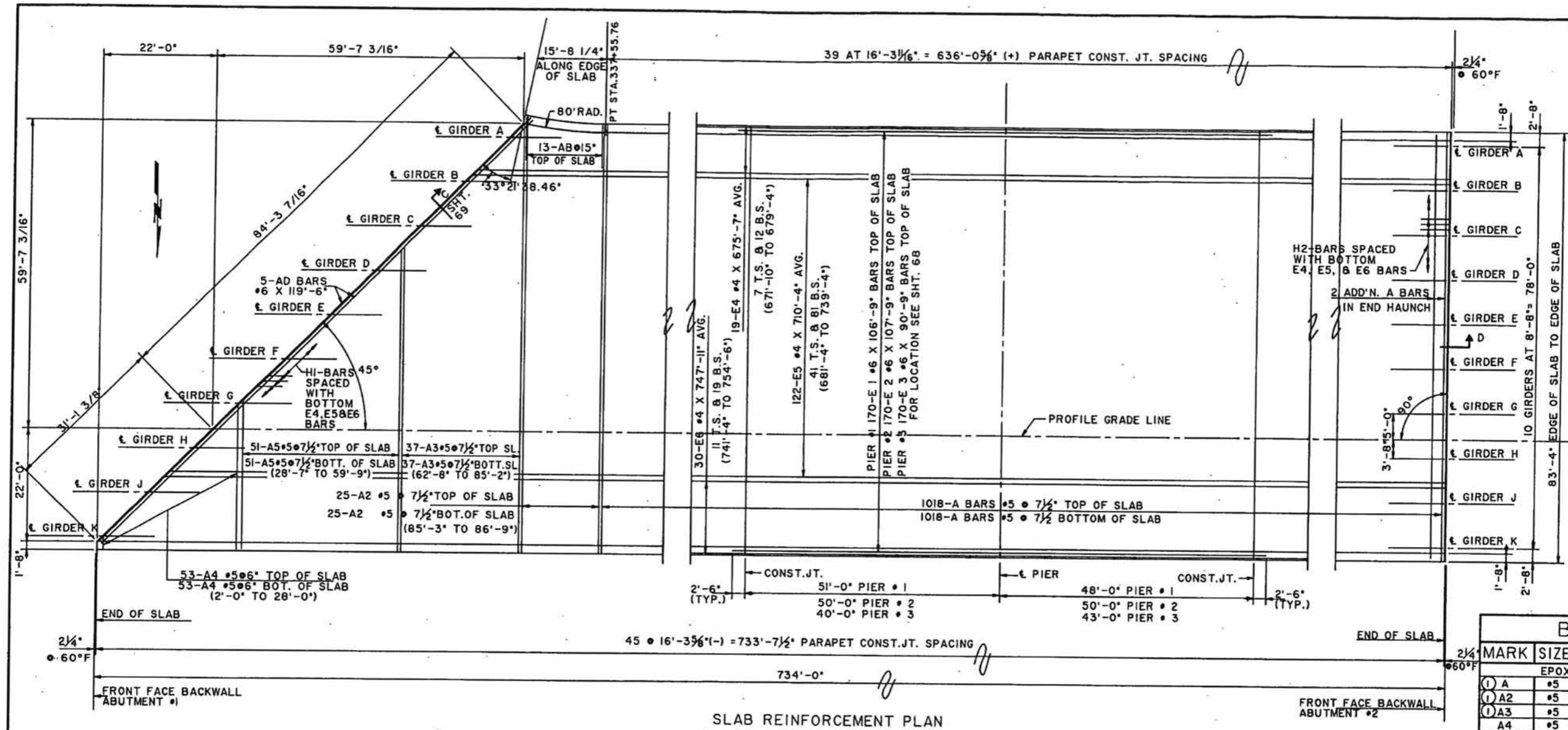
NOTE: FOR SHEAR CONNECTOR SPACING, SPANS NO. 3 & 4, SEE GIRDER ELEVATION SHEET 65.

| | |
|----------|-----|
| Design | LRs |
| Drawn | LDM |
| Checked | LRs |
| Approved | |
| Squad | |

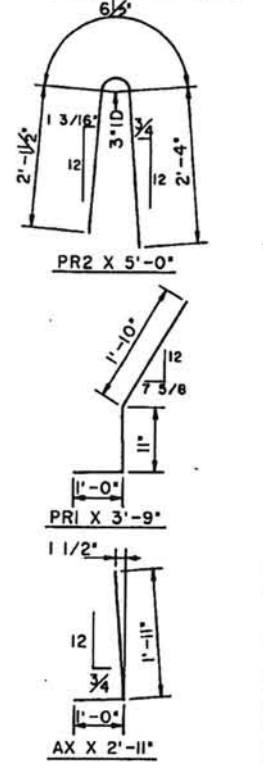
**BRIDGE F
DETAILS OF SUPERSTRUCTURE
(SHT. 4 OF 7)**

| FED. ROAD DIST. NO. | STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|-----------|-----------|--------------|
| 6 | OKLA. | | | |

DESCRIPTION REVISIONS DATE



BAR BENDING DIAGRAMS

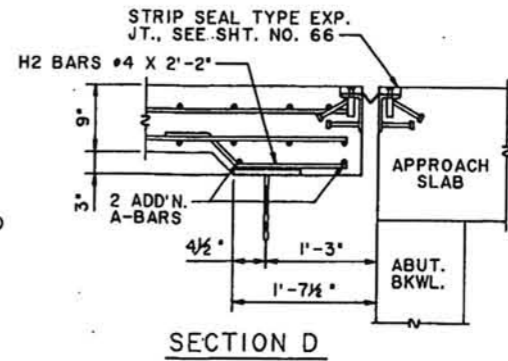
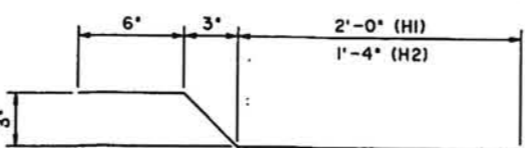


GENERAL NOTES FOR DECK CONSTRUCTION

- ALL CONCRETE IN THE SUPERSTRUCTURE SHALL BE CLASS AA CONCRETE.
- ALL REINFORCING STEEL SHALL BE EPOXY COATED AND CONFORM TO A-615, GRADE 60.
- ALL EXPOSED EDGES, UNLESS SHOWN OR NOTED OTHERWISE, SHALL HAVE A 3/4" CHAMFER. ALL CHAMFER STRIPS SHALL BE SIZED LUMBER.
- TRANSVERSE REINFORCING STEEL SHALL BEGIN AT A MAXIMUM OF 2" FROM END OF SLAB. LONGITUDINAL BARS SHALL BEGIN AND END 1' CLEAR FROM END OF SLAB.
- ALL TOP OF SLAB REINF. BARS SHALL BE SUPPORTED ON APPROVED METAL HIGH CHAIRS. BOTTOM OF SLAB REINF. SHALL BE SUPPORTED ON APPROVED METAL LOW CHAIRS. THE MAXIMUM SPACING OF THE CHAIRS (HIGH & LOW) SHALL BE ONE HALF GIRDER SPACING OR A MAXIMUM SPACING OF 4'-0" O.C.
- PLAN QUANTITIES FOR CLASS AA CONCRETE INCLUDES 51.5 C.Y. FOR HAUNCHES OVER GIRDERS. THE HAUNCH HEIGHTS WILL BE SET AFTER ERECTION OF STRUCTURAL STEEL TO PROVIDE FOR DEAD LOAD DEFLECTION AND GRADE ADJUSTMENT, BUT THE PAY QUANTITIES FOR HAUNCHES WILL BE AS SHOWN ABOVE.
- WHEN VIBRATING CONCRETE CONTAINING EPOXY COATED REINF. STEEL, THE VIBRATOR SHALL BE EQUIPPED WITH A SHEATH DESIGNED TO PREVENT DAMAGE TO THE EPOXY COATING.
- AFTER ALL STRUCTURAL STEEL HAS BEEN ERECTED THE ENGINEER WILL FURNISH THE STATE BRIDGE ENGINEER WITH THE PROFILE OF THE TOPS OF THE GIRDERS WITH THE ELEVATIONS TAKEN AT THE PANEL POINTS. THE BRIDGE ENGINEER WILL DETERMINE THE THICKENING OF THE HAUNCHES THAT WILL BE NECESSARY TO PROVIDE FOR DEAD LOAD DEFLECTION AND FURNISH THIS INFORMATION TO THE ENGINEER.

FORMWORK BRACING FOR EXTERIOR GIRDERS

THE EXTERIOR GIRDER SHALL BE SECURELY BRACED TO ELIMINATE TWISTING DUE TO THE WEIGHT OF THE OVERHANGING SLAB AND THE FINISHING MACHINE. BRACING SHALL CONSIST OF A TENSION MEMBER FROM THE TOP FLANGE OF THE EXTERIOR GIRDER TO THE TOP FLANGE OF THE ADJACENT GIRDER, AND A COMPRESSION MEMBER FROM THE BOTTOM FLANGE OF THE EXTERIOR GIRDER TO THE TOP FLANGE OF THE ADJACENT GIRDER. BRACING SHALL BE PERPENDICULAR TO THE GIRDERS. BRACING SHALL BE SPACED TO PROVIDE THE REQUIRED RIGIDITY, BUT IN NO CASE SHALL THE SPACING EXCEED 8'-0". BRACING MEMBERS REMAINING IN CONCRETE SHALL HAVE SUITABLE CLEARANCE AND COVER. DRAWINGS OF THE PROPOSED BRACING SYSTEM SHALL BE APPROVED BY THE BRIDGE ENGINEER BEFORE ANY DECK CONCRETE IS POURED.



QUANTITIES

| ITEM | UNIT | AMOUNT | Design | JTB | Drawn | JRS | LDM |
|---|------|------------|----------|-----|-------|-----|-----|
| CLASS AA CONCRETE | C.Y. | 1,664.8 | | | | | |
| CONCRETE PARAPET | L.F. | 1,385.9 | | | | | |
| RESET EXPANSION BEARINGS | EA. | 20. | | | | | |
| EPOXY COATED REINF. STEEL (GR. 60) | LB. | 358,360. | | | | | |
| STRUCTURAL STEEL A-36 | LB. | 1,477,370. | | | | | |
| STRUCTURAL STEEL A-588 | LB. | 704,760. | | | | | |
| STRIP SEAL TYPE EXPANSION JOINT WITH ANGLES | L.F. | 200.8 | Checked | LR | | | |
| PENETRATING WATER REPELLENT SURFACE TREATMENT | S.Y. | 6,721.5 | Approved | | | | |

BAR LIST

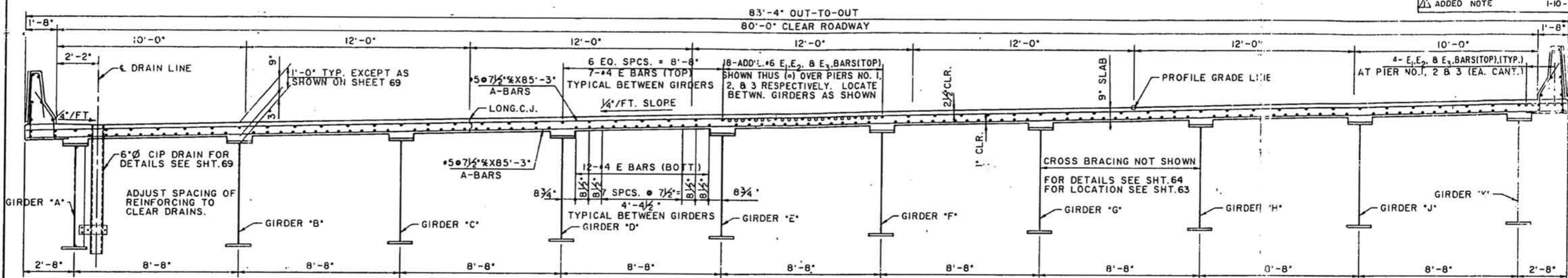
| MARK | SIZE | FORM NO. | LENGTH |
|-------------------|------|-----------|---------------|
| EPOXY COATED BARS | | | |
| ① A | #5 | STR. 2038 | 85'-3" |
| ① A2 | #5 | STR. 50 | 86'-0" AVG. |
| ① A3 | #5 | STR. 74 | 73'-11" AVG. |
| A4 | #5 | STR. 106 | 15'-0" AVG. |
| A5 | #5 | STR. 102 | 44'-2" AVG. |
| ③ AD | #6 | STR. 5 | 119'-6" |
| ④ AX | #5 | BENT 1110 | 2'-11" |
| ④ E6 | #4 | STR. 30 | 747'-11" AVG. |
| ③ E1 | #6 | STR. 170 | 106'-9" |
| ③ E2 | #6 | STR. 170 | 107'-9" |
| ③ E3 | #6 | STR. 170 | 90'-9" |
| ② E4 | #4 | STR. 19 | 675'-7" AVG. |
| ③ E5 | #4 | STR. 122 | 710'-4" AVG. |
| ④ PH | #4 | STR. 6 | 15'-6" |
| ④ PHI | #4 | STR. 504 | 16'-1" |
| ④ PRI | #5 | BENT 1110 | 3'-9" |
| ④ PR2 | #5 | BENT 1110 | 5'-0" |
| AB | #5 | STR. 13 | 8'-2" |
| HI | #4 | BENT 59 | 2'-10" |
| H2 | #4 | BENT 59 | 2'-2" |

- INCLUDES ONE 27" LAP. LAPS STAGGERED TO AVOID MORE THAN 50% AT ONE LOCATION
 - INCLUDES 12-18" LAPS AT SPLICES
 - INCLUDES ONE 33' LAP
 - INCLUDES 14-18" LAPS AT SPLICES
 - INCLUDES 13-18" LAPS AT SPLICES
- ⑥ ALL COST TO BE INCLUDED IN THE PRICE BID PER L.F. OF CONC. PARAPET

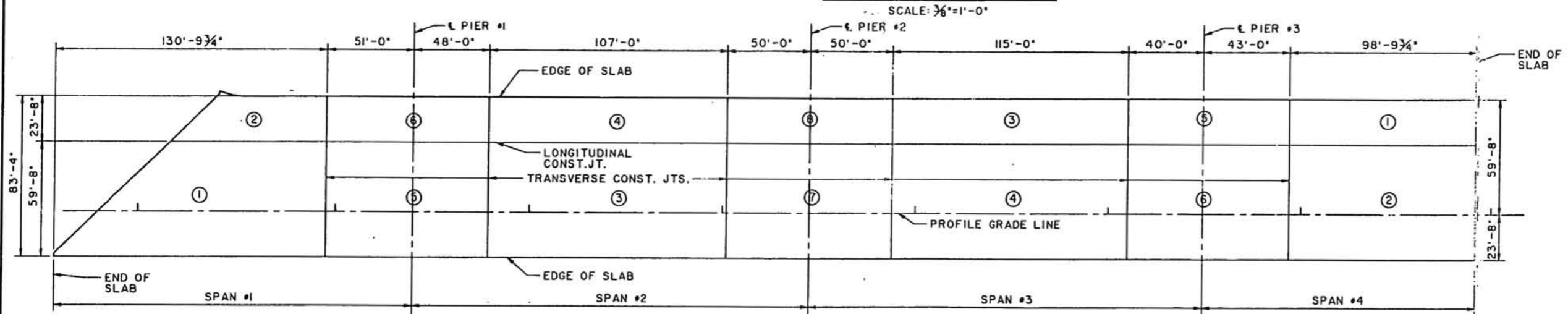
BRIDGE F
DETAILS OF SUPERSTRUCTURE
(SHT. 5 OF 7)
F.A. Project No. 1-235-1(109)000 Sheet No. 67

FOR GENERAL NOTES FOR DECK
CONSTRUCTION SEE SHT. NO. 67

| | | | | | |
|---------------------|-------|-------------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJECT NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| 6 | OKLA. | | | | |
| REVISIONS | | | | | DATE |
| ADDED NOTE | | | | | 1-10-86 |

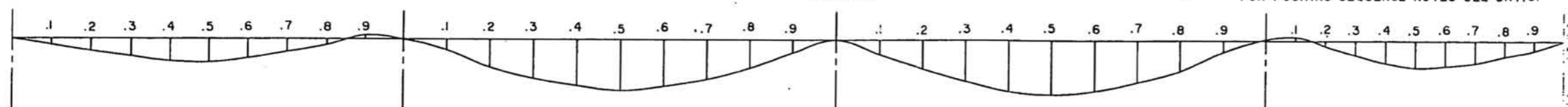


SECTION THROUGH DECK



POURING SEQUENCE DIAGRAM

NOTES: FOR CONSTRUCTION JOINT DETAILS AND NOTES SEE SHT. 69
FOR POURING SEQUENCE NOTES SEE SHT. 51

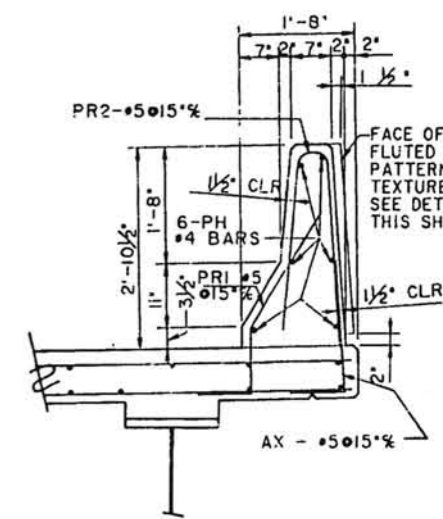


DEAD LOAD DEFLECTION DIAGRAM

CONCRETE ALONE = 75% FOR EXT. GIRDER
CONCRETE ALONE = 80% FOR INTERIOR GIRDER

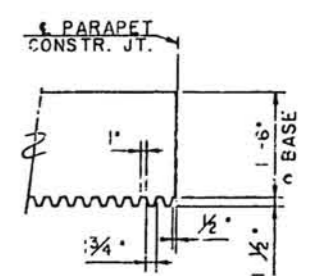
DEAD LOAD DEFLECTION SCHEDULE (INCHES)

| GIRDER | SPAN #1 | | | | | | | | | SPAN #2 | | | | | | | | | SPAN #3 | | | | | | | | | SPAN #4 | | | | | | | | | | | | | | | | | | | | | | | |
|--------|---------|------|------|------|------|------|------|------|------|---------|------|------|------|------|------|------|------|------|---------|----|------|------|------|------|------|------|------|---------|------|----|------|------|------|------|------|------|------|------|------|----|---|---|---|---|---|---|---|---|---|---|---|
| | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | | | | | | | | | | |
| A | 0 | .01 | -.02 | -.04 | -.07 | -.10 | -.15 | -.22 | -.34 | 0 | 1.19 | 2.75 | 4.10 | 4.90 | 5.01 | 4.42 | 3.27 | 1.83 | 0.58 | 0 | 0.21 | 1.16 | 2.38 | 3.38 | 3.87 | 3.73 | 2.99 | 1.83 | 0.65 | 0 | -.02 | 0.25 | 0.74 | 1.27 | 1.67 | 1.85 | 1.76 | 1.39 | 0.78 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| B | 0 | 0.18 | 0.28 | 0.36 | 0.41 | 0.45 | 0.47 | 0.45 | 0.31 | 0 | 1.42 | 3.31 | 4.95 | 5.94 | 6.09 | 5.38 | 3.97 | 2.22 | 0.70 | 0 | 0.28 | 1.45 | 2.95 | 4.19 | 4.79 | 4.50 | 3.68 | 2.24 | 0.79 | 0 | -.02 | 0.33 | 0.95 | 1.61 | 2.12 | 2.34 | 2.22 | 1.75 | 0.98 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| C | 0 | 0.36 | 0.61 | 0.70 | 0.62 | 0.40 | 0.08 | -.07 | -.42 | 0 | 1.30 | 3.14 | 4.79 | 5.79 | 5.96 | 5.27 | 3.88 | 2.14 | 0.66 | 0 | 0.31 | 1.51 | 3.02 | 4.25 | 4.85 | 4.56 | 3.72 | 2.27 | 0.80 | 0 | -.02 | 0.32 | 0.93 | 1.60 | 2.10 | 2.33 | 2.21 | 1.74 | 0.98 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| D | 0 | 0.57 | 1.00 | 1.22 | 1.20 | 0.97 | 0.58 | 0.14 | -.19 | 0 | 1.14 | 2.88 | 4.48 | 5.48 | 5.67 | 5.02 | 3.69 | 2.02 | 0.60 | 0 | 0.36 | 1.58 | 3.12 | 4.35 | 4.94 | 4.73 | 3.78 | 2.31 | 0.82 | 0 | -.03 | 0.30 | 0.91 | 1.57 | 2.08 | 2.31 | 2.19 | 1.73 | 0.97 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| E | 0 | 0.83 | 1.48 | 1.85 | 1.91 | 1.57 | 1.19 | 0.60 | 0.09 | 0 | 0.99 | 2.63 | 4.20 | 5.20 | 5.42 | 4.80 | 3.52 | 1.91 | 0.55 | 0 | 0.40 | 1.66 | 3.21 | 4.45 | 5.03 | 4.81 | 3.84 | 2.34 | 0.84 | 0 | -.04 | 0.29 | 0.89 | 1.55 | 2.06 | 2.29 | 2.18 | 1.72 | 0.97 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| F | 0 | 1.09 | 1.94 | 2.45 | 2.57 | 2.32 | 1.76 | 1.03 | 0.36 | 0 | 0.79 | 2.28 | 3.80 | 4.82 | 5.09 | 4.52 | 3.29 | 1.74 | 0.47 | 0 | 0.47 | 1.78 | 3.35 | 4.59 | 5.16 | 4.92 | 3.92 | 2.39 | 0.86 | 0 | -.06 | 0.26 | 0.86 | 1.51 | 2.02 | 2.26 | 2.15 | 1.70 | 0.96 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| G | 0 | 1.46 | 2.62 | 3.35 | 3.59 | 3.33 | 2.67 | 1.74 | 0.82 | 0 | 0.57 | 1.91 | 3.39 | 4.42 | 4.72 | 4.22 | 3.05 | 1.58 | 0.39 | 0 | 0.53 | 1.88 | 3.48 | 4.72 | 5.28 | 5.02 | 4.00 | 2.44 | 0.88 | 0 | -.07 | 0.24 | 0.83 | 1.48 | 1.99 | 2.23 | 2.13 | 1.69 | 0.95 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| H | 0 | 1.75 | 3.15 | 4.07 | 4.43 | 4.20 | 3.46 | 2.37 | 1.20 | 0 | 0.45 | 1.70 | 3.15 | 4.22 | 4.56 | 4.08 | 2.94 | 1.49 | 0.35 | 0 | 0.57 | 1.94 | 3.55 | 4.79 | 5.35 | 5.08 | 4.04 | 2.47 | 0.89 | 0 | -.07 | 0.23 | 0.81 | 1.47 | 1.97 | 2.21 | 2.12 | 1.68 | 0.94 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| J | 0 | 1.96 | 3.56 | 4.64 | 5.09 | 4.91 | 4.15 | 2.96 | 1.62 | 0 | 0.26 | 1.23 | 2.62 | 3.57 | 3.90 | 3.52 | 2.54 | 1.27 | 0.25 | 0 | 0.65 | 2.08 | 3.71 | 4.96 | 5.51 | 5.21 | 4.14 | 2.54 | 0.92 | 0 | -.09 | 0.20 | 0.78 | 1.42 | 1.93 | 2.18 | 2.09 | 1.66 | 0.93 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| K | 0 | .99 | 3.62 | 4.74 | 5.24 | 5.10 | 4.37 | 3.20 | 1.81 | 0 | 0.02 | 0.75 | 1.73 | 2.19 | 2.29 | 2.53 | 1.81 | 0.85 | 0.12 | 0 | 0.60 | 1.82 | 3.17 | 4.19 | 4.63 | 4.77 | 3.47 | 2.13 | 0.79 | 0 | -.10 | 0.11 | 0.56 | 1.07 | 1.48 | 1.62 | 1.29 | 0.73 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |



SECTION THROUGH PARAPET

SCALE: 3/4" = 1'-0"
NOTE: ALL PARAPET DIMENSIONS ARE PERPENDICULAR OR PARALLEL TO SLOPE OF DECK SLAB



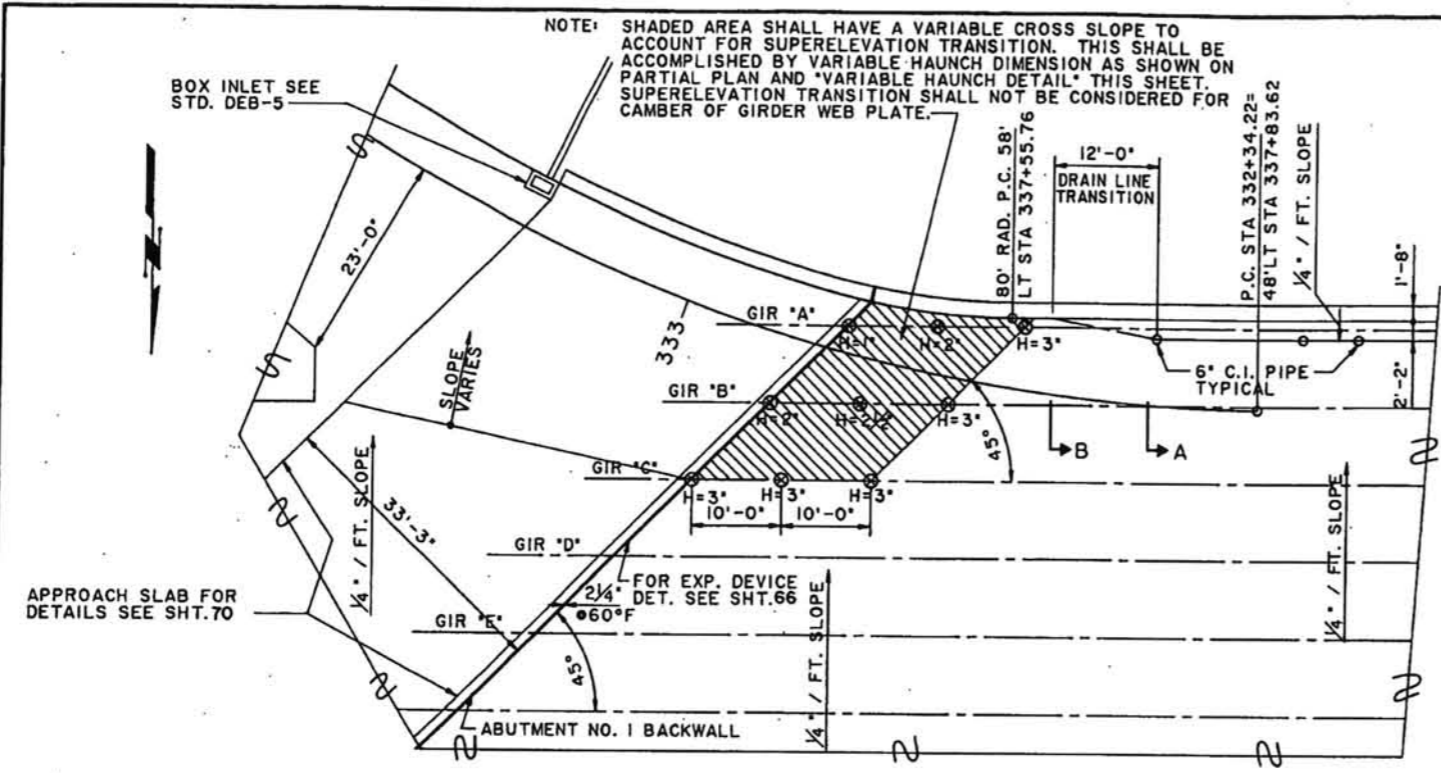
FOR PARAPET CONSTR. JT. DETAILS SEE SHT. 69

FLUTED PATTERN TEXTURE AT EXT. FACE OF PARAPET

| | |
|----------|-----|
| Design | JTB |
| Drawn | JTS |
| Checked | LQS |
| Approved | |
| Squad | |

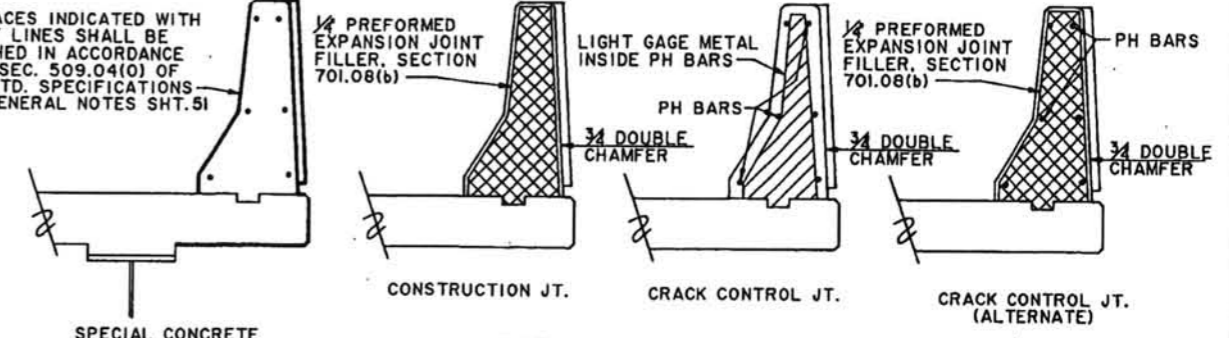
BRIDGE F
DETAILS OF SUPERSTRUCTURE
(SHT. 6 OF 7)
F.A. Project No. 1-235-1(10)000 Sheet No. 68

| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|-----------|-------------|-----------|--------------|
| 6 | OKLA. | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |



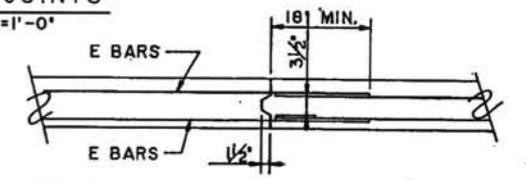
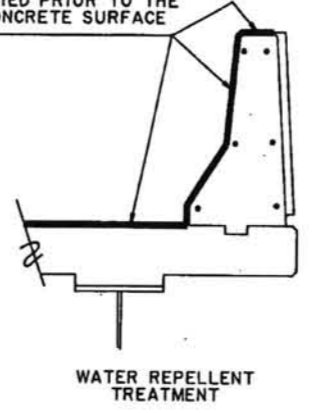
PARTIAL PLAN OF SLAB
SCALE 1" = 10'

SURFACES INDICATED WITH HEAVY LINES SHALL BE FINISHED IN ACCORDANCE WITH SEC. 509.04(1) OF THE STD. SPECIFICATIONS SEE GENERAL NOTES SHT. 51



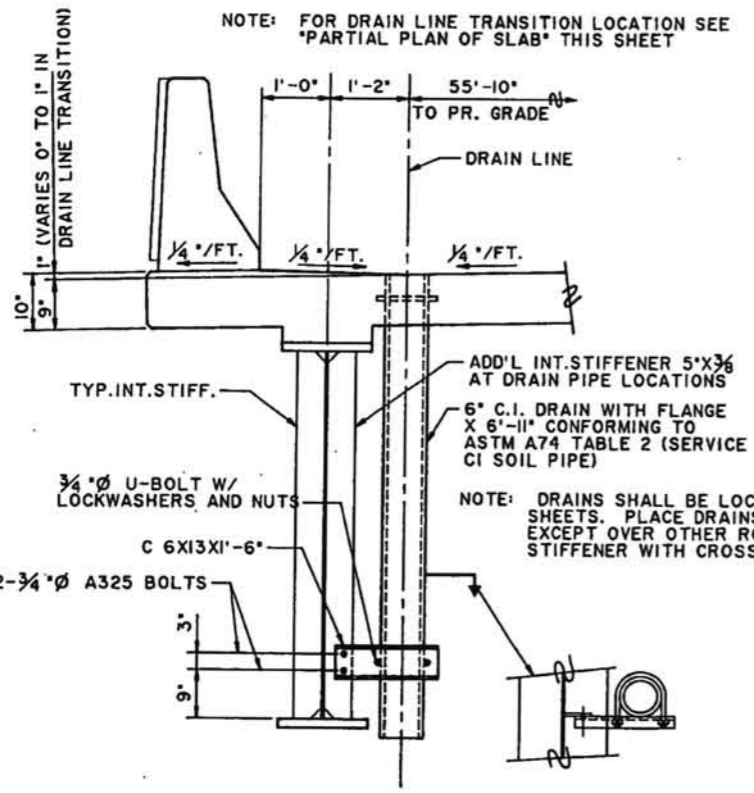
PARAPET JOINTS
SCALE: 3/4" = 1'-0"

SURFACES INDICATED WITH HEAVY LINES SHALL BE TREATED WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT. TREATMENT TO BE APPLIED PRIOR TO THE APPLICATION OF THE CONCRETE SURFACE FINISH.

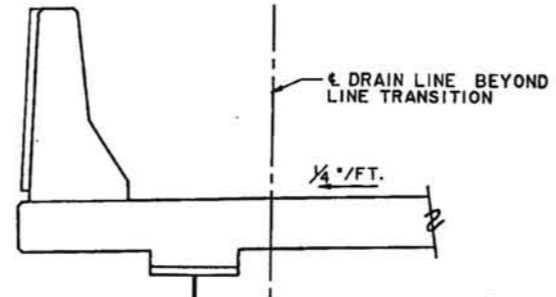


SLAB TRANSVERSE CONSTRUCTION JOINT

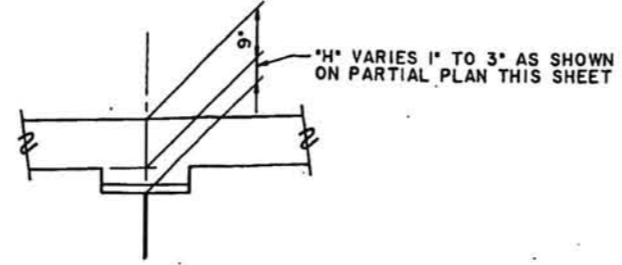
NOTE: NO HEAVY CONSTRUCTION MACHINES OR OTHER EQUIPMENT WILL BE PERMITTED ON THE FINISHED SLAB WITHIN 5'-0" OF THE TRANSVERSE AND LONGITUDINAL CONSTRUCTION JOINTS UNTIL AFTER SLAB IS IN PLACE ON BOTH SIDES OF THE JOINT.



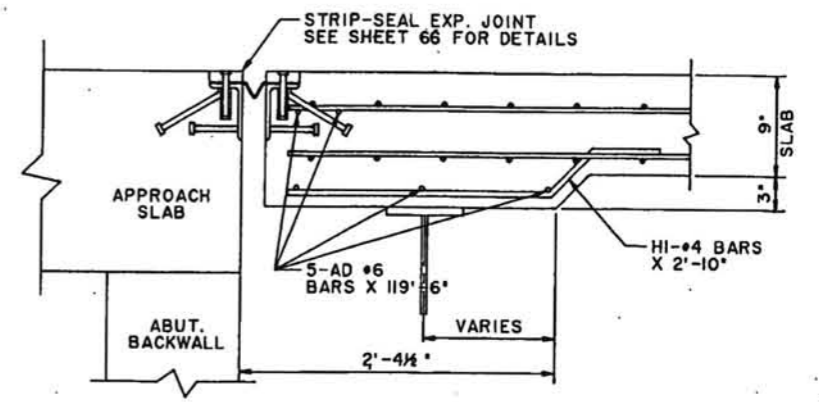
SECTION A*
DETAIL OF CAST IRON DRAIN
SCALE: 3/4" = 1'-0"



SECTION B*
SCALE: 3/4" = 1'-0"



VARIABLE HAUNCH DETAIL
SCALE: 3/4" = 1'-0"



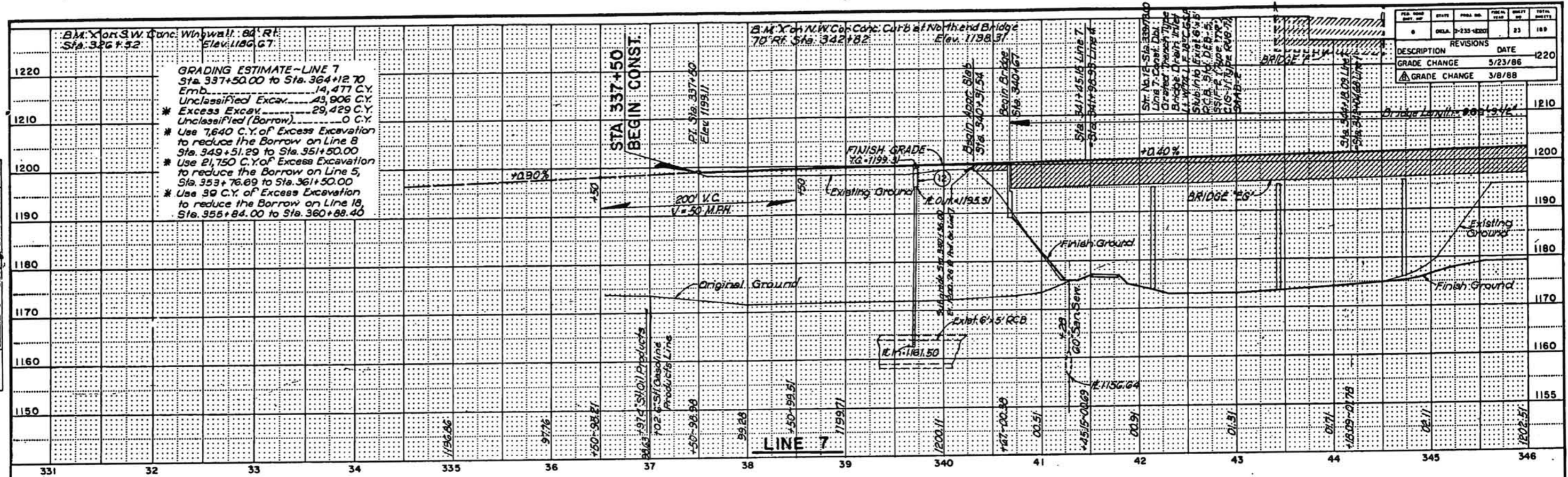
SECTION C
SEE SHEET 67

| | |
|----------|-----|
| Design | JTB |
| Drawn | JRS |
| Checked | LRS |
| Approved | |
| Squad | |

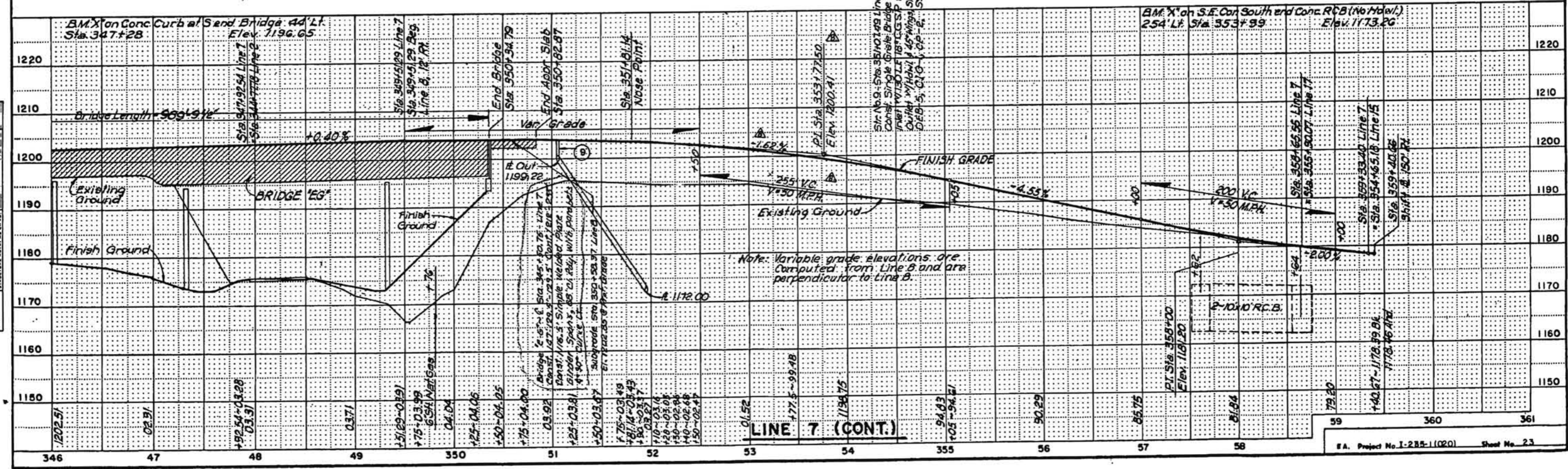
BRIDGE F
DETAILS OF SUPERSTRUCTURE
(SHT. 7 OF 7)

| | |
|-----------------------|--|
| DATE | |
| BY | |
| PLAN | |
| SUBMITTED | |
| APPROVED | |
| NOTE BOOK | |
| NO. OF SHEETS CHECKED | |
| NO. | |

| | |
|-----------------------|--|
| DATE | |
| BY | |
| PROFILE | |
| SUBMITTED | |
| APPROVED | |
| NOTE BOOK | |
| NO. OF SHEETS CHECKED | |
| NO. | |



| | | | | | | | |
|--------------|---|-----------|---------|--------------|----|--------------|-----|
| REV. NO. | 0 | DATE | 5-23-88 | TOTAL SHEETS | 23 | TOTAL SHEETS | 189 |
| DESCRIPTION | | REVISIONS | DATE | | | | |
| GRADE CHANGE | | 5/23/88 | | | | | |
| GRADE CHANGE | | 3/8/88 | | | | | |

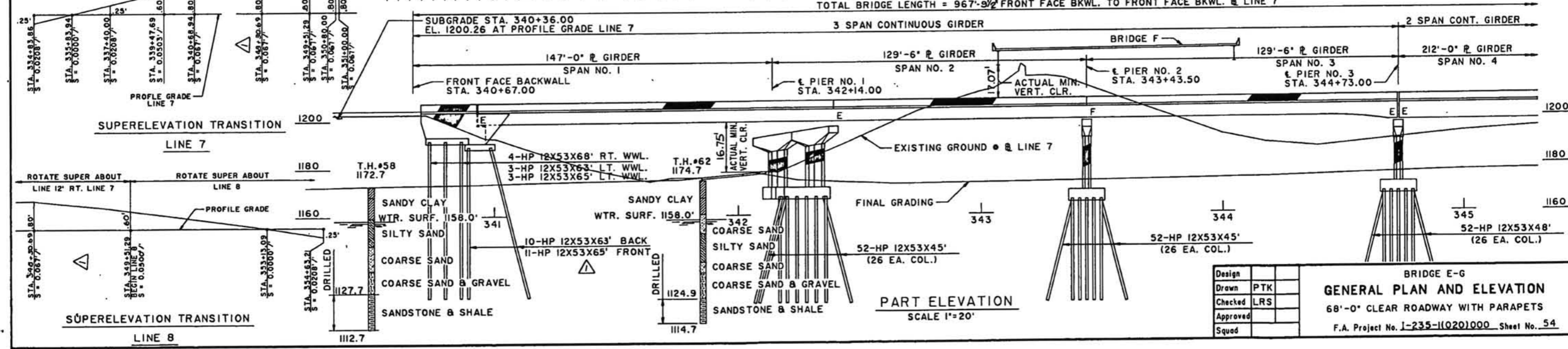
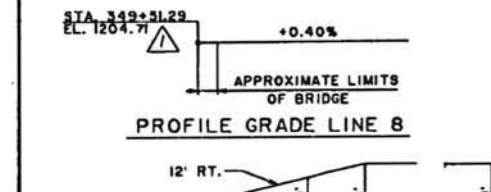
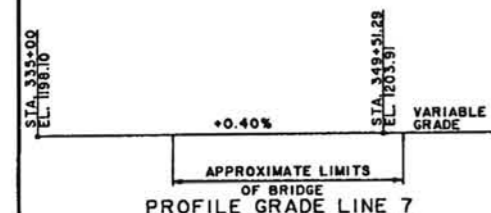
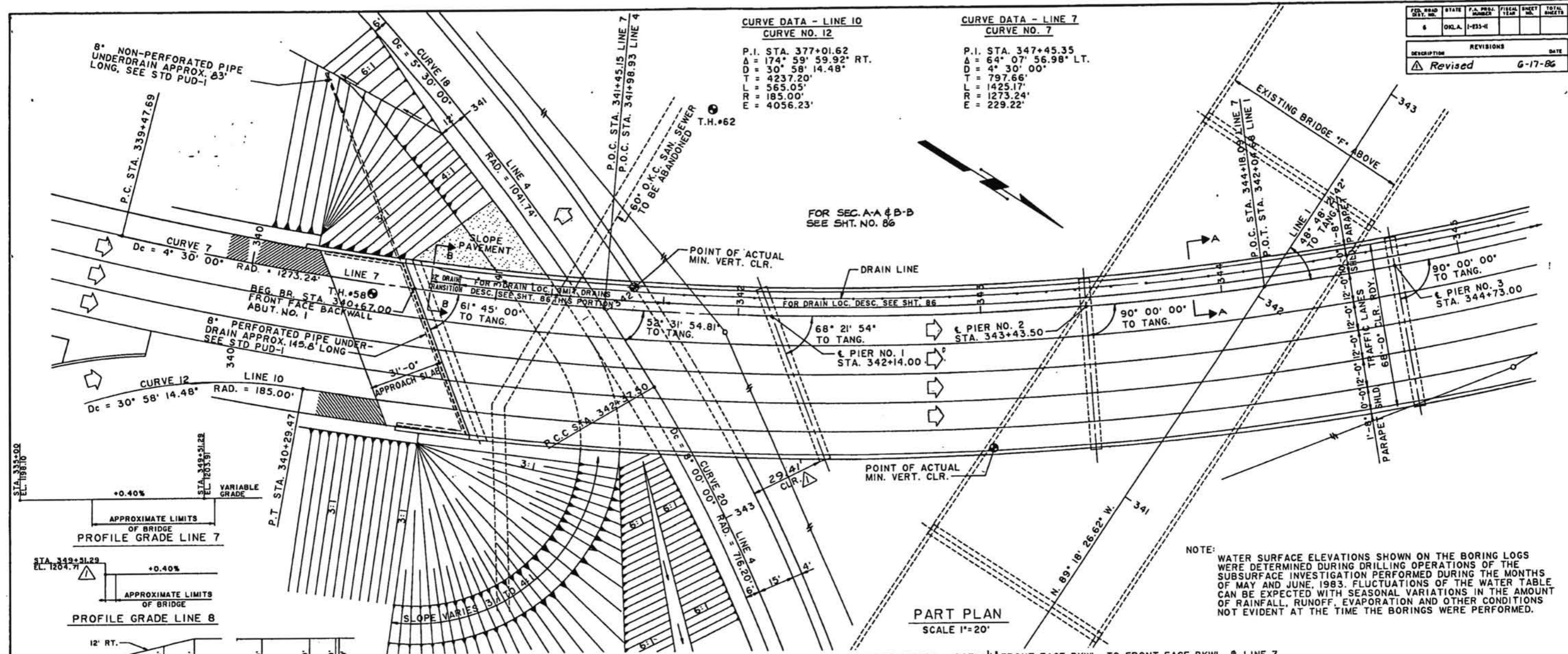


| FED. ROAD DIST. NO. | STATE | F.A. PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------------|-------------|-----------|--------------|
| 6 | OKLA. | 1-235-E | | | |

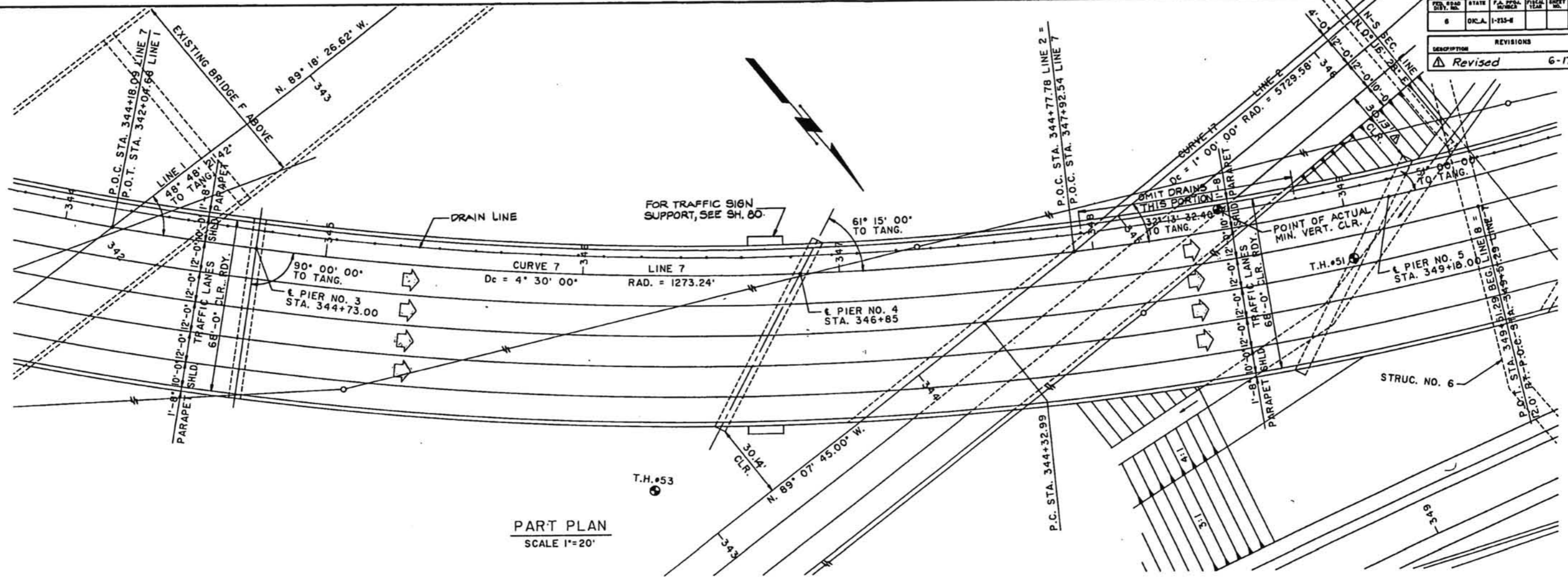
| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|---------|
| Revised | | 6-17-86 |

CURVE DATA - LINE 10
CURVE NO. 12
P.I. STA. 377+01.62
Δ = 174° 59' 59.92" RT.
D = 30° 58' 14.48"
T = 4237.20'
L = 565.05'
R = 185.00'
E = 4056.23'

CURVE DATA - LINE 7
CURVE NO. 7
P.I. STA. 347+45.35
Δ = 64° 07' 56.98" LT.
D = 4° 30' 00"
T = 797.66'
L = 1425.17'
R = 1273.24'
E = 229.22'

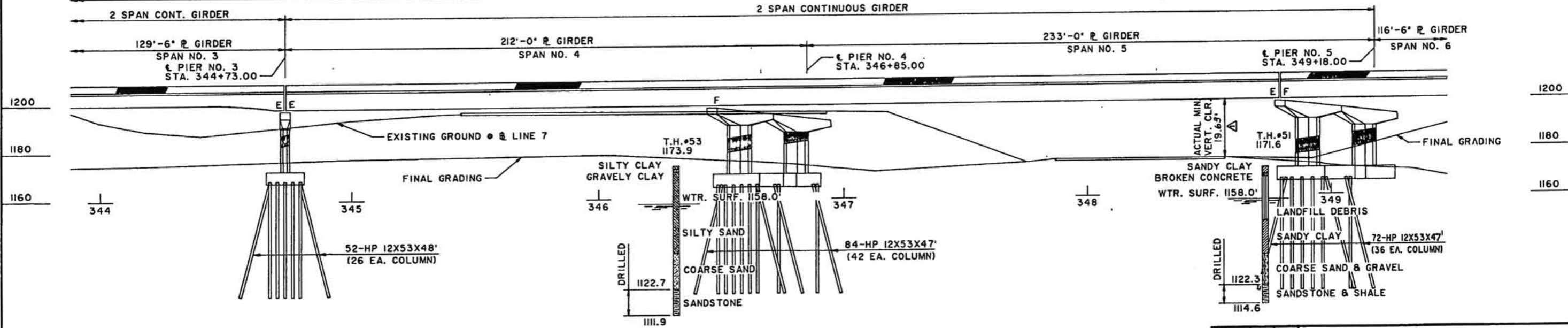


| NO. | DATE | BY | REVISIONS | DATE |
|-----|---------|----|-----------|------|
| 1 | 6-17-86 | | Revised | |



PART PLAN
SCALE 1"=20'

TOTAL BRIDGE LENGTH = 967'-9 1/2" FRONT FACE BKWL. TO FRONT FACE BKWL. @ LINE 7



PART ELEVATION
SCALE 1"=20'

| | | | | |
|----------|-----|--|--|--|
| Design | | | | |
| Drawn | PTK | | | |
| Checked | LRS | | | |
| Approved | | | | |
| Squed | | | | |

BRIDGE E-G
GENERAL PLAN AND ELEVATION
68'-0" CLEAR ROADWAY WITH PARAPETS
F.A. Project No. I-235-(1020)000 Sheet No. 55

| FED. ROAD DIST. NO. | STATE | F.A. PROJECT NUMBER | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|---------------------|-------------|-----------|--------------|
| 6 | OKLA. | 1-235-1020 | | 36 | |

DESIGN DATA

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|---------|
| Revised | | 6-17-86 |

LOADING HS20+20 psf FUT. WS.
OKLA. O. TRUCK

CONCRETE
CL AA $f'_c = 3000$ PSI
CL A $f'_c = 3000$ PSI

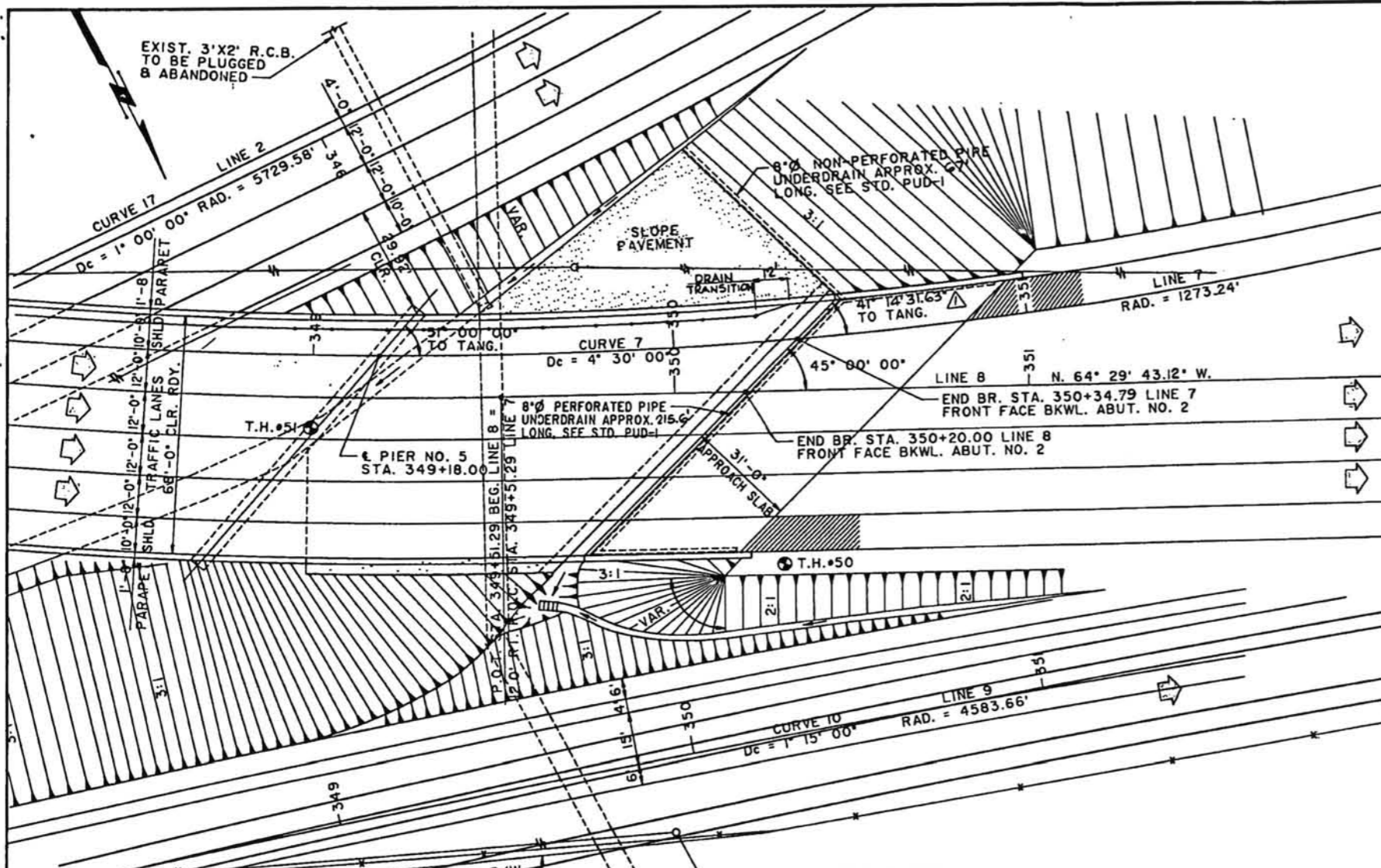
DESIGN WSD AASHTO - SUPERSTRUCTURE
LFD AASHTO - SUBSTRUCTURE

REINFORCING STEEL
GRADE 60 $f_y = 60,000$ PSI
GRADE 40 $f_y = 40,000$ PSI

DESIGN SPEC: AASHTO SPECIFICATIONS 1977 EDITION
AND INTERIM SPECIFICATIONS

STRUCTURAL STEEL
A-588 $f_y = 50,000$ PSI
A-36 $f_y = 36,000$ PSI

FOUNDATION PRESSURE
ABUTMENT (DIRECT) 62.6 TONS/PILE
PIER (DIRECT) 57.0 TONS/PILE

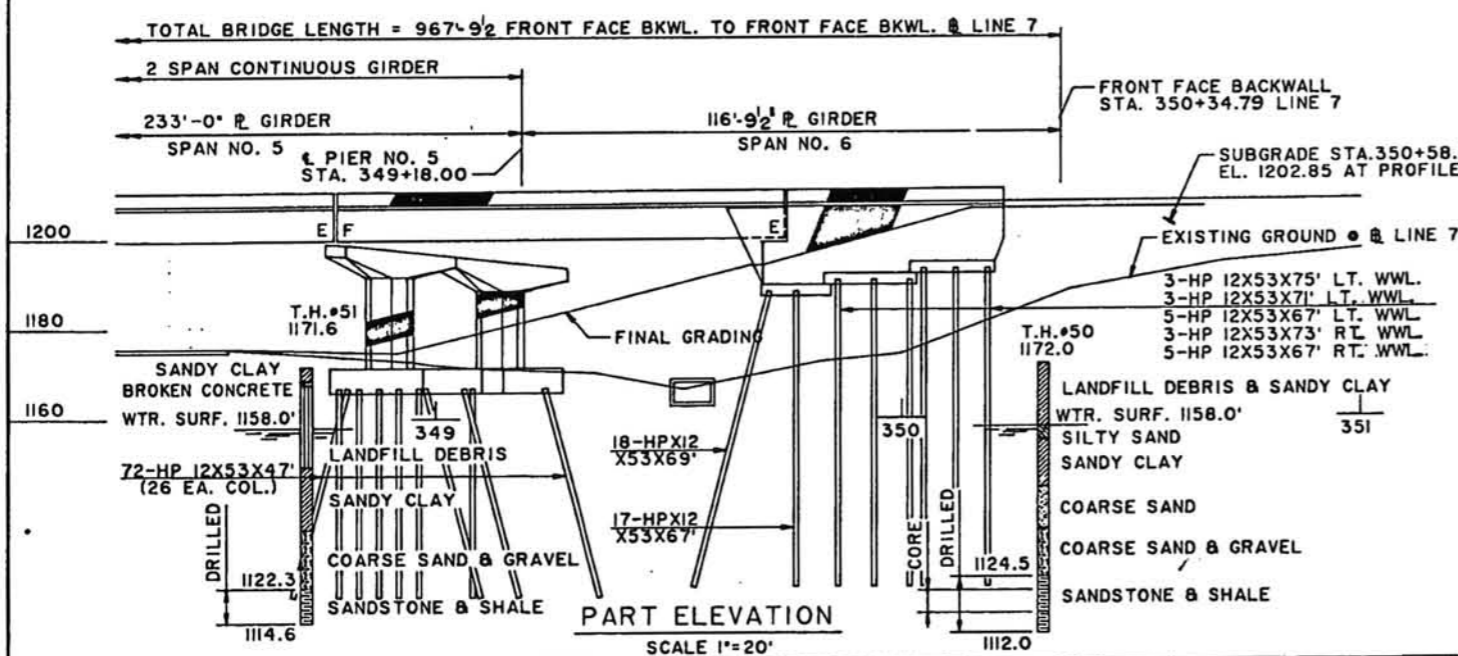


PART PLAN
SCALE 1"=20'

| SUMMARY OF QUANTITIES | | | | | | |
|-----------------------|---|------|---------|----------|------------|------------|
| ITEM NO. | ITEM | UNIT | ABUTS. | PIERS | SUPER | TOTAL |
| 202.06(F) | SELECT BORROW | C.Y. | 1280. | | | 1280. |
| 414.06(C) | APPROACH SLABS | S.Y. | 626.2 | | | 626.2 |
| 501.06(B) | SUBSTRUCTURE EXC. COMMON | C.Y. | 1,380. | 2,853. | | 4,233. |
| 504.06(A) | CLASS AA CONC. | C.Y. | | | 1,973.9 | 1,973.9 |
| 505.06(B) | CONCRETE PARAPET | L.F. | 178.0 | | 1,893.4 | 2,071.4 |
| 506.06(A) | STRUCTURAL STEEL A-36 | LB. | | | 693,320. | 693,320. |
| 506.06(A) | STRUCTURAL STEEL A-588 | LB. | | | 2,861,310. | 2,861,310. |
| 509.06(B) | CLASS A CONC. | C.Y. | 699.7 | 1,676.4 | | 2376.1 |
| 511.06 | REINFORCING STEEL (GR. 40) | LB. | 54,760. | 223,740. | | 278,500. |
| 511A.06 | EPOXY COATED REINF. STEEL (GR. 40) | LB. | | 37,740. | | 37,740. |
| 511A.06 | EPOXY COATED REINF. STEEL (GR. 60) | LB. | | | 436,760. | 436,760. |
| 514.06(E) | STEEL PILING (HP 12X53) | L.F. | 5,709. | 14,508. | | 20,217. |
| 514.06(F) | STEEL PILING SPLICES (HP 12X53) | EA. | 85. | 312. | | 397. |
| 5155.06 | PENETRATING WATER REPELLENT SURFACE TREATMENT | S.Y. | 441.7 | 714.0 | 7,917.8 | 9,073.5 |
| 613.06(Q) | PERFORATED PIPE UNDERDRAIN | L.F. | 365.9 | | | 365.9 |
| 613.06(R) | NON-PERFORATED PIPE UNDERDRAIN | L.F. | 210.9 | | | 210.9 |
| 613.06(S) | PIPE UNDERDRAIN COVER MATERIAL | C.Y. | 145. | | | 145. |
| 613.06(T) | TRENCH EXCAVATION | C.Y. | | | | 69.2 |
| 613.06(U) | STANDARD BEDDING MATERIAL | C.Y. | | | | 28.0 |
| 900.23 SP | STRIP SEAL TYPE EXP. JOINT WITH ANGLES | L.F. | | | 343.2 | 343.2 |
| 900.04 SP | 5" CONCRETE SLOPE WALL | S.Y. | 1,788.6 | | | 1,788.6 |
| 900.65 SP | APPROACH SLAB BASE | S.Y. | 626.2 | | | 626.2 |
| 900.66 SP | RESET EXPAN. BEARINGS | EA. | | | 8. | 8. |

SHEET INDEX

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| 53 | GENERAL NOTES - BRIDGES E-G AND I | 73 | DETAILS OF PIERS |
| 54 | GENERAL PLAN AND ELEVATION | 74 | DETAILS OF SUPERSTRUCTURE |
| 55 | GENERAL PLAN AND ELEVATION | 75 | DETAILS OF SUPERSTRUCTURE |
| 56 | GENERAL PLAN AND ELEVATION | 76 | DETAILS OF SUPERSTRUCTURE |
| 57 | BRIDGE STAKING DIAGRAM | 77 | DETAILS OF SUPERSTRUCTURE |
| 58 | BRIDGE STAKING DIAGRAM | 78 | DETAILS OF SUPERSTRUCTURE |
| 59 | ANCHOR BOLT LAYOUT | 79 | DETAILS OF SUPERSTRUCTURE |
| 60 | ANCHOR BOLT LAYOUT | 80 | DETAILS OF SUPERSTRUCTURE |
| 61 | DETAILS OF ABUTMENT NO. 1 | 81 | DETAILS OF SUPERSTRUCTURE |
| 62 | DETAILS OF ABUTMENT NO. 1 | 82 | DETAILS OF SUPERSTRUCTURE |
| 63 | DETAILS OF ABUTMENT NO. 2 | 83 | DETAILS OF SUPERSTRUCTURE |
| 64 | DETAILS OF ABUTMENT NO. 2 | 84 | DETAILS OF SUPERSTRUCTURE |
| 65 | DETAILS OF ABUTMENT NO. 2 | 85 | DETAILS OF SUPERSTRUCTURE |
| 66 | DETAILS OF ABUTMENTS | 86 | DETAILS OF SUPERSTRUCTURE |
| 67 | DETAILS OF PIER NO. 1 | 87 | DETAILS OF APPROACH SLABS |
| 68 | DETAILS OF PIER NO. 2 | 88 | DETAILS OF SLOPE WALLS |
| 69 | DETAILS OF PIER NO. 3 | 89 | DETAILS OF SLOPE WALLS |
| 70 | DETAILS OF PIER NO. 4 | 56A | CONSTRUCTION REFERENCE LINE LAYOUT STD PUD-1 |
| | | 85A | DETAILS OF SUPERSTRUCTURE |

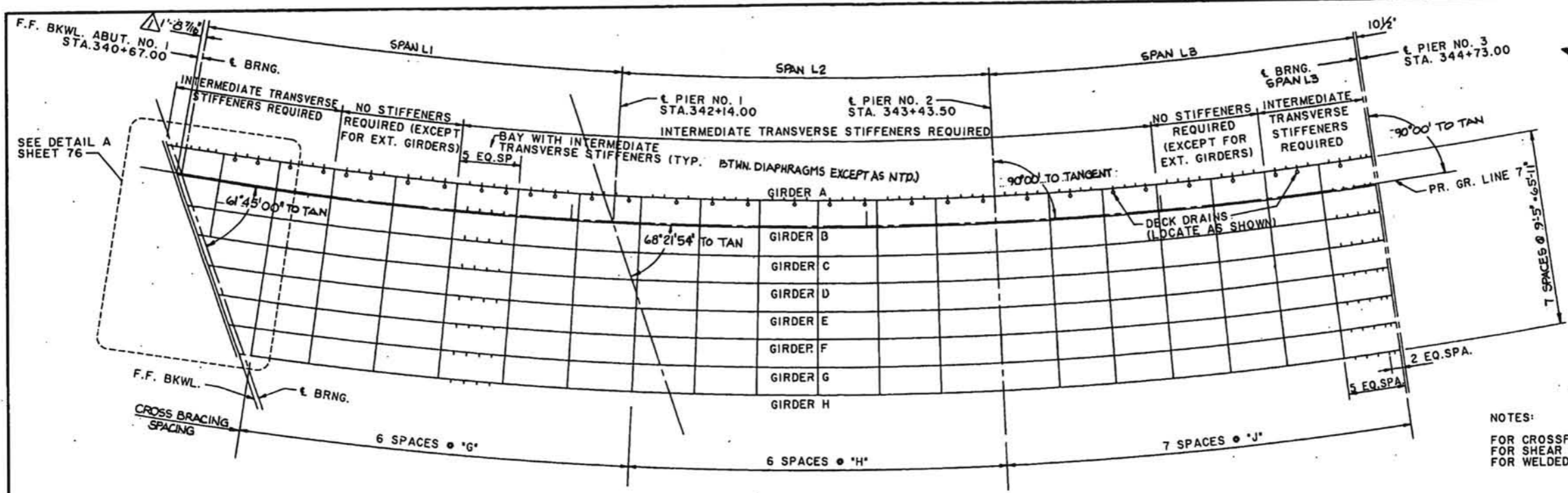


PART ELEVATION
SCALE 1"=20'

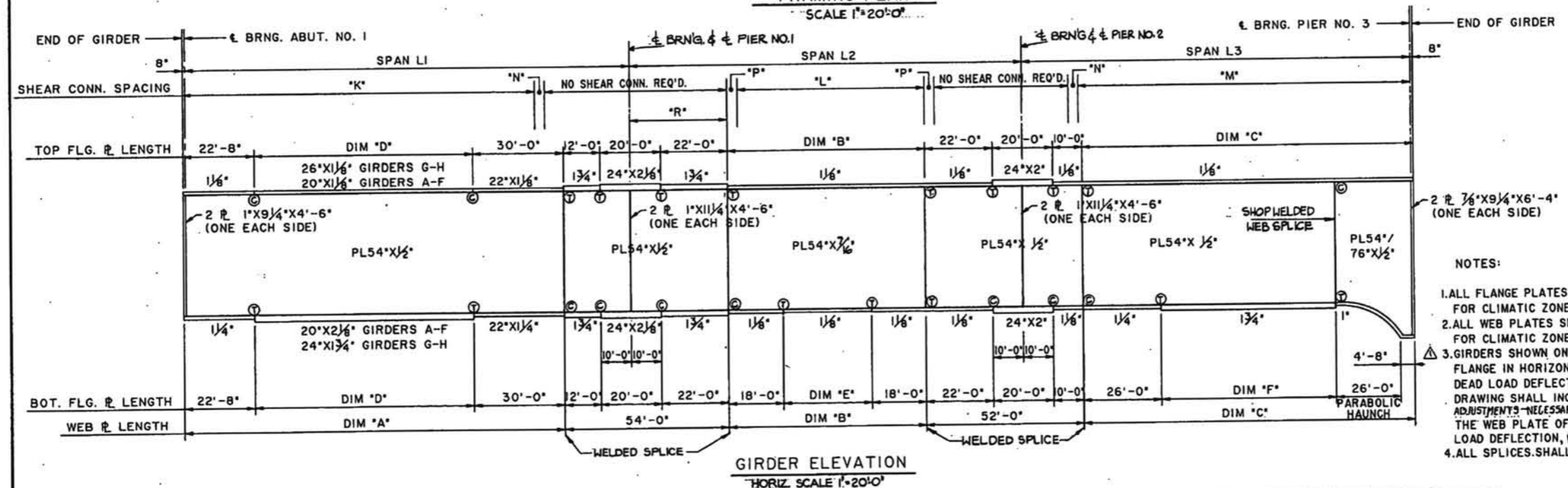
STEEL PILING:
ALL ABUTMENT PILING SHALL BE DRIVEN THROUGH THE COMPACTED FILL.
STEEL PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN ON THE PLANS. IF PRACTICAL REFUSAL IS NOT OBTAINED AT THIS ELEVATION, DRIVING SHALL CONTINUE UNTIL THE MINIMUM REQUIRED BEARING OF (120) TONS IS OBTAINED.
THE LENGTH OF STEEL PILING SHOWN ON THE PLANS ARE FOR ESTIMATING PURPOSES ONLY. THE NUMBER OF STEEL PILING SPLICES SHOWN ON THE PLANS IS ESTIMATED AT ONE SPLICE PER PILE. ACTUAL NUMBER OF SPLICES WILL BE DETERMINED IN THE FIELD DURING CONSTRUCTION.

| | | | |
|----------|--------|---|--|
| Design | | BRIDGE E-G | |
| Drawn | P.T.K. | GENERAL PLAN AND ELEVATION | |
| Checked | L.R.S. | 68'-0" CLEAR ROADWAY WITH PARAPETS | |
| Approved | | F.A. Project No. 1-235-1(020)000 Sheet No. 56 | |
| Squad | | | |

| | | | | | |
|---------------------|-------|-------------------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | F.A. PROJ. NUMBER | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| 6 | OKLA. | 1-235-E | | | |
| REVISIONS | | | | | DATE |
| Revised | | | | | 6-17-86 |
| Revised | | | | | 9-3-86 |



NOTES:
FOR CROSSFRAMING DETAILS, SEE SHT. 77
FOR SHEAR CONNECTOR DETAILS, SEE SHEET 78
FOR WELDED SPLICE DETAILS, SEE SHEET 78



NOTES:
1. ALL FLANGE PLATES SHALL CONFORM TO THE CHARPY V-NOTCH REQUIREMENTS FOR CLIMATIC ZONE 2.
2. ALL WEB PLATES SHALL CONFORM TO THE CHARPY V-NOTCH REQUIREMENTS FOR CLIMATIC ZONE 2.
3. GIRDERS SHOWN ON THIS SHEET ARE SHOWN AND DIMENSIONED WITH THE TOP FLANGE IN HORIZONTAL POSITION. VERTICAL CURVATURE AND EFFECTS OF DEAD LOAD DEFLECTION HAVE NOT BEEN TAKEN INTO CONSIDERATION. SHOP DRAWING SHALL INCLUDE A BLOCKING DIAGRAM FOR EACH GIRDER SHOWING ALL ADJUSTMENTS NECESSARY FOR D.L. DEFLECTION, VERTICAL CURVATURE AND SUPERELEVATION TRANSITION. THE WEB PLATE OF THE GIRDERS SHALL BE FABRICATED WITH CAMBER FOR DEAD LOAD DEFLECTION, VERTICAL CURVATURE AND SUPERELEVATION TRANSITION.
4. ALL SPLICES SHALL BE WELDED SPLICES - SEE SHT. 78

DIMENSION AND SHEAR CONNECTOR SPACING SCHEDULE

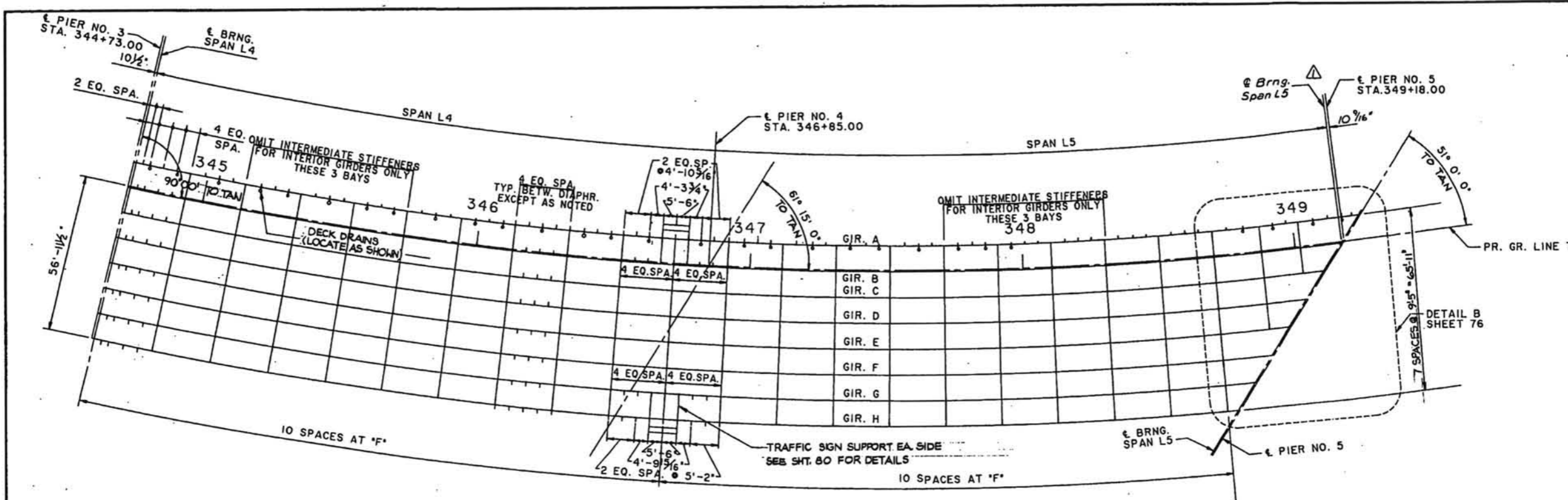
NOTES: (1) ALL FLANGE & WEB PLATES SHALL BE A-588 (M-222), ALL OTHER STRUCTURAL STEEL SHALL BE A-36 (M-103) Δ
(2) ALL FLANGE PLATES ARE 20' UNLESS NOTED

| GIRDER | SPAN L1 | SPAN L2 | SPAN L3 | RADIUS | DIM. 'A' | DIM. 'B' | DIM. 'C' | DIM. 'D' | DIM. 'E' | DIM. 'F' | 'G' | 'H' | 'I' | 'J' | 'K' | 'L' | 'M' | 'N' | 'P' | 'R' |
|--------|--------------|-------------|-------------|---------|--------------|------------|-------------|-------------|------------|------------|-------------|-------------|-------------|--------------------|-----------------|-------------------|--------------|--------------|--------------|--------|
| A | 145'-6 1/4" | 132'-1 1/4" | 127'-8 3/4" | 1264.28 | 124'-2 1/4" | 68'-1 3/4" | 108'-4 3/4" | 71'-6 1/4" | 32'-1 3/4" | 51'-8 3/4" | 20'-3 1/4" | 19'-10 1/4" | 18'-3" | 87'0 16"=116'-0" | 32'0 16"=42'-8" | 75'0 16"=100'-0" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 49'-3" |
| B | 145'-3 1/6" | 129'-4 3/8" | 128'-8 1/6" | 1273.70 | 123'-11 1/6" | 65'-4 3/8" | 109'-4 1/6" | 71'-3 1/6" | 29'-4 3/8" | 52'-8 1/6" | 20'-5 1/6" | 20'-0 1/2" | 18'-4 3/8" | 108'0 13"=117'-0" | 36'0 13"=39'-0" | 94'0 13"=101'-10" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 46'-8" |
| C | 145'-0 1/6" | 126'-7 1/6" | 129'-7 3/6" | 1283.12 | 123'-8 1/6" | 62'-7 1/6" | 110'-3 3/6" | 71'-0 1/6" | 26'-7 1/6" | 53'-7 3/6" | 20'-7 3/6" | 20'-2 3/6" | 18'-6 3/6" | 99'0 14"=115'-6" | 29'0 15"=36'-3" | 82'0 15"=102'-6" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 47'-6" |
| D | 144'-10" | 123'-9 1/6" | 130'-7 1/6" | 1292.53 | 123'-6" | 59'-9 1/6" | 111'-3 1/6" | 70'-10" | 23'-9 1/6" | 54'-7 1/6" | 20'-9 1/6" | 20'-4" | 18'-7 3/6" | 92'0 15"=115'-0" | 21'0 16"=28'-0" | 84'0 15"=105'-0" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 50'-6" |
| E | 144'-7 3/8" | 121'-0 3/8" | 131'-6 3/8" | 1301.95 | 123'-3 3/8" | 57'-0 3/8" | 112'-2 3/8" | 70'-7 3/8" | 21'-0 3/8" | 55'-6 9/8" | 20'-10 3/8" | 20'-5 3/8" | 18'-9 3/8" | 92'0 15"=115'-0" | 16'0 15"=20'-0" | 91'0 14"=106'-2" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 50'-6" |
| F | 144'-4 1/6" | 118'-3 1/2" | 132'-6 1/6" | 1311.37 | 123'-0 1/6" | 54'-3 1/2" | 113'-2 1/6" | 70'-4 1/6" | 18'-3 1/2" | 56'-6 1/6" | 21'-0 1/6" | 20'-7 3/6" | 18'-11 1/6" | 99'0 14"=115'-6" | NO V-CONN | 93'0 14"=108'-6" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 46'-0" |
| G | 144'-2 3/6" | 115'-6 3/6" | 133'-5 1/2" | 1320.78 | 122'-10 3/6" | 51'-6 3/6" | 114'-1 1/2" | 70'-2 3/6" | 15'-6 3/6" | 57'-5 1/6" | 21'-2 3/6" | 20'-9 3/6" | 19'-0 3/6" | 107'0 13"=115'-11" | NO V-CONN | 101'0 13"=109'-5" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 46'-0" |
| H | 143'-11 1/6" | 112'-9 3/6" | 134'-5" | 1330.20 | 122'-7 1/6" | 48'-9 3/6" | 115'-1" | 69'-11 1/6" | 12'-9 3/6" | 58'-5" | 21'-4 3/6" | 20'-11 1/6" | 19'-2 1/6" | 94'0 15"=117'-6" | NO V-CONN | 83'0 16"=110'-8" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 8'0 5"=3'-4" | 46'-0" |

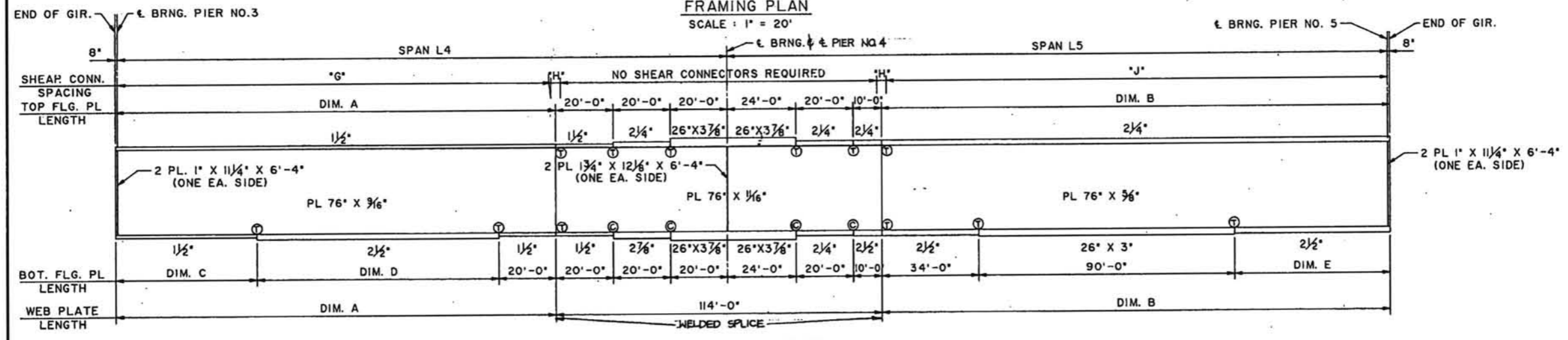
| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
(SHT. 1 OF 14)
F.A. Project No. 1-235-(1020)000 Sheet No. 74

| FED. ROAD DIST. NO. | STATE | F.A. PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------------|-----------|--------------|
| 6 | OKLA. | 235-1020 | 75 | |
| DESCRIPTION | | REVISIONS | DATE | |
| Revised | | | 6-17-86 | |
| Revised | | | 9-3-86 | |



FRAMING PLAN
SCALE: 1" = 20'



GIRDER ELEVATION
HORIZ. SCALE: 1" = 20'

DIMENSION & SHEAR CONNECTOR SPACING SCHEDULE

| GIRDER | SPAN L4 | SPAN L5 | RADIUS | DIM. A | DIM. B | DIM. C | DIM. D | DIM. E | DIM. F | *G* | *H* | *J* |
|--------|--------------|-------------|---------|--------------|-------------|-------------|--------|------------|-------------|---------------------|----------------|----------------------|
| A | 214'-6 3/8" | 232'-10" | 1264.28 | 155'-2 3/8" | 179'-6" | 49'-2 3/8" | 86'-0" | 55'-6" | 19'-7 3/8" | 92 @ 20" = 153'-4" | 8 @ 5" = 3'-4" | 93 @ 23" = 178'-3" |
| B | 210'-11 3/8" | 232'-1 1/8" | 1273.70 | 151'-7 3/8" | 178'-9 1/8" | 47'-7 3/8" | 84'-0" | 54'-9 1/8" | 19'-9 3/8" | 105 @ 17" = 148'-9" | 8 @ 5" = 3'-4" | 106 @ 20" = 176'-8" |
| C | 207'-4 3/8" | 231'-4 1/8" | 1283.11 | 148'-0 3/8" | 178'-0 1/8" | 46'-0 3/8" | 82'-0" | 54'-0 1/8" | 19'-11 1/8" | 102 @ 17" = 144'-6" | 8 @ 5" = 3'-4" | 105 @ 20" = 175'-0" |
| D | 203'-9 3/8" | 230'-8 1/8" | 1292.53 | 144'-5 3/8" | 177'-4 1/8" | 44'-5 3/8" | 80'-0" | 53'-4 1/8" | 20'-0 1/8" | 85 @ 20" = 141'-8" | 8 @ 5" = 3'-4" | 105 @ 20" = 175'-0" |
| E | 200'-2 1/2" | 230'-0 1/8" | 1301.95 | 140'-10 1/2" | 176'-8 1/8" | 42'-10 1/2" | 78'-0" | 52'-8 1/8" | 20'-2 1/8" | 83 @ 20" = 138'-4" | 8 @ 5" = 3'-4" | 104 @ 20" = 173'-4" |
| F | 196'-7 3/4" | 229'-4 3/8" | 1311.36 | 137'-3 3/4" | 176'-0 3/8" | 41'-3 3/4" | 76'-0" | 52'-0 3/8" | 20'-4 1/2" | 95 @ 17" = 134'-7" | 8 @ 5" = 3'-4" | 104 @ 20" = 173'-4" |
| G | 193'-1 3/4" | 228'-8 3/4" | 1320.78 | 133'-9 3/4" | 175'-4 3/4" | 39'-9 3/4" | 74'-0" | 51'-4 3/4" | 20'-6 3/8" | 93 @ 17" = 131'-9" | 8 @ 5" = 3'-4" | 122 @ 17" = 172'-10" |
| H | 189'-6 1/4" | 228'-1 1/8" | 1330.20 | 130'-2 1/8" | 174'-9 1/8" | 38'-2 1/8" | 72'-0" | 50'-9 1/8" | 20'-7 1/8" | 90 @ 17" = 127'-6" | 8 @ 5" = 3'-4" | 148 @ 14" = 172'-8" |

NOTES: (1) ALL FLANGE AND WEB PLATES SHALL BE A-588 (M-22Z). ALL OTHER STRUCTURAL STEEL SHALL BE A-36 (M-183). (2) ALL FLANGE PLATES 24" WIDE UNLESS NOTED.

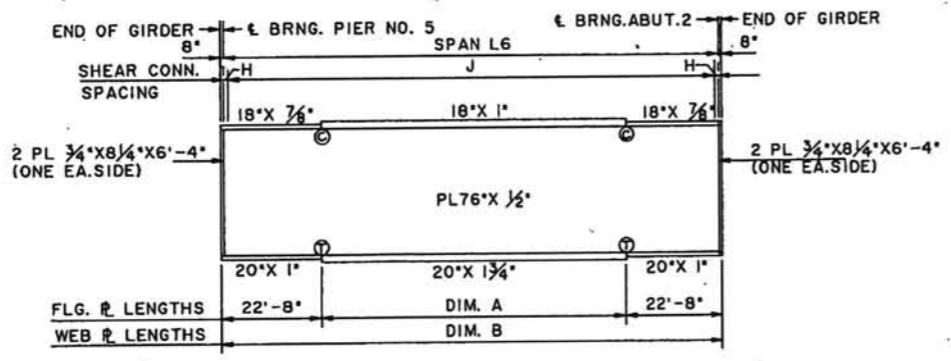
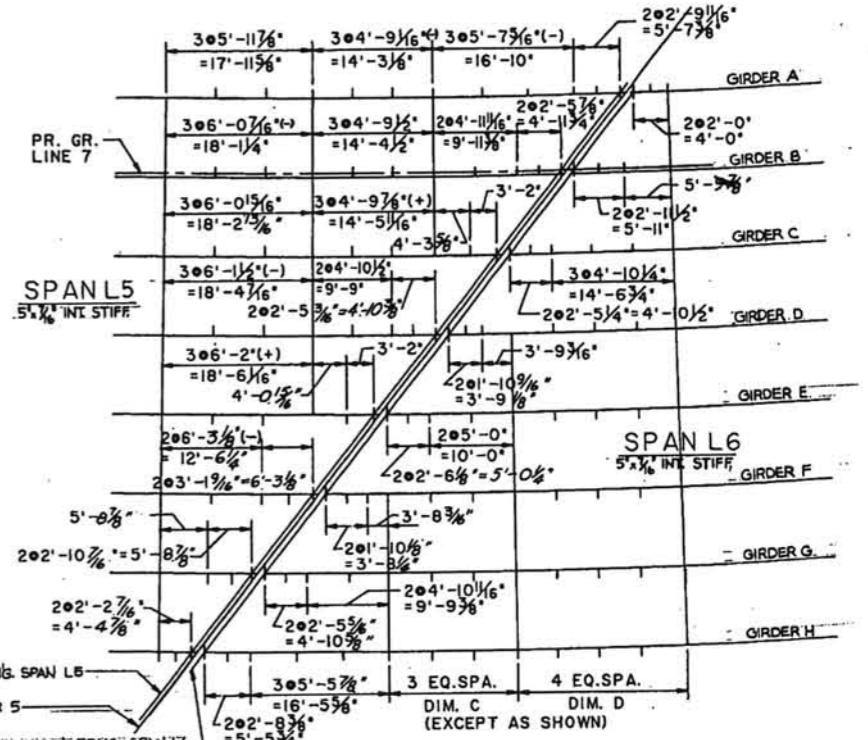
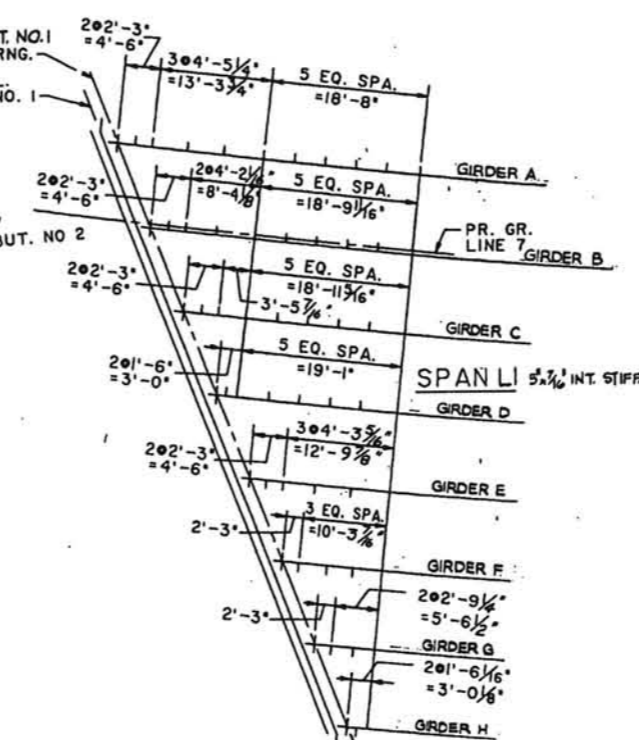
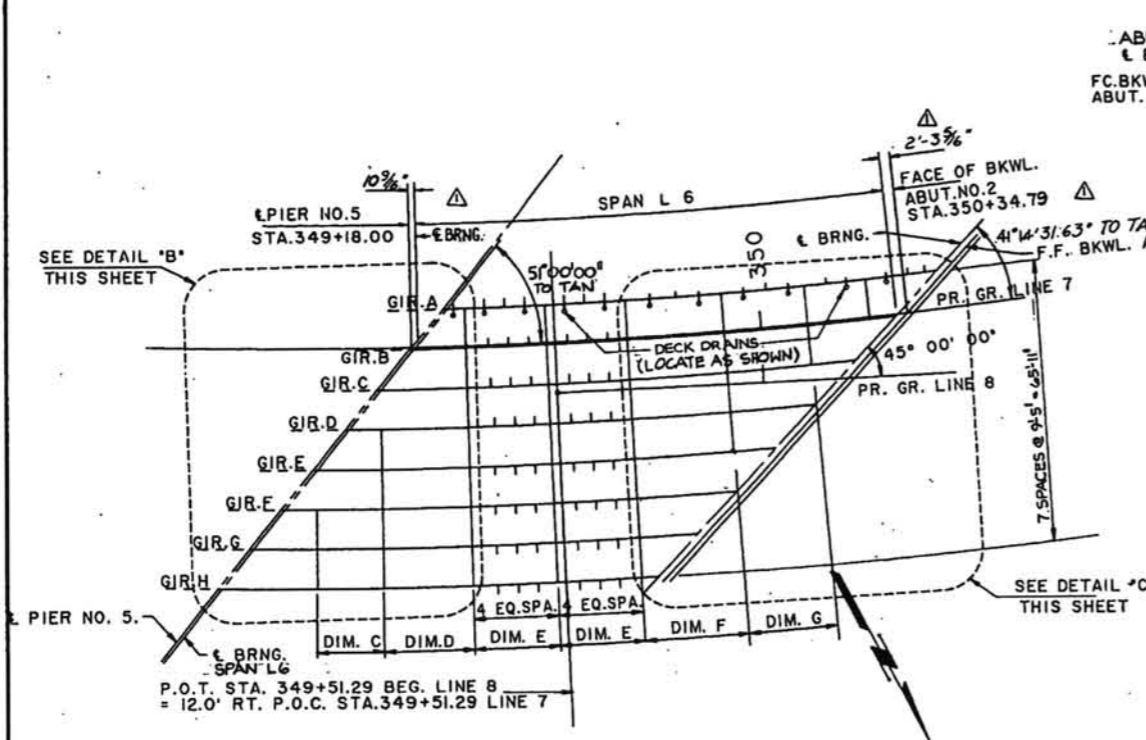
NOTES: FOR CROSSFRAMING DETAILS, SEE SHT. 77 FOR SHEAR CONNECTOR DETAILS, SEE SHT. 78 FOR WELDED SPLICE DETAILS, SEE SHT. 78 SEE SHEET 74 FOR ADDITIONAL NOTES.

SEE CHANGE OF PLANS SHT. NO. 76A

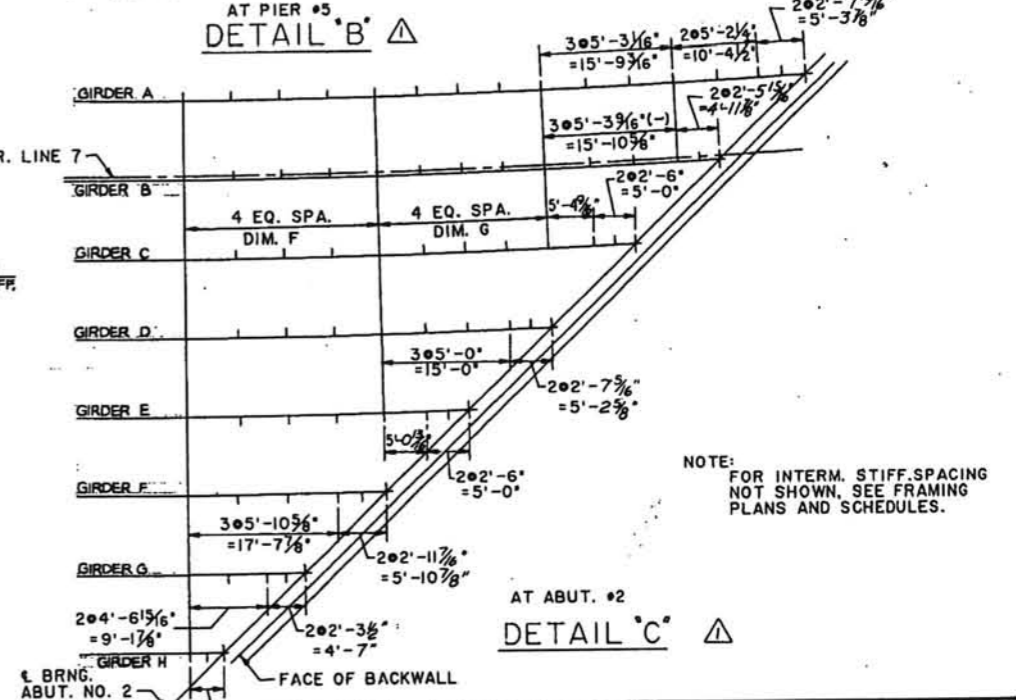
| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squid | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
(SHT. 2 OF 14)
F.A. Project No. 1-235-1(020)000 Sheet No. 75

| NO. | DATE | BY | CHKD. | APPD. | DATE |
|-----------|---------------|----|-------|-------|---------|
| 1 | OKLA. 1-13-84 | | | | |
| REVISIONS | | | | | DATE |
| REVISED | | | | | 6-17-86 |



NOTES:
FOR CROSS FRAMING DETAILS, SEE SHEET 77
FOR SHEAR CONNECTOR DETAILS SEE SHEET 78
FOR WELDED SPlice DETAILS SEE SHEET 78
SEE SHEET 74 FOR ADDITIONAL NOTES.



NOTE: ALL STRUCTURAL STEEL SHALL BE A-36 (M-185).

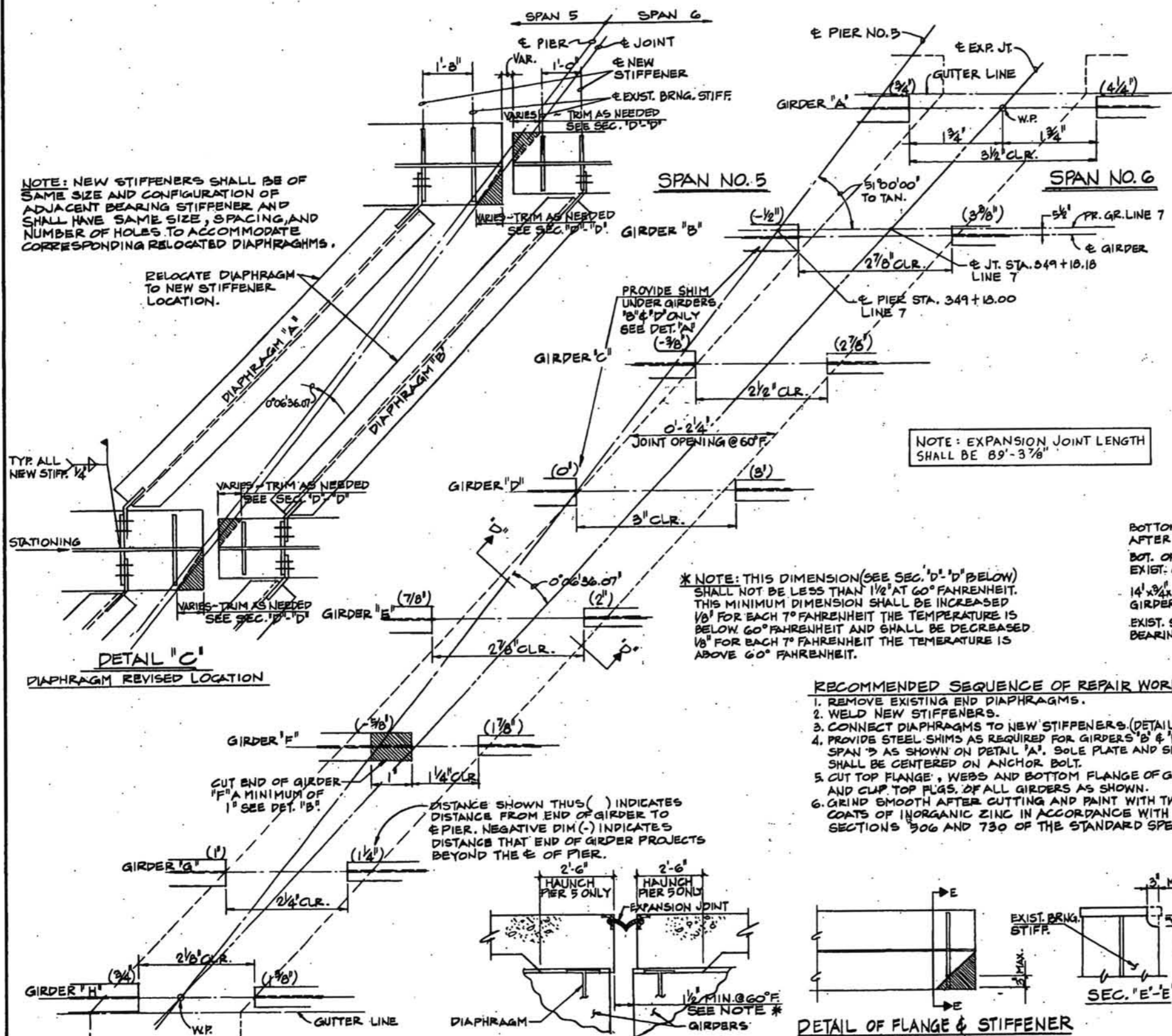
| GIRDER | SPAN L6 | RADIUS | DIM. A | DIM. B | DIM. C | DIM. D | DIM. E | DIM. F | DIM. G | DIM. H | DIM. J | |
|--------|-------------|---------|------------|--------------|------------|-------------|-------------|-------------|-------------|-------------|--------------------|--------------------|
| A | 115'-9 1/2" | 1264.28 | 71'-9 1/2" | 117'-1 1/2" | - | - | 18'-11" | 22'-8 1/2" | 19'-9 3/8" | 1 • 14 1/2" | 90 • 17" = 113'-4" | |
| B | 113'-6 1/2" | 1273.70 | 69'-6 1/2" | 114'-10 1/2" | - | - | 19'-0 3/4" | 22'-10 3/8" | 19'-11 1/8" | 1 • 11 1/2" | 67 • 20" = 111'-8" | |
| C | 111'-4 3/8" | 1283.11 | 67'-4 3/8" | 112'-8 3/8" | - | - | 19'-2 3/8" | 23'-0 3/8" | 20'-0 3/8" | 1 • 13 3/8" | 77 • 17" = 109'-1" | |
| D | 109'-2 3/8" | 1292.53 | 65'-2 3/8" | 110'-6 3/8" | - | - | 19'-6 1/8" | 19'-4 1/8" | 23'-2 3/8" | - | 1 • 9 3/8" | 76 • 17" = 107'-8" |
| E | 107'-1 3/8" | 1301.95 | 63'-1 3/8" | 108'-5 3/8" | - | - | 19'-8 3/8" | 19'-5 3/8" | 23'-4 1/8" | - | 1 • 12 3/8" | 63 • 20" = 105'-0" |
| F | 105'-1 3/8" | 1311.36 | 61'-1 3/8" | 106'-5 3/8" | 15'-1 3/8" | 19'-10 3/8" | 19'-7 1/8" | - | - | - | 1 • 10 1/2" | 73 • 17" = 103'-5" |
| G | 103'-2" | 1320.78 | 59'-2" | 104'-6" | 15'-2 3/8" | 20'-0 3/8" | 19'-9 3/8" | - | - | - | 1 • 7" | 72 • 17" = 102'-0" |
| H | 101'-3" | 1330.20 | 57'-3" | 102'-7" | 15'-4" | 20'-11 3/8" | 19'-10 3/8" | - | - | - | 1 • 12 1/2" | 70 • 17" = 99'-2" |

| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
(SHT. 3 OF 14)
F.A. Project No. I-235-1(1020)000 Sheet No. 76

| REV. NO. | DATE | BY | CHKD. | APP. BY |
|----------|------|----|-------|---------|
| | | | | |

NOTE: NEW STIFFENERS SHALL BE OF SAME SIZE AND CONFIGURATION OF ADJACENT BEARING STIFFENER AND SHALL HAVE SAME SIZE, SPACING, AND NUMBER OF HOLES TO ACCOMMODATE CORRESPONDING RELOCATED DIAPHRAGMS.



RELOCATE DIAPHRAGM TO NEW STIFFENER LOCATION.

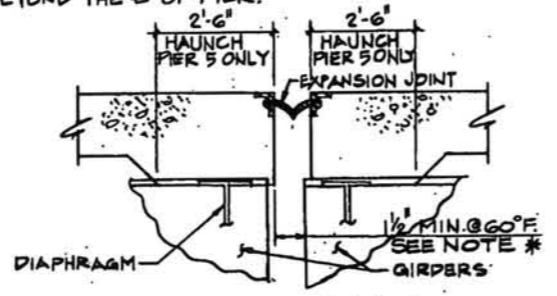
DETAIL "C" DIAPHRAGM REVISED LOCATION

* NOTE: THIS DIMENSION (SEE SEC. "D-D" BELOW) SHALL NOT BE LESS THAN 1 1/2" AT 60° FAHRENHEIT. THIS MINIMUM DIMENSION SHALL BE INCREASED 1/8" FOR EACH 7° FAHRENHEIT THE TEMPERATURE IS BELOW 60° FAHRENHEIT AND SHALL BE DECREASED 1/8" FOR EACH 7° FAHRENHEIT THE TEMPERATURE IS ABOVE 60° FAHRENHEIT.

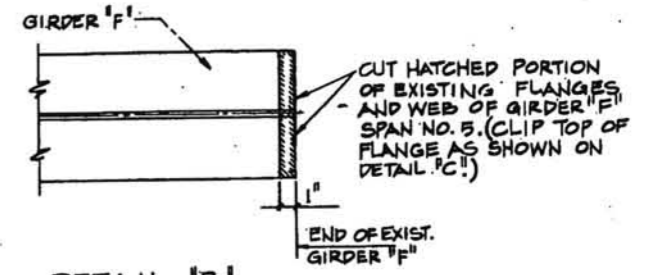
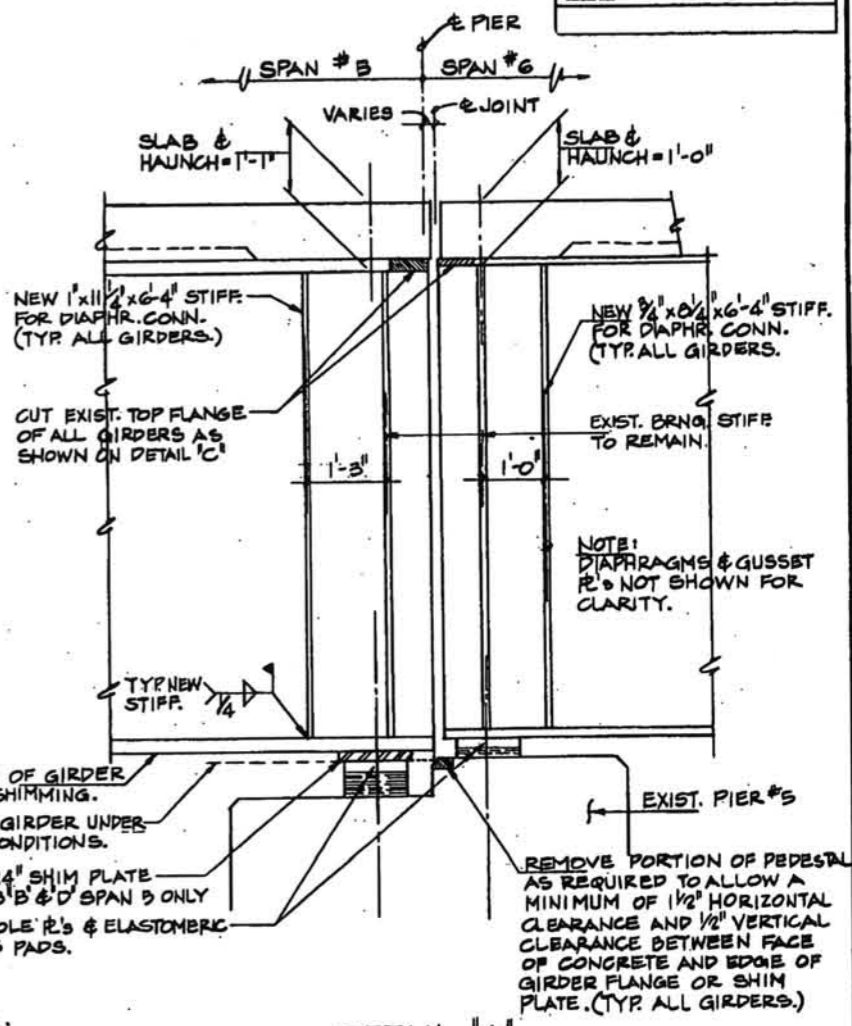
NOTE: EXPANSION JOINT LENGTH SHALL BE 69'-3 7/8"

RECOMMENDED SEQUENCE OF REPAIR WORK:

1. REMOVE EXISTING END DIAPHRAGMS.
2. WELD NEW STIFFENERS.
3. CONNECT DIAPHRAGMS TO NEW STIFFENERS (DETAIL "C").
4. PROVIDE STEEL SHIMS AS REQUIRED FOR GIRDERS "B" & "D" - SPAN 5 AS SHOWN ON DETAIL "A". SOLE PLATE AND SHIMS SHALL BE CENTERED ON ANCHOR BOLT.
5. CUT TOP FLANGE, WEBS AND BOTTOM FLANGE OF GIRDER "F" AND CLIP TOP FLGS. OF ALL GIRDERS AS SHOWN.
6. GRIND SMOOTH AFTER CUTTING AND PAINT WITH TWO COATS OF INORGANIC ZINC IN ACCORDANCE WITH SECTIONS 306 AND 730 OF THE STANDARD SPECIFICATIONS.



DETAIL OF FLANGE & STIFFENER SPAN 5 ONLY



PLAN OF EXP. JT. SCALE: 1/4" = 1'-0" TRANSVERSE, 1" = 1' LONGITUDINAL

| | |
|----------|--|
| Design | |
| Drawn | |
| Checked | |
| Approved | |
| Squad | |

CHANGE IN PLAN NO. _____

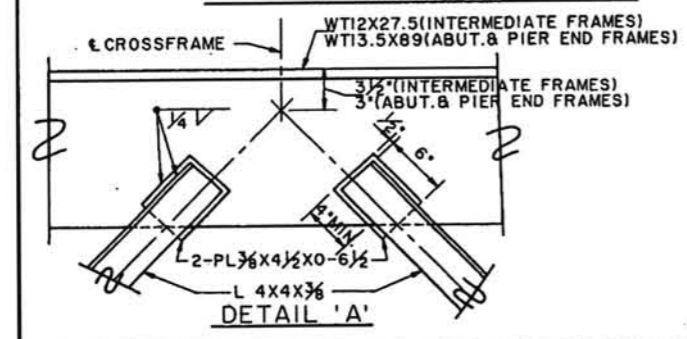
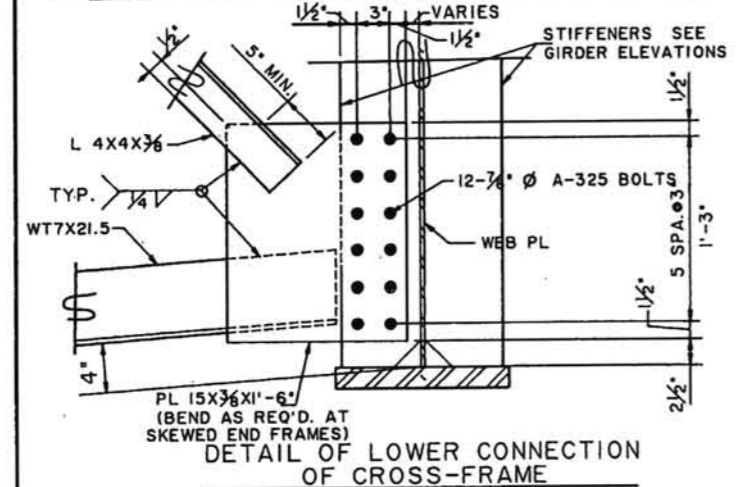
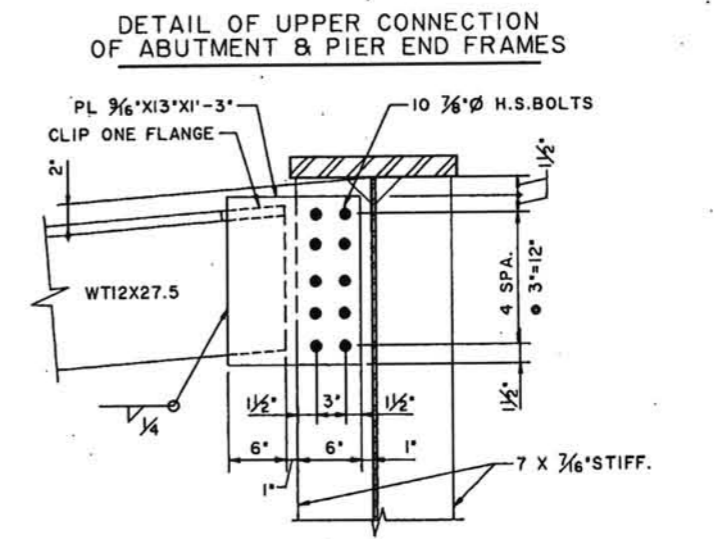
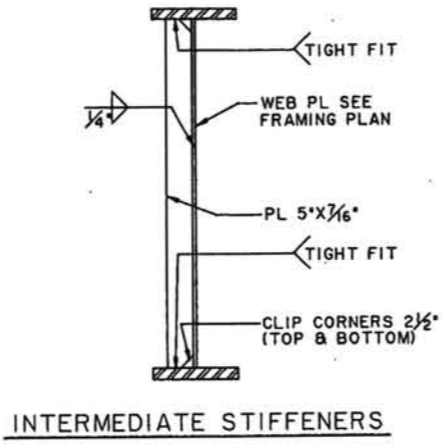
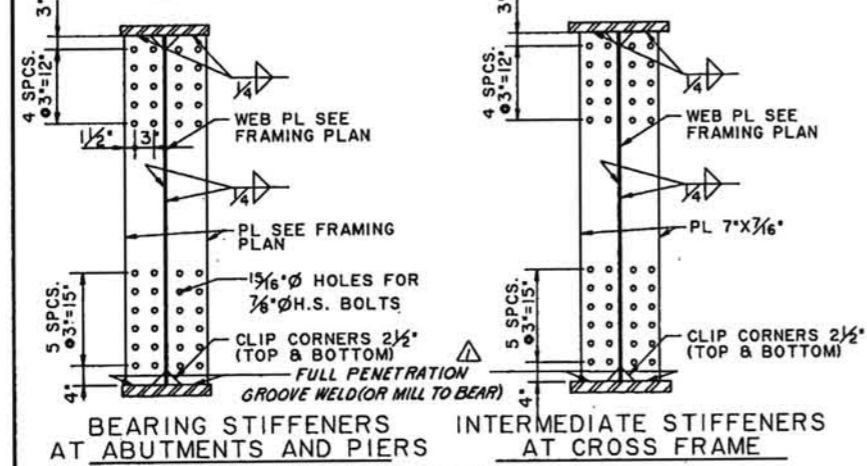
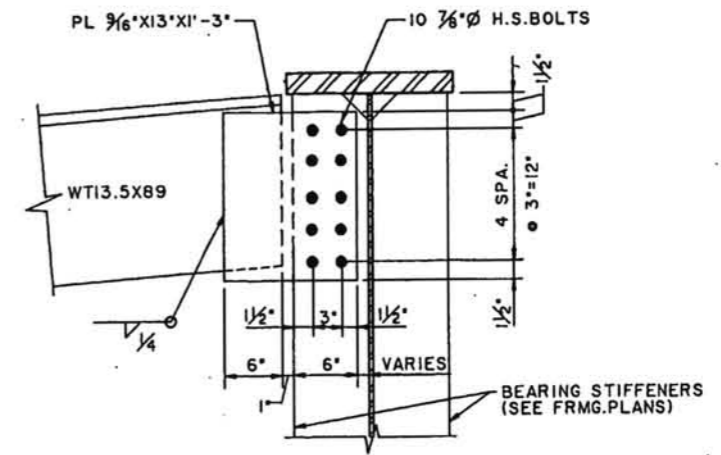
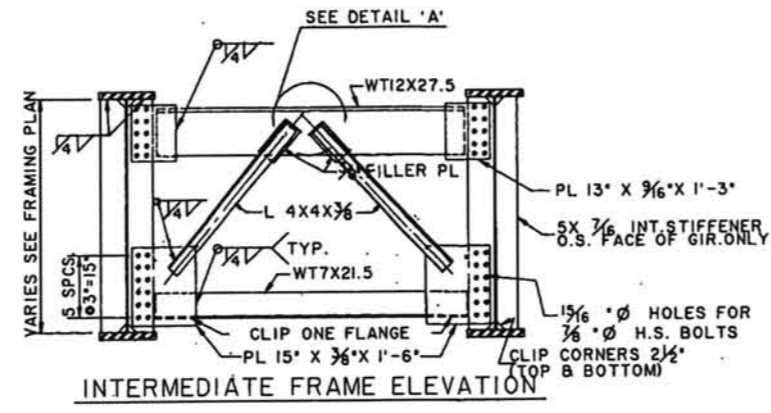
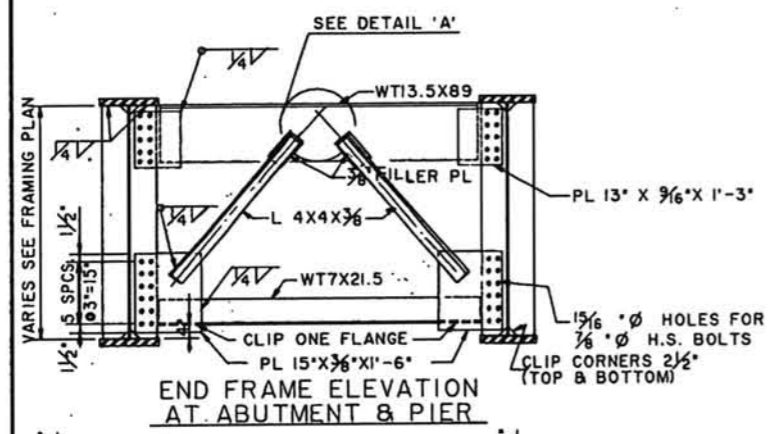
BRIDGE E-G

DETAILS OF SUPERSTRUCTURE

(SHEET 3 OF 14)

F.A. Project No. 1-235-1(020)000 Sheet No. 78A

| REV. NO. | DATE | DESCRIPTION |
|-----------|---------------|-------------|
| 0 | OKLA. 1-23-81 | |
| REVISIONS | | DATE |
| Revised | | 6-17-86 |

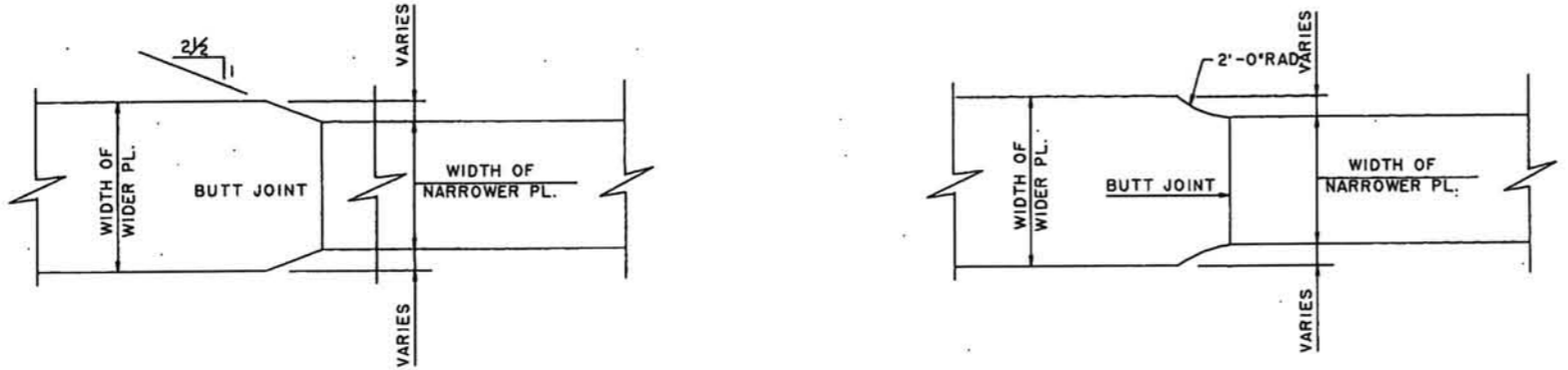


SEE CHANGE OF PLANS SHT. NO. 76A

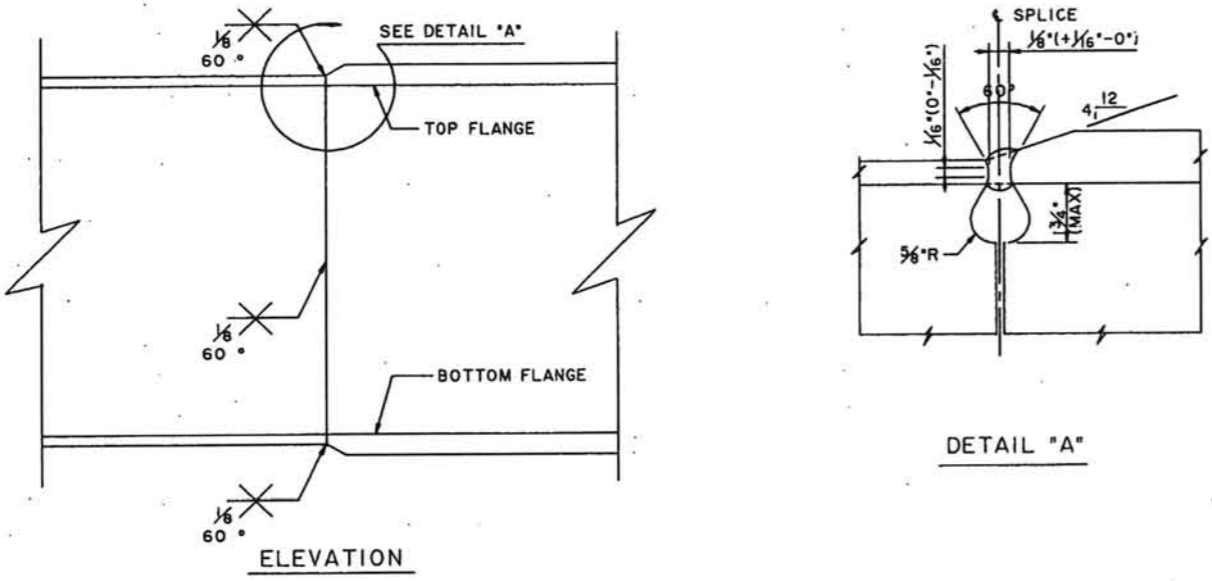
| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
 (SHT. 4 OF 14)
 F.A. Project No. I-235-1(020)000 Sheet No. 77

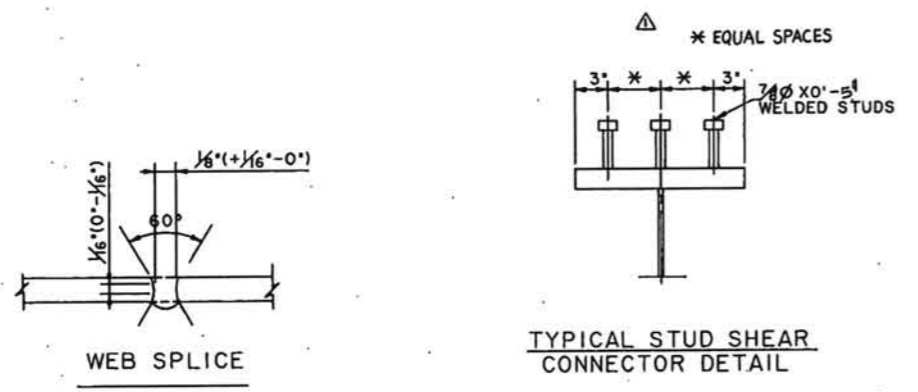
| | | | | | |
|-------------|-------|----------------|-------------|-----------|--------------|
| DES. NO. | STATE | F.A. PROJ. NO. | DESIGN YEAR | SHEET NO. | TOTAL SHEETS |
| 6 | OKLA. | I-433-E | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| △ REVISED | | G-17-86 | | | |



ALLTERNATE FLANGE WIDTH TRANSITIONS



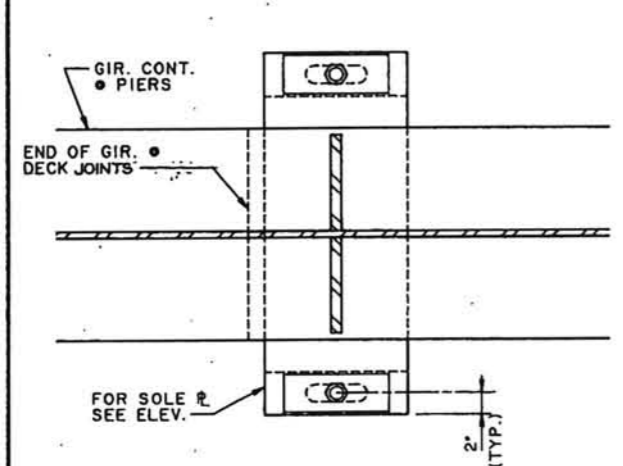
DETAILS OF WELDED FIELD SPLICE



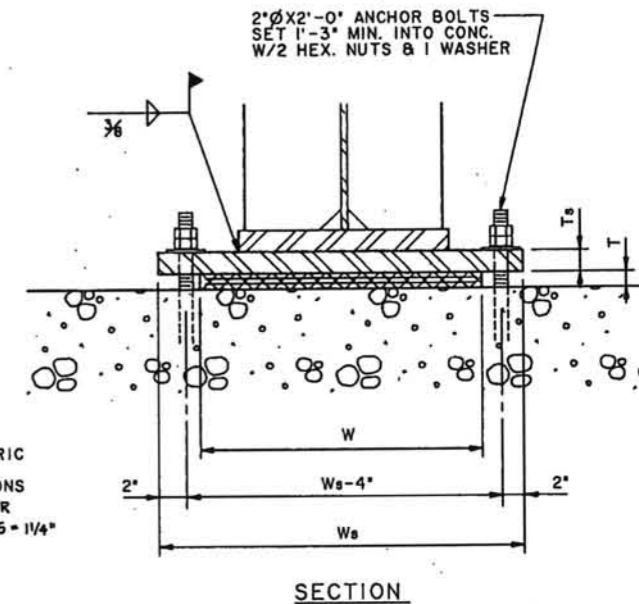
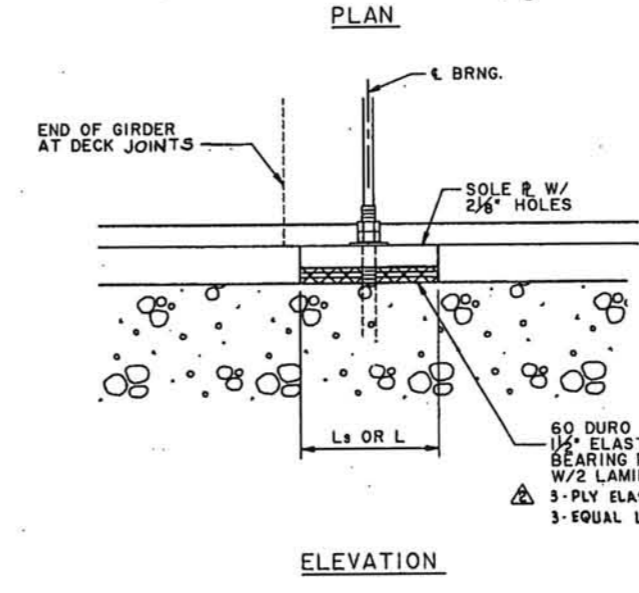
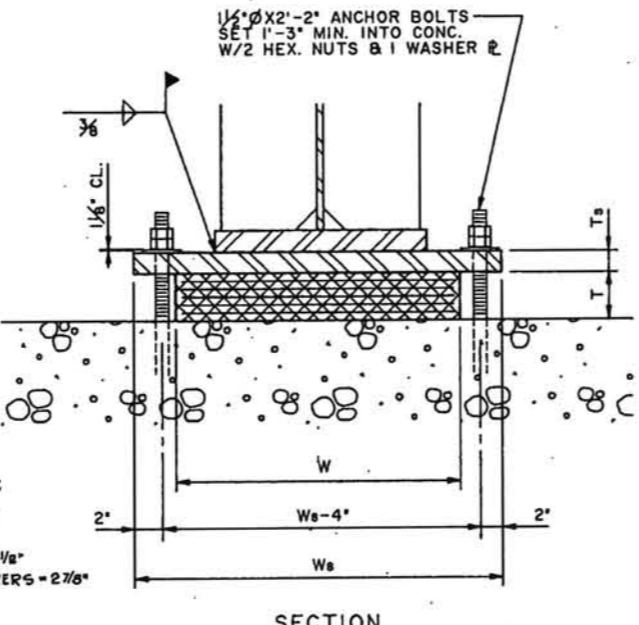
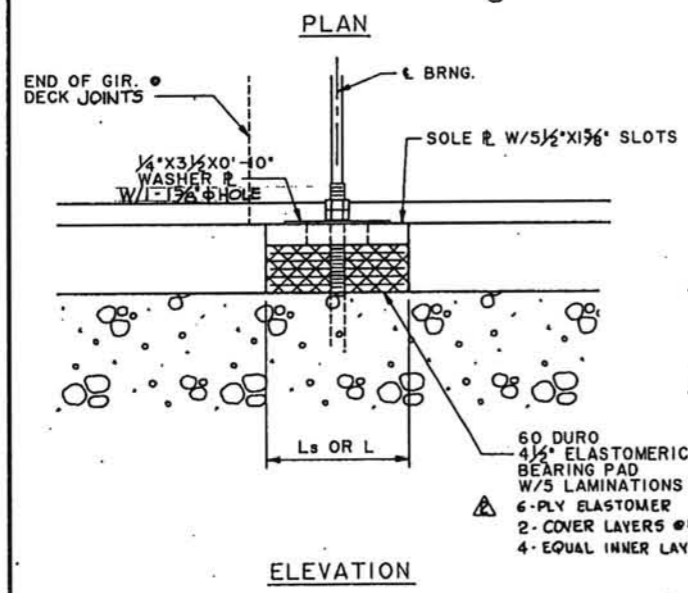
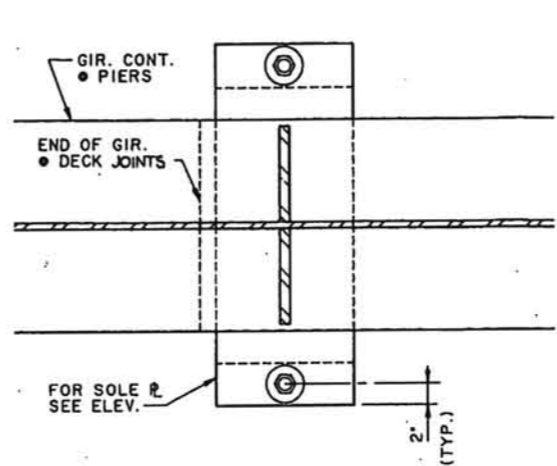
| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
(SHT. 5 OF 14)
F.A. Project No. I-235-(1020)000 Sheet No. 78

| GEN. ROAD DIST. NO. | STATE | F.A. PROJ. NUMBER | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|---------|-------------------|-------------|-----------|--------------|
| 6 | OKLA. | I-235-0200 | | 79 | |
| REVISIONS | | | | | |
| DESCRIPTION | DATE | | | | |
| REVISD | 6-17-86 | | | | |
| REVISD | 9-3-86 | | | | |



NOTES:
THE ANCHOR BOLTS AT EXPANSION BEARINGS SHALL CENTER THE SLOTS OF THE ANCHOR PLATE AT THE MEAN TEMPERATURE OF 60 DEGREES F. FOR EACH 100' OF EXPANSION TO THE BEARING ASSEMBLY. THE ANCHOR BOLT SHALL BE SET 1/8" FROM CENTERLINE SLOT IN THE DIRECTION OF THE EXPANSION FOR EACH 15 DEGREES THE TEMPERATURE IS ABOVE 60 DEGREES F. OR SHALL BE SET 1/8" FROM CENTERLINE SLOT IN THE DIRECTION OF THE CONTRACTION FOR EACH 15 DEGREES THE TEMPERATURE IS BELOW 60 DEGREES F.
SETTING OF EXPANSION BEARINGS AT ABUT. NO. 1 AT TEMPERATURES BELOW 40 DEGREES F. OR ABOVE 80 DEGREES F. WILL BE ALLOWED PROVIDED THAT THE CONTRACTOR JACKS THE GIRDERS AT THE ABUTMENTS AND RESETS THE BEARING WHEN THE TEMPERATURE FALLS BETWEEN 40 DEGREES F. AND 80 DEGREES F. IT IS INTENDED THAT THE BEARING PADS SHALL BE VERTICAL BETWEEN 40 DEGREES AND 80 DEGREES F. IF BEARING PADS ARE NOT VERTICAL WITHIN THIS TEMPERATURE RANGE THEN THEY SHALL BE RESET WITH ALL LOADS REMOVED FROM BEARING PADS. RESETTING OF THE EXPANSION BEARINGS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "RESET EXP. BEARINGS". THIS ITEM WILL BE DELETED IF RESETTING IS NOT REQUIRED. BEARING PAD AND SOLE PLATE IN EXPANSION BEARINGS SHALL BE BONDED TOGETHER IN SHOP. FOR ANCHOR BOLT DETAILS SEE SHEET 59.
THE MAXIMUM ALLOWABLE DISPLACEMENT OF SOLE PLATE FROM CENTERLINE OF BEARING STIFFENERS AT FIXED BRNGS. SHALL BE 2"



TYPICAL EXPANSION BEARING

TYPICAL FIXED BEARING

BEARING SCHEDULE

| GIRDER | ABUTMENT NO. 1 EXPANSION | | | | | PIER NO. 1 EXPANSION | | | | | PIER NO. 2 FIX | | | | | PIER NO. 3 SPAN NO. 3 EXPANSION | | | | | PIER NO. 3 SPAN NO. 4 EXPANSION | | | | | PIER NO. 4 FIX | | | | | | | | | | | | | | | | |
|--------------|--------------------------|-----|-------------|-----|-----|----------------------|----|-----|-------------|----|----------------|------------|--------|----|-------------|---------------------------------|----|------------|-----|--------|---------------------------------|-----|-----|------------|-----|----------------|-------------|---|-----|-----|----|-----|-----|--------|---|-----|-----|--------|-----|-----|--------|---|
| | SOLE PLATE | | BEARING PAD | | | SOLE PLATE | | | BEARING PAD | | | SOLE PLATE | | | BEARING PAD | | | SOLE PLATE | | | BEARING PAD | | | SOLE PLATE | | | BEARING PAD | | | | | | | | | | | | | | | |
| | Ws | Ls | Ts | W | L | T | Ws | Ls | Ts | W | L | T | Ws | Ls | Ts | W | L | T | Ws | Ls | Ts | W | L | T | Ws | Ls | Ts | W | L | T | Ws | Ls | Ts | W | L | T | | | | | | |
| *A & *H | 30' | 12' | 2' | 21' | 12' | 4 1/2" | 5 | 32' | 22' | 2' | 22' | 20' | 4 1/2" | 5 | 32' | 20' | 2' | 22' | 20' | 1 1/2" | 2 | 30' | 12' | 2' | 21' | 12' | 4 1/2" | 5 | 32' | 12' | 2' | 24' | 12' | 4 1/2" | 5 | 42' | 30' | 2 1/2" | 34' | 30' | 1 1/2" | 2 |
| *B* THRU *G* | 30' | 12' | 2' | 21' | 12' | 4 1/2" | 5 | 32' | 22' | 2' | 24' | 20' | 4 1/2" | 5 | 32' | 20' | 2' | 24' | 20' | 1 1/2" | 2 | 30' | 12' | 2' | 21' | 12' | 4 1/2" | 5 | 32' | 12' | 2' | 24' | 12' | 4 1/2" | 5 | 44' | 30' | 2 1/2" | 36' | 30' | 1 1/2" | 2 |

BEARING SCHEDULE

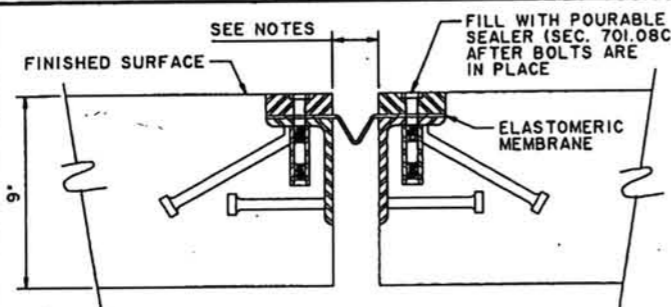
| GIRDER | PIER NO. 5 SPAN NO. 5 EXPANSION | | | | | PIER NO. 5 SPAN NO. 6 FIX | | | | | ABUTMENT NO. 2 EXPANSION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|----------------|---------------------------------|-----|-------------|-----|-----|---------------------------|----|-----|-------------|----|--------------------------|------------|--------|----|-------------|-----|----|-----|-----|--------|----|---|---|---|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
| | SOLE PLATE | | BEARING PAD | | | SOLE PLATE | | | BEARING PAD | | | SOLE PLATE | | | BEARING PAD | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Ws | Ls | Ts | W | L | T | Ws | Ls | Ts | W | L | T | Ws | Ls | Ts | W | L | T | Ws | Ls | Ts | W | L | T | | | | | | | | | | | | | | | | | | |
| A, B, C, D & E | 38' | 12' | 2' | 30' | 12' | 4 1/2" | 5 | 30' | 12' | 2' | 19' | 12' | 1 1/2" | 2 | 30' | 12' | 2' | 19' | 12' | 4 1/2" | 5 | | | | | | | | | | | | | | | | | | | | | |
| F & G | 43' | 12' | 2' | 35' | 12' | 4 1/2" | 5 | 30' | 12' | 2' | 19' | 12' | 1 1/2" | 2 | 30' | 12' | 2' | 19' | 12' | 4 1/2" | 5 | | | | | | | | | | | | | | | | | | | | | |
| H | 48' | 12' | 2' | 36' | 12' | 4 1/2" | 5 | 30' | 12' | 2' | 19' | 12' | 1 1/2" | 2 | 30' | 12' | 2' | 19' | 12' | 4 1/2" | 5 | | | | | | | | | | | | | | | | | | | | | |

NOTES:
1. ALL COSTS OF ELASTOMERIC BEARING PADS SHALL BE INCLUDED IN PRICE BID FOR OTHER ITEMS OF WORK.
2. LAMINATIONS SHALL CONFORM TO A-36.

SEE CHANGE OF PLANS SHT. NO. 76A

| | | | | | |
|----------|-----|--|--|--|--|
| Design | LRS | | | | |
| Drawn | LDM | | | | |
| Checked | LRS | | | | |
| Approved | | | | | |
| Squad | | | | | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
(SHEET 6 OF 14)
F.A. Project No. I-235-1(020)000 Sheet No. 79



TYPICAL SECTION THRU JOINT
(ELASTOMERIC SHEET TYPE STRIP SEAL)

STRIP SEAL EXPANSION JOINT NOTES

THE STRIP-SEAL EXPANSION JOINT SHALL HAVE A TOTAL MOVEMENT RANGE OF UP TO 4" AND SEAL THE DECK TO PREVENT MOISTURE OR OTHER CONTAMINANTS FROM DESCENDING ONTO THE LOWER STRUCTURE COMPONENTS. THE MAXIMUM EXPANSION LENGTH TO THE JOINT SHALL BE 400' FOR STEEL BEAM SPANS. OTHER STRIP-SEAL JOINTS MAY BE USED WHEN APPROVED BY THE BRIDGE ENGINEER. THE TYPE OF STRIP-SEAL SELECTED FOR THE BRIDGE MUST BE USED FOR ALL STRIP-SEAL JOINTS ON THAT BRIDGE, EXCEPT AS SHOWN ON DRAWINGS.

ELASTOMER: THE ELASTOMER SHEET SHALL BE 1/8" THICK 100% VIRGIN POLYISOPRENE OR 100% VIRGIN CHLOROPRENE REINFORCED WITH A NON-WICKING SYNTHETIC FABRIC MEETING THE PROPERTIES SPECIFIED IN SECTION 733.05, AND HAVING A DUROMETER HARDNESS FALLING WITHIN THE RANGE OF 55-80. THE WIDTH OF SHEET SHALL BE 10 1/2". THE SHEET SHALL BE COMPLETELY SEATED ON THE EMBEDDED ANGLES AND SHALL BE CONTINUOUS FOR THE FULL LENGTH OF THE JOINT INCLUDING PARAPET FACE. HOLES IN THE ELASTOMER SHEET SHALL BE SHOP PUNCHED 1/4" DIAMETER TO ACCEPT THE 1/4" X 3/32" WASHER. ALL LOCATIONS WHERE MATERIALS ARE SHOWN TO BE MITERED TO ACCOMMODATE SHARP SKEWS, THE MATERIAL SHALL BE SCARFED AND HEAT VULCANIZED IN AN APPROVED MANNER. SPECIAL SHAPE GLANDS MAY ALSO BE USED FOR SHARP SKEWS.

STRUCTURAL STEEL: ALL STRUCTURAL STEEL SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M-183 OR ASTM A-36. 1/2" X 2" BOLTS SHALL BE TYPE 304 STAINLESS STEEL.

PAINT: THE ENTIRE STEEL ASSEMBLY SHALL RECEIVE A SHOP COAT OF INORGANIC ZINC PRIMER.

FABRICATION OF JOINT: SHOP DRAWINGS SHOWING DIMENSIONS AND DETAILS OF THE COMPLETE EXPANSION JOINT SHALL BE SUBMITTED TO THE BRIDGE ENGINEER FOR APPROVAL. WELDED STUDS, OR THREADED INSERTS SHALL BE ELECTRIC ARC AND WELDED TO THE ANGLES WITH COMPLETE FUSION.

THE ALIGNMENT OF THE ASSEMBLED DEVICE AND THE PROTECTION REQUIRED FOR THE THREADED SLEEVE NUT TO INSURE THE RETAINING BOLTS WILL GO TO FULL DEPTH ARE CRITICAL TO THE INSTALLATION AND FUNCTION OF THE EXPANSION DEVICE. THE CONTRACTOR AND HIS FABRICATOR WILL BE REQUIRED TO TAKE ALL STEPS NECESSARY TO ASSURE THE SATISFACTORY INSTALLATION. COMPLETE PRE-ASSEMBLY, TEMPORARY SEALS FOR SLEEVE NUTS, AND OTHER STEPS MAY BE REQUIRED. THE SHOP DRAWINGS SHALL ADDRESS THE PROPOSED METHOD OF PROTECTION AND ASSEMBLY.

ALL THREADED SLEEVE NUTS SHALL BE BLOWN CLEAN WITH AN "AIR-NEEDLE" BEFORE THE STAINLESS STEEL BOLTS ARE INSERTED.

INSTALLATION OF JOINT: ELASTOMERIC SEAL AND CONTACTING STEEL SURFACES SHALL BE CLEANED PRIOR TO ASSEMBLY. THE JOINT SHALL BE SET TO ITS PROPER LINE, GRADE, AND OPENING AND SHALL BE ADEQUATELY BRACED IN A MANNER APPROVED BY THE ENGINEER. THE JOINT SHALL BE SET WITH AN OPENING OF 2 1/4" MEASURED PARALLEL TO THE ROADWAY BASED ON A TEMPERATURE OF 60° FAHRENHEIT. FOR EACH 100' OF EXPANSION TO THE JOINT TO BE PROVIDED FOR, THE SETTING OF THE OPENING SHALL BE INCREASED 1/4" FOR EACH 15° FAHRENHEIT THE TEMPERATURE IS BELOW 60° FAHRENHEIT AND DECREASED 1/4" FOR EACH 15° FAHRENHEIT THE TEMPERATURE IS ABOVE 60° FAHRENHEIT.

THE PARAPET OPENING AT THE JOINT SHALL BE THE SAME AS THE JOINT OPENING.

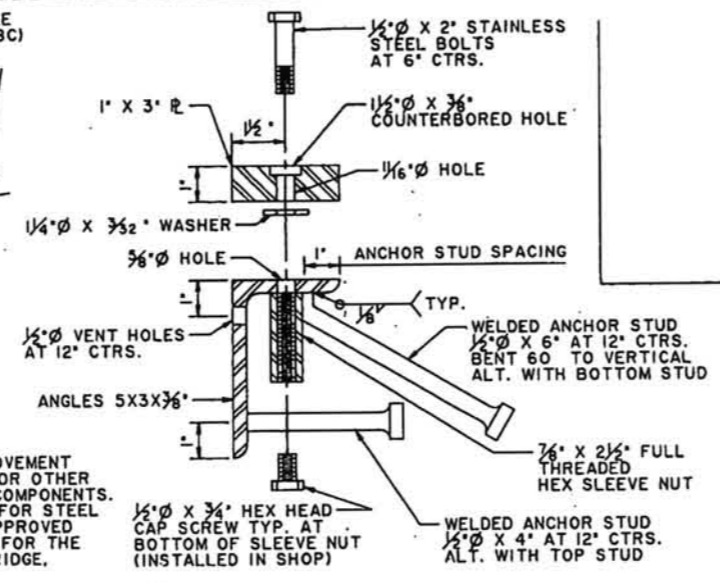
BOLTS SHALL BE TIGHTENED TO A MINIMUM TORQUE OF 100 FOOT-POUNDS. AFTER SEVERAL BOLTS HAVE BEEN TIGHTENED, BOLTS PREVIOUSLY TIGHTENED SHALL BE CHECKED AGAIN UNTIL ALL ARE TIGHTENED TO THE PRESCRIBED AMOUNT. ALL BOLTS WILL AGAIN BE TIGHTENED TO 100 FOOT-POUNDS 24 HOURS AFTER ORIGINAL TIGHTENING.

METHOD OF MEASUREMENT & BASIS OF PAYMENT: THE STRIP-SEAL TYPE EXPANSION JOINT SHALL BE MEASURED BY THE LINEAR FOOT ALONG THE CENTERLINE OF JOINT FROM END TO END OF JOINT AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID FOR:

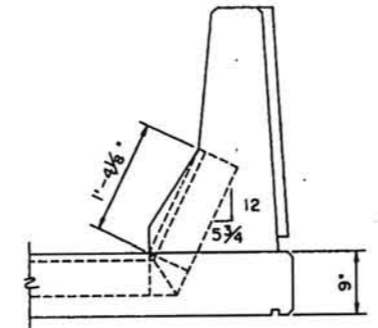
SP. STRIP SEAL EXP. JOINT W/ANGLES LIN. FT.

THE PRICE BID SHALL BE FULL COMPENSATION FOR FURNISHING ALL MATERIALS IN THE COMPLETED JOINT INCLUDING ELASTOMER SHEET, STRUCTURAL STEEL, ANCHORS, WELDING, EQUIPMENT, LABOR, AND INCIDENTALS NECESSARY TO INSTALL THE JOINT COMPLETE IN PLACE.

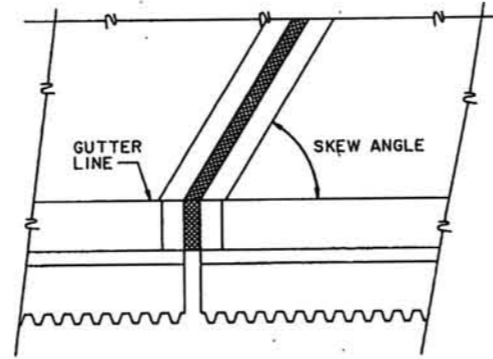
STRIP SEAL EXPANSION JOINT



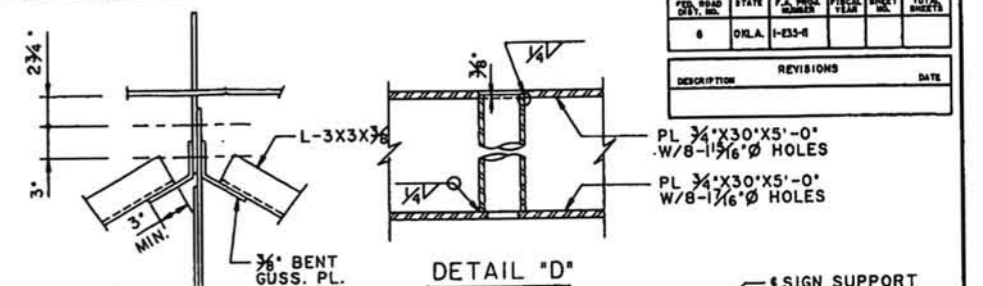
DETAIL OF JOINT STRUCTURAL STEEL



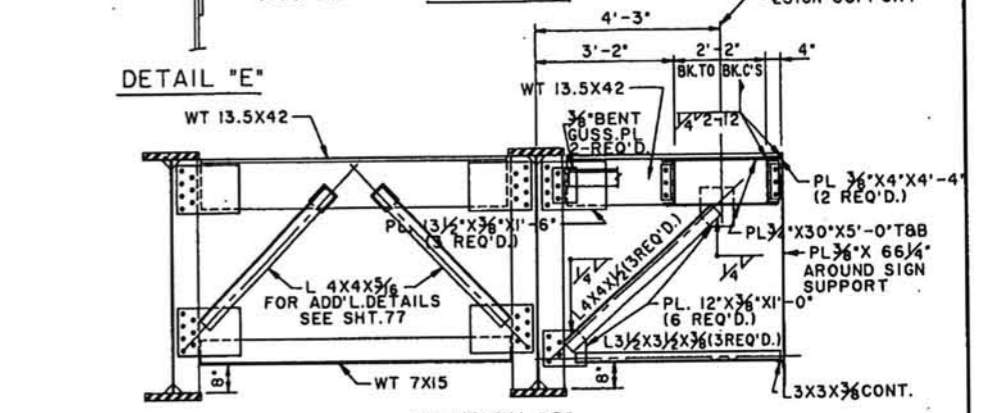
EXP. JT. DETAIL AT PARAPET



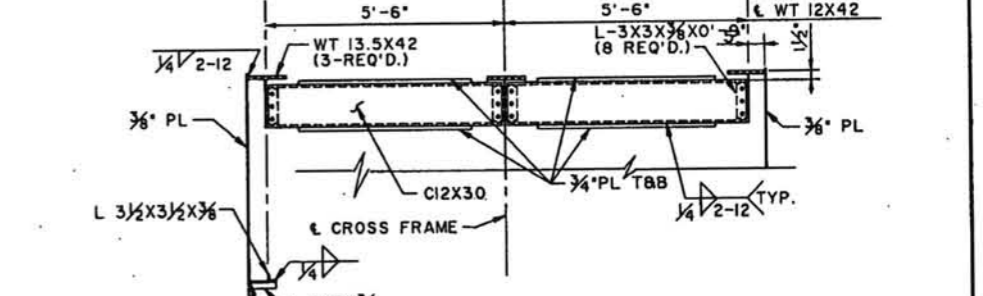
PLAN DETAIL AT GUTTER



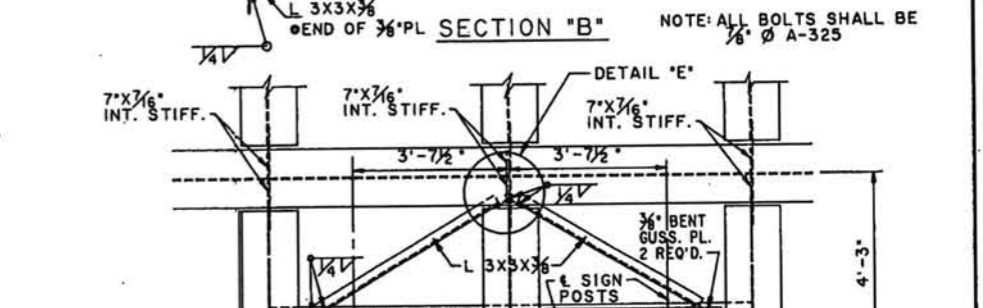
DETAIL "D"



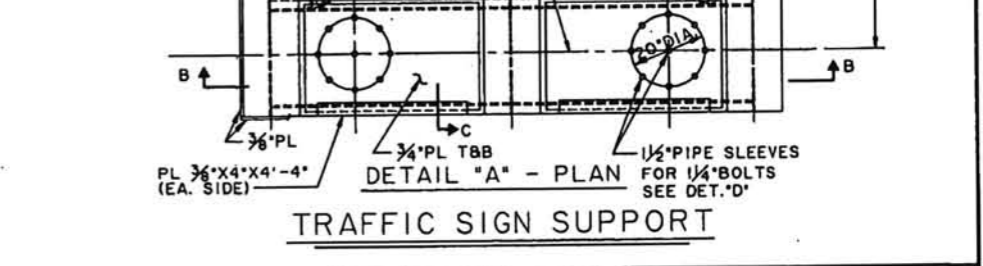
DETAIL "E"



SECTION "C"



SECTION "B"



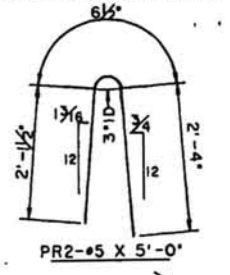
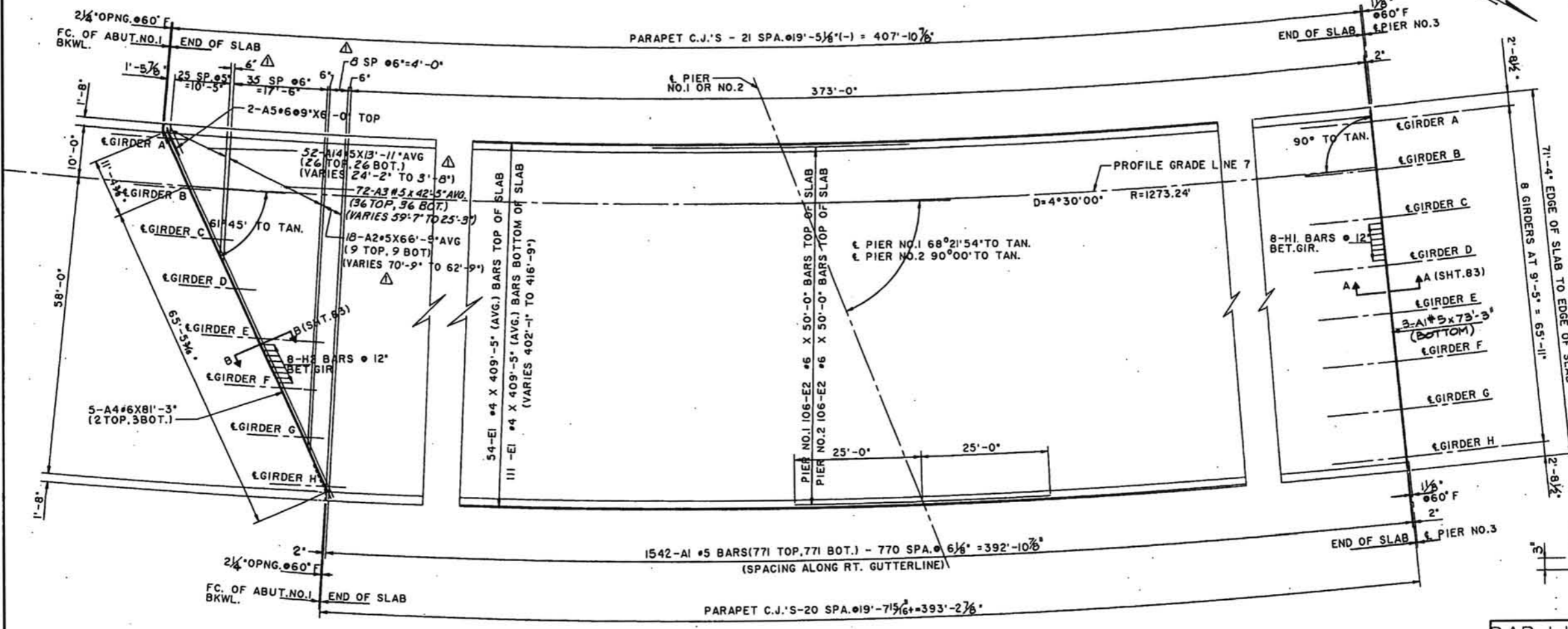
TRAFFIC SIGN SUPPORT

| | |
|----------|-----|
| Design | LRS |
| Drawn | PTK |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE E-6
DETAILS OF SUPERSTRUCTURE
(SHT. 7 OF 14)

F.A. Project No. J-235-1(020)000 Sheet No. 80

| DESIGN YEAR | STATE | F.A. PROJECT NO. | SCALE | DATE | TOTAL SHEETS |
|-------------|--------|------------------|-------|------|--------------|
| 8 | D.K.A. | I-235-4 | | | |
| REVISIONS | | DATE | | | |
| Revised | | 4-17-86 | | | |



GENERAL NOTES FOR DECK CONSTRUCTION

ALL CONCRETE IN THE SUPERSTRUCTURE SHALL BE CLASS AA CONCRETE.
 ALL REINFORCING STEEL SHALL BE EPOXY COATED AND SHALL CONFORM TO ASTM A-615, GRADE 60.
 ALL EXPOSED EDGES, UNLESS SHOWN OR NOTED OTHERWISE, SHALL HAVE A 3/4\"/>

PLAN QUANTITIES FOR CLASS AA CONCRETE INCLUDE 7% C.Y. FOR HAUNCHES. THE HAUNCH HEIGHTS WILL BE SET AFTER ERECTION OF STRUCTURAL STEEL TO PROVIDE FOR DEAD LOAD DEFLECTION AND GRADE ADJUSTMENT BUT THE PAY QUANTITIES FOR HAUNCHES WILL BE AS SHOWN ABOVE.
 AFTER ALL STRUCTURAL STEEL HAS BEEN ERECTED THE ENGINEER WILL FURNISH THE STATE BRIDGE ENGINEER WITH THE PROFILE OF THE TOPS OF THE GIRDERS WITH THE ELEVATIONS TAKEN AT THE PANEL POINTS. THE BRIDGE ENGINEER WILL DETERMINE THE THICKENING OF THE HAUNCHES THAT WILL BE NECESSARY TO PROVIDE FOR DEAD LOAD DEFLECTION AND FURNISH THIS INFORMATION TO THE ENGINEER.
 WHEN VIBRATING CONCRETE CONTAINING EPOXY COATED REINF. STEEL THE VIBRATOR SHALL BE EQUIPPED WITH A SHEATH DESIGNED TO PREVENT DAMAGE TO THE EPOXY COATING.

SLAB REINFORCEMENT PLAN
(SPANS NO.1,2 & 3)

FORMWORK BRACING FOR EXTERIOR GIRDERS

THE EXTERIOR GIRDER SHALL BE SECURELY BRACED TO ELIMINATE TWISTING DUE TO THE WEIGHT OF THE OVERHANGING SLAB AND THE FINISHING MACHINE. BRACING SHALL CONSIST OF A TENSION MEMBER FROM THE TOP FLANGE OF THE EXTERIOR GIRDER TO THE TOP FLANGE OF THE ADJACENT GIRDER, AND A COMPRESSION MEMBER FROM THE BOTTOM FLANGE OF THE EXTERIOR GIRDER TO THE TOP FLANGE OF THE ADJACENT GIRDER. BRACING SHALL BE PERPENDICULAR TO THE GIRDERS. BRACING SHALL BE SPACED TO PROVIDE THE REQUIRED RIGIDITY, BUT IN NO CASE SHALL THE SPACING EXCEED 8'-0\"/>

| QUANTITIES | | |
|---|------|------------|
| ITEM | UNIT | AMOUNT |
| CLASS AA CONCRETE | C.Y. | 1,973.9 |
| CONCRETE PARAPET | L.F. | 1893.4 |
| EPOXY COATED REINF. STEEL (GR.60) | LBS. | 436,760. |
| STRUCTURAL STEEL A-36 | LBS. | 693,320. |
| STRUCTURAL STEEL A-588 | LBS. | 2,861,310. |
| PENETRATING WATER REPELLENT SURFACE TREATMENT | S.Y. | 7917.8 |
| STRIP SEAL TYPE EXPANSION JOINT W/ANGLES | L.F. | 343.2 |
| RESET EXPANSION BEARINGS | EA. | 8 |

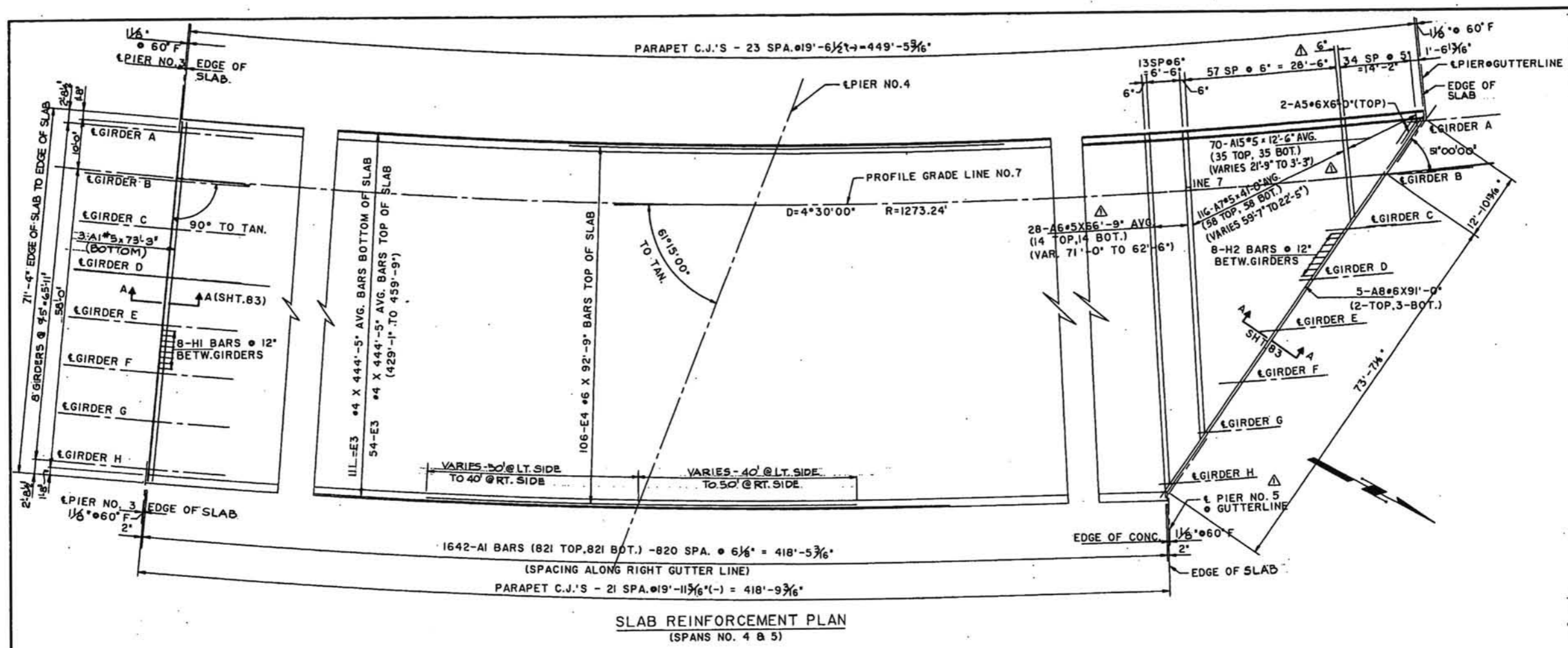
| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BAR LIST-SPANS 1,2&3

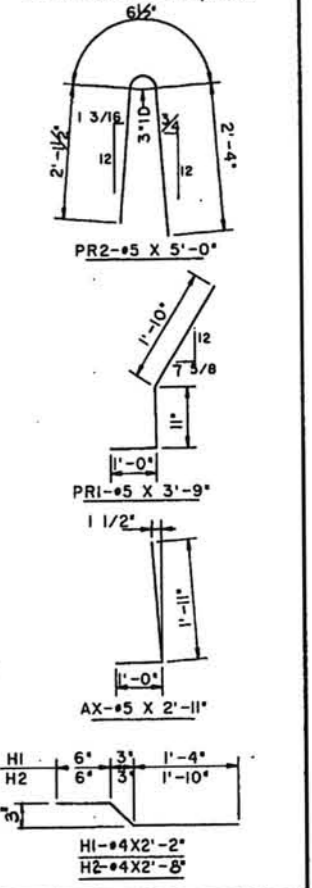
| MARK | SIZE | FORM NO. | LENGTH |
|-------------------|------|-----------|--------------|
| EPOXY COATED BARS | | | |
| ① A1 | #5 | STR. 1545 | 73'-3" |
| ① A2 | #5 | STR. 18 | 66'-9" AVG. |
| A3 | #5 | STR. 72 | 42'-5" AVG. |
| ② A4 | #6 | STR. 5 | 81'-3" |
| A5 | #6 | STR. 2 | 6'-0" |
| ③ E1 | #4 | STR. 165 | 409'-5" AVG. |
| E2 | #6 | STR. 212 | 50'-0" |
| PR1 | #5 | BNT. 840 | 3'-9" |
| PR2 | #5 | BNT. 840 | 5'-0" |
| AX | #5 | BNT. 840 | 2'-11" |
| A14 | #5 | STR. 52 | 13'-11" AVG. |
| PH1 | #4 | STR. 126 | 19'-1" |
| PH2 | #4 | STR. 120 | 19'-4" |
| H1 | #4 | BNT. 56 | 2'-2" |
| H2 | #4 | BNT. 56 | 2'-8" |

① INCLUDES ONE 27\"/>

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
 (SHT. 8 OF 14)
 F.A. Project No. I-235-1(020)000, Sheet No. 81



| REV. NO. | DATE | DESCRIPTION | REVISIONS | DATE |
|----------|------|----------------------|-----------|------|
| 0 | | | | |
| | | REVISIONS | | |
| | | BAR BENDING DIAGRAMS | | |
| | | REVISED G-17-86 | | |



| BAR LIST-SPANS 4 & 5 | | | | |
|----------------------|------|-----------|--------------|--|
| MARK | SIZE | FORM NO. | LENGTH | |
| EPOXY COATED BARS | | | | |
| ① A1 | #5 | STR. 1645 | 73'-3" | |
| A5 | #6 | STR. 2 | 6'-0" | |
| ① A6 | #5 | STR. 28 | 66'-9" AVG. | |
| A7 | #5 | STR. 116 | 41'-0" AVG. | |
| ② A8 | #6 | STR. 5 | 91'-0" | |
| ③ E3 | #4 | STR. 165 | 444'-5" AVG. | |
| ② E4 | #4 | STR. 106 | 92'-9" | |
| A15 | #5 | STR. 70 | 12'-6" AVG. | |
| AX | #5 | BENT 901 | 2'-11" | |
| PRI | #5 | BENT 901 | 3'-9" | |
| PR2 | #5 | BENT 901 | 5'-0" | |
| PH3 | #4 | STR. 138 | 19'-2" | |
| PH4 | #4 | STR. 126 | 19'-7" | |
| HI | #4 | BENT 56 | 2'-2" | |
| H2 | #4 | BENT 56 | 2'-8" | |

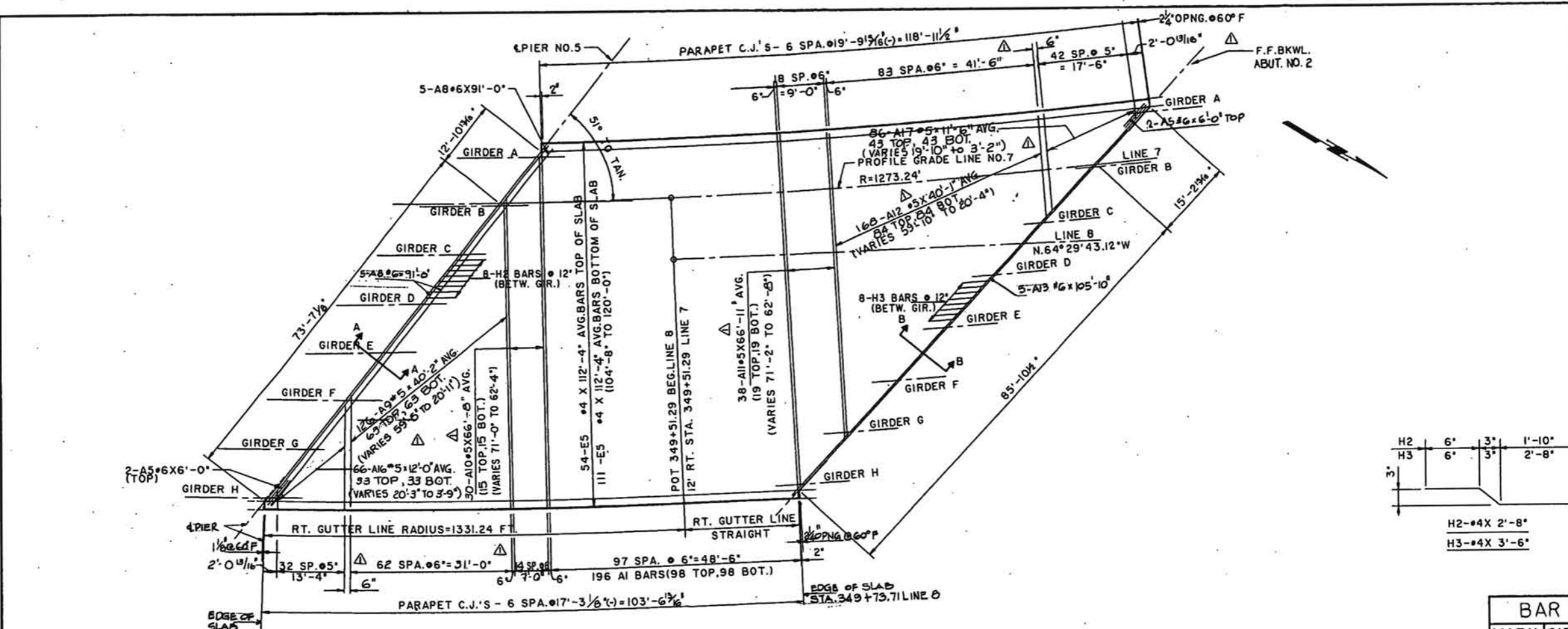
① INCLUDES ONE 27' LAP AT SPLICE
② INCLUDES ONE 33' LAP AT SPLICE
③ INCLUDES 7-18" LAPS AT SPLICES
INCLUDED IN PRICE BID PER L.F. OF PARAPET
NOTE: SPLICES TO BE STAGGERED TO AVOID MORE THAN 50% AT ONE LOCATION

SEE CHANGE OF PLANS SHT. NO. 76A

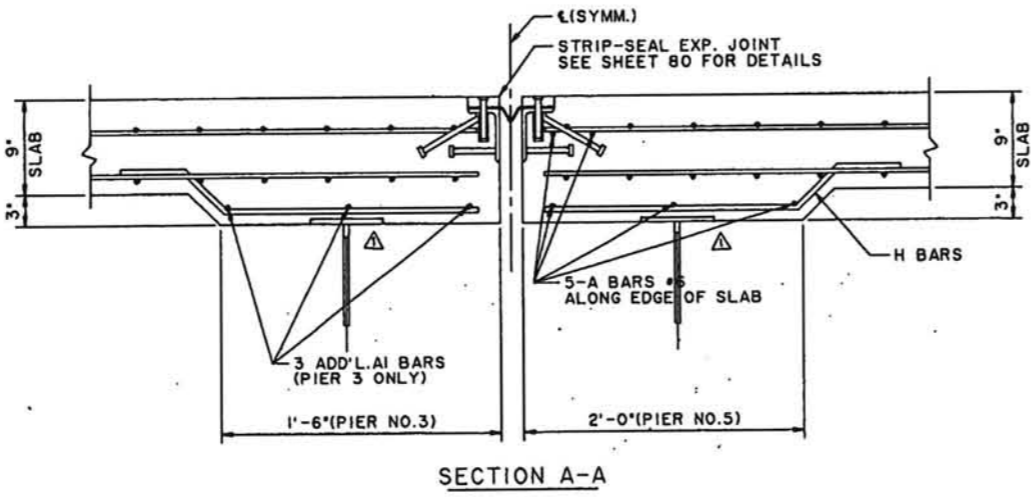
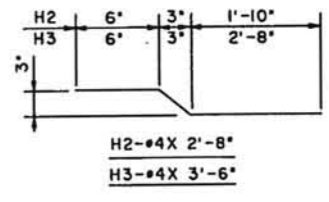
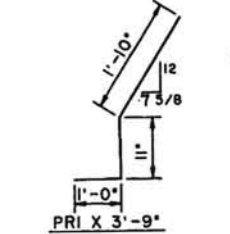
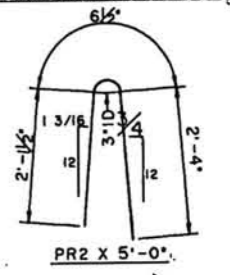
| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
(SHT. 9 OF 14)
F.A. Project No. I-235-(1020)000 Sheet No. 82

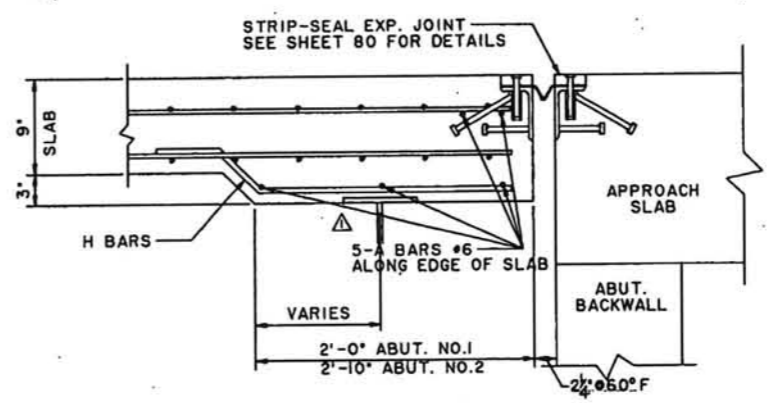
| REV. NO. | DATE | DESCRIPTION |
|----------|---------|------------------------------|
| 1 | 6-17-86 | REVISED BAR BENDING DIAGRAMS |



SLAB REINFORCEMENT PLAN
(SPAN NO. 6)
SEE SHEET NO. 81 FOR NOTES



SECTION A-A



SECTION B-B

BAR LIST-SPAN 6

| MARK | SIZE | FORM NO. | LENGTH |
|-------------------|------|----------|--------------|
| EPOXY COATED BARS | | | |
| ① A1 | #5 | STR. 196 | 73'-3" |
| A5 | #6 | STR. 4 | 6'-0" |
| ② A8 | #6 | STR. 5 | 91'-0" |
| A9 | #5 | STR. 126 | 40'-2" AVG. |
| ① A10 | #5 | STR. 30 | 66'-8" AVG. |
| ① A11 | #5 | STR. 38 | 66'-10" AVG. |
| ① A12 | #5 | STR. 168 | 40'-1" AVG. |
| ② A13 | #6 | STR. 5 | 105'-10" |
| ① A16 | #5 | STR. 66 | 12'-0" AVG. |
| ③ E5 | #4 | STR. 163 | 112'-4" AVG. |
| A17 | #5 | STR. 86 | 11'-7" AVG. |
| PRI | #5 | BENT 234 | 3'-9" |
| PR2 | #5 | BENT 234 | 5'-0" |
| AX | #5 | BENT 234 | 2'-11" |
| PH5 | #4 | STR. 36 | 16'-11" |
| PH6 | #4 | STR. 36 | 19'-6" |
| H2 | #4 | BENT 56 | 2'-8" |
| H3 | #4 | BENT 56 | 3'-6" |

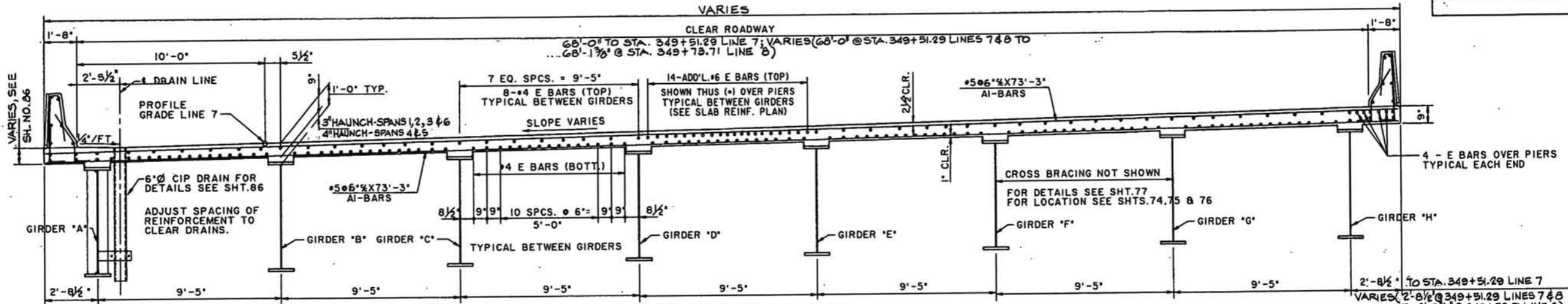
① INCLUDES ONE 27" LAP AT SPLICE
 ② INCLUDES ONE 33" LAP AT SPLICE
 ③ INCLUDES ONE 18" LAP AT SPLICE
 NOTE: SPLICES TO BE STAGGERED TO AVOID MORE THAN 50% AT ONE LOCATION.

| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

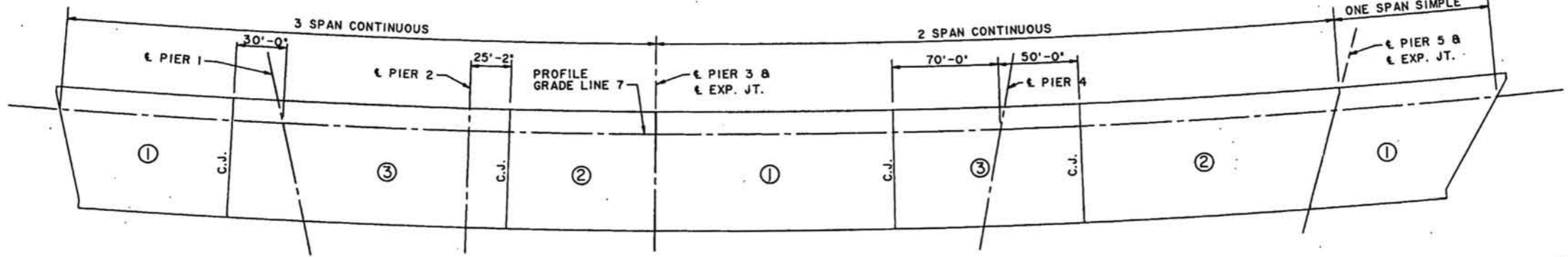
BRIDGE E-G
 DETAILS OF SUPERSTRUCTURE
 (SHT. 10 OF 14)
 F.A. Project No. I-235-1(020)000 Sheet No. 83

| DESIGN | REVISIONS | DATE |
|--------|-----------|------|
| | | |

FOR GENERAL NOTES FOR DECK
CONSTRUCTION SEE SHT. NO.81

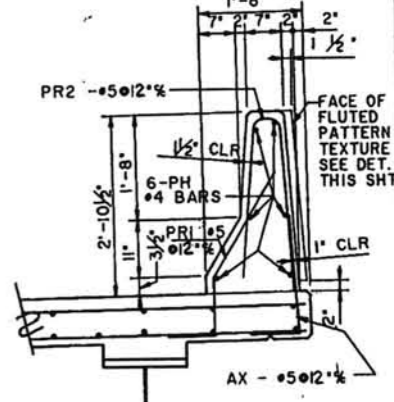


SECTION THROUGH DECK
SCALE: 3/8"=1'-0"



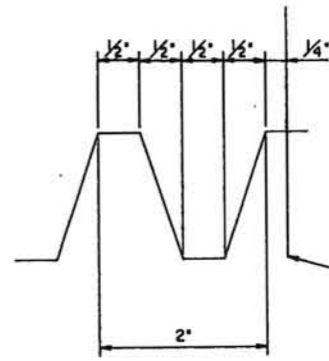
POURING SEQUENCE DIAGRAM
NO SCALE

NOTES: FOR CONSTRUCTION JOINT DETAILS
AND NOTES SEE SHT.86
FOR POURING SEQUENCE NOTES SEE SHT.53



SECTION THROUGH PARAPET
SCALE: 3/8"=1'-0"

NOTE: ALL PARAPET DIMENSIONS ARE
PARALLEL OR PERPENDICULAR TO SLOPE
OF BRIDGE DECK.



FOR PARAPET CONSTR.
JT. DETAILS SEE SHT.86

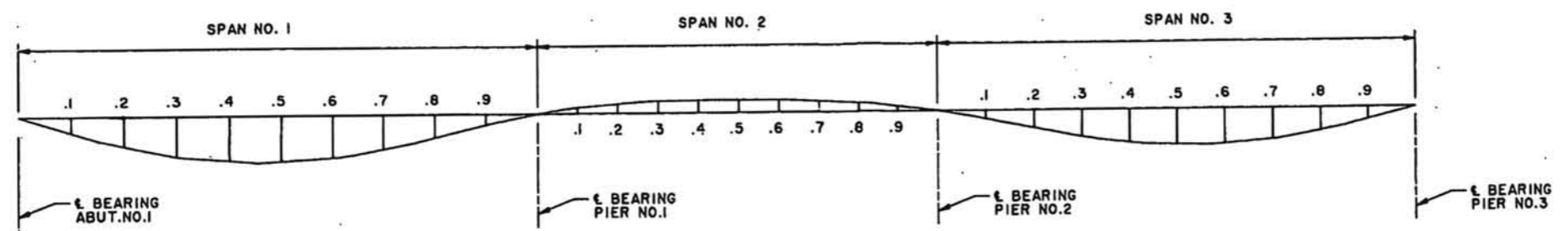
FLUTED PATTERN TEXTURE
AT EXT. FACE OF PARAPET

| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
(SHT. 11 OF 14)

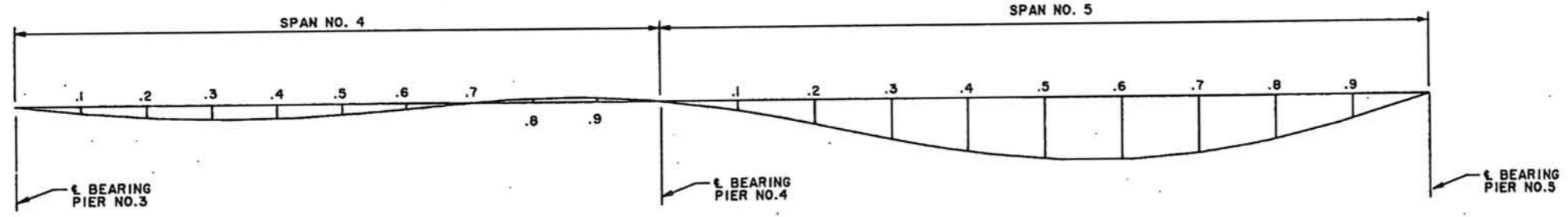
F.A. Project No. I-235-1(020)000 Sheet No. 84

| | | | | | |
|-------------|------|-----------|-------|--------|------|
| NO. | DATE | BY | CHKD. | APPD. | DATE |
| 1 | OKLA | 1-23-86 | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| REVISED | | | | 9-3-86 | |

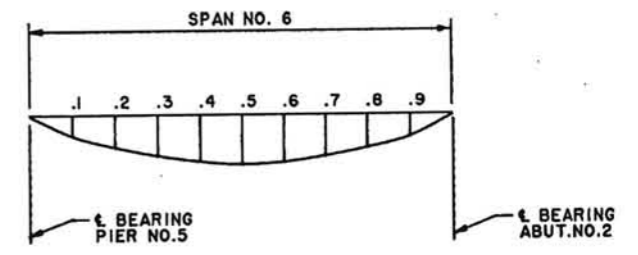


DEAD LOAD DEFLECTION DIAGRAM
 CONCRETE ALONE = 75% FOR EXT. GIRDER
 CONCRETE ALONE = 80% FOR INTERIOR GIRDER

| GIRDER | SPAN #1 | | | | | | | | | SPAN #2 | | | | | | | | | SPAN #3 | | | | | | | | | | | | |
|--------|---------|------|------|------|------|------|------|------|------|---------|---|-------|-------|-------|-------|-------|-------|-------|---------|-------|------|------|------|------|------|------|------|------|------|------|---|
| | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 |
| A | 0 | 1.83 | 3.33 | 4.32 | 4.93 | 4.94 | 4.38 | 3.42 | 2.17 | 0.98 | 0 | -0.46 | -0.59 | -0.56 | -0.46 | -0.36 | -0.37 | -0.42 | -0.34 | 0 | 0.62 | 1.40 | 2.21 | 2.93 | 3.10 | 3.12 | 2.77 | 2.05 | 1.10 | 0 | |
| B | 0 | 1.85 | 3.45 | 4.48 | 5.03 | 5.15 | 4.51 | 3.55 | 2.31 | 1.03 | 0 | -0.47 | -0.69 | -0.67 | -0.50 | -0.50 | -0.46 | -0.50 | -0.52 | -0.40 | 0 | 0.70 | 1.59 | 2.48 | 3.16 | 3.45 | 3.47 | 3.09 | 2.28 | 1.22 | 0 |
| C | 0 | 1.88 | 3.46 | 4.69 | 5.15 | 5.21 | 4.76 | 3.68 | 2.38 | 1.05 | 0 | -0.51 | -0.76 | -0.78 | -0.72 | -0.65 | -0.63 | -0.63 | -0.64 | -0.48 | 0 | 0.78 | 1.75 | 2.73 | 3.46 | 3.77 | 3.79 | 3.37 | 2.48 | 1.33 | 0 |
| D | 0 | 1.93 | 3.59 | 4.74 | 5.35 | 5.30 | 4.86 | 3.81 | 2.45 | 1.10 | 0 | -0.55 | -0.84 | -0.91 | -0.89 | -0.83 | -0.81 | -0.81 | -0.75 | -0.52 | 0 | 0.86 | 1.90 | 2.95 | 3.75 | 4.05 | 4.06 | 3.60 | 2.64 | 1.42 | 0 |
| E | 0 | 2.00 | 3.64 | 4.75 | 5.51 | 5.41 | 4.93 | 3.91 | 2.51 | 1.14 | 0 | -0.60 | -0.91 | -1.05 | -1.06 | -1.03 | -1.01 | -0.98 | -0.87 | -0.57 | 0 | 0.94 | 2.05 | 3.16 | 3.97 | 4.30 | 4.31 | 3.81 | 2.80 | 1.50 | 0 |
| F | 0 | 2.00 | 3.71 | 4.80 | 5.48 | 5.58 | 5.00 | 3.98 | 2.61 | 1.20 | 0 | -0.64 | -0.98 | -1.20 | -1.25 | -1.26 | -1.23 | -1.17 | -0.99 | -0.63 | 0 | 1.02 | 2.19 | 3.36 | 4.20 | 4.54 | 4.53 | 4.01 | 2.94 | 1.58 | 0 |
| G | 0 | 2.00 | 3.72 | 4.91 | 5.52 | 5.69 | 5.09 | 4.07 | 2.74 | 1.27 | 0 | -0.67 | -1.05 | -1.37 | -1.45 | -1.49 | -1.46 | -1.35 | -1.12 | -0.69 | 0 | 1.10 | 2.35 | 3.58 | 4.43 | 4.75 | 4.76 | 4.20 | 3.08 | 1.66 | 0 |
| H | 0 | 2.03 | 3.76 | 5.20 | 5.70 | 5.78 | 5.36 | 4.25 | 2.85 | 1.33 | 0 | -0.74 | -1.21 | -1.58 | -1.73 | -1.77 | -1.72 | -1.57 | -1.26 | -0.76 | 0 | 1.18 | 2.49 | 3.77 | 4.67 | 5.03 | 5.01 | 4.41 | 3.22 | 1.73 | 0 |



DEAD LOAD DEFLECTION DIAGRAM
 CONCRETE ALONE = 75% FOR EXT. GIRDER
 CONCRETE ALONE = 80% FOR INTERIOR GIRDER



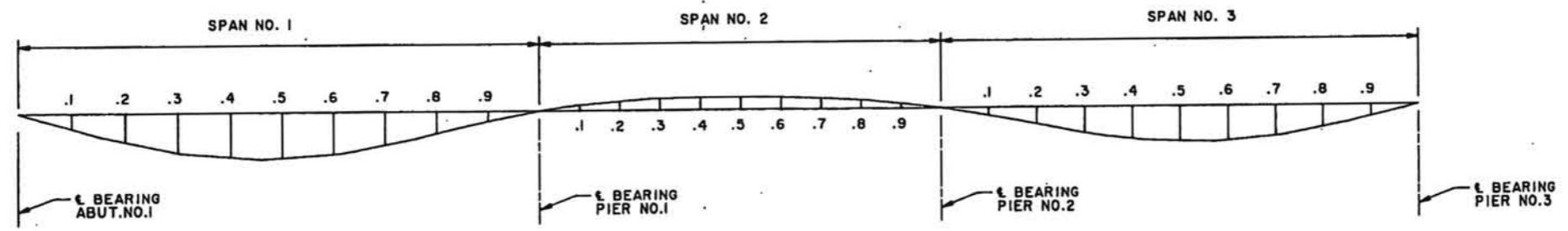
DEAD LOAD DEFLECTION DIAGRAM
 CONCRETE ALONE = 75% FOR EXT. GIRDER
 CONCRETE ALONE = 80% FOR INTERIOR GIRDER

| GIRDER | SPAN #4 | | | | | | | | | SPAN #5 | | | | | | | | | SPAN #6 | | | | | | | | | | | | | |
|--------|---------|------|------|------|------|------|------|-------|-------|---------|---|------|------|------|------|-------|-------|-------|---------|------|---|----|------|------|------|------|------|------|------|------|------|---|
| | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | |
| A | 0 | 2.29 | 4.11 | 5.11 | 5.39 | 4.92 | 3.86 | 2.38 | 0.93 | -0.06 | 0 | 0.87 | 2.54 | 4.33 | 6.24 | 7.28 | 7.58 | 6.90 | 5.25 | 2.87 | 0 | 0 | 0.84 | 1.61 | 2.10 | 2.43 | 2.51 | 2.43 | 2.10 | 1.61 | 0.84 | 0 |
| B | 0 | 2.17 | 3.90 | 4.83 | 5.07 | 4.60 | 3.56 | 2.13 | 0.74 | -0.04 | 0 | 1.01 | 2.81 | 4.90 | 6.64 | 7.70 | 7.89 | 7.13 | 5.49 | 2.97 | 0 | 0 | 0.77 | 1.49 | 1.89 | 2.16 | 2.23 | 2.16 | 1.89 | 1.43 | 0.77 | 0 |
| C | 0 | 2.05 | 3.65 | 4.53 | 4.73 | 4.24 | 3.22 | 1.84 | 0.54 | -0.14 | 0 | 1.13 | 3.05 | 5.26 | 7.11 | 8.19 | 8.32 | 7.47 | 5.67 | 3.08 | 0 | 0 | 0.67 | 1.28 | 1.72 | 1.95 | 2.09 | 1.95 | 1.72 | 1.28 | 0.67 | 0 |
| D | 0 | 1.90 | 3.37 | 4.19 | 4.35 | 3.86 | 2.85 | 1.54 | 0.36 | -0.20 | 0 | 1.24 | 3.30 | 5.62 | 7.53 | 8.64 | 8.64 | 7.90 | 5.97 | 3.23 | 0 | 0 | 0.62 | 1.22 | 1.59 | 1.86 | 1.90 | 1.68 | 1.59 | 1.22 | 0.62 | 0 |
| E | 0 | 1.74 | 3.07 | 3.80 | 3.90 | 3.41 | 2.40 | 1.21 | 0.13 | -0.32 | 0 | 1.37 | 3.57 | 6.00 | 7.97 | 9.09 | 9.24 | 8.31 | 6.35 | 3.42 | 0 | 0 | 0.61 | 1.14 | 1.53 | 1.71 | 1.84 | 1.71 | 1.53 | 1.14 | 0.61 | 0 |
| F | 0 | 1.97 | 2.75 | 3.38 | 3.41 | 2.90 | 2.00 | 0.85 | -0.13 | -0.46 | 0 | 1.52 | 3.83 | 6.41 | 8.48 | 9.67 | 9.79 | 8.76 | 6.68 | 3.59 | 0 | 0 | 0.56 | 1.07 | 1.41 | 1.69 | 1.70 | 1.69 | 1.41 | 1.07 | 0.56 | 0 |
| G | 0 | 1.40 | 2.43 | 2.94 | 2.91 | 2.38 | 1.53 | 0.46 | -0.37 | -0.57 | 0 | 1.68 | 4.12 | 6.84 | 9.12 | 10.40 | 10.52 | 9.40 | 7.09 | 3.82 | 0 | 0 | 0.58 | 1.08 | 1.43 | 1.69 | 1.73 | 1.69 | 1.43 | 1.08 | 0.58 | 0 |
| H | 0 | 1.28 | 2.13 | 2.53 | 2.43 | 1.87 | 1.03 | -0.04 | -0.64 | -0.70 | 0 | 1.76 | 4.38 | 7.23 | 9.62 | 11.04 | 11.31 | 10.25 | 7.81 | 4.23 | 0 | 0 | 0.56 | 1.09 | 1.45 | 1.71 | 1.75 | 1.71 | 1.45 | 1.09 | 0.56 | 0 |

| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

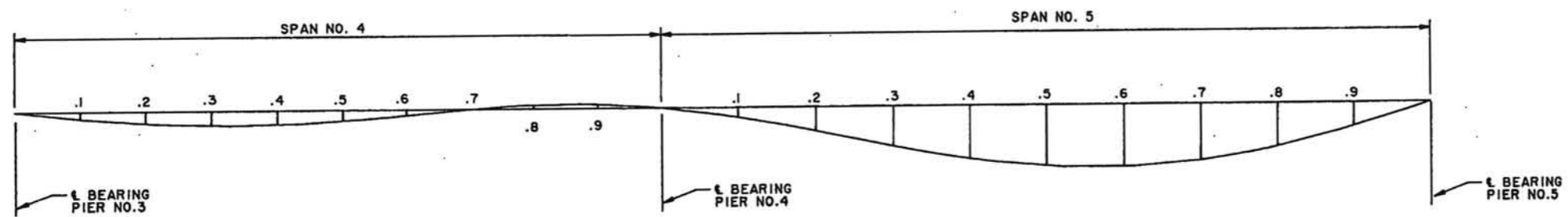
BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
 (SHT. 12 OF 13)
 F.A. Project No. I-235-(1020)000 Sheet No. 85

| | | | | | | |
|--|-------|------------|---------|-------|--------------|----------|
| CITY | STATE | F.A. DIST. | PROJECT | SHEET | TOTAL SHEETS | |
| 0 | OKLA. | I-435-E | | | | |
| DESCRIPTION | | | | | REVISIONS | DATE |
| REVISD | | | | | | 9-3-86 |
| Revised D.L.D. for Heat Curved Girders | | | | | | 11-17-86 |

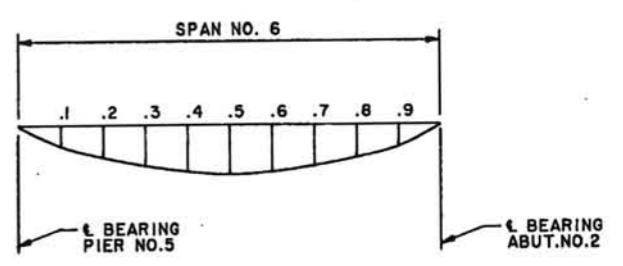


DEAD LOAD DEFLECTION DIAGRAM FOR HEAT-CURVED GIRDERS
 CONCRETE ALONE = 75% FOR EXT. GIRDER
 CONCRETE ALONE = 80% FOR INTERIOR GIRDER

| GIRDER | SPAN #1 | | | | | | | | | | SPAN #2 | | | | | | | | | | SPAN #3 | | | | | | | | | | |
|--------|---------|------|------|------|------|------|------|------|------|------|---------|-------|-------|-------|-------|-------|-------|-------|-------|-------|---------|------|------|------|------|------|------|------|------|------|---|
| | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 |
| A | 0 | 2.67 | 4.62 | 5.99 | 6.83 | 6.83 | 6.07 | 4.98 | 3.29 | 1.47 | 0 | -0.72 | -0.89 | -0.84 | -0.78 | -0.61 | -0.54 | -0.58 | -0.64 | -0.52 | 0 | 0.93 | 2.11 | 3.43 | 4.23 | 4.64 | 4.67 | 4.15 | 3.07 | 1.66 | 0 |
| B | 0 | 2.66 | 4.73 | 6.14 | 6.89 | 7.03 | 6.18 | 5.10 | 3.56 | 1.51 | 0 | -0.69 | -1.02 | -0.99 | -0.90 | -0.78 | -0.72 | -0.78 | -0.77 | -0.59 | 0 | 1.04 | 2.35 | 3.73 | 4.60 | 5.02 | 5.05 | 4.50 | 3.32 | 1.79 | 0 |
| C | 0 | 2.67 | 4.68 | 6.35 | 6.97 | 7.05 | 6.44 | 5.22 | 3.47 | 1.54 | 0 | -0.73 | -1.13 | -1.15 | -0.93 | -0.84 | -0.81 | -0.96 | -0.94 | -0.68 | 0 | 1.15 | 2.37 | 3.69 | 4.94 | 5.38 | 5.40 | 4.81 | 3.54 | 1.92 | 0 |
| D | 0 | 2.72 | 4.78 | 6.38 | 7.21 | 7.14 | 6.55 | 5.37 | 3.82 | 1.83 | 0 | -0.82 | -1.25 | -1.33 | -1.32 | -0.94 | -0.91 | -1.25 | -1.16 | -0.80 | 0 | 1.33 | 2.76 | 4.29 | 5.25 | 5.70 | 5.72 | 5.07 | 3.72 | 2.02 | 0 |
| E | 0 | 2.82 | 4.90 | 6.40 | 7.42 | 7.29 | 6.84 | 5.51 | 3.79 | 1.73 | 0 | -0.91 | -1.38 | -1.60 | -1.62 | -1.61 | -1.60 | -1.55 | -1.38 | -0.90 | 0 | 1.48 | 2.95 | 4.55 | 5.54 | 6.00 | 6.01 | 5.32 | 3.91 | 2.11 | 0 |
| F | 0 | 2.83 | 5.02 | 6.49 | 7.41 | 7.33 | 7.08 | 5.63 | 3.70 | 1.82 | 0 | -0.97 | -1.49 | -1.81 | -1.89 | -1.90 | -1.95 | -1.88 | -1.59 | -1.01 | 0 | 1.64 | 3.13 | 4.66 | 5.82 | 6.29 | 6.28 | 5.56 | 4.07 | 2.21 | 0 |
| G | 0 | 2.81 | 5.10 | 6.73 | 7.57 | 7.80 | 6.98 | 5.72 | 3.85 | 1.91 | 0 | -1.00 | -1.58 | -2.06 | -2.18 | -2.24 | -2.26 | -2.12 | -1.76 | -1.08 | 0 | 1.73 | 3.34 | 5.06 | 6.11 | 6.55 | 6.56 | 5.79 | 4.24 | 2.31 | 0 |
| H | 0 | 2.83 | 5.12 | 7.08 | 7.78 | 7.88 | 7.29 | 5.93 | 3.97 | 1.94 | 0 | -1.08 | -1.77 | -2.29 | -2.51 | -2.57 | -2.60 | -2.40 | -1.93 | -1.17 | 0 | 1.81 | 3.51 | 5.32 | 6.40 | 6.89 | 6.86 | 6.04 | 4.41 | 2.39 | 0 |



DEAD LOAD DEFLECTION DIAGRAM FOR HEAT-CURVED GIRDERS
 CONCRETE ALONE = 75% FOR EXT. GIRDER
 CONCRETE ALONE = 80% FOR INTERIOR GIRDER



DEAD LOAD DEFLECTION DIAGRAM
 CONCRETE ALONE = 75% FOR EXT. GIRDER
 CONCRETE ALONE = 80% FOR INTERIOR GIRDER

| GIRDER | SPAN #4 | | | | | | | | | | SPAN #5 | | | | | | | | | | SPAN #6 | | | | | | | | | | | |
|--------|---------|------|------|------|------|------|------|-------|-------|-------|---------|------|------|------|-------|-------|-------|-------|-------|------|---------|----|------|------|------|------|------|------|------|------|------|---|
| | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | .1 | .2 | .3 | .4 | .5 | .6 | .7 | .8 | .9 | 0 | |
| A | 0 | 3.41 | 6.12 | 7.30 | 7.71 | 7.03 | 5.32 | 3.57 | 1.40 | -0.09 | 0 | 1.31 | 3.81 | 6.57 | 9.05 | 10.56 | 10.96 | 10.00 | 7.61 | 4.16 | 0 | 0 | 1.27 | 2.43 | 3.04 | 3.52 | 3.84 | 3.92 | 3.04 | 2.43 | 1.27 | 0 |
| B | 0 | 3.30 | 5.93 | 7.03 | 7.35 | 6.67 | 5.16 | 3.15 | 1.10 | -0.06 | 0 | 1.49 | 4.16 | 7.00 | 9.49 | 10.01 | 11.28 | 10.20 | 7.85 | 4.25 | 0 | 0 | 1.16 | 2.16 | 2.74 | 3.13 | 3.23 | 3.13 | 2.74 | 2.16 | 1.16 | 0 |
| C | 0 | 3.18 | 5.66 | 6.66 | 6.95 | 6.23 | 4.73 | 2.67 | 0.78 | -0.20 | 0 | 1.64 | 4.42 | 7.42 | 10.02 | 11.53 | 11.73 | 10.53 | 7.99 | 4.34 | 0 | 0 | 1.01 | 1.93 | 2.49 | 2.83 | 3.03 | 2.93 | 2.49 | 1.93 | 1.01 | 0 |
| D | 0 | 3.00 | 5.32 | 6.29 | 6.53 | 5.79 | 4.28 | 2.20 | 0.51 | -0.20 | 0 | 1.77 | 4.72 | 7.81 | 10.47 | 12.00 | 12.29 | 10.98 | 8.30 | 4.49 | 0 | 0 | 0.84 | 1.85 | 2.31 | 2.71 | 2.78 | 2.71 | 2.31 | 1.85 | 0.84 | 0 |
| E | 0 | 2.80 | 4.94 | 5.81 | 5.97 | 5.22 | 3.67 | 1.71 | 0.18 | -0.43 | 0 | 1.93 | 5.03 | 8.28 | 11.00 | 12.54 | 12.75 | 11.47 | 8.73 | 4.72 | 0 | 0 | 0.92 | 1.72 | 2.22 | 2.48 | 2.67 | 2.48 | 2.22 | 1.72 | 0.92 | 0 |
| F | 0 | 2.57 | 4.51 | 5.21 | 5.29 | 4.50 | 3.10 | 1.18 | -0.18 | -0.64 | 0 | 2.11 | 5.35 | 8.72 | 11.53 | 13.15 | 13.31 | 11.91 | 9.08 | 4.88 | 0 | 0 | 0.85 | 1.61 | 2.04 | 2.44 | 2.48 | 2.44 | 2.04 | 1.61 | 0.85 | 0 |
| G | 0 | 2.13 | 4.06 | 4.62 | 4.57 | 3.74 | 2.40 | 0.63 | -0.30 | -0.78 | 0 | 2.26 | 5.60 | 9.17 | 12.22 | 13.94 | 14.10 | 12.60 | 9.50 | 5.12 | 0 | 0 | 0.85 | 1.61 | 2.04 | 2.33 | 2.31 | 2.33 | 2.04 | 1.61 | 0.85 | 0 |
| H | 0 | 2.18 | 3.82 | 4.03 | 3.89 | 2.99 | 1.85 | -0.05 | -0.86 | -0.94 | 0 | 2.38 | 5.84 | 9.54 | 12.70 | 14.57 | 14.93 | 13.53 | 10.31 | 5.58 | 0 | 0 | 0.85 | 1.64 | 2.11 | 2.47 | 2.53 | 2.47 | 2.11 | 1.64 | 0.85 | 0 |

| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
 (SHT. 12 OF 13)
 F.A. Project No. I-235-1(020)000 Sheet No. 85A

| CON. NO. | STATE | F.A. PROJ. NO. | DESIGN | DATE |
|-------------|-------|----------------|---------|------|
| 6 | OILA | 1-235-11 | | |
| DESCRIPTION | | REVISIONS | DATE | |
| Revised | | | 6-17-86 | |

NOTE: PLACE CRACK CONTROL JT. AT MIDPOINT BETWEEN PARAPET CONSTRUCTION JOINTS.

NOTE: ALL EXPOSED EDGES SHALL HAVE 3/4" CHAMFER UNLESS OTHERWISE SHOWN OR NOTED. ALL CHAMFER STRIPS SHALL BE SIZED LUMBER.

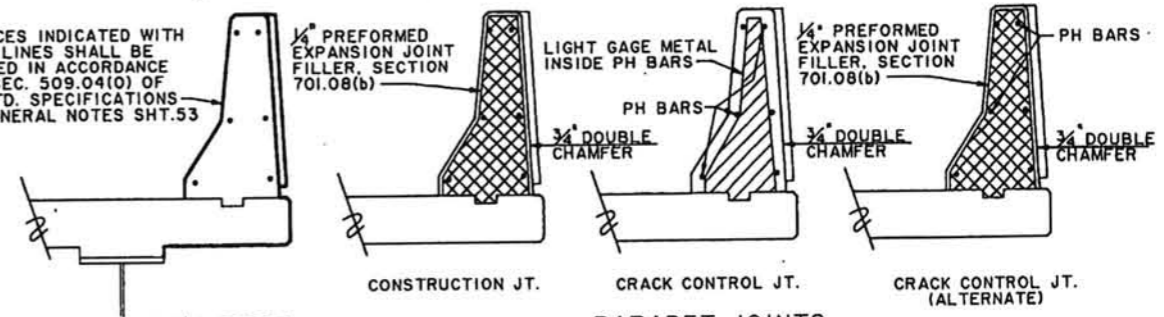
SURFACES INDICATED WITH HEAVY LINES SHALL BE FINISHED IN ACCORDANCE WITH SEC. 509.04101 OF THE STD. SPECIFICATIONS SEE GENERAL NOTES SHT.53

1/4" PREFORMED EXPANSION JOINT FILLER, SECTION 701.08(b)

LIGHT GAGE METAL INSIDE PH BARS

1/4" PREFORMED EXPANSION JOINT FILLER, SECTION 701.08(b)

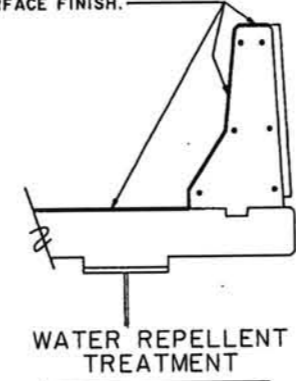
PH BARS



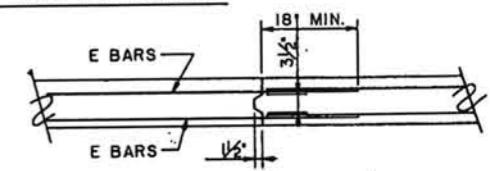
SPECIAL CONCRETE SURFACE FINISH

PARAPET JOINTS

SURFACES INDICATED WITH HEAVY LINES SHALL BE TREATED WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT. TREATMENT TO BE APPLIED PRIOR TO THE APPLICATION OF THE CONCRETE SURFACE FINISH.

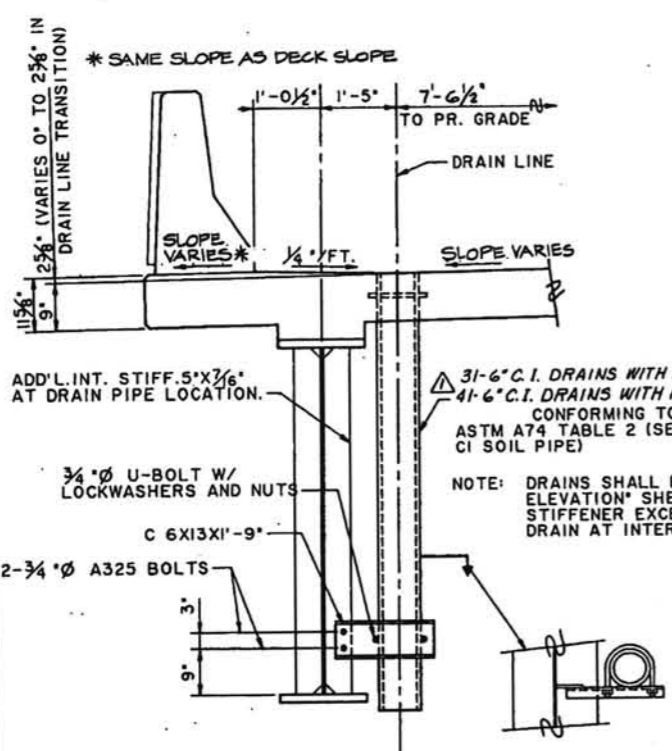


WATER REPELLENT TREATMENT

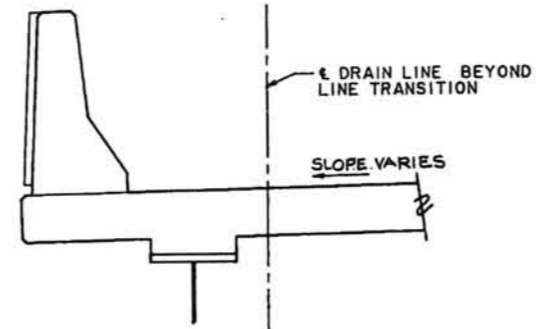


SLAB TRANSVERSE CONSTRUCTION JOINT

NOTE: NO HEAVY CONSTRUCTION MACHINES OR OTHER EQUIPMENT WILL BE PERMITTED ON THE FINISHED SLAB WITHIN 5'-0" OF THE TRANSVERSE CONSTRUCTION JOINTS UNTIL AFTER SLAB IS IN PLACE ON BOTH SIDES OF THE JOINT.



SECTION "A"
DETAIL OF CAST IRON DRAIN
SCALE: 3/4"=1'-0"



SECTION "B"
SCALE: 3/4"=1'-0"

ADD'L. INT. STIFF. 5" X 3/16" AT DRAIN PIPE LOCATION.

3/4" U-BOLT W/ LOCKWASHERS AND NUTS

C 6X13X1'-9"

2-3/4" A325 BOLTS

31'-6" C.I. DRAINS WITH FLANGE x 5'-11" (SPAN L1, L2 & L3)

41'-6" C.I. DRAINS WITH FLANGE x 7'-9" (SPAN L3, L4, L5 & L6)

CONFORMING TO ASTM A74 TABLE 2 (SERVICE CI SOIL PIPE)

NOTE: DRAINS SHALL BE LOCATED AS SHOWN ON "GENERAL PLAN AND ELEVATION" SHEETS. PLACE DRAINS AT EACH INTERMEDIATE STIFFENER EXCEPT OVER OTHER ROADWAYS. DO NOT LOCATE DRAIN AT INTERMEDIATE STIFFENER WITH CROSS BRACING.

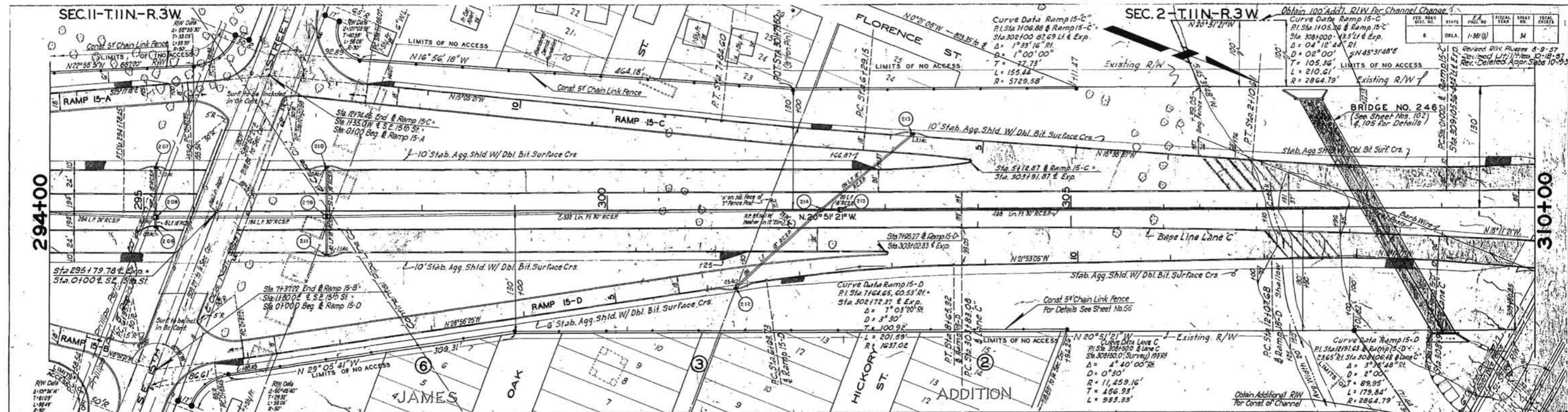
| | |
|----------|-----|
| Design | JTB |
| Drawn | LDM |
| Checked | LRS |
| Approved | |
| Squad | |

BRIDGE E-G
DETAILS OF SUPERSTRUCTURE
(SHT. 14 OF 14)
F.A. Project No. 1-235-1(020)000 Sheet No. 86

SEC. II-T.IIN-R.3W

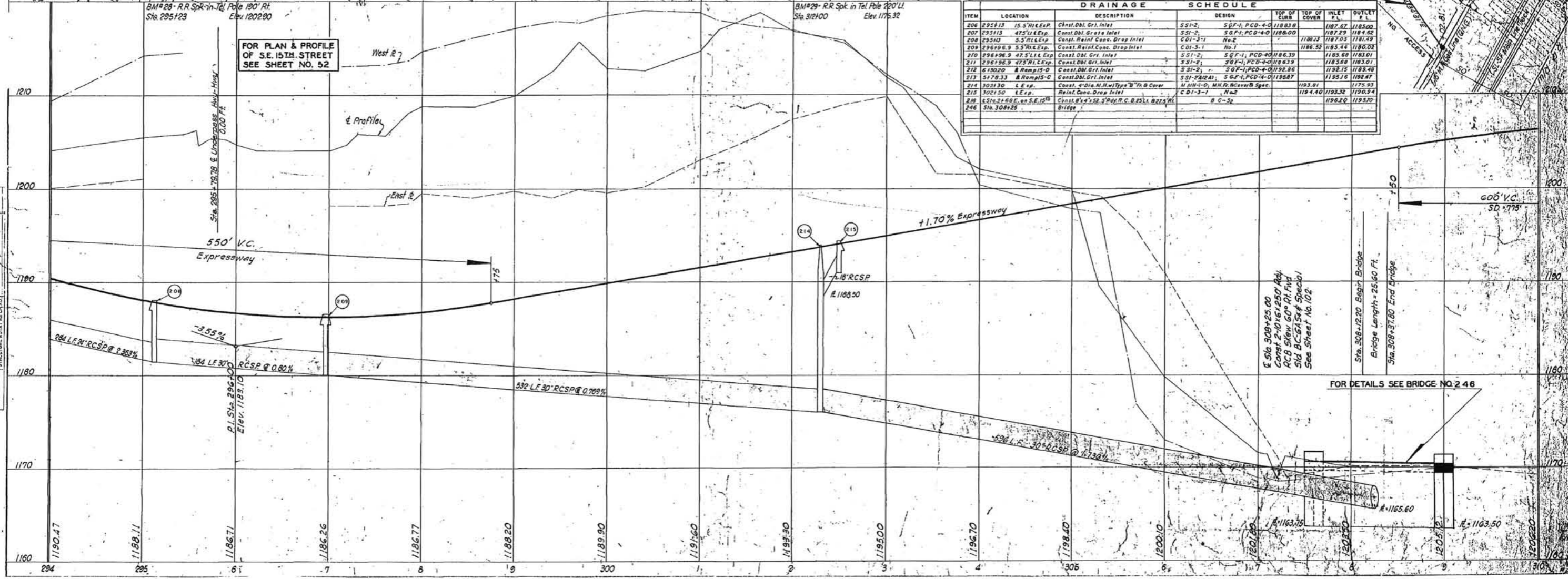
SEC. 2-T.IIN-R.3W

| FED. ROAD DIST. NO. | STATE | PROJECT NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|-------------|-------------|-----------|--------------|
| 1-58(9) | OKLA. | 1-58(9) | | 34 | |



FOR PLAN & PROFILE OF S.E. 15TH STREET SEE SHEET NO. 52

| DRAINAGE | | SCHEDULE | | TOP OF COVER | TOP OF INLET | OUTLET |
|----------|----------|---|-----------------------------------|----------------|--------------|---------|
| ITEM | LOCATION | DESCRIPTION | DESIGN | FT. | FT. | FT. |
| 206 | 295+13 | 15.5' R/L Exp. Const. Dbl. Grit Inlet | SSI-2 | 1183.8 | 1187.67 | 1185.00 |
| 207 | 295+13 | 47.5' L/E Exp. Const. Dbl. Grit Inlet | SSI-2 | 1183.8 | 1187.29 | 1184.62 |
| 208 | 295+10 | 5.5' R/L Exp. Const. Reinf. Conc. Drop Inlet | CDI-3-1 | No. 2 | 1187.05 | 1181.49 |
| 209 | 296+19 | 5.5' R/L Exp. Const. Reinf. Conc. Drop Inlet | CDI-3-1 | No. 1 | 1186.52 | 1183.44 |
| 210 | 296+26 | 9.47.5' L/E Exp. Const. Dbl. Grit Inlet | SSI-2 | SGF-1, PCD-4-0 | 1185.88 | 1183.01 |
| 211 | 296+26 | 9.47.5' R/L Exp. Const. Dbl. Grit Inlet | SSI-2 | SGF-1, PCD-4-0 | 1185.69 | 1183.01 |
| 212 | 6+30.20 | Ramp 15-D Const. Dbl. Grit Inlet | SSI-2 | SGF-1, PCD-4-0 | 1192.36 | 1192.15 |
| 213 | 5+78.33 | Ramp 15-C Const. Dbl. Grit Inlet | SSI-2 | SGF-1, PCD-4-0 | 1195.87 | 1195.87 |
| 214 | 302+30 | L/E Exp. Const. 4'-Dia. M.H. w/Type 'B' Fr. & Cover | MNH-3-D, M.H. Fr. & Cover B Spec. | 1193.81 | 1175.93 | |
| 215 | 302+30 | L/E Exp. Reinf. Conc. Drop Inlet | CDI-3-1 | No. 2 | 1194.40 | 1193.94 |
| 216 | 310+25 | 6.68' Exp. Const. 8'-x-4'-x-25' Box Culvert | BC-3-1 | No. 2 | 1196.20 | 1193.70 |
| 246 | 308+25 | Bridge | # C-3g | | | |



FOR DETAILS SEE BRIDGE NO. 246

UPFILE SURVEYED
 PLOTTED
 AND CHECKED
 DATE BOOK
 & M. M. NOTED
 STRUCTURE NOT AT NS DNYO

Sub: June 21st, 2010

| DESIGN DATA | I-40 | I-35/235 |
|----------------------------------|-------------|-----------|
| AADT 2005 | = 111,400 | 98,800 |
| AADT 2030 | = 165,200 | 148,200 |
| K (DHV / ADT-TWO WAY) | = 9 % | 9 % |
| D (DIRECTIONAL DIST.) | = 55 % | 55 % |
| T (% OF DHV) | = 10 % | 10 % |
| T (% OF AADT) | = 14 % | 14 % |
| T ³ OVERLOADS (AXLES) | = 10 % | 10 % |
| T ³ OVERLOADS | = 15 AXLES | |
| 20-YR FLEX ESALs | = 69.22 MIL | 61.74 MIL |
| I-40/I-35/I-235 | V = 70 MPH | |
| BOULEVARD | V = 50 MPH | |
| RAMPS | V = 50 MPH | |

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED
INTERSTATE HIGHWAY
GRADING, DRAINAGE, PAVING, SIGNING & STRIPING, LIGHTING, AND BRIDGE PLANS
INTERSTATE 40 CROSSTOWN
OKLAHOMA COUNTY

FOR INDEX OF SHEETS AND STANDARDS, SEE SHEET 2.

| FED. ROAD DIST. NO. | STATE | JOB PIECE NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|---------------|-------------|-----------|--------------|
| 6 | OKLA | 17428(34) | 08 | 1 | 484 |

MANDATORY TIE :
This project is a mandatory tie with Project No. IMY-0040-4(432) 152SS, Job Piece Number 17428(32) and shall be bid accordingly.

CONTROL SECTION 55-69P
PROJECT NO. OKCY-XTWN(015)TI
PROJECT NO. SEC-115-155N(806)SG
STATE JOB NO. 17428(34) AND STATE JOB NO. 22873(08)
WORK PACKAGE 4.3

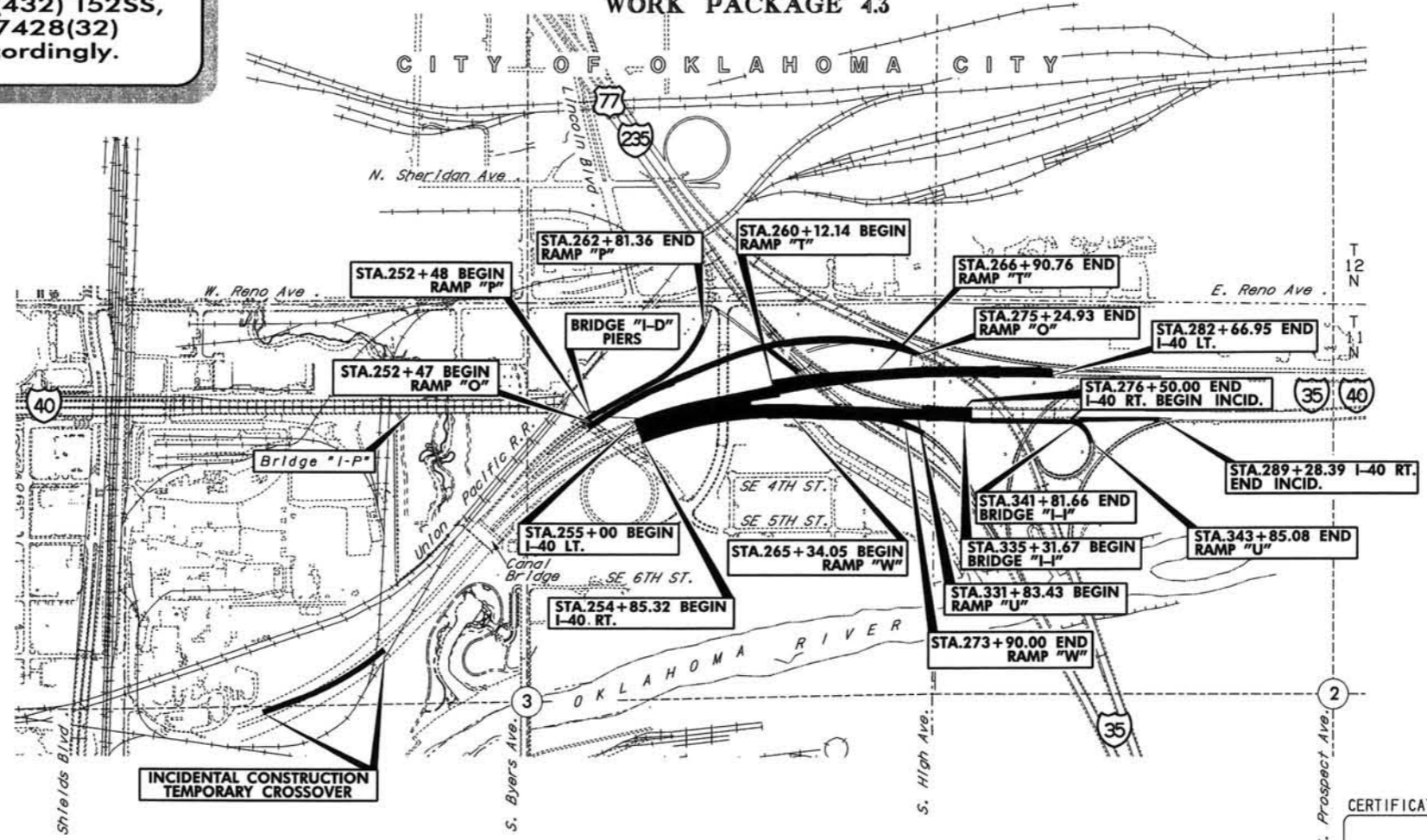


SCALES
PLAN 1" = 50'
PROFILE HOR. 1" = 50'
VER. 1" = 5'
LAYOUT MAP 1" = 1000'

CONVENTIONAL SYMBOLS

| | | | |
|--|---|--|-----------------------|
| | PROPOSED ROAD | | TELEPHONE UNDERGROUND |
| | RAILROADS | | SANITARY SEWER |
| | RANGE & TOWNSHIP | | GAS LINE |
| | SECTION LINES | | WATER LINE |
| | QUARTER SECTION LINES | | |
| | FENCES | | |
| | GROUND LINE | | |
| | EXISTING ROADS | | |
| | BASE LINE | | |
| | GRADE LINES | | |
| | TELEPHONE & TELEGRAPH | | |
| | POWER LINES | | |
| | BUILDINGS | | |
| | OIL WELL | | |
| | DRAINAGE STRUCTURES - IN PLACE | | |
| | DRAINAGE STRUCTURES - NEW | | |
| | RIGHT-OF-WAY LINES - EXISTING | | |
| | RIGHT-OF-WAY LINES - NEW | | |
| | RIGHT-OF-WAY MARKERS - IN PLACE | | |
| | RIGHT-OF-WAY MARKERS - REMOVE & REPLACE | | |
| | RIGHT-OF-WAY MARKERS - NEW | | |
| | CONTROLLED ACCESS | | |
| | RIGHT-OF-WAY FENCE | | |

FOR SURVEY CONTROL DATA SEE SURVEY DATA SHEETS 60 TO 81



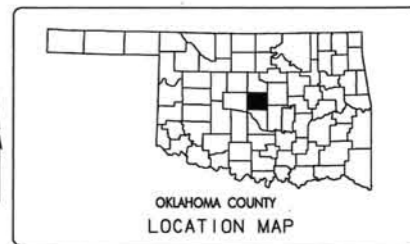
BRIDGE "I-I"
NBI NO. EXIST.: 21722
LENGTH: 649.99'

BEGIN BRIDGE "I-I"
STA. 335+31.67
(RAMP U)

END BRIDGE "I-I"
STA. 341+81.66
(RAMP U)

Bridge "I-P"
(Exist. I-40 Crosstown)
NBI No. Exist.: 16197

BRIDGE "I-D"
NBI No. 28598
PIERS 11, 12, 13 & 14



ROADWAY LENGTH ----- 2,766.95 FT. 0.524 MI.
BRIDGE LENGTH ----- 649.99 FT. 0.123 MI.
PROJECT LENGTH ----- 0.524 MI.

EQUATIONS: NONE
EXCEPTIONS: NONE

CERTIFICATE OF AUTHORIZATION NO. 3722 P.E., L.S. RENEWAL DATE 6-30-11

PREPARED BY:
THE BENHAM COMPANIES, LLC

STEVEN R. JACOBY, P.E.
OKLA. REG. NO. 14866
DATE 6.16.2010

| | |
|--|--|
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION |
| DATE APPROVED _____ | DATE APPROVED _____ |
| BY _____ | BY _____ |
| CHIEF ENGINEER | DIVISION ADMINISTRATOR |
| S.W.O. 3367(1) | Project No. 17428(34) AND 22873(08) Sheet No. 1 |

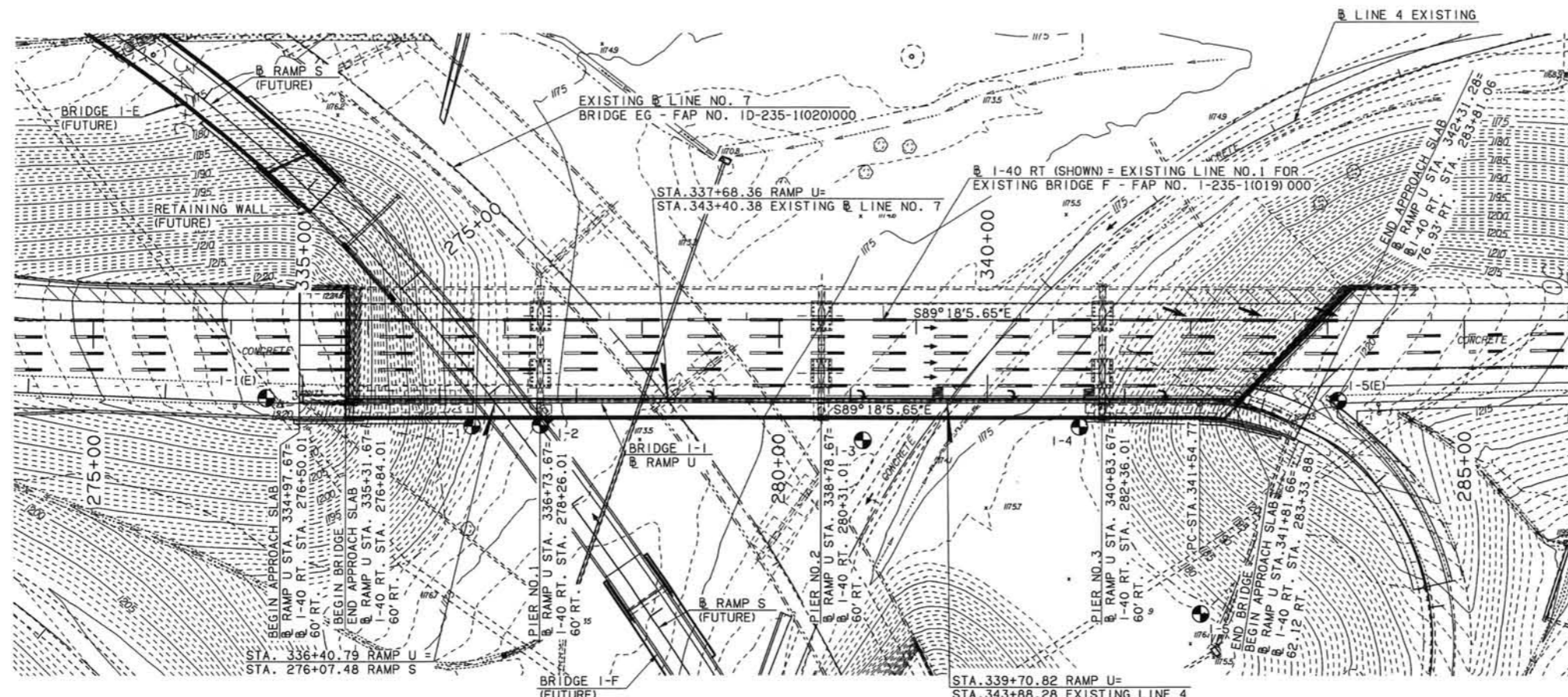
1999 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION-ENGLISH GOVERN, APPROVED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION, SEPTEMBER 21, 1999.

6/15/2010

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| DESIGN NO. | STATE | J/P PROJ NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|------------|-------|--------------|-------------|-----------|--------------|
| 6 | OKLA | 1742834 | 10 | 254 | 484 |



DESIGN DATA

LOADING
 HL-93 OR OKLAHOMA OVERLOAD TRUCK
 20 PSF FUTURE WEARING SURFACE.
 5 PSF STEEL STAY-IN-PLACE FORMS.

LFD OPERATING RATING HS-32

DESIGN
 AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 3RD EDITION INCLUDING 2006 INTERIMS, EXCEPT FOR BEARING PADS WHICH SATISFY AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 16TH EDITION WITH 1999 INTERIMS, USING HL-93 LIVE LOADING, AND EXCEPT FOR PILES WHICH SATISFY AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 16TH EDITION WITH NO INTERIMS, USING HL-93 LIVE LOADING.

MATERIAL

CONCRETE
 CLASS AA $f'_c = 4,000$ PSI
 CLASS A $f'_c = 3,000$ PSI
 REINFORCING STEEL $F_y = 60,000$ PSI
 STRUCTURAL STEEL M270 (GRADE 50W) $F_y = 50,000$ PSI

FOUNDATION DESIGN

ABUTMENTS (HP 14X73 PILING)
 FACTORED PILE REACTION = 120.0 TONS/PILE

WINGWALL (HP 12X53 PILING)
 FACTORED PILE REACTION = 61.2 TONS/PILE

PIERS (6' DIA. DRILLED SHAFTS)
 MAX. FACTORED LOAD (SERVICE I) = 746.2 TONS/SHAFT
 MAX. FACTORED LOAD (STRENGTH II) = 1044.6 TONS/SHAFT
 FRICTION CAPACITY (7.90 TSF) = 668.3 TONS/SHAFT
 END BEARING CAPACITY (50 TSF) = 990.5 TONS/SHAFT
 TOTAL CAPACITY = 1658.8 TONS/SHAFT

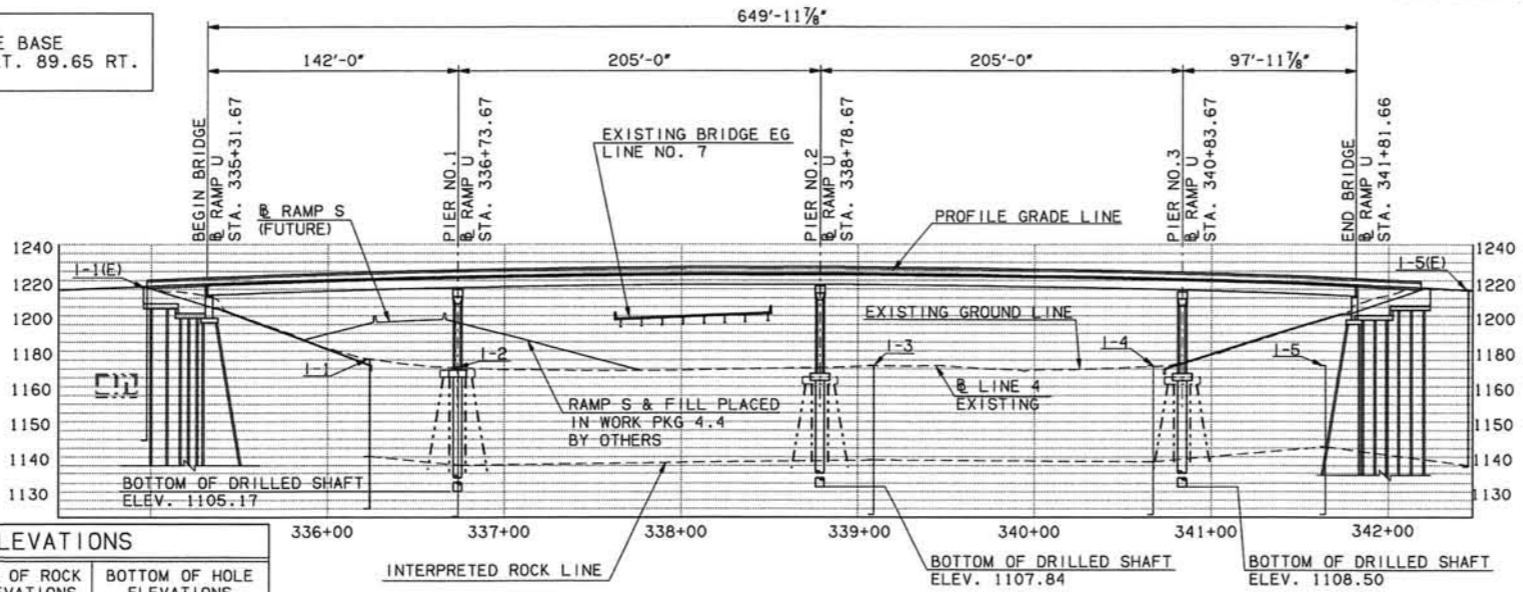
BENCHMARK: 40
 CHISELED "X" ON THE SOUTHWEST SIDE
 STA. 279+62.01 (I-40) LT. 525.35 RT.
 ELEV. 1174.87

BENCHMARK: 41
 CHISELED "X" LAMP POLE BASE
 STA. 287+73.12 (I-40) LT. 89.65 RT.
 ELEV. 1172.15

PLAN - BRIDGE I-1

SCALE: 1 = 50'

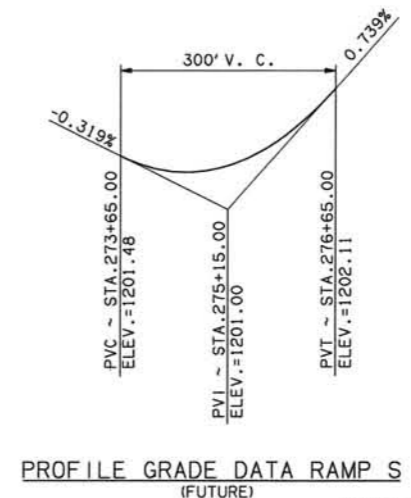
ALL ABUTMENT PILING SHALL BE DRIVEN THROUGH THE COMPACTED FILL. PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN ON THE PLANS. IF THE AXIAL LOAD RESISTANCE IS NOT OBTAINED AT THIS ELEVATION, DRIVING SHALL CONTINUE UNTIL THE AXIAL LOAD RESISTANCE IS OBTAINED. THE LENGTH OF STEEL PILING SHOWN ON THE PLANS IS FOR ESTIMATING PURPOSES ONLY.



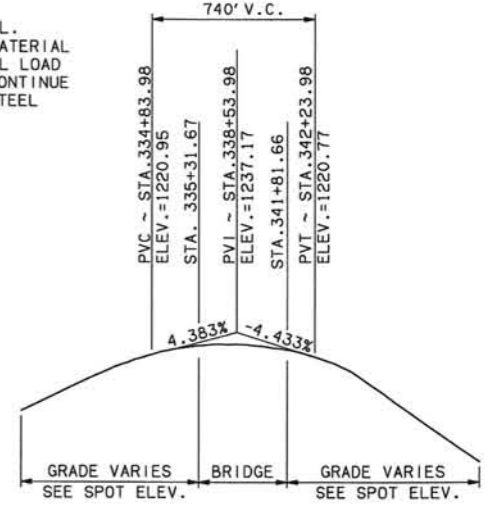
ELEVATION - BRIDGE I-1 - RAMP U

SCALE HORIZ. 1 = 50'
VERT. 1 = 50'

| BORING ELEVATIONS | | | |
|-------------------|------------------------|------------------------|---------------------------|
| BORINGS | TOP OF HOLE ELEVATIONS | TOP OF ROCK ELEVATIONS | BOTTOM OF HOLE ELEVATIONS |
| I-1(E) | 1220.83 | - | 1134.33 |
| I-1 | 1180.24 | 1125.24 | 1094.99 |
| I-2 | 1175.70 | 1120.20 | 1090.45 |
| I-3 | 1176.67 | 1123.17 | 1091.42 |
| I-4 | 1176.75 | 1122.25 | 1091.50 |
| I-5 | 1176.85 | 1123.85 | 1091.60 |
| I-5(E) | 1219.81 | 1120.81 | 1119.31 |



PROFILE GRADE DATA RAMP S (FUTURE)



PROFILE GRADE DATA RAMP U

PROFILE GRADE SHOWN IS BEST FIT OF FIELD DATA. FINAL GRADE WILL BE BASED ON EXTENDING 2% X-SLOPE FROM BREAK LINE. SEE SHEETS 257 & 285.

WIDEN BRIDGE I-1 AT STA. 338+56.67 CRL RAMP U (EXIST. I-40 RT.). 142' - 205' - 205' - 98' CONTINUOUS STEEL PLATE GIRDER SPANS W/92' CLEAR ROADWAY. F-SHAPED PARAPET RT., SLOPED FACE PARAPET LT., SKEW 45° LT.FWD AT ABUTMENT 2.

| | | | | |
|----------|--------|-------|-------------------------------------|-----------------|
| Design | SOT | 1/05 | I-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RWS | 1/05 | I-40 RELOCATION BRIDGE I-1 | |
| Checked | SOT | 06/10 | 1-40 RT. & RAMP U | |
| Approved | SRJ | 06/10 | GENERAL PLAN AND ELEVATION | |
| Squad | BENHAM | | @ STA. 338+56.67 PLATE GIRDER SPANS | |
| | | | Job Piece No. 17428(34) | Sheet No. 254 |

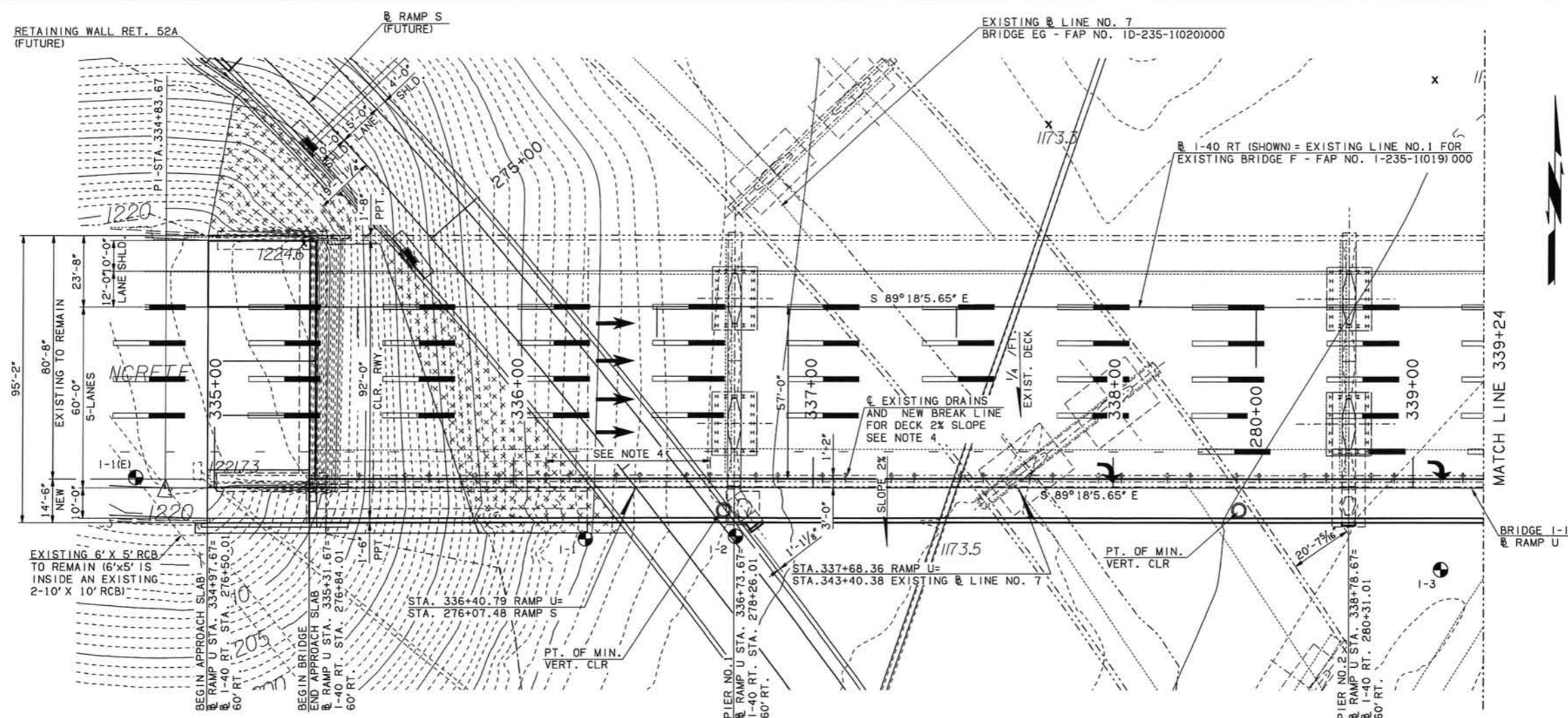
NOTE:
 FOR HORIZONTAL AND VERTICAL CLEARANCES AND ADDITIONAL INFORMATION, SEE SHEET 255 AND 256.

6/15/2010

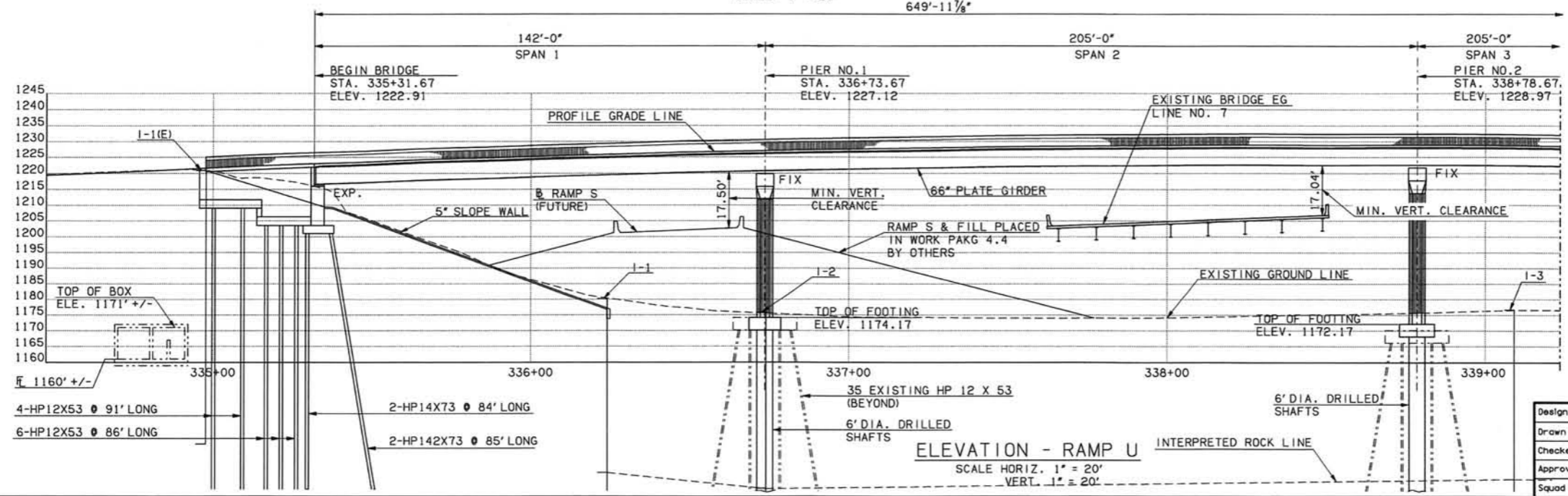
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| FED. ROAD DIST. NO. | STATE | JOB PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|---------------|-------------|-----------|--------------|
| 6 | OKLA | 1742834 | 10 | 255 | 484 |



PLAN - BRIDGE I-1
SCALE: 1" = 20'



ELEVATION - RAMP U
SCALE HORIZ. 1" = 20'
VERT. 1" = 20'

NOTES:

1. NEW FILL FOR RAMPS ADDS LOAD TO EXISTING PILES. EXISTING PILE LOAD (DIRECT) = 61.2 TONS/PILE. ADDITIONAL LOAD = 16.2 TONS/PILE. TOTAL PILE LOAD (PROPOSED) = 77.4 TONS/PILE (10,000 PSI)
2. NOTE EXISTING PLANS ARE ROTATED 180°. SPANS, PIERS AND ABUTMENTS ARE RE-NUMBERED FOR SIMPLICITY.
3. FOR BOTTOM OF BORING AND BOTTOM OF DRILLED SHAFT, SEE SHEET 254.
4. FOR SAWCUT DETAILS SEE DETAIL 1 TYPICAL SECTION SHEET 257

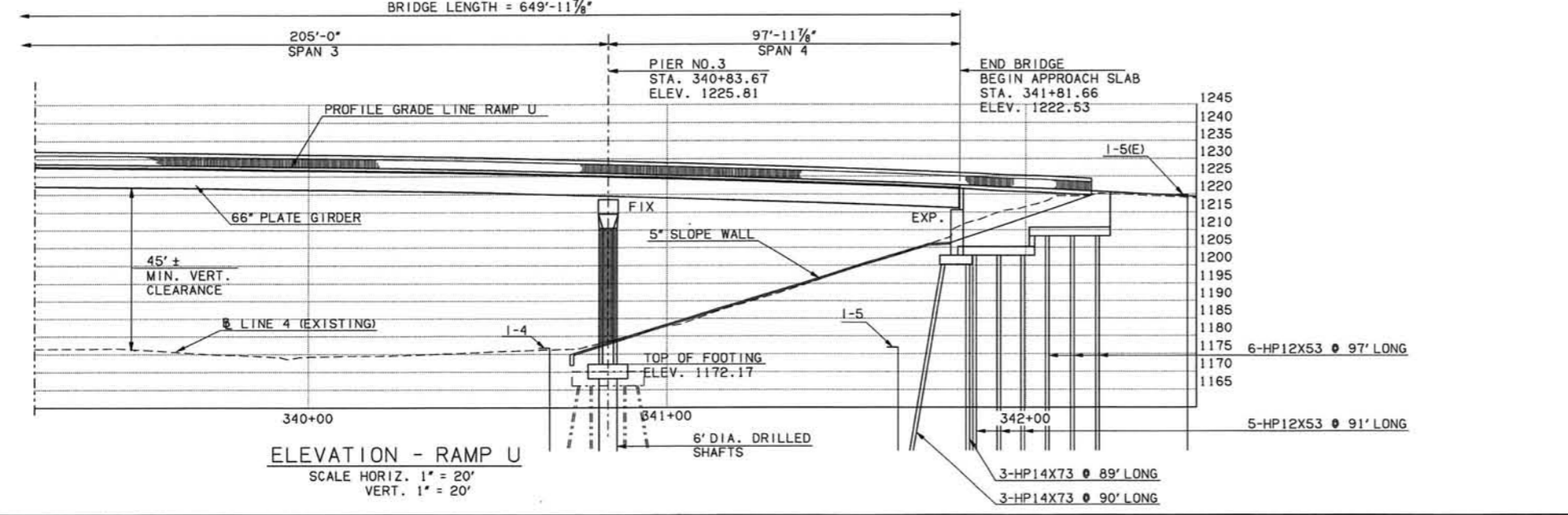
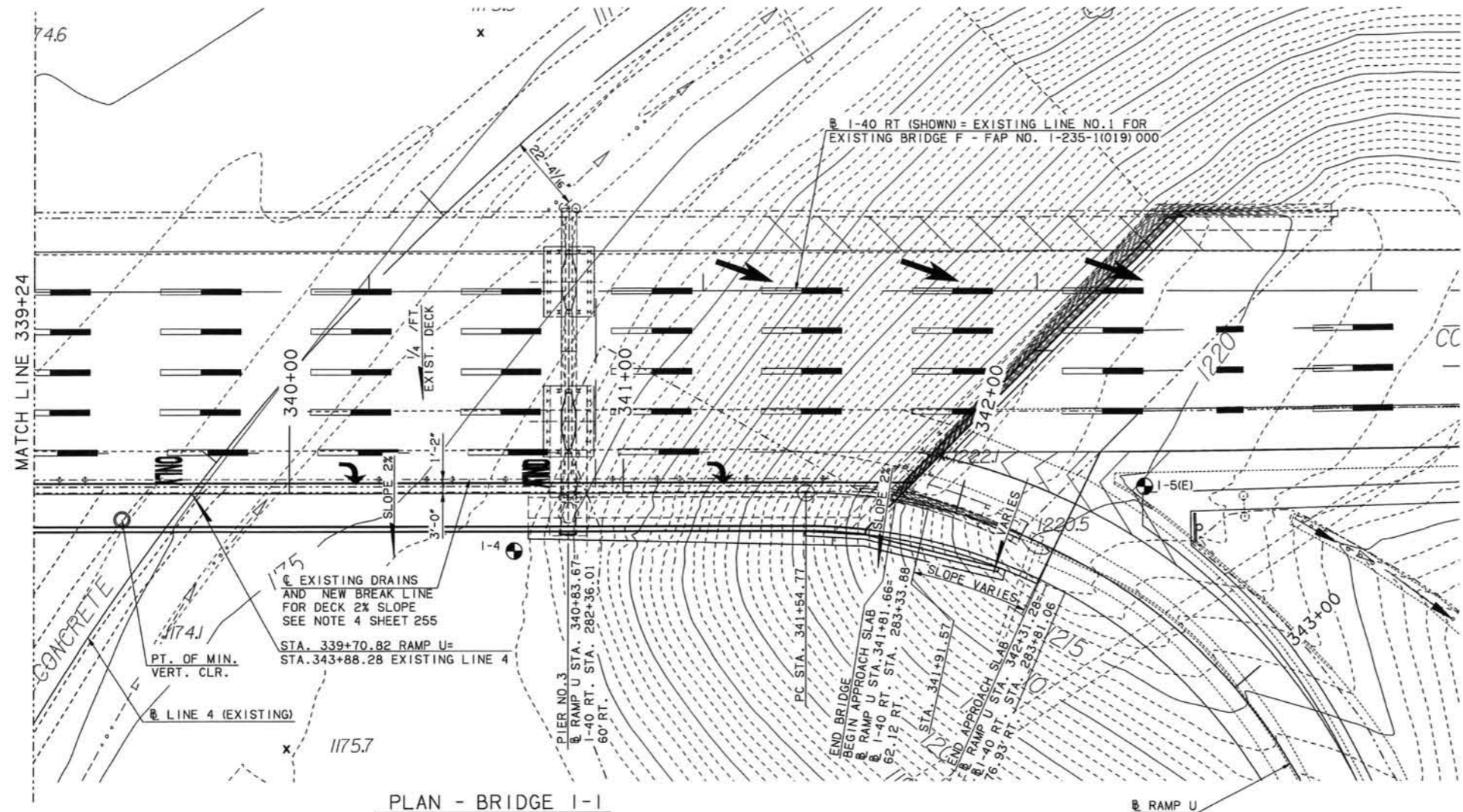
| | | | | |
|-------------------------|--------|-------|--|-----------------|
| Design | SOT | 1/05 | I-40 CROSSTOWN I-40 RELOCATION BRIDGE I-1 I-40 RT. & RAMP U GENERAL PLAN AND ELEVATION @ STA. 338+56.67 PLATE GIRDER SPANS | OKLAHOMA COUNTY |
| Drawn | RWS | 1/05 | | |
| Checked | SOT | 06/10 | | |
| Approved | SRJ | 06/10 | | |
| Squad | BENHAM | | | |
| Job Piece No. 17428(34) | | | Sheet No. 255 | |

6/15/2010

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| PROJ. NO. | STATE | J.P. PROJ. NO. | FOCAL YEAR | SHEET NO. | TOTAL SHEETS |
|-----------|-------|----------------|------------|-----------|--------------|
| 6 | OKLA | 17428340 | 10 | 256 | 484 |

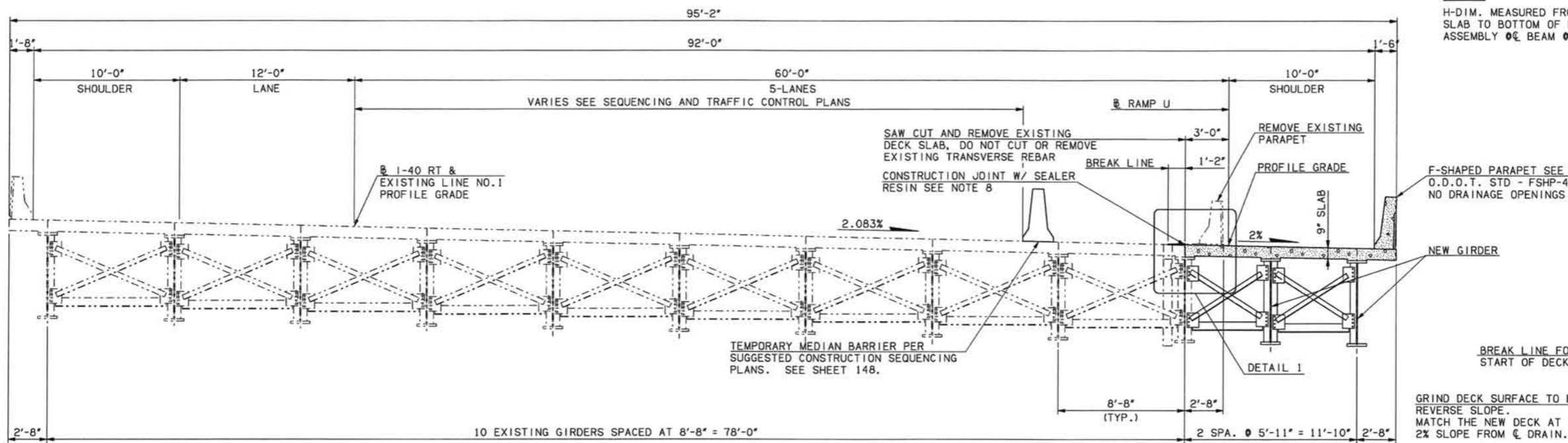


| Design | SOT | 1/05 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
|----------|--------|-------|---|-----------------|
| Drawn | RWS | 1/05 | I-40 RELOCATION BRIDGE I-1 I-40 RT. & RAMP U | |
| Checked | SOT | 06/10 | GENERAL PLAN AND ELEVATION | |
| Approved | SRJ | 06/10 | @ STA. 338+56.67 PLATE GIRDER SPANS | |
| Squad | BENHAM | | Job Piece No. 17428(34) | Sheet No. 256 |

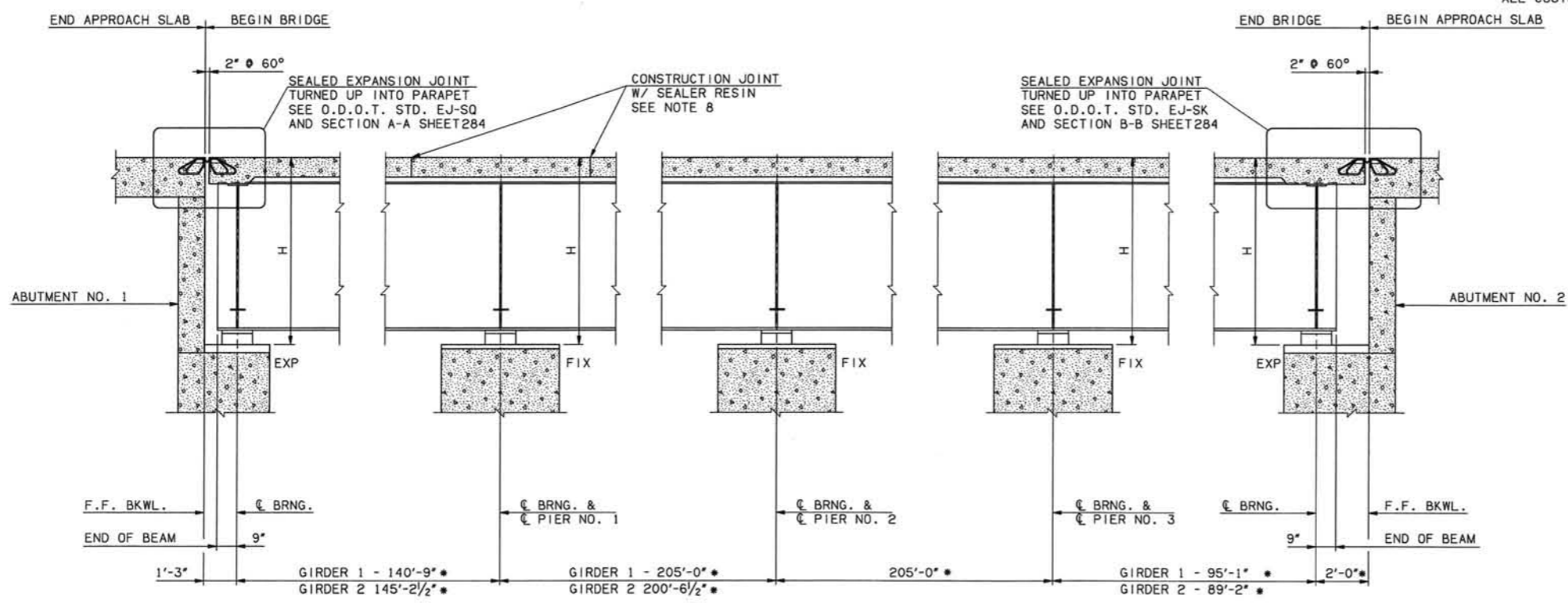
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TYPICAL SECTION
SCALE: 1/4" = 1'-0"



LONGITUDINAL SECTION
SCALE: 3/8" = 1'-0"

*** NOTE:**
DIMENSIONS SHOWN ARE ALONG GIRDERS

NOTE:
H-DIM. MEASURED FROM TOP OF SLAB TO BOTTOM OF BEARING ASSEMBLY @ CL BEAM @ CL BEARING.

| HEIGHT DIMENSION | | |
|------------------|----------|------------|
| LOCATION | GIRDER | H |
| ABUTMENT 1 | EXISTING | 7'-1 1/8" |
| ABUTMENT 1 | INTERIOR | 7'-1 1/8" |
| ABUTMENT 1 | EXTERIOR | 7'-1 1/8" |
| PIER 1 | EXISTING | 7'-0 3/4" |
| PIER 1 | INTERIOR | 6'-10 1/4" |
| PIER 1 | EXTERIOR | 6'-10 1/4" |
| PIER 2 | EXISTING | 7'-1" |
| PIER 2 | INTERIOR | 6'-10 3/8" |
| PIER 2 | EXTERIOR | 6'-10 3/8" |
| PIER 3 | EXISTING | 7'-0" |
| PIER 3 | INTERIOR | 6'-10 1/4" |
| PIER 3 | EXTERIOR | 6'-10 1/4" |
| ABUTMENT 2 | EXISTING | 7'-1" |
| ABUTMENT 2 | INTERIOR | 7'-2 1/2" |
| ABUTMENT 2 | EXTERIOR | 7'-2 1/2" |

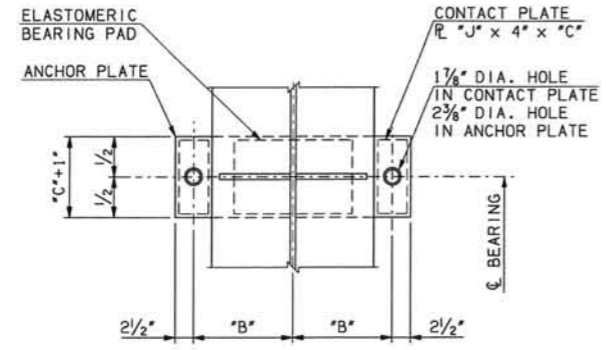
- BRIDGE DECK NOTES:**
- BRIDGE DECK SHALL BE CLASS AA CONCRETE WITH MAXIMUM 1" DIA. AGGREGATE.
 - ALL CONSTRUCTION SHALL COMPLY WITH THE REQUIREMENTS OF THE 1999 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.
 - ALL EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER UNLESS OTHERWISE NOTED.
 - TRANSVERSE REINFORCING STEEL IN THE BRIDGE DECK SHALL BEGIN AND END A MAXIMUM OF 2" FROM THE END OF CONCRETE.
 - LONGITUDINAL REINFORCING STEEL SHALL BEGIN AND END 1" FROM END OF SLAB.
 - ALL LONGITUDINAL TOP REINFORCING STEEL IN THE DECK SHALL BE SUPPORTED ON APPROVED CONTINUOUS EPOXY COATED METAL HIGH CHAIRS (HMC) SPACED AT 4'-0" MAXIMUM AND BOTTOM LAYER OF REINFORCING STEEL SHALL BE SUPPORTED ON APPROVED EPOXY COATED METAL SLAB BOLSTERS (SB) SPACED AT 4'-0" MAXIMUM
 - ALL TIE WIRES USED TO TIE THE REINFORCING STEEL IN THE DECK SHALL BE PLASTIC COATED.
 - ALL CONSTRUCTION JOINTS WITHIN THE DECK SLAB SHALL BE SEALED USING HIGH MOLECULAR WEIGHT METHACRYLATE IN ACCORDANCE WITH THE SPECIAL PROVISION "CONCRETE SURFACE REPAIR BY SEALING". ALL COST OF THE HIGH MOLECULAR WEIGHT METHACRYLATE SEALER SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF "(SP) SEALER RESIN". ALL COST OF EQUIPMENT AND LABOR FOR THE INSTALLATION OF THE HIGH MOLECULAR WEIGHT METHACRYLATE SEALER SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF "(SP) SEALER CRACK PREPARATION".
 - FOR DECK REINFORCING AND DETAILS SEE SHEETS 283, 284 & 285.

| | | | | |
|----------|--------|-------|----------------------------------|-----------------|
| Design | KSJ | 9/07 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RWS | 9/07 | 1-40 RELOCATION BRIDGE 1-1 | |
| Checked | SOT | 06/10 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 06/10 | TYPICAL AND LONGITUDINAL SECTION | |
| Squad | BENHAM | | Job Piece No. 17428(34) | Sheet No. 257 |

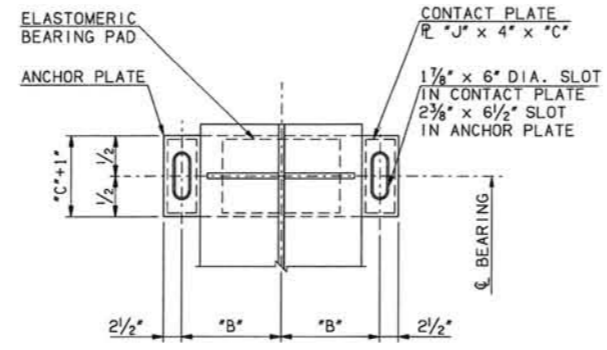
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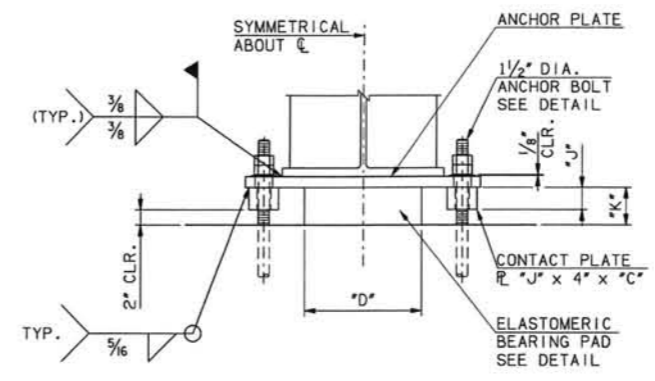
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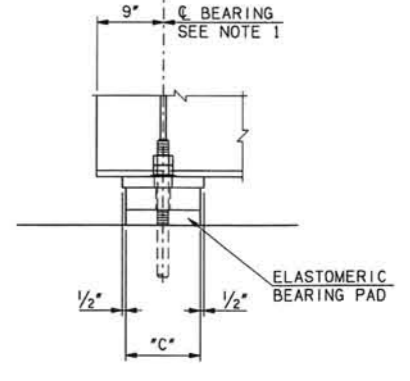
FIX - PLAN AT PIERS



EXPANSION - PLAN AT ABUTMENTS



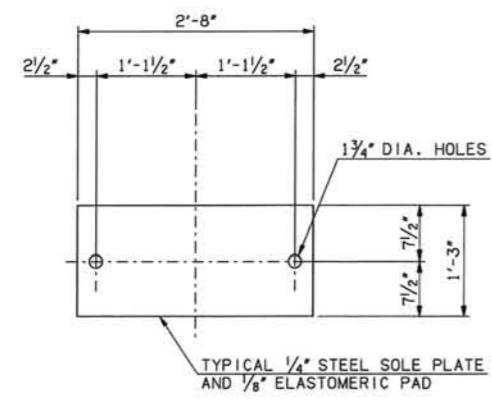
END VIEW



SIDE VIEW

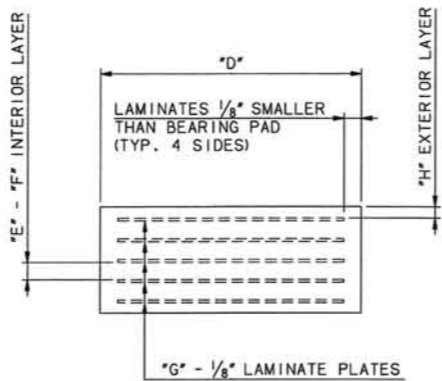
BEARING PAD DETAILS

SCALE: NONE



SOLE PLATE DETAIL

SCALE: 1" = 1'-0"

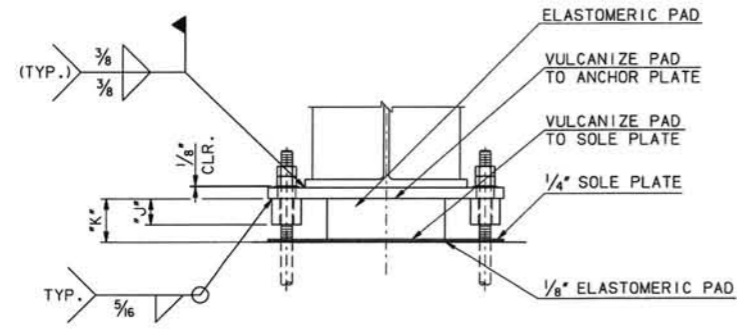


BEARING PAD ELEVATION

(60 DUROMETER HARDNESS)
INCLUDE ALL COSTS OF BEARING PADS
IN PRICE BID FOR FIXED OR EXPANSION
BEARING ASSEMBLY.

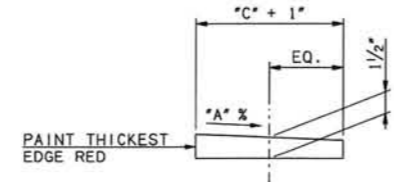
| ANCHOR PLATE AND BEARING PAD SCHEDULE | | | | | | | | | | | | |
|---------------------------------------|----------|---------------------|-----------------------------------|------------------------|-----------------------|-------------------------------|------------------------------------|-------------------------|-----------------------------------|----------------------------------|-----------------------|------------------------------|
| LOCATION | GIRDER | *A* BEVEL SLOPE (%) | *B* CL GIRDER TO ANCHOR BOLT (IN) | *C* LENGTH OF PAD (IN) | *D* WIDTH OF PAD (IN) | *E* NUMBER OF INTERIOR LAYERS | *F* HEIGHT OF INTERIOR LAYERS (IN) | *G* NUMBER OF LAMINATES | *H* HEIGHT OF EXTERIOR LAYER (IN) | *J* CONTACT PLATE THICKNESS (IN) | *K* TOTAL HEIGHT (IN) | NON-SLIDING BASE PL REQUIRED |
| ABUTMENT NO. 1 | INTERIOR | 3.8 | 13.5 | 14 | 10 | 7 | 0.5 | 8 | 0.25 | 3 | * 5.375 | YES |
| ABUTMENT NO. 1 | EXTERIOR | 3.8 | 13.5 | 14 | 10 | 7 | 0.5 | 8 | 0.25 | 3 | * 5.375 | YES |
| PIER NO. 1 | INTERIOR | 2.13 | 15.5 | 20 | 26 | 1 | 0.75 | 2 | 0.25 | - | 1.5 | NO |
| PIER NO. 1 | EXTERIOR | 2.07 | 15.5 | 20 | 26 | 1 | 0.75 | 2 | 0.25 | - | 1.5 | NO |
| PIER NO. 2 | INTERIOR | 0 | 16.5 | 22 | 28 | 1 | 0.75 | 2 | 0.25 | - | 1.5 | NO |
| PIER NO. 2 | EXTERIOR | 0 | 16.5 | 22 | 28 | 1 | 0.75 | 2 | 0.25 | - | 1.5 | NO |
| PIER NO. 3 | INTERIOR | 2.76 | 14.5 | 20 | 24 | 1 | 0.75 | 2 | 0.25 | - | 1.5 | NO |
| PIER NO. 3 | EXTERIOR | 2.76 | 14.5 | 20 | 24 | 1 | 0.75 | 2 | 0.25 | - | 1.5 | NO |
| ABUTMENT NO. 2 | INTERIOR | 3.9 | 13.5 | 14 | 10 | 8 | 0.5 | 9 | 0.25 | 4 | * 6 | YES |
| ABUTMENT NO. 2 | EXTERIOR | 3.83 | 13.5 | 14 | 10 | 8 | 0.5 | 9 | 0.25 | 4 | * 6 | YES |

* INCLUDES 1/4" FOR SOLE PLATE AND 1/8" FOR ELASTOMERIC PAD.



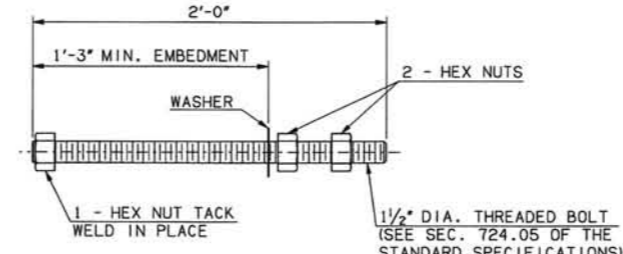
NON-SLIDING BASE PLATE

SCALE: NONE



BEVELED ANCHOR PLATE DETAIL

BEVELED ANCHOR PLATE IS REQUIRED WHEN ANGLE BETWEEN UNDERSIDE OF GIRDER AND HORIZONTAL EXCEEDS 1%. ORIENT BEVEL SLOPE TO MATCH PROFILE GRADE AT PIER OR ABUTMENT.



DETAIL OF ANCHOR BOLT

NOTES:

- ANCHOR BOLTS SHALL BE CENTERED IN SLOTS DURING SETTING OF BEAMS. DIMENSION MAY VARY AT EXPANSION BEARING DEPENDING ON TEMPERATURE AT THE TIME OF BEAM SETTING.
- STRUCTURAL STEEL FOR ANCHOR PLATES, CONTACT PLATES, AND ANCHOR BOLTS SHALL CONFORM TO AASHTO M270 (ASTM A709), GRADE 50W (WEATHERING STEEL, CHARPY V-NOTCH TESTING NOT REQUIRED). NUTS, WASHERS AND WELDING SHALL HAVE WEATHERING CHARACTERISTICS.
- VULCANIZE PAD ACCORDING TO ODOT SPECIFICATION 733.06 (b) WHEN NOTED.

| | | | |
|----------|--------|-------|-----------------------------------|
| Design | DE | 9/07 | I-40 CROSSTOWN OKLAHOMA COUNTY |
| Drawn | RWS | 9/07 | |
| Checked | KJ | 06/10 | |
| Approved | SRJ | 06/10 | |
| Squad | BENHAM | | |

I-40 RELOCATION BRIDGE I-1
BEARING PAD DETAILS

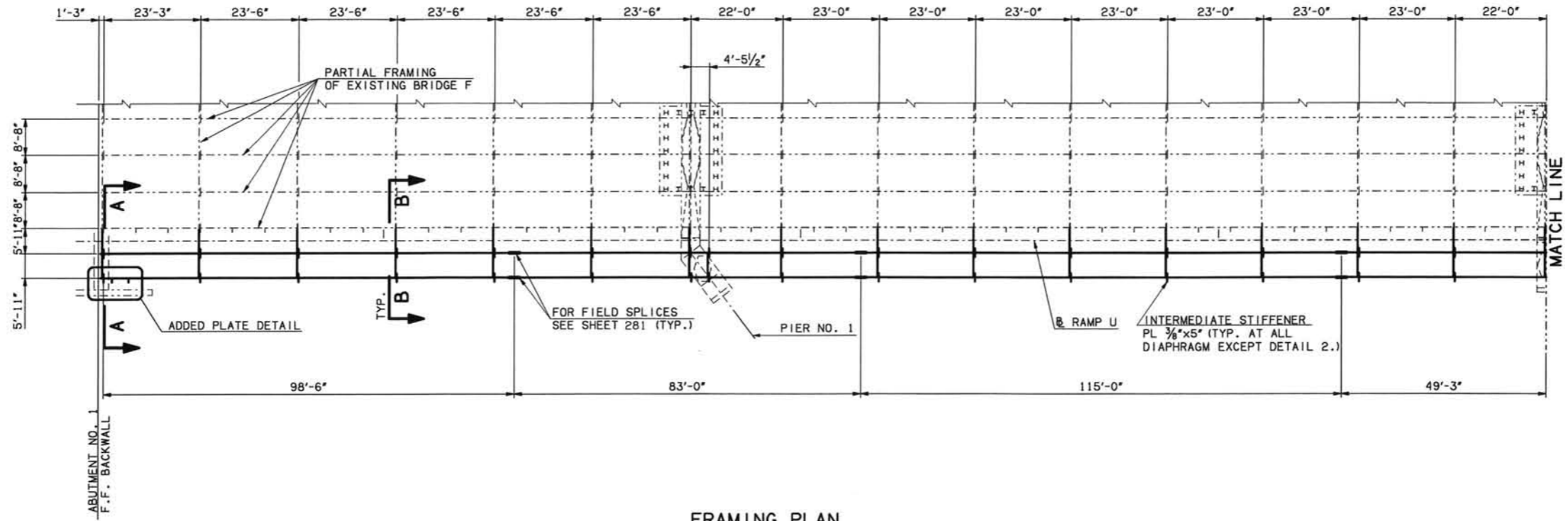
Job Piece No. 17428(34) Sheet No. 275

| FED. ROAD DIST. NO. | STATE | JOB PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|---------------|-------------|-----------|--------------|
| 6 | OKLA | 17428(34) | 10 | 278 | 484 |

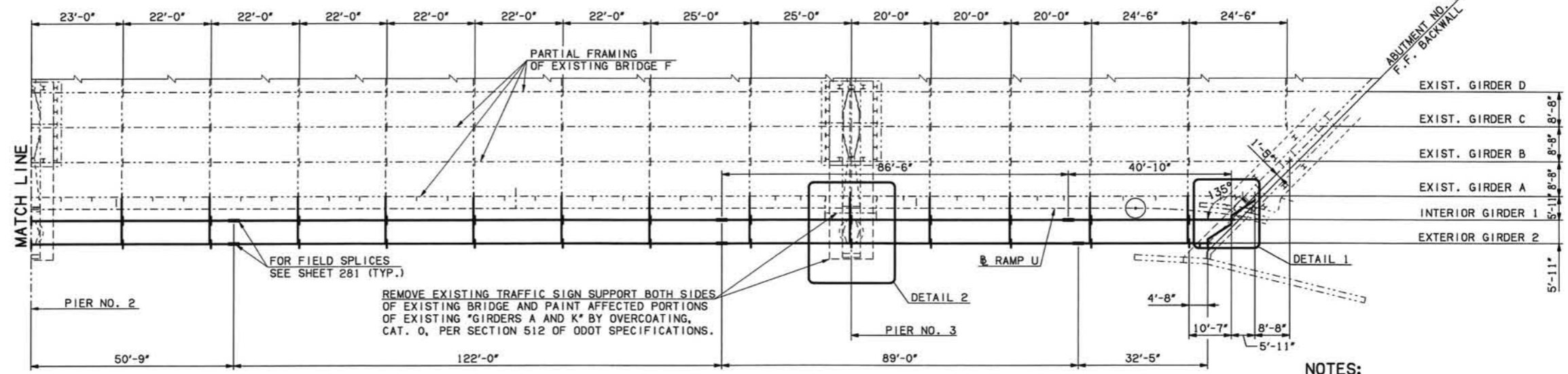
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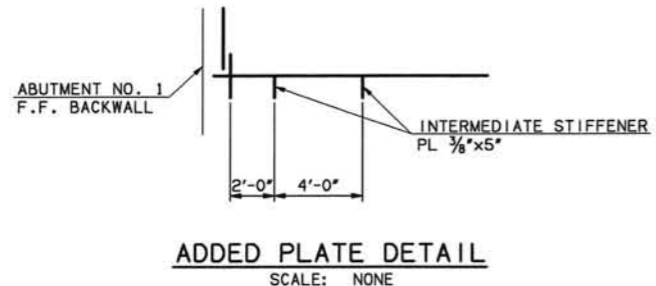
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FRAMING PLAN
SCALE: 1" = 15'-0"



FRAMING PLAN
SCALE: 1" = 15'-0"



ADDED PLATE DETAIL
SCALE: NONE

- NOTES:**
1. GENERAL STEEL NOTES SEE SHEET 278.
 2. FRAMING PLAN SECTIONS AND DETAILS SEE SHEETS 277 & 278.
 3. GIRDER ELEVATION SEE SHEET 279 & 280.
 4. FIELD SPLICE DETAILS SEE SHEET 281.
 5. DEAD LOAD DEFLECTION DETAILS SEE SHEET 282.
 6. INCLUDED ALL COST OF TOUCHING UP PAINT ON EXISTING GIRDERS IN PRICE BID FOR PAINTING EXISTING STRUCTURES.

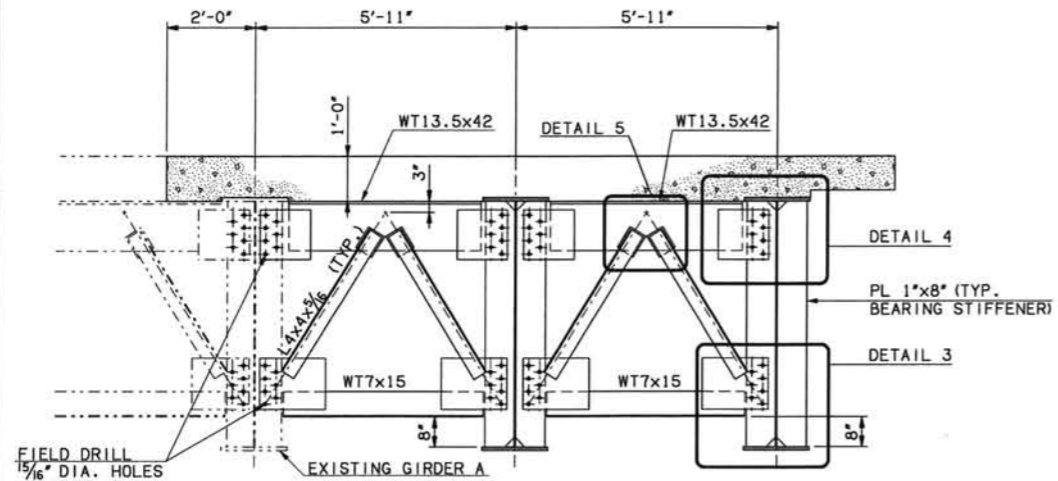
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|----------|--------|-------|-------------------------------|-----------------|
| Design | DE | 5/05 | I-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RWS | 2/06 | I-40 RELOCATION BRIDGE 1-1 | |
| Checked | DE | 06/10 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 06/10 | FRAMING PLAN | |
| Squad | BENHAM | | Job Piece No. 17428(34) | Sheet No. 276 |

6/15/2010

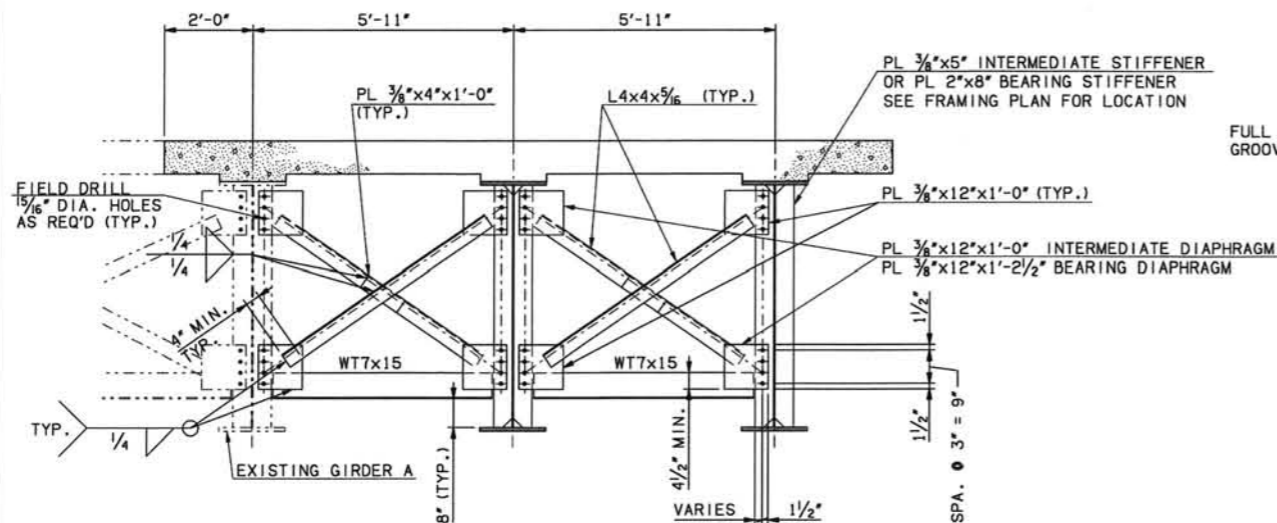
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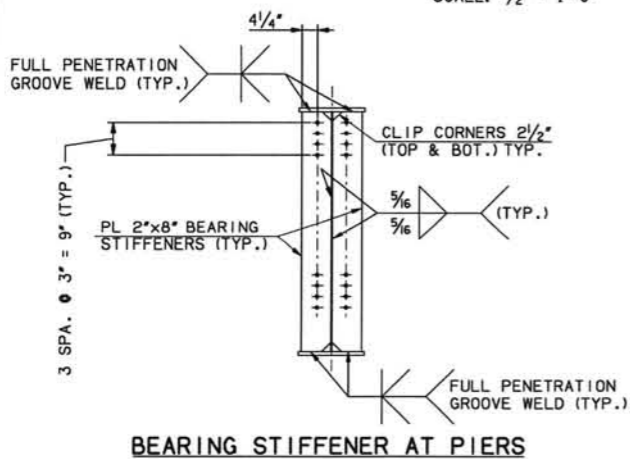
| DESIGN | DATE | BY | CHKD | TOTAL SHEETS | CURRENT SHEET |
|--------|-------|-----|------|--------------|---------------|
| 1 | 06/10 | SRJ | DE | 278 | 277 |



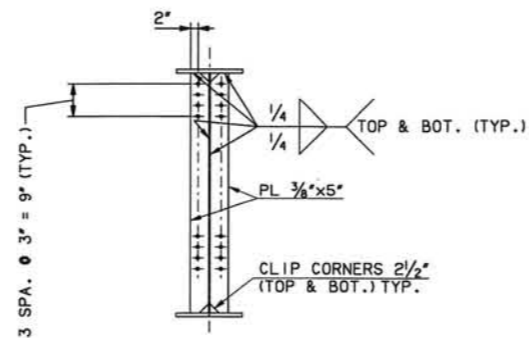
DIAPHRAGM AT ABUTMENT NO. 1
DIAPHRAGM AT ABUTMENT NO. 2 SIMILAR SEE DETAIL 1
SECTION A-A
SCALE: 1/2" = 1'-0"



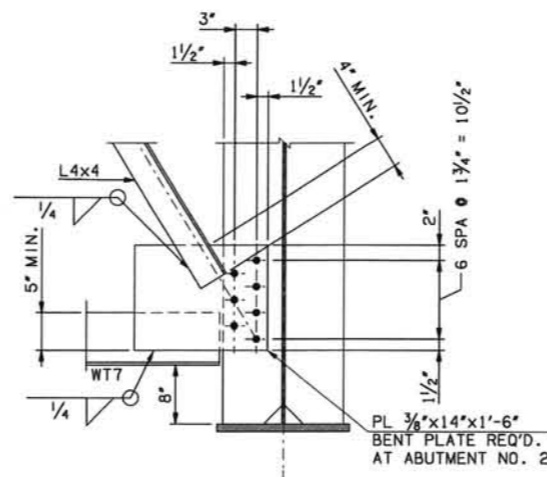
TYPICAL INTERMEDIATE DIAPHRAGMS AND
INTERMEDIATE DIAPHRAGM AT PIER NO. 1, 2 & 3
SECTION B-B
SCALE: 1/2" = 1'-0"



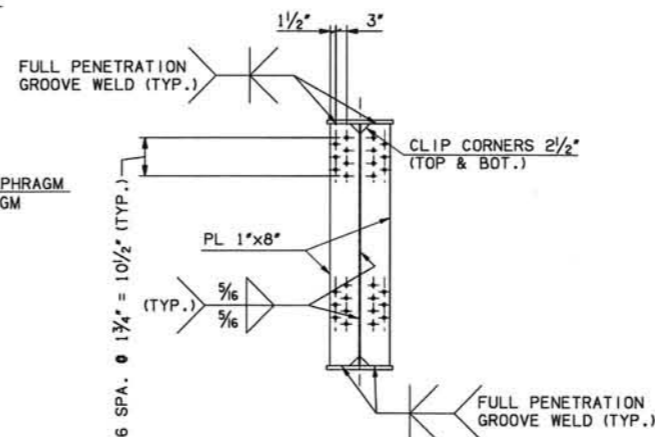
BEARING STIFFENER AT PIERS



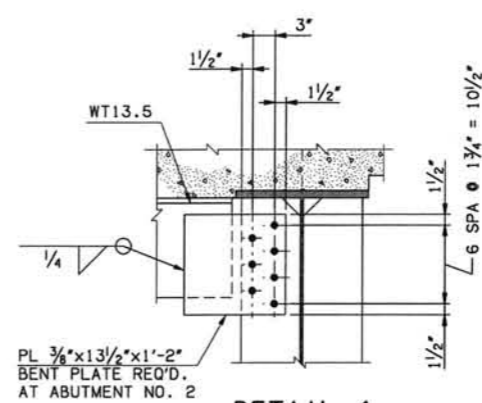
INTERMEDIATE STIFFENER



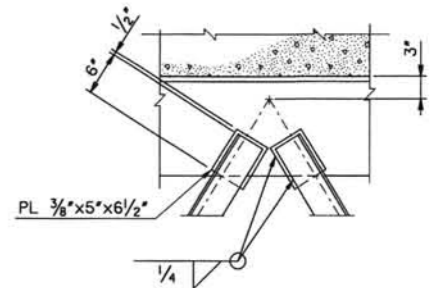
DETAIL 3
SCALE: 1" = 1'-0"



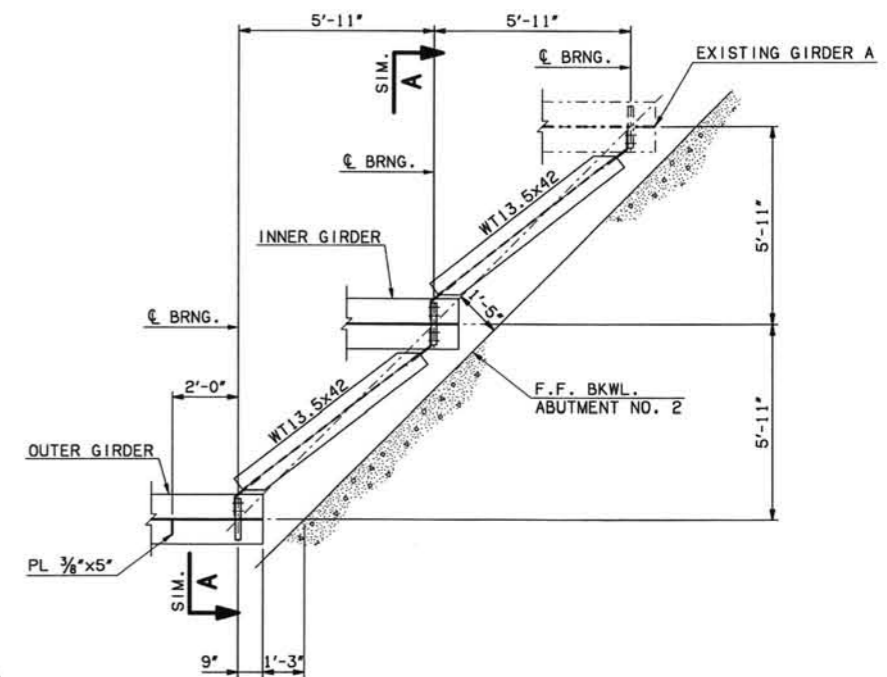
BEARING STIFFENERS AT ABUTMENTS



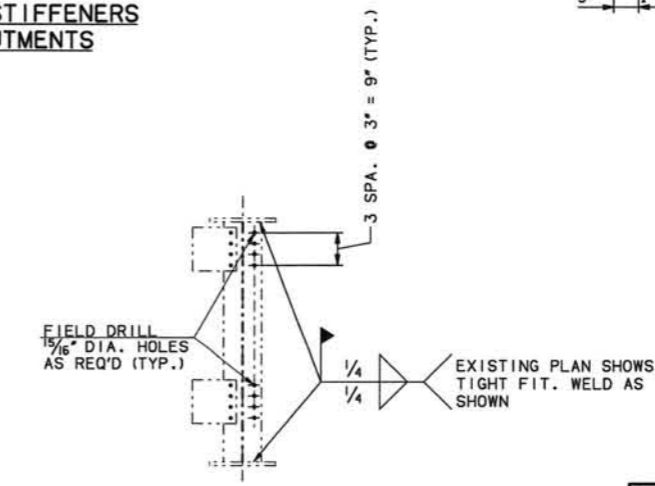
DETAIL 4
SCALE: 1" = 1'-0"



DETAIL 5
SCALE: 1" = 1'-0"



DETAIL 1
SCALE: 3/8" = 1'-0"



EXISTING INTERMEDIATE STIFFENER

NOTE:

1. GENERAL STEEL NOTES AND FOR DETAILS AT PIER NO. 3 SEE SHEET 278.
2. FRAMING PLAN & DETAILS SEE SHEETS 276 & 278.
3. GIRDER ELEVATION SEE SHEET 279 & 280.
4. FIELD SPICE DETAILS SEE SHEET 281.
5. DEAD LOAD DEFLECTION DETAILS SEE SHEET 282.

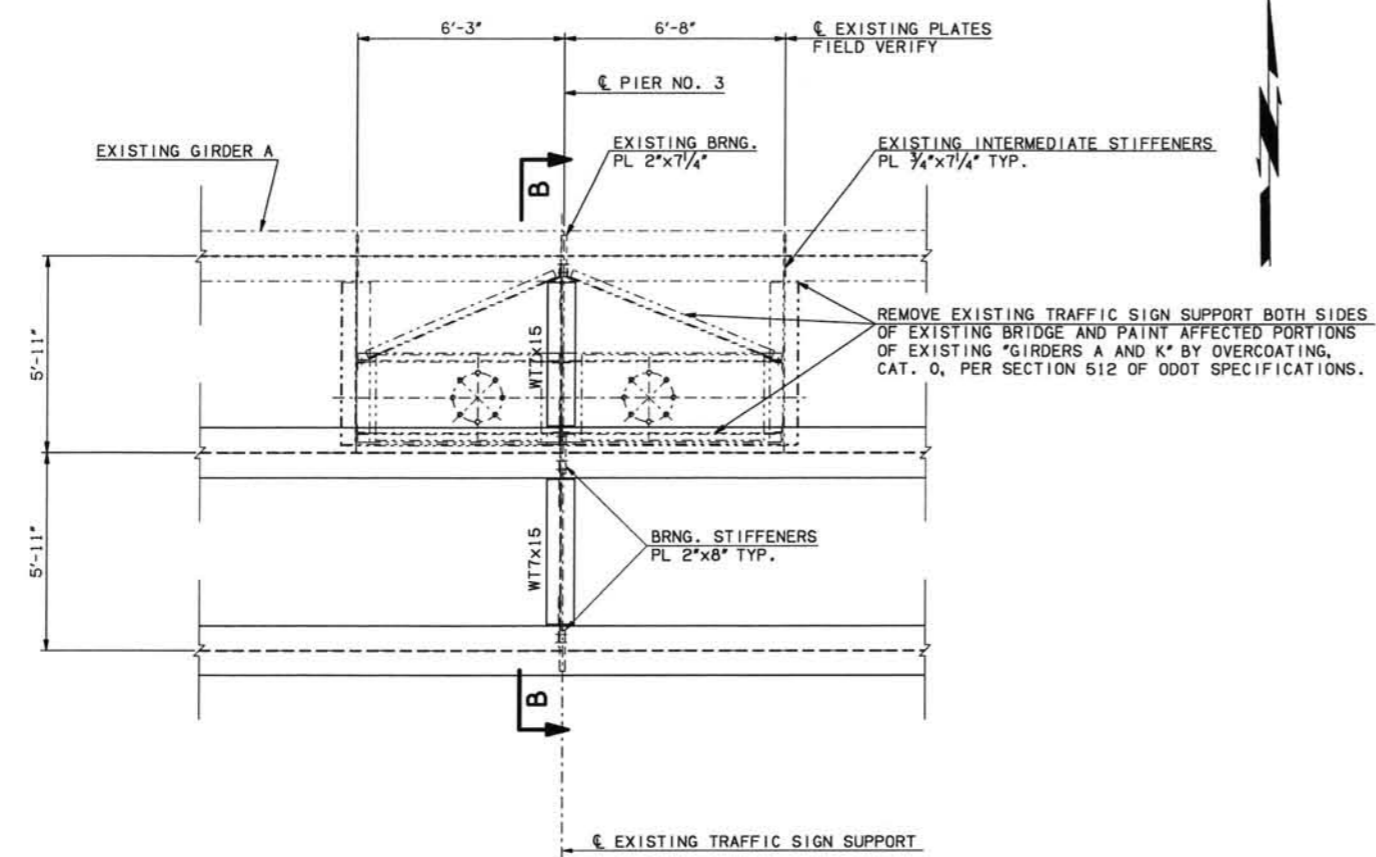
| Design | DE | 9/07 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
|----------|--------|-------|--|-----------------|
| Drawn | RWS | 8/07 | 1-40 RELOCATION BRIDGE 1-1 | |
| Checked | DE | 06/10 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 06/10 | FRAMING PLAN DETAILS & SECTIONS | |
| Squad | BENHAM | | Job Piece No. 17428(34) | Sheet No. 277 |

6/15/2010

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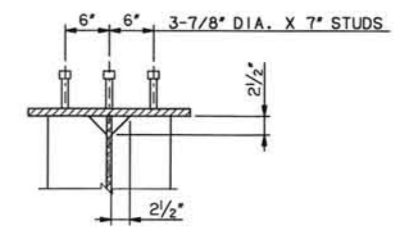
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| FED ROAD DIST NO | STATE | JOB PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|------------------|-------|-------------|-------------|----------|--------------|
| 6 | OKLA | 1742830 | 10 | 278 | 484 |



PLAN - REMOVE EXISTING TRAFFIC SIGN SUPPORT AT PIER NO. 3

DETAIL 2
SCALE: 3/8" = 1'-0"



STIFFENER CLIP AND SHEAR STUDS
SCALE: 1" = 1'-0"

GENERAL STEEL NOTES:

- ALL FLANGE PLATES, WEB PLATES, STIFFENER PLATES AND FIELD SPLICE PLATES SHALL CONFORM TO THE CHARPY V-NOTCH REQUIREMENTS.
- ALL STRUCTURAL STEEL SHALL BE M270 GRADE 50W STEEL, EXCEPT EMBEDDED ANCHOR PLATES, WHICH MAY BE A36.
- ALL DIAPHRAGM BOLTED CONNECTIONS SHALL BE 7/8" DIA. HIGH STRENGTH BOLTS (A325) WITH DIRECT TENSION INDICATORS AS SPECIFIED IN SECTION 506 OF THE STANDARD SPECIFICATIONS. THE "CALIBRATED WRENCH" METHOD SHALL NOT BE USED. ALL BOLT HOLES SHALL BE 15/16" DIA UNLESS NOTED.
- ALL FIELD SPLICE BOLTED CONNECTIONS SHALL BE 3/4" DIA. HIGH STRENGTH BOLTS (A325) WITH DIRECT TENSION INDICATORS AS SPECIFIED IN SECTION 506 OF THE STANDARD SPECIFICATIONS. THE "CALIBRATED WRENCH" METHOD SHALL NOT BE USED. ALL BOLT HOLES SHALL BE 7/8" DIA UNLESS NOTED.
- BEARING TO BEARING LENGTH IS TAKEN ALONG THE GIRDER WEB AND TOP FLANGE WITH DIAPHRAGMS PLACED AS SHOWN ON THE FRAMING PLAN.
- GIRDERS SHOWN ON THE GIRDER ELEVATION SHEETS ARE DRAWN AND DIMENSIONS SHOWN AS IF THE TOP FLANGE OF GIRDERS WERE IN A TRULY HORIZONTAL POSITION. SHOP DRAWINGS SHALL INCLUDE ADJUSTMENTS AS NECESSARY TO ACCOUNT FOR VERTICAL CURVE AND DEAD LOAD DEFLECTIONS.
- ALL FILLET WELDS SHALL BE TERMINATED 3/8" +/- 1/8" FROM EDGES OF STIFFENERS AS PER AWS D1.5 SECT 9.15.
- DRIP PLATES SHALL BE PLACED ON THE OUTSIDE OF THE EXTERIOR GIRDERS ON THE UP GRADE SIDE OF ABUTMENTS AND PIERS 1 AND 3.
- ALL COST OF DRIP PLATE, WELD, CAULK, AND LABOR NEEDED FOR INSTALLATION SHALL BE INCLUDED IN THE UNIT PRICE BID PER LB. FOR "STRUCTURAL STEEL."
- FRAMING PLAN & DETAILS SEE SHEETS 276 & 277.
- GIRDER ELEVATION SEE SHEET 279 & 280.
- FIELD SPLICE DETAILS SEE SHEET 281.
- DEAD LOAD DEFLECTION DETAILS SEE SHEET 282.

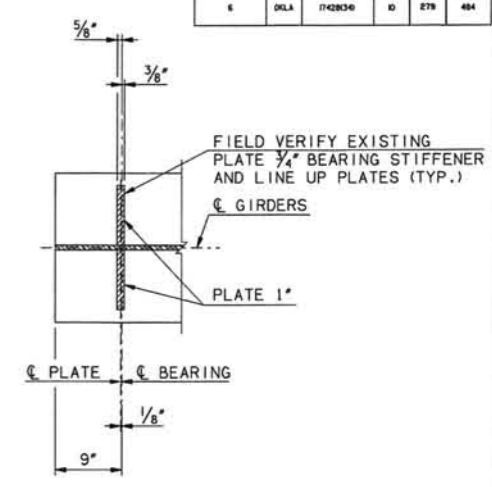
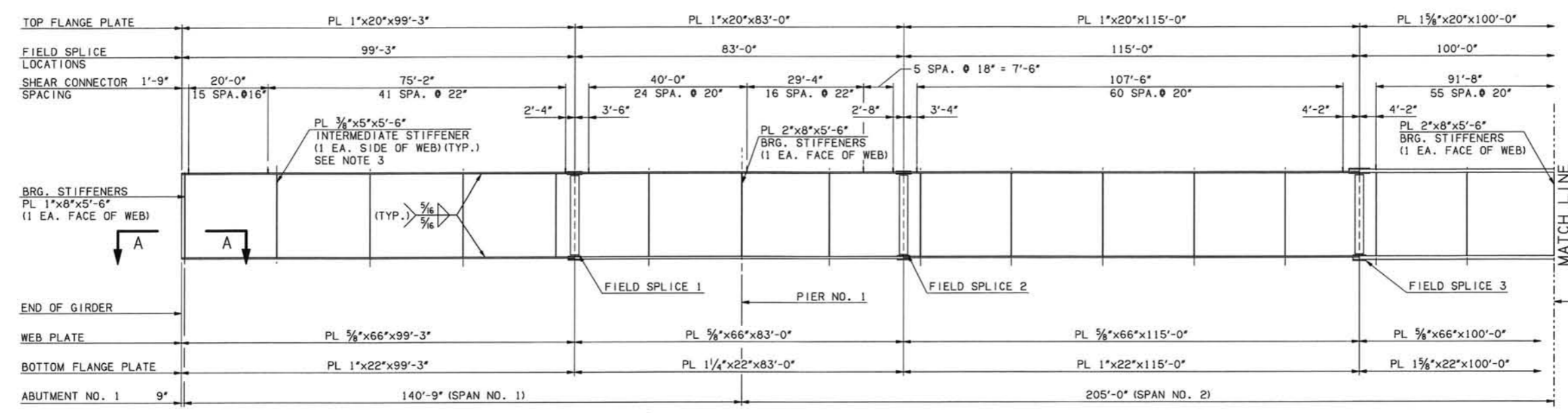
| | | | | |
|----------|--------|-------|--|-----------------|
| Design | DE | 8/07 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RWS | 8/07 | 1-40 RELOCATION BRIDGE 1-1 | |
| Checked | DE | 06/10 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 06/10 | FRAMING PLAN DETAILS & SECTIONS | |
| Squad | BENHAM | | Job Piece No. 17428(34) | Sheet No. 278 |

| FILE NO. | STATE | J.P.P. PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|----------|-------|------------------|-------------|-----------|--------------|
| 6 | OKLA | 17428(34) | 0 | 279 | 484 |

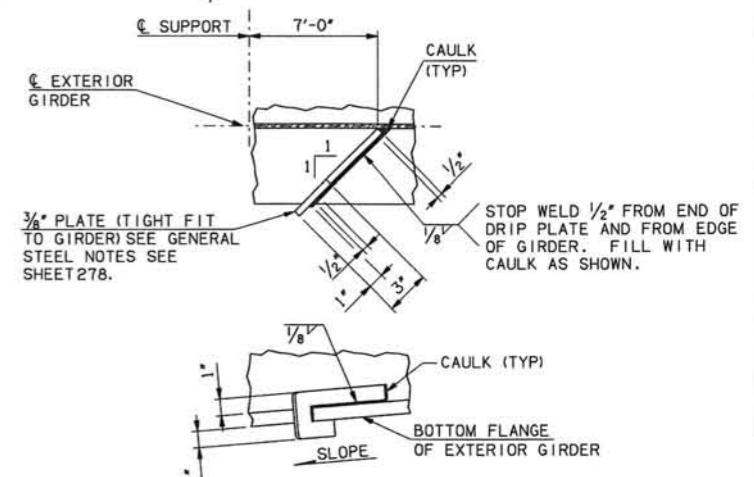
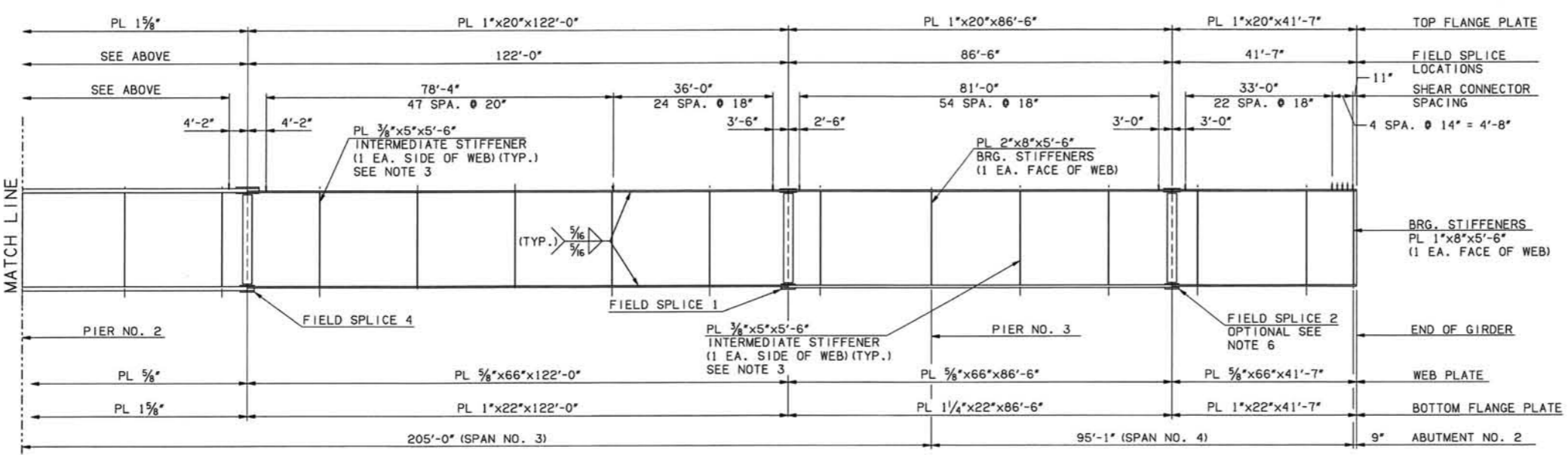
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SECTION A-A
SCALE: 1" = 1'-0"



TYPICAL DRIP PLATE DETAIL FOR EXTERIOR GIRDERS
SCALE: NONE

- NOTES:
1. ALL DIMENSIONS SHOWN ARE HORIZONTAL.
 2. GENERAL STEEL NOTES, STIFFENER DETAILS, SHEAR CONNECTORS, DETAILS AND SECTIONS SEE SHEETS 277 AND 278.
 3. FRAMING PLAN SEE SHEET 276.
 4. FIELD SPLICE DETAILS SEE SHEET 281.
 5. DEAD LOAD DEFLECTIONS SEE SHEET 282.
 6. IF OMITTING OPTIONAL FIELD SPLICE, PLACE ADDITIONAL SHEAR STUD ROWS @ 18" SPACING.

INTERIOR GIRDER NO.1 ELEVATION

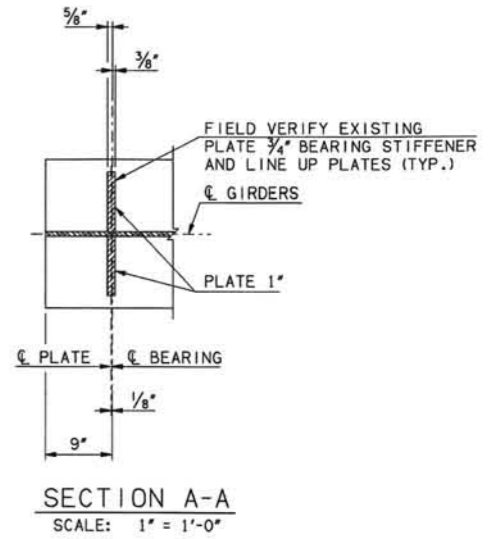
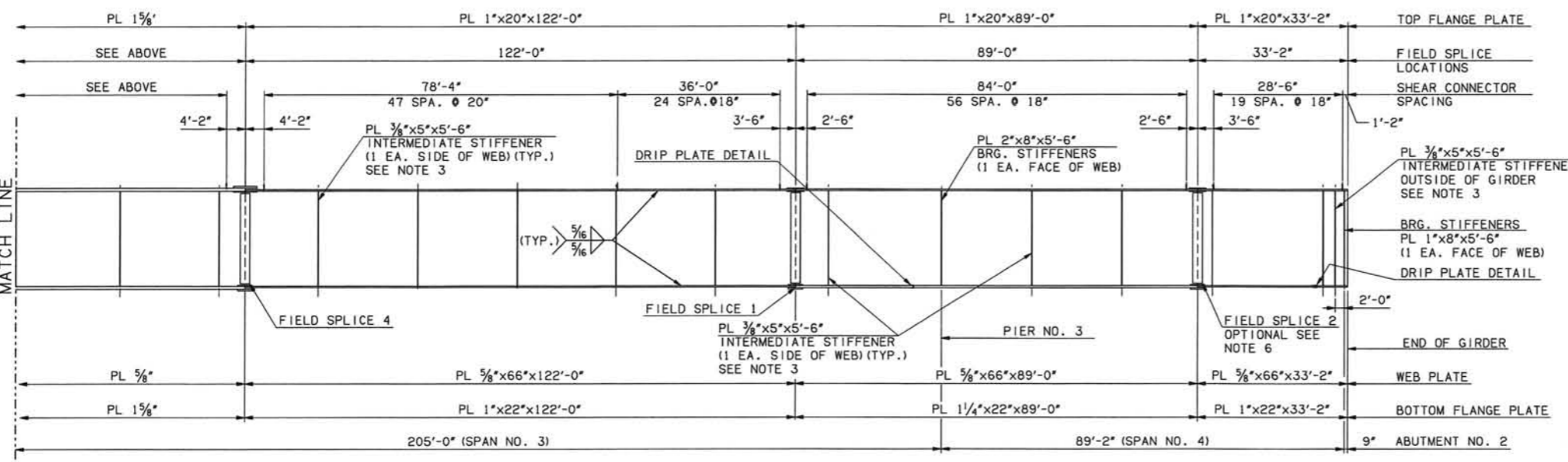
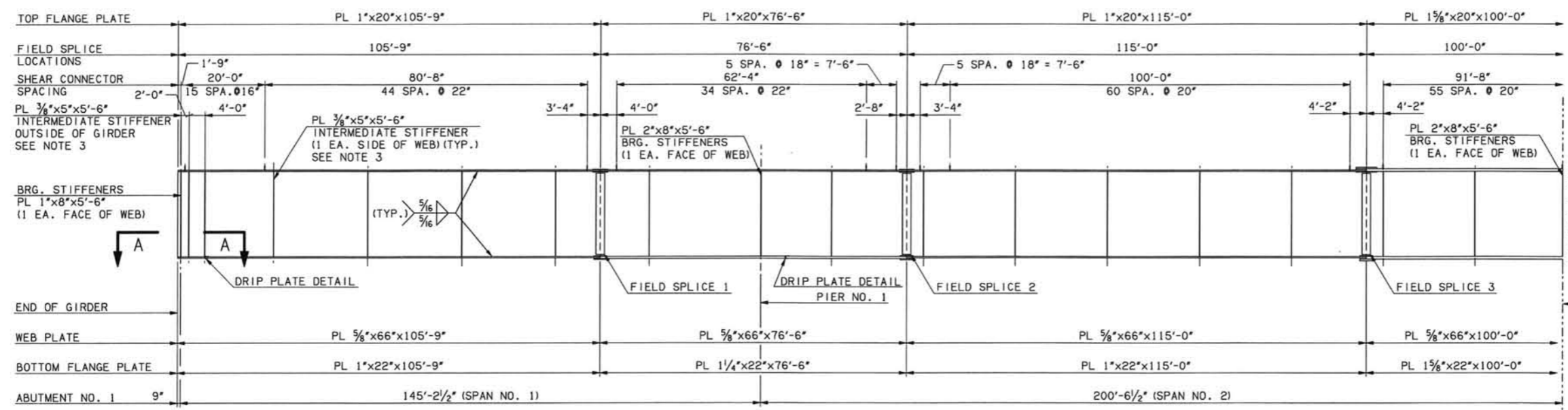
| | | | | |
|----------|--------|-------|----------------------------|-----------------|
| Design | DE | 11/07 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RWS | 11/07 | 1-40 RELOCATION BRIDGE 1-1 | |
| Checked | DE | 06/10 | GIRDER ELEVATION DETAILS | |
| Approved | SRJ | 06/10 | INTERIOR GIRDER NO. 1 | |
| Squad | BENHAM | | Job Piece No. 17428(34) | Sheet No. 279 |

| FED. ROAD DIST. NO. | STATE | J.P.P. PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|------------------|-------------|-----------|--------------|
| 6 | OKLA | 17428(34) | 0 | 280 | 484 |

6/15/2010

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- NOTES:**
1. ALL DIMENSIONS SHOWN ARE HORIZONTAL.
 2. GENERAL STEEL NOTES, STIFFENER DETAILS, SHEAR CONNECTORS, DETAILS AND SECTIONS SEE SHEETS 277 AND 278.
 3. FRAMING PLAN SEE SHEET 276.
 4. FIELD SPLICE DETAILS SEE SHEET 281.
 5. DEAD LOAD DEFLECTIONS SEE SHEET 282.
 6. IF OMITTING OPTIONAL FIELD SPLICE, PLACE ADDITIONAL SHEAR STUD ROWS @ 18" SPACING.

EXTERIOR GIRDER NO.2 ELEVATION

| | | | | |
|----------|--------|-------|---|-----------------|
| Design | DE | 11/07 | I-40 CROSSTOWN I-40 RELOCATION BRIDGE I-1 GIRDER ELEVATION DETAILS EXTERIOR GIRDER NO. 2 | OKLAHOMA COUNTY |
| Drawn | RWS | 11/07 | | |
| Checked | DE | 06/10 | | |
| Approved | SRJ | 06/10 | | |
| Squad | BENHAM | | | |

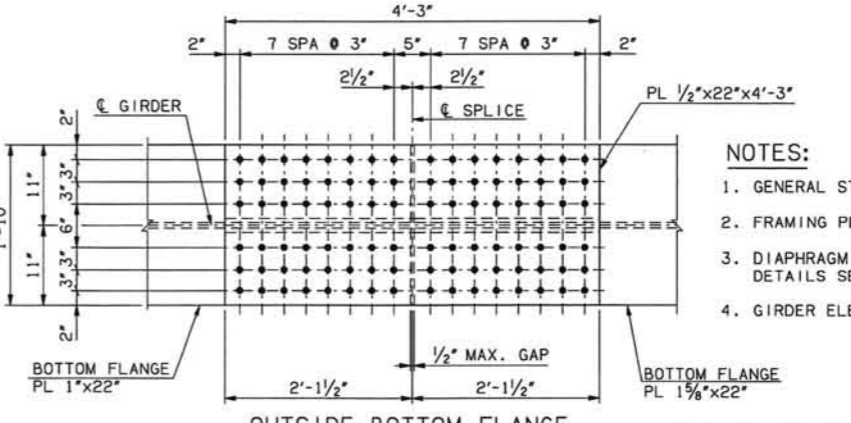
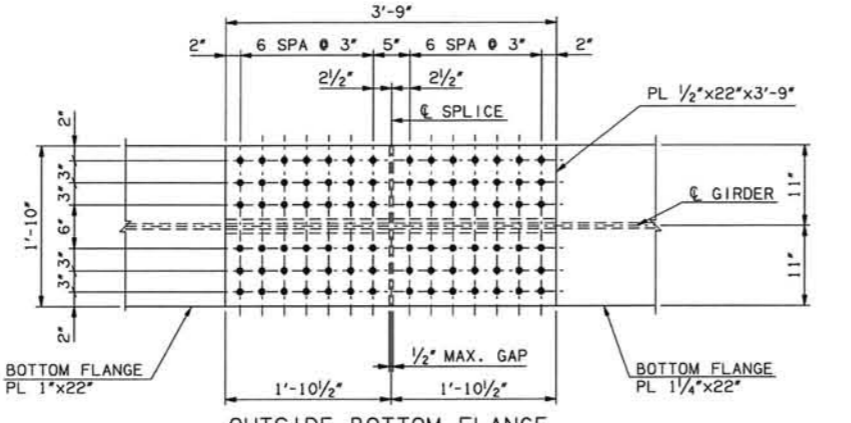
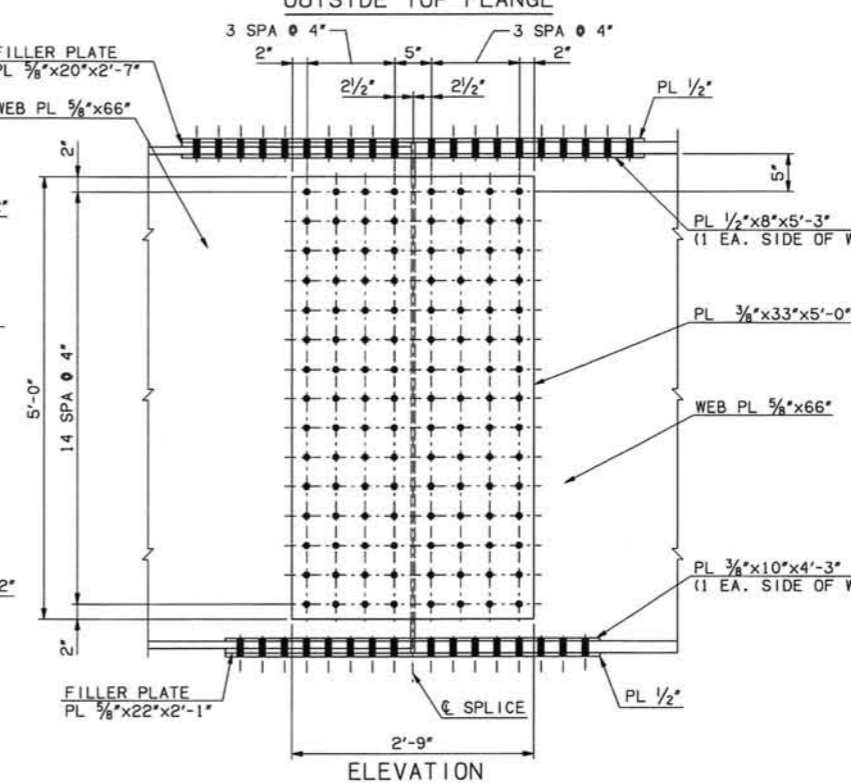
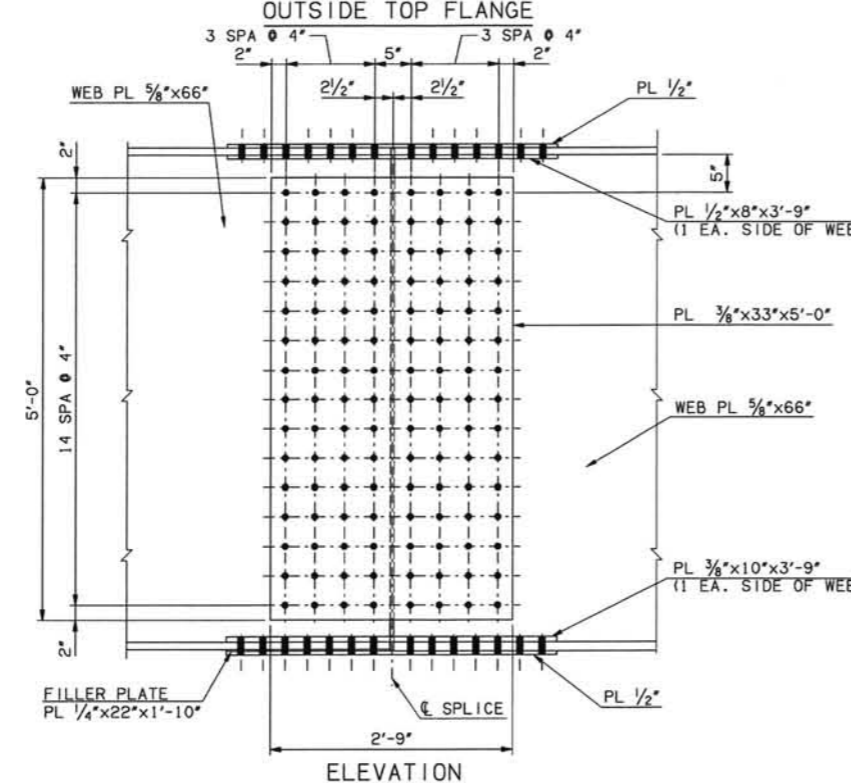
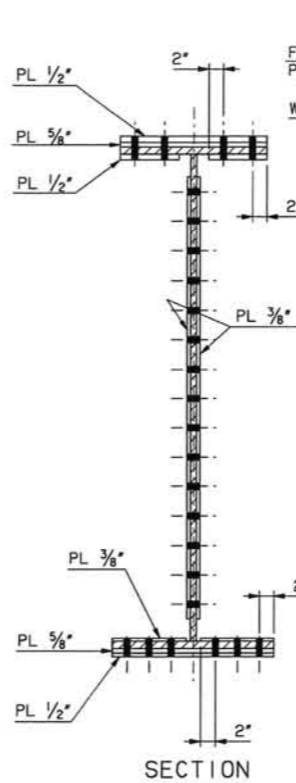
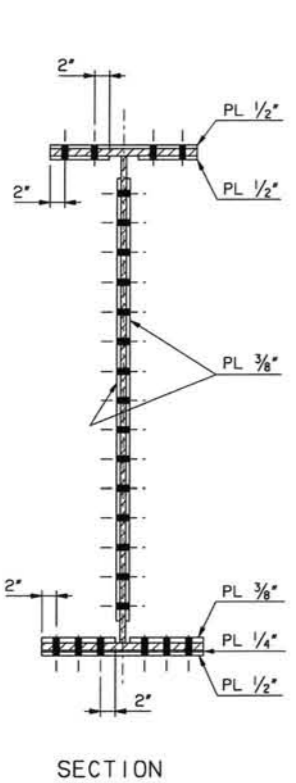
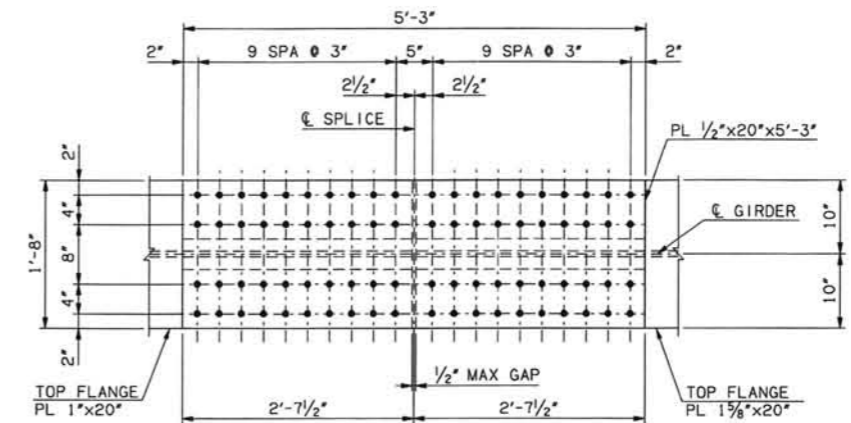
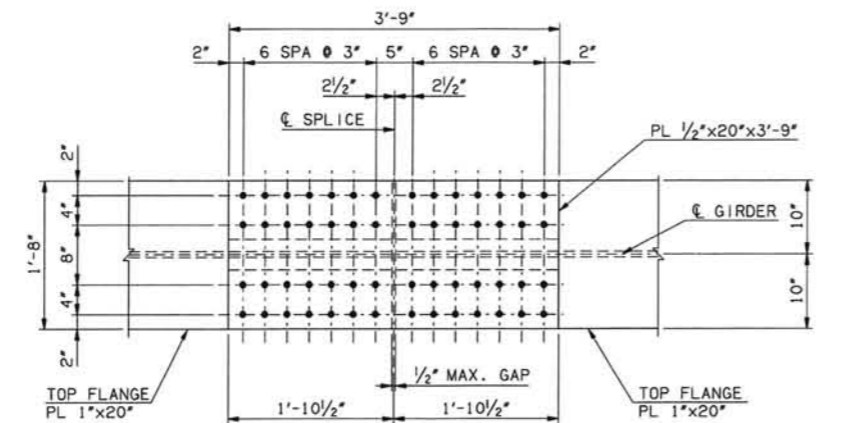
Job Piece No. 17428(34) Sheet No. 280

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| FED. ROAD DIST. NO. | STATE | JVP PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|---------------|-------------|-----------|--------------|
| 6 | OKLA | 1742834 | 10 | 281 | 484 |



BOLTED FIELD SPLICE "1" - AS SHOWN
 SCALE: 1" = 1'-0"
BOLTED FIELD SPLICE "2" - OPPOSITE HAND
 SCALE: 1" = 1'-0"

BOLTED FIELD SPLICE "3" - AS SHOWN
 SCALE: 1" = 1'-0"
BOLTED FIELD SPLICE "4" - OPPOSITE HAND
 SCALE: 1" = 1'-0"

- NOTES:**
1. GENERAL STEEL NOTES, TRAFFIC SIGN SUPPORT SEE SHEET 278.
 2. FRAMING PLAN SEE SHEET 276.
 3. DIAPHRAGM DETAILS, STIFFENER DETAILS, SHEAR CONNECTORS, DETAILS SEE SHEET 277 & 278.
 4. GIRDER ELEVATION SEE SHEET 279 & 280.

| | | | |
|----------|--------|-------|---|
| Design | DE | 11/07 | 1-40 CROSSTOWN OKLAHOMA COUNTY 1-40 RELOCATION BRIDGE I-1 BOLTED FIELD SPLICE DETAILS SPLICES 1, 2, 3 AND 4 |
| Drawn | RWS | 11/07 | |
| Checked | DE | 06/10 | |
| Approved | SRJ | 06/10 | |
| Squad | BENHAM | | |

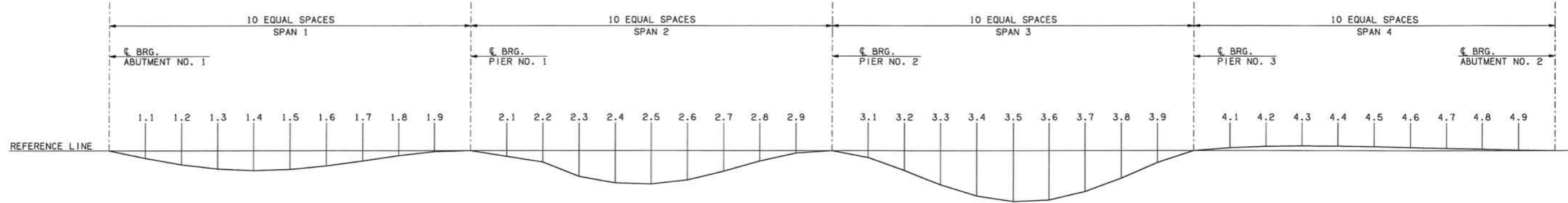
Job Piece No. 17428(34) Sheet No. 281

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| FED ROAD DIST NO | STATE | APP PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|------------------|-------|-------------|-------------|----------|--------------|
| 6 | OKLA | 17428(34) | 10 | 282 | 484 |



| | | DEAD LOAD DEFLECTION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|-------------------|----------|----------------------|------|------|------|------|------|------|------|------|-------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|-------|-------|-------|-------|-------|-------|-------|-------|-------|------|--|--|--|--|--|--|--|--|
| ORDINATES | | - | 1.1 | 1.2 | 1.3 | 1.4 | 1.5 | 1.6 | 1.7 | 1.8 | 1.9 | - | 2.1 | 2.2 | 2.3 | 2.4 | 2.5 | 2.6 | 2.7 | 2.8 | 2.9 | - | 3.1 | 3.2 | 3.3 | 3.4 | 3.5 | 3.6 | 3.7 | 3.8 | 3.9 | - | 4.1 | 4.2 | 4.3 | 4.4 | 4.5 | 4.6 | 4.7 | 4.8 | 4.9 | - | | | | | | | | |
| GIRDER 1 INTERIOR | STEEL | 0.00 | 0.19 | 0.35 | 0.45 | 0.49 | 0.46 | 0.38 | 0.25 | 0.12 | 0.02 | 0.00 | 0.15 | 0.40 | 0.65 | 0.82 | 0.85 | 0.75 | 0.53 | 0.28 | 0.06 | 0.00 | 0.18 | 0.51 | 0.87 | 1.16 | 1.29 | 1.25 | 1.03 | 0.69 | 0.30 | 0.00 | -0.07 | -0.11 | -0.12 | -0.11 | -0.10 | -0.08 | -0.06 | -0.04 | -0.02 | 0.00 | | | | | | | | |
| | CONCRETE | 0.00 | 0.57 | 1.04 | 1.36 | 1.48 | 1.40 | 1.14 | 0.78 | 0.38 | 0.08 | 0.00 | 0.42 | 1.15 | 1.88 | 2.36 | 2.45 | 2.13 | 1.50 | 0.76 | 0.17 | 0.00 | 0.51 | 1.45 | 2.49 | 3.34 | 3.77 | 3.66 | 3.03 | 2.02 | 0.89 | 0.00 | -0.21 | -0.32 | -0.36 | -0.34 | -0.30 | -0.24 | -0.17 | -0.11 | -0.05 | 0.00 | | | | | | | | |
| | TOTAL | 0.00 | 0.76 | 1.39 | 1.81 | 1.97 | 1.86 | 1.52 | 1.03 | 0.50 | 0.10 | 0.00 | 0.57 | 1.55 | 2.53 | 3.18 | 3.30 | 2.88 | 2.03 | 1.04 | 0.23 | 0.00 | 0.69 | 1.96 | 3.36 | 4.50 | 5.06 | 4.91 | 4.06 | 2.71 | 1.19 | 0.00 | -0.28 | -0.43 | -0.48 | -0.45 | -0.40 | -0.32 | -0.23 | -0.15 | -0.07 | 0.00 | | | | | | | | |
| GIRDER 2 EXTERIOR | STEEL | 0.00 | 0.15 | 0.28 | 0.36 | 0.38 | 0.35 | 0.28 | 0.17 | 0.07 | 0.00 | 0.00 | 0.19 | 0.50 | 0.79 | 0.98 | 1.02 | 0.90 | 0.66 | 0.36 | 0.10 | 0.00 | 0.16 | 0.47 | 0.82 | 1.10 | 1.24 | 1.21 | 1.00 | 0.67 | 0.29 | 0.00 | -0.07 | -0.10 | -0.11 | -0.10 | -0.08 | -0.06 | -0.04 | -0.02 | -0.01 | 0.00 | | | | | | | | |
| | CONCRETE | 0.00 | 0.47 | 0.87 | 1.12 | 1.19 | 1.10 | 0.86 | 0.54 | 0.21 | -0.01 | 0.00 | 0.58 | 1.51 | 2.40 | 2.97 | 3.08 | 2.70 | 1.94 | 1.04 | 0.28 | 0.00 | 0.46 | 1.39 | 2.44 | 3.32 | 3.77 | 3.68 | 3.06 | 2.04 | 0.90 | 0.00 | -0.22 | -0.32 | -0.34 | -0.31 | -0.25 | -0.19 | -0.12 | -0.07 | -0.03 | 0.00 | | | | | | | | |
| | TOTAL | 0.00 | 0.62 | 1.15 | 1.48 | 1.57 | 1.45 | 1.14 | 0.71 | 0.28 | -0.01 | 0.00 | 0.77 | 2.01 | 3.19 | 3.95 | 4.10 | 3.60 | 2.60 | 1.40 | 0.38 | 0.00 | 0.62 | 1.86 | 3.26 | 4.42 | 5.01 | 4.89 | 4.06 | 2.71 | 1.19 | 0.00 | -0.29 | -0.42 | -0.45 | -0.41 | -0.33 | -0.25 | -0.16 | -0.09 | -0.04 | 0.00 | | | | | | | | |

- NOTES:**
1. DEFLECTION IS IN INCHES.
 2. POSITIVE VALUE INDICATES DEFLECTION IS DOWNWARD.
 3. CONCRETE LOAD INCLUDES SLAB, PARAPETS AND STAY-IN-PLACE FORMS.
 4. GIRDER ELEVATION SEE SHEETS 279 AND 280.

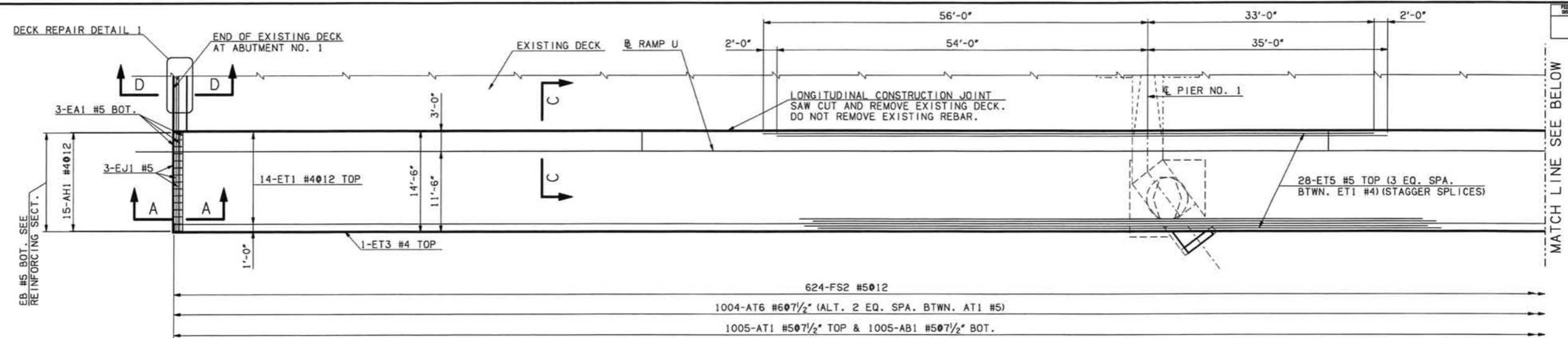
| | | | | |
|----------|--------|-------|----------------------------|-----------------|
| Design | DE | 11/07 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RWS | 11/07 | 1-40 RELOCATION BRIDGE 1-1 | |
| Checked | DE | 06/10 | DEAD LOAD DEFLECTION | |
| Approved | SRJ | 06/10 | | |
| Squad | BENHAM | | Job Piece No. 17428(34) | Sheet No. 282 |

6/15/2010

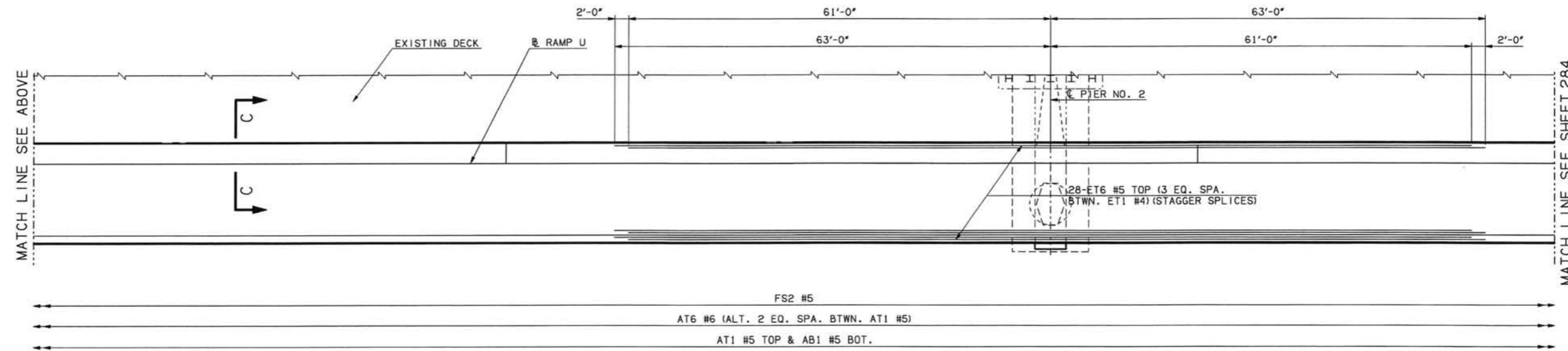
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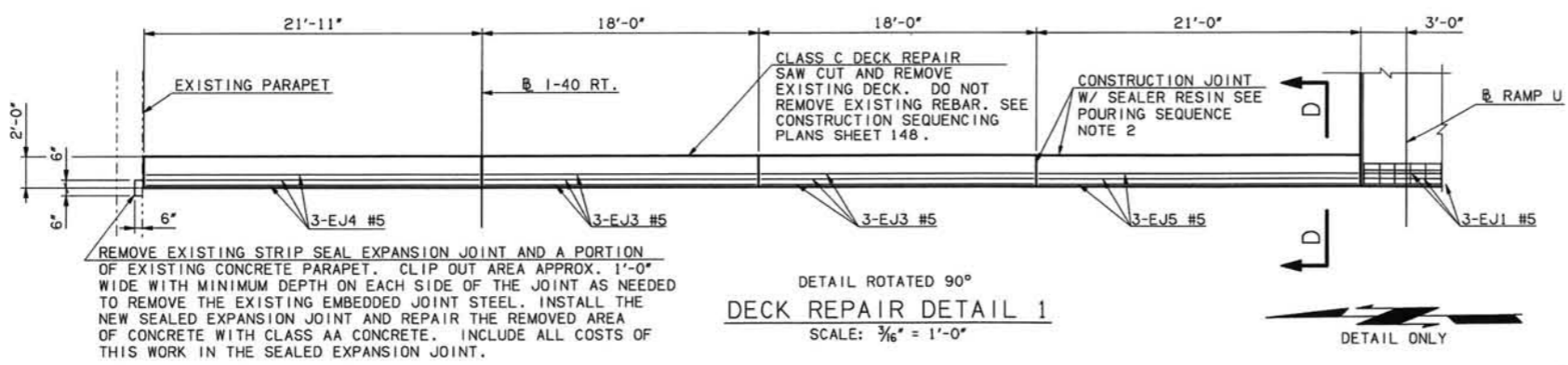
| PROJECT NO. | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|-------------|-------|-------------|-------------|----------|--------------|
| 1 | OKLA | 1742834 | 10 | 283 | 484 |



PLAN DECK REINFORCING
SCALE: 1/8" = 1'-0"



PLAN DECK REINFORCING
SCALE: 1/8" = 1'-0"



DECK REPAIR DETAIL 1
SCALE: 3/16" = 1'-0"

- NOTE:**
1. ADDITIONAL DECK REINFORCEMENT DETAILS SEE SHEETS 284 & 285.
 2. FOR TYPICAL SECTION, LONGITUDINAL SECTION AND DECK NOTES SEE SHEET 257.
 3. FOR SECTIONS, SEE SHEET 284.

| | | | | |
|----------|--------|-------|----------------------------|-----------------|
| Design | KSJ | 10/07 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RWS | 10/07 | 1-40 RELOCATION BRIDGE 1-1 | |
| Checked | KSJ | 06/10 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 06/10 | DECK REINFORCEMENT | |
| Squad | BENHAM | | Job Piece No. 17428(34) | Sheet No. 283 |

MATCH LINE SEE BELOW

MATCH LINE SEE ABOVE

MATCH LINE SEE SHEET 284

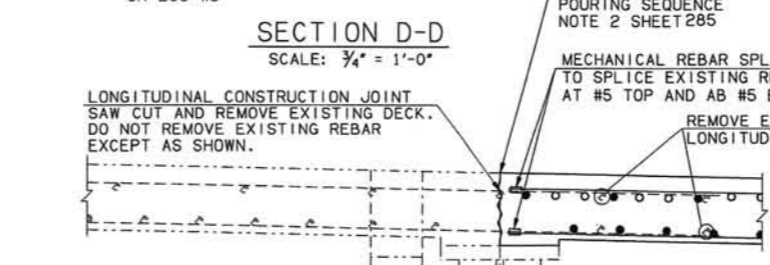
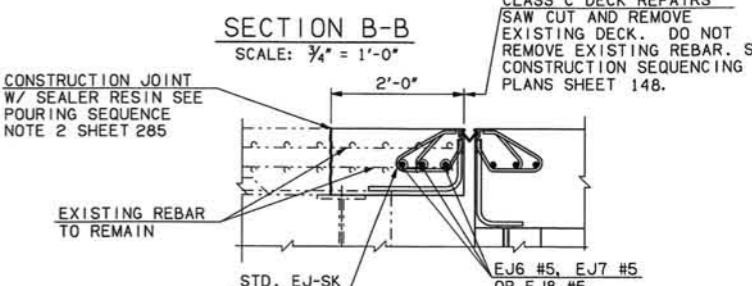
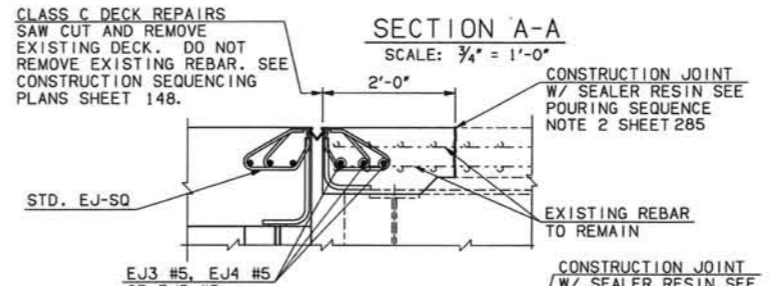
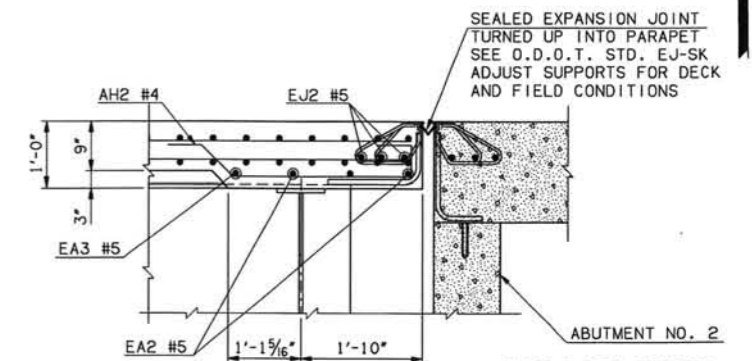
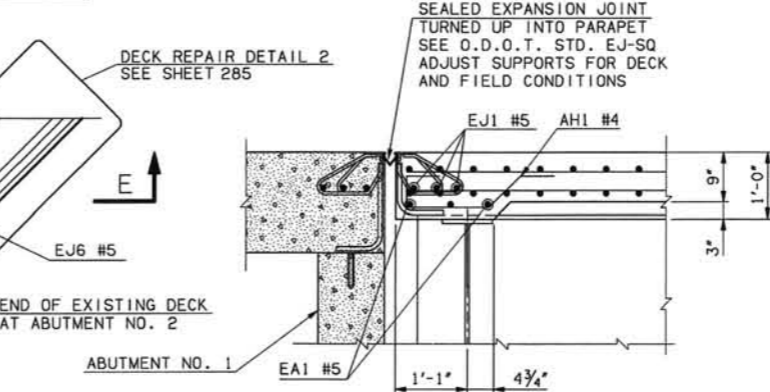
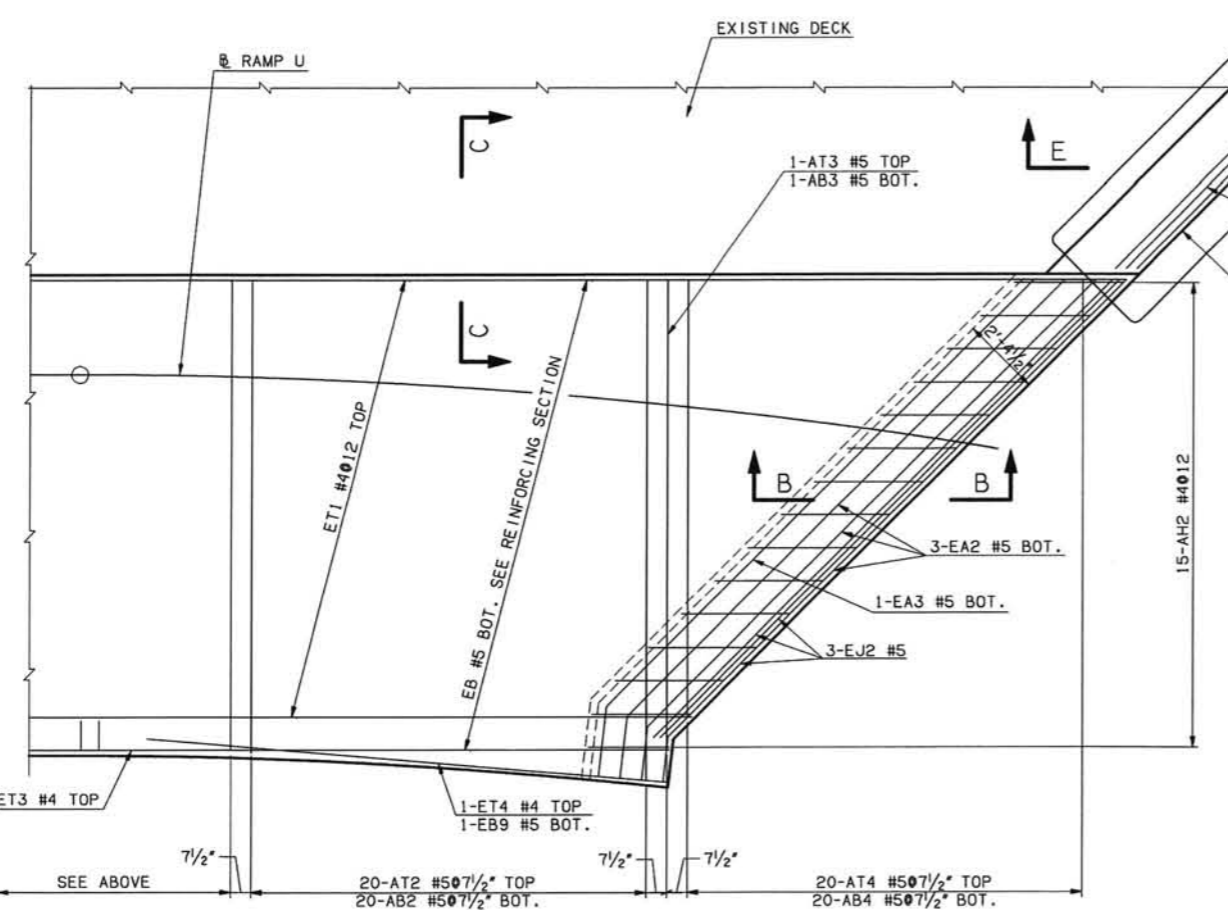
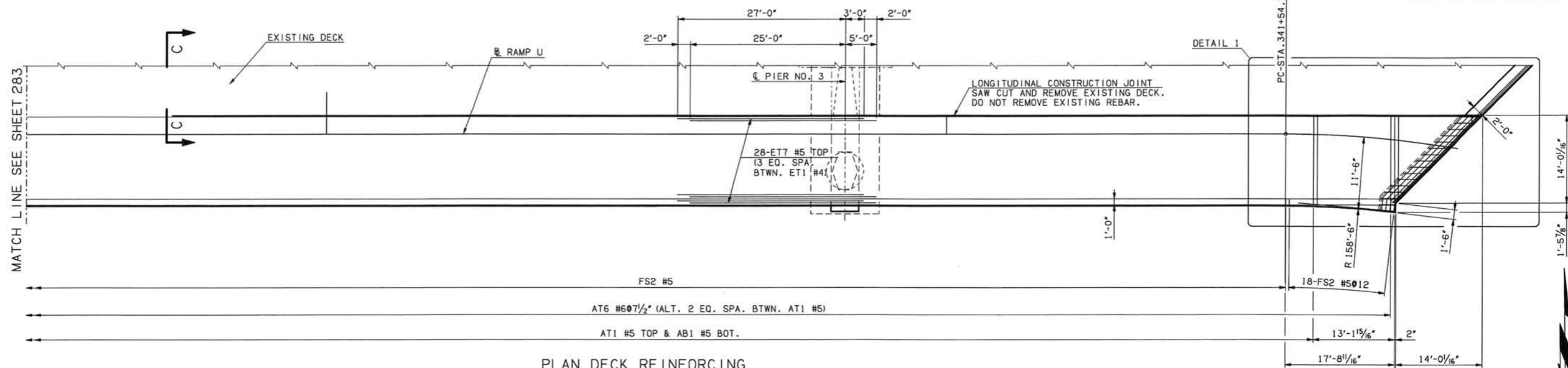
DETAIL ONLY

| FED. ROAD DIST. NO. | STATE | J.P. PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------------|-------------|-----------|--------------|
| 6 | OKLA | 17428(34) | 10 | 284 | 484 |

6/15/2010

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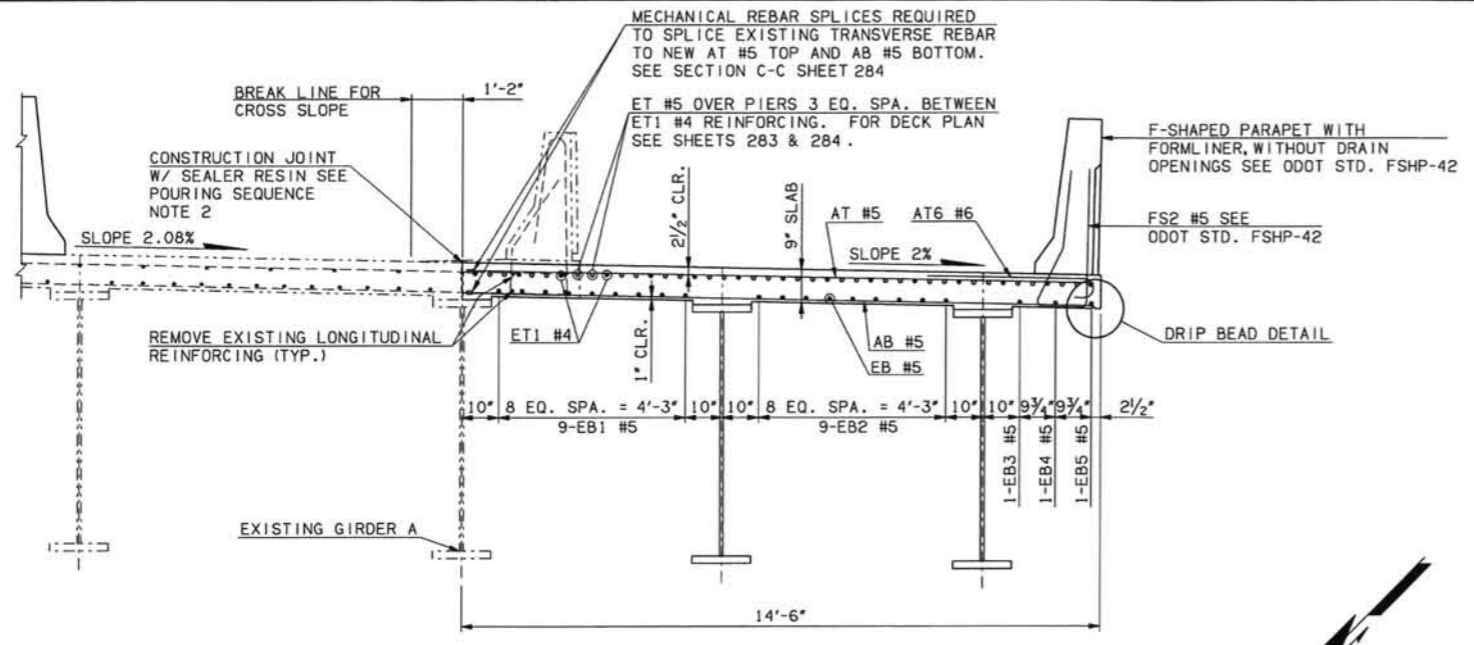
- NOTES:**
1. ADDITIONAL DECK REINFORCEMENT DETAILS SEE SHEETS 283 & 285.
 2. FOR TYPICAL SECTION, LONGITUDINAL SECTION AND DECK NOTES SEE SHEET 257.

| | | | | |
|----------|--------|-------|----------------------------|-----------------|
| Design | KSJ | 10/07 | I-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RWS | 10/07 | 1-40 RELOCATION BRIDGE 1-1 | |
| Checked | KSJ | 06/10 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 06/10 | DECK REINFORCEMENT | |
| Squad | BENHAM | | Job Piece No. 17428(34) | Sheet No. 284 |

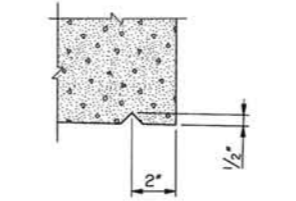
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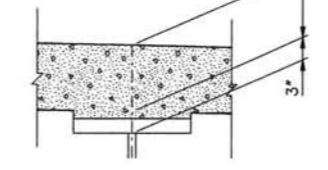
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REINFORCEMENT SECTION
SCALE: 1/2" = 1'-0"



DRIP BEAD DETAIL
SCALE: NONE

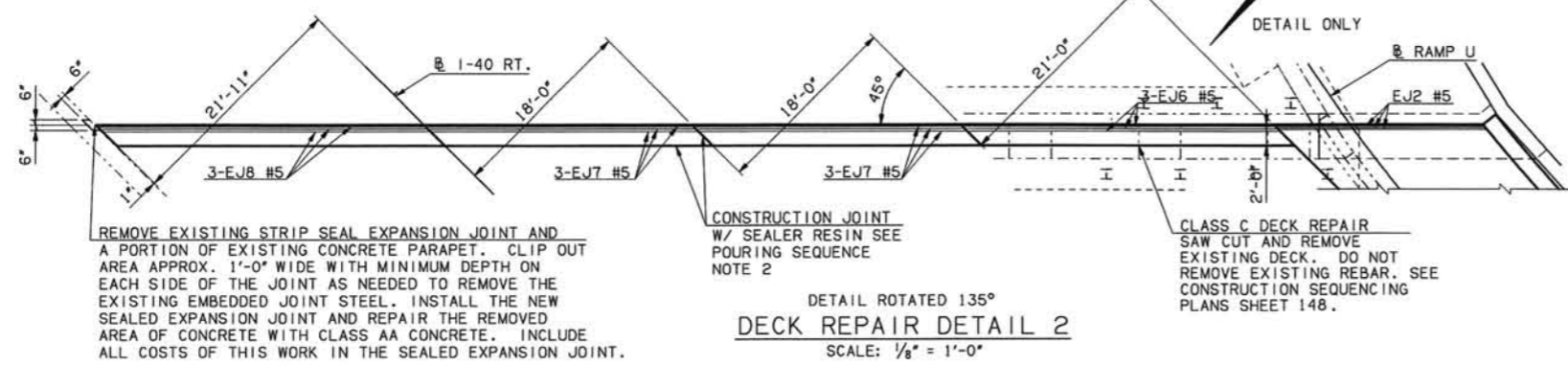


TYPICAL HAUNCH DETAIL
SCALE: 1" = 1'-0"

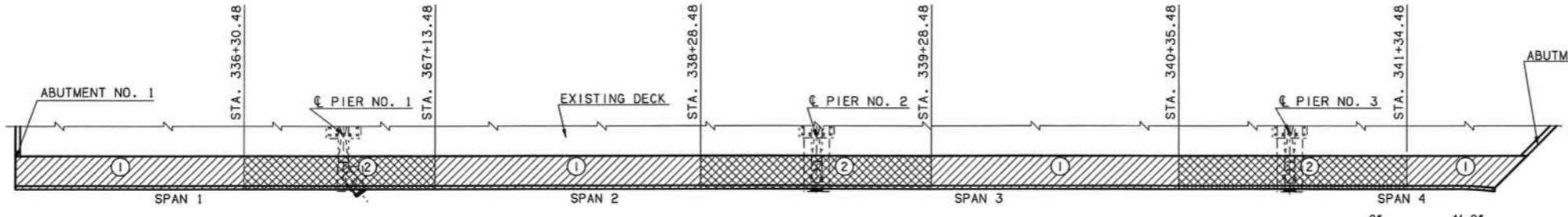
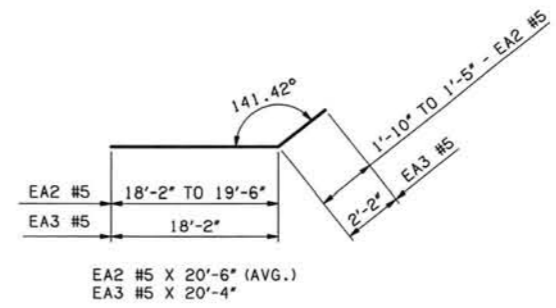
- REBAR NOTES:**
1. 11 - 31" MIN. LAPS, BAR AVG. 684'-1" TO 669'-11"
 2. 11 - 39" MIN. LAPS, BAR AVG. 689'-9" TO 685'-6"
 3. 11 - 39" MIN. LAPS, BAR AVG. 683'-10" TO 679'-7"
 4. REBAR REQUIRES MECHANICAL SPLICE. ADJUST LENGTH AS REQUIRED BY MANUFACTURER AT NO ADDITIONAL COST.

| REV | DATE | BY | CHKD | APPD | TOTAL SHEETS |
|-----|------------|----|------|------|--------------|
| 1 | 06/15/2010 | | | | 484 |

| DECK BAR LIST (ONE REQUIRED) | | | | | |
|---------------------------------|------|------|------|----------------|-------------------|
| EPOXY COATED REINFORCING | | | | | |
| MARK | SIZE | QTY. | FORM | LENGTH | REMARKS |
| AT1 | #5 | 1005 | STR. | 14'-2" | |
| AT2 | #5 | 20 | STR. | 14'-8" (AVG.) | 14'-3" TO 15'-1" |
| AT3 | #5 | 1 | STR. | 15'-2" | |
| AT4 | #5 | 20 | STR. | 7'-3" (AVG.) | 13'-2" TO 1'-4" |
| AT6 | #6 | 1004 | BNT. | 4'-8" | |
| AB1 | #5 | 1005 | STR. | 14'-2" | |
| AB2 | #5 | 20 | STR. | 14'-8" (AVG.) | 14'-3" TO 15'-1" |
| AB3 | #5 | 1 | STR. | 15'-2" | |
| AB4 | #5 | 20 | STR. | 7'-3" (AVG.) | 13'-2" TO 1'-4" |
| ET1 | #4 | 14 | STR. | 677'-0" (AVG.) | SEE NOTE 1 |
| ET3 | #4 | 1 | STR. | 669'-3" | 11-31" MIN. LAPS |
| ET4 | #4 | 1 | STR. | 14'-3" | |
| ET5 | #5 | 28 | STR. | 92'-3" | 1-39" MIN. LAP |
| ET6 | #5 | 28 | STR. | 130'-6" | 2-39" MIN. LAP |
| ET7 | #5 | 28 | STR. | 30'-0" | |
| EB1 | #5 | 9 | STR. | 687'-8" (AVG.) | SEE NOTE 2 |
| EB2 | #5 | 9 | STR. | 681'-9" (AVG.) | SEE NOTE 3 |
| EB3 | #5 | 1 | STR. | 677'-11" | 11-39" MIN. LAPS |
| EB4 | #5 | 1 | STR. | 677'-1" | 11-39" MIN. LAPS |
| EB5 | #5 | 1 | STR. | 676'-5" | 11-39" MIN. LAPS |
| EB9 | #5 | 1 | STR. | 15'-0" | |
| AH1 | #4 | 15 | BNT. | 4'-0" | |
| AH2 | #4 | 15 | BNT. | 5'-5" | |
| EA1 | #5 | 3 | STR. | 14'-2" | |
| EA2 | #5 | 3 | BNT. | 20'-6" (AVG.) | 20'-0" TO 20'-11" |
| EA3 | #5 | 1 | BNT. | 20'-4" | |
| FS2 | #5 | 642 | BNT. | 7'-4" | STD-FSHP-42 |
| EJ1 | #5 | 3 | STR. | 14'-2" | |
| EJ2 | #5 | 3 | STR. | 19'-6" | |
| EJ3 | #5 | 6 | STR. | 17'-8" | |
| EJ4 | #5 | 3 | STR. | 21'-7" | |
| EJ5 | #5 | 3 | STR. | 20'-8" | |
| EJ6 | #5 | 3 | STR. | 29'-3" | |
| EJ7 | #5 | 6 | STR. | 25'-0" | |
| EJ8 | #5 | 3 | STR. | 30'-6" | |



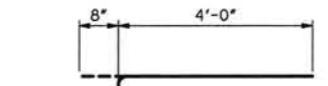
DECK REPAIR DETAIL 2
SCALE: 1/8" = 1'-0"



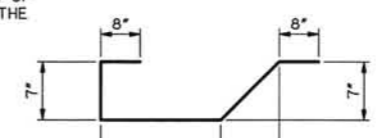
PLAN - BRIDGE 1-1 DECK POUR SEQUENCE
SCALE: 1" = 30'

POURING SEQUENCE NOTES:

1. THE DECK SLAB IS DIVIDED INTO SECTIONS 1 OR 2 BETWEEN CONSTRUCTION JOINTS AS SHOWN. THE CONCRETE SHALL BE POURED IN EACH SECTION OF THE DECK SLAB IN THE NUMERICAL SEQUENCE INDICATED. SECTIONS OF THE DECK SLAB WITH THE SAME NUMBER MAY BE POURED IN ANY ORDER. NO SECTION SHALL BE POURED BEFORE THE ADJACENT SECTION(S) HAVE BEEN IN PLACE FOR AT LEAST 48 HOURS.
2. ALL CONSTRUCTION JOINTS WITHIN THE NEW DECK SLAB SHALL BE SEALED USING HIGH MOLECULAR WEIGHT METHACRYLATE IN ACCORDANCE WITH THE SPECIAL PROVISION "CONCRETE SURFACE REPAIR BY SEALING". ALL COST OF THE HIGH MOLECULAR WEIGHT METHACRYLATE SEALER SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF "(SP) SEALER RESIN". ALL COST OF EQUIPMENT AND LABOR FOR THE INSTALLATION OF THE HIGH MOLECULAR WEIGHT METHACRYLATE SEALER SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF "(SP) SEALER CRACK PREPARATION".
3. IN THE EVENT OF AN EMERGENCY, POURING OF THE DECK SLAB MAY BE HALTED WITH A KEYED CONSTRUCTION JOINT MADE PERPENDICULAR TO THE DIRECTION OF TRAFFIC AS DIRECTED BY THE ENGINEER.
4. LONGITUDINAL REINFORCING SHALL BE CONTINUOUS THRU ALL CONSTRUCTION JOINTS. NO HEAVY EQUIPMENT WILL BE PERMITTED ON THE FINISHED DECK SLAB WITHIN 15' OF ANY CONSTRUCTION JOINT UNTIL THE DECK SLAB IS IN PLACE ON BOTH SIDES OF THE RESPECTIVE JOINT.
5. DO NOT TIE WITHIN 6" OF ANY CONSTRUCTION JOINT.
6. DO NOT PLACE THE CONCRETE FOR THE DECK SLAB OR APPLY OTHER HEAVY EQUIPMENT LOADS TO THE BEAMS UNTIL THE DIAPHRAGMS HAVE BEEN PLACED AND ALL BOLTS HAVE BEEN TIGHTENED.



AT6 #6 X 4'-8"



AH1 #4 X 4'-0"
AH2 #4 X 5'-5"

| SUPERSTRUCTURE QUANTITIES | | |
|------------------------------------|------|--------|
| ITEM | UNIT | TOTAL |
| DIAMOND GRINDING CONCRETE PAVEMENT | SY | 85.0 |
| SEALED EXPANSION JOINT | LF | 225.4 |
| 42" F-SHAPED PARAPET | LF | 640.9 |
| CLASS C BRIDGE DECK REPAIR | SY | 42.4 |
| STRUCTURAL STEEL | LB | 431710 |
| CLASS AA CONCRETE | CY | 275.8 |
| GRAFFITI TREATMENT | SF | 27995 |
| (PL) MECHANICAL SPLICES | EA | 2092 |
| EPOXY COATED REINFORCING STEEL | LB | 72670 |
| WATER REPELLENT | SY | 313 |
| PAINTING EXISTING STRUCTURES | LS | 1 |
| (SP) SEALER CRACK PREPARATION | LF | 947.1 |
| (SP) SEALER RESIN | GAL | 28 |

- NOTES:**
1. ADDITIONAL DECK REINFORCEMENT DETAILS SEE SHEETS 283 & 284.
 2. FOR TYPICAL SECTION, LONGITUDINAL SECTION AND DECK NOTES SEE SHEET 257.
 3. FOR DECK REMOVAL SEE PAY ITEM NOTE (9) SHEET 36.

| | | | |
|----------|--------|-------|-----------------------------------|
| Design | KSJ | 10/07 | I-40 CROSSTOWN OKLAHOMA COUNTY |
| Drawn | RWS | 10/07 | |
| Checked | KSJ | 06/10 | |
| Approved | SRJ | 06/10 | |
| Squad | BENHAM | | |

1-40 RELOCATION BRIDGE 1-1
**SUPERSTRUCTURE DETAILS
DECK REINFORCEMENT**

Job Place No. 17428(34) Sheet No. 285

Sub: May 2013

5/2/2013 11:16:29 AM

| DESIGN DATA | | I-40 | I-35 / 235 |
|------------------------------------|-------|-----------|------------|
| AADT 2005 | | 111,400 | 98,800 |
| AADT 2030 | | 165,200 | 148,200 |
| K (DHV / ADT-TWO WAY) | | 9 % | 9 % |
| D (DIRECTIONAL DIST.) | | 55 % | 55 % |
| T (% OF DHV) | | 10 % | 10 % |
| T (% OF AADT) | | 14 % | 14 % |
| T ³ OVERLOADS (AXLES) | | 10 % | 10 % |
| T ³ OVERLOADS | | 15 AXLES | 15 AXLES |
| 20-YR FLEX ESALS | | 76.14 MIL | 67.92 MIL |
| I-40 / I-35 / I-235 V=70 MPH | | | |
| BOULEVARD V=35 MPH | | | |
| RAMPS V=50 MPH | | | |

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED
INTERSTATE HIGHWAY
GRADE, DRAINING, BRIDGE & SURFACE PLANS
INTERSTATE 40 CROSSTOWN
OKLAHOMA COUNTY
CONTROL SECTION 40-55-69
PROJECT NO. OKCY-XTWN(047)SS
STATE JOB NO. 17428(59)
WORK PACKAGE 4.4

FOR INDEX OF SHEETS
AND STANDARDS,
SEE SHEET 2.

| JOB DIVISION | STATE | JOB NO. | PIECE NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|--------------|-------|-----------|-----------|-------------|-----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 1 | 559 | |
| DESCRIPTION | | REVISION | DATE | | | |

BRIDGE "I-A" SPANS 1-8
NBI NO. NEW: 28596
LENGTH: 803.50'

BRIDGE "I-A" SPANS 9B-10B & BRIDGE "I-B"
NBI NO. Exist: 28597
LENGTH: 675.72'

BRIDGE "I-A" SPANS 9A-11A & BRIDGE "I-D"
NBI NO. Exist: 28598
LENGTH: 594.88'

BRIDGE "I-A" SPAN 12A
NBI NO. NEW: 28599
LENGTH: 110.67'

BRIDGE "I-C"
NBI NO. NEW: 28608
LENGTH: 1055.89'

BRIDGE "I-E"
NBI NO. NEW: 28602
LENGTH: 492.50'

BRIDGE "I-F"
NBI NO. NEW: 28603
LENGTH: 350.00'

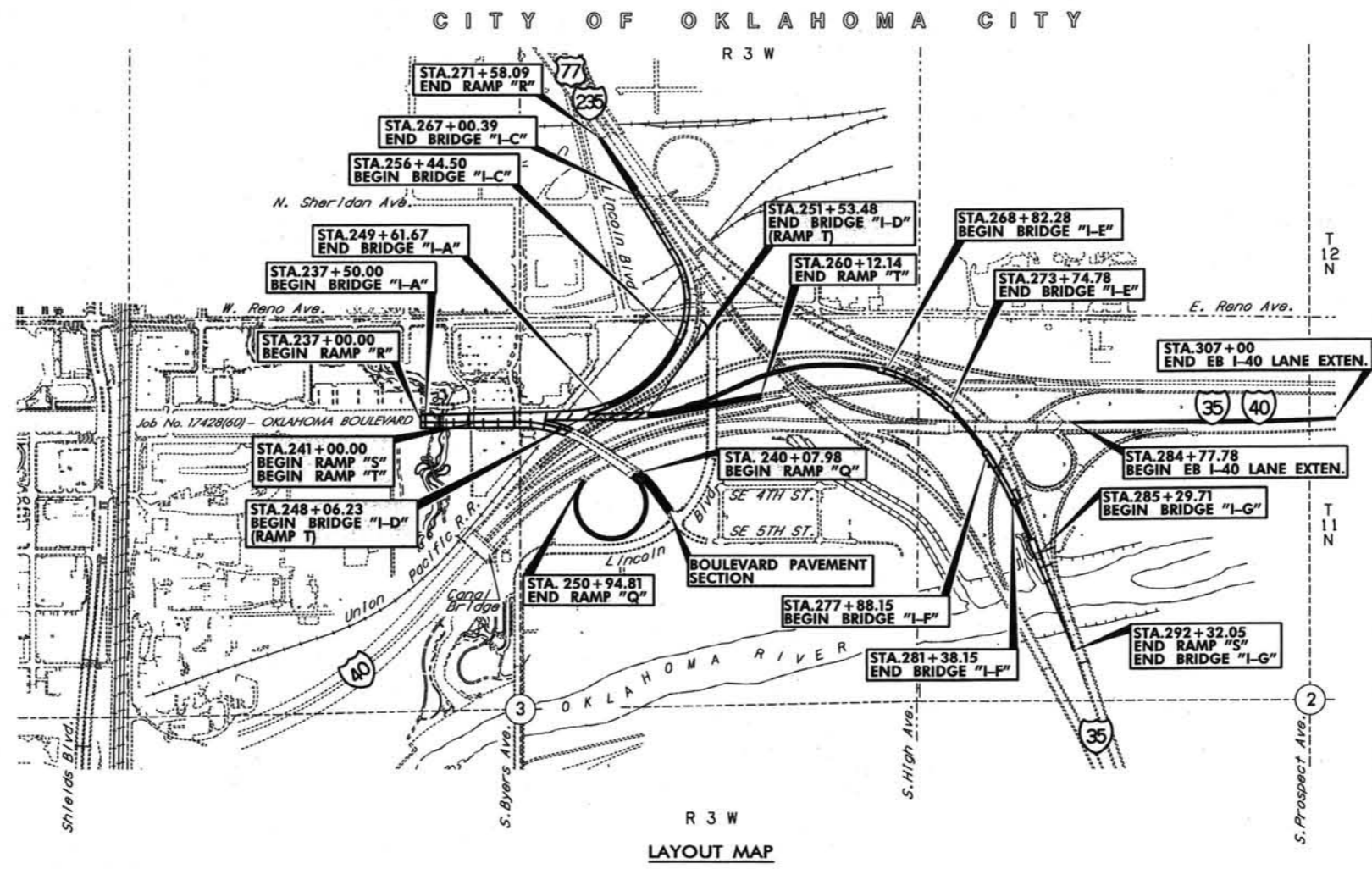
BRIDGE "I-G"
NBI NO. Exist: 21356
LENGTH: 802.50'

SCALES

PLAN 1" = 50'
PROFILE HOR. 1" = 50'
VER. 1" = 5'
LAYOUT MAP 1" = 1000'

CONVENTIONAL SYMBOLS

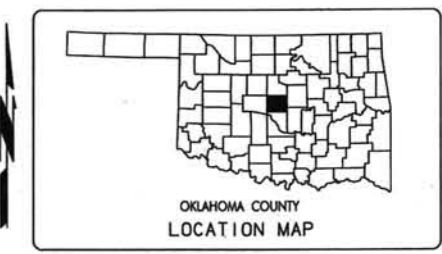
| | | | |
|---|---|-------|-----------------------|
| — | PROPOSED ROAD | —TUG— | TELEPHONE UNDERGROUND |
| — | RAILROADS | —SS— | SANITARY SEWER |
| — | RANGE & TOWNSHIP | —G— | GAS LINE |
| — | SECTION LINES | —W— | WATER LINE |
| — | QUARTER SECTION LINES | | |
| — | FENCES | | |
| — | GROUND LINE | | |
| — | EXISTING ROADS | | |
| — | BASE LINE | | |
| — | GRADE LINES | | |
| — | TELEPHONE & TELEGRAPH | | |
| — | POWER LINES | | |
| — | BUILDINGS | | |
| — | OILWELL | | |
| — | DRAINAGE STRUCTURES - IN PLACE | | |
| — | DRAINAGE STRUCTURES - NEW | | |
| — | RIGHT-OF-WAY LINES - EXISTING | | |
| — | RIGHT-OF-WAY LINES - NEW | | |
| — | RIGHT-OF-WAY MARKERS - IN PLACE | | |
| — | RIGHT-OF-WAY MARKERS - REMOVE & REPLACE | | |
| — | RIGHT-OF-WAY MARKERS - NEW | | |
| — | CONTROLLED ACCESS | | |
| — | RIGHT-OF-WAY FENCE | | |



FOR SURVEY CONTROL DATA
SEE SURVEY DATA SHEETS
51 TO 72

ROADWAY LENGTH 7701.72 FT. 1.458 MI.
BRIDGE LENGTH 4159.81 FT. 0.787 MI.
PROJECT LENGTH 2.245 MI.

EQUATIONS: NONE
EXCEPTIONS: NONE



CERTIFICATE OF AUTHORIZATION NO. 3722 P.E., L.S. RENEWAL DATE 10-30-13

SAIC
From Science to Solutions

SAIC Energy, Environment & Infrastructure, LLC
One West Third Street, Suite 100
Tulsa, Oklahoma 74103
(918) 492-1600

Susan O. Tryon, P.E.
OK P.E. NO. 15852
PROJECT ENGINEER
DATE: 5/2/13

REGISTERED PROFESSIONAL ENGINEER
SUSAN OWEN TRYON
15852
OKLAHOMA

OKLAHOMA
DEPARTMENT OF TRANSPORTATION

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

DATE APPROVED _____
BY _____
CHIEF ENGINEER

DATE APPROVED _____
BY _____
DIVISION ADMINISTRATOR

S.W.O. 3367(1) Project No. 17428(59) Sheet No. 1

2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION-ENGLISH GOVERN, APPROVED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION, JANUARY 4, 2010.

5/16/2013

ID:4,28 AM

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INDEX OF SHEETS

ROADWAY DRAWINGS
JOB PIECE No. 17428(59)
1 TITLE SHEET
2 INDEX OF SHEETS
TYPICAL SECTIONS
3-9 TYPICAL SECTIONS
10-14 MISCELLANEOUS DETAILS
PAY ITEMS AND NOTES
15-168 GENERAL CONSTRUCTION NOTES
17 UNION PACIFIC RAILROAD GENERAL CONSTRUCTION NOTES
18 ENVIRONMENTAL NOTES
19 ROADWAY PAY ITEMS
20-21 ROADWAY PAY ITEM NOTES
22 TRAFFIC PAY QUANTITIES
23-25 TRAFFIC PAY ITEM NOTES
26-37 BRIDGE PAY ITEMS AND NOTES
SUMMARIES
38-40 SUMMARY SHEETS
41-42 SUMMARY OF DRAINAGE STRUCTURES
43 STORMWATER MANAGEMENT PLAN
TRAFFIC DATA
44 TRAFFIC DATA
GEOMETRICS
45-50 GEOMETRIC DATA
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51-72 SURVEY DATA SHEETS
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COMMON DETAILS
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CROSS SECTIONS

CROSS SECTIONS - RAMP "R"
CROSS SECTIONS - RAMP "S"
CROSS SECTIONS - RAMP "T"
CROSS SECTIONS - EB I-40 LANE EXTENSION
CROSS SECTIONS - RAMP "Q"

SHEETS 13-24 AND 31-41 OF 45 OF THE SURVEY SWO 3367(1) ARE NOT REQUIRED FOR THIS PROJECT.

THE FOLLOWING ODOT STANDARDS WILL BE REQUIRED

Table with 5 columns: ROADWAY DESIGN, TRAFFIC SIGNING, TRAFFIC CONTROL, HIGHWAY LIGHTING, BRIDGE DESIGN. Lists various standard codes like SSS-1-1, PM1-1-01, TCS1-1-01, CCD1-1-00, FSH-42-2-00E, etc.

OBSOLETE STDS.

- OSS9-1-00E
OSS10-1-00E
OWE-1-00E



RESPONSIBLE FOR: ROADWAY SHEETS - 3-9, 19-21, 38-39, 41-43, 45-50, 73-132, 143-149



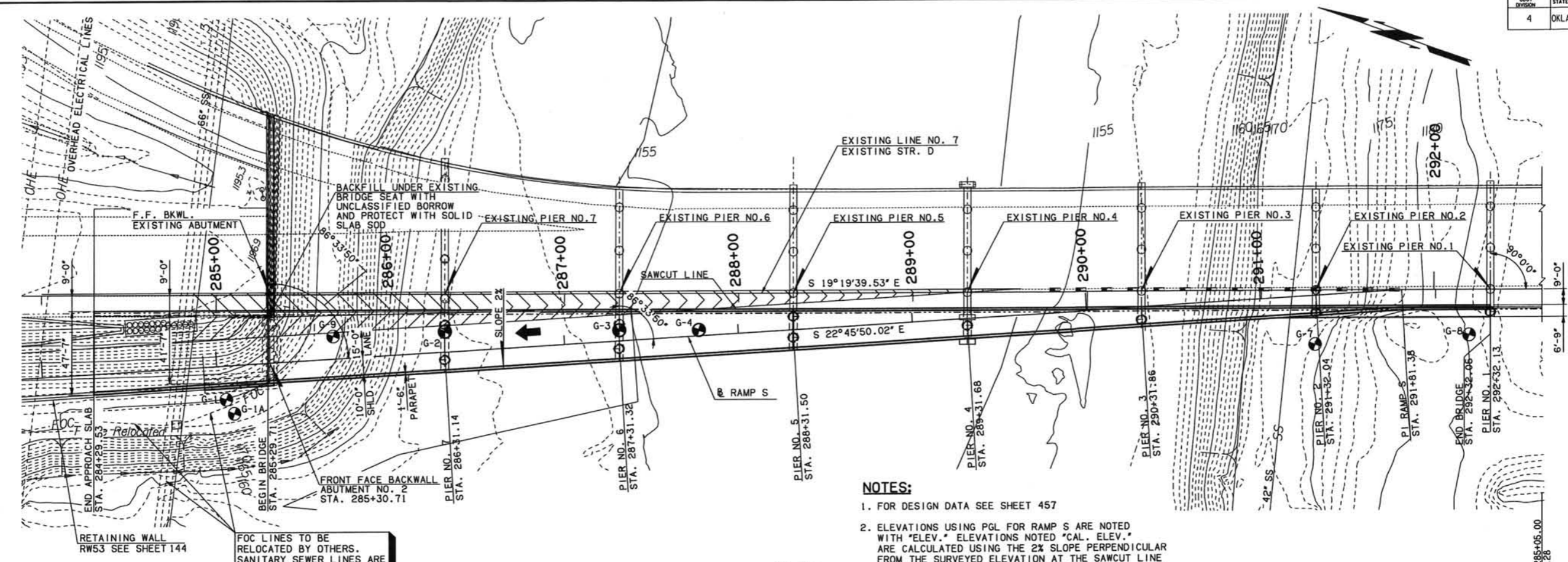
RESPONSIBLE FOR: TRAFFIC SHEETS - 22-25, 40, 44, 133-142, 150-154

Design, Drawn, Checked, Approved, Squad table. I-40 CROSSTOWN OKLAHOMA COUNTY. INDEX OF SHEETS AND ODOT STANDARD DRAWINGS. Job Piece No. 17428(59) Sheet No. 2

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NOTES:

- FOR DESIGN DATA SEE SHEET 457
- ELEVATIONS USING PGL FOR RAMP S ARE NOTED WITH "ELEV." ELEVATIONS NOTED "CAL. ELEV." ARE CALCULATED USING THE 2% SLOPE PERPENDICULAR FROM THE SURVEYED ELEVATION AT THE SAWCUT LINE OF THE EXISTING DECK TO THE RAMP S. AT PIERS 3 THROUGH 1, THE SAWCUT LINE AND RAMP S COINCIDE AND THE ELEVATIONS ARE TAKEN DIRECTLY FROM THE SURVEY.

PLAN
SCALE: 1" = 30'

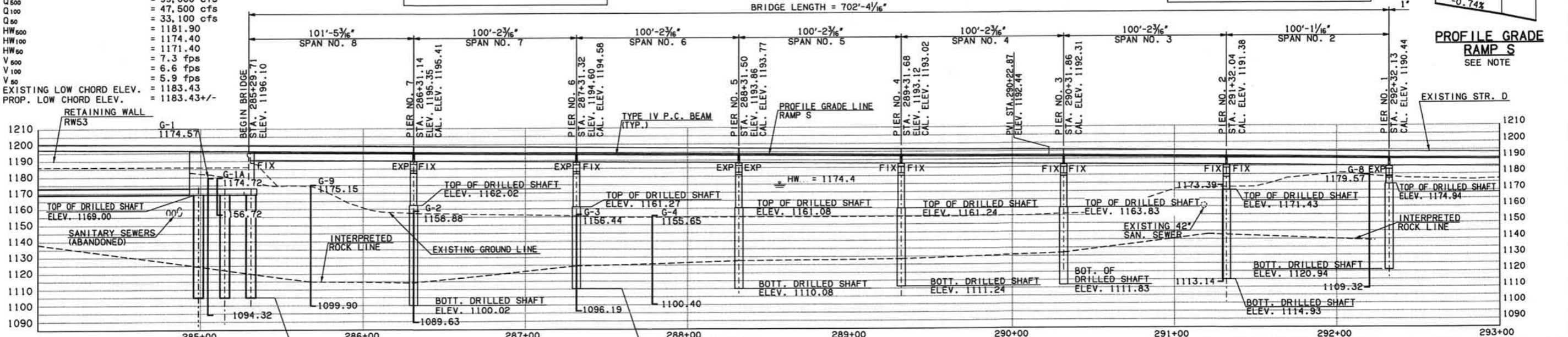
HYDRAULIC DATA
FROM F.A.P. NO. I-35-3(107)126:

| | |
|--------------------------|------------------|
| DRAINAGE AREA | = 13,354 SQ. MI. |
| Q ₅₀₀ | = 99,000 cfs |
| Q ₁₀₀ | = 47,500 cfs |
| Q ₅₀ | = 33,100 cfs |
| HW ₅₀₀ | = 1181.90 |
| HW ₁₀₀ | = 1174.40 |
| HW ₅₀ | = 1171.40 |
| V ₅₀₀ | = 7.3 fps |
| V ₁₀₀ | = 6.6 fps |
| V ₅₀ | = 5.9 fps |
| EXISTING LOW CHORD ELEV. | = 1183.43 |
| PROP. LOW CHORD ELEV. | = 1183.43 +/- |

BENCHMARK: 41
CHISELED "X" LAMP POLE BASE
STA. 287+73.12 (I-40); 89.65' RT.
ELEV. = 1172.15

BENCHMARK: 40
CHISELED "X" ON THE SOUTHWEST SIDE
STA. 279+62.02 (I-40); 525.35' RT.
ELEV. = 1174.87

BENCHMARK: 40
CHISELED "X" ON THE SOUTHWEST SIDE
STA. 279+62.02 (I-40); 525.35' RT.
ELEV. = 1174.87

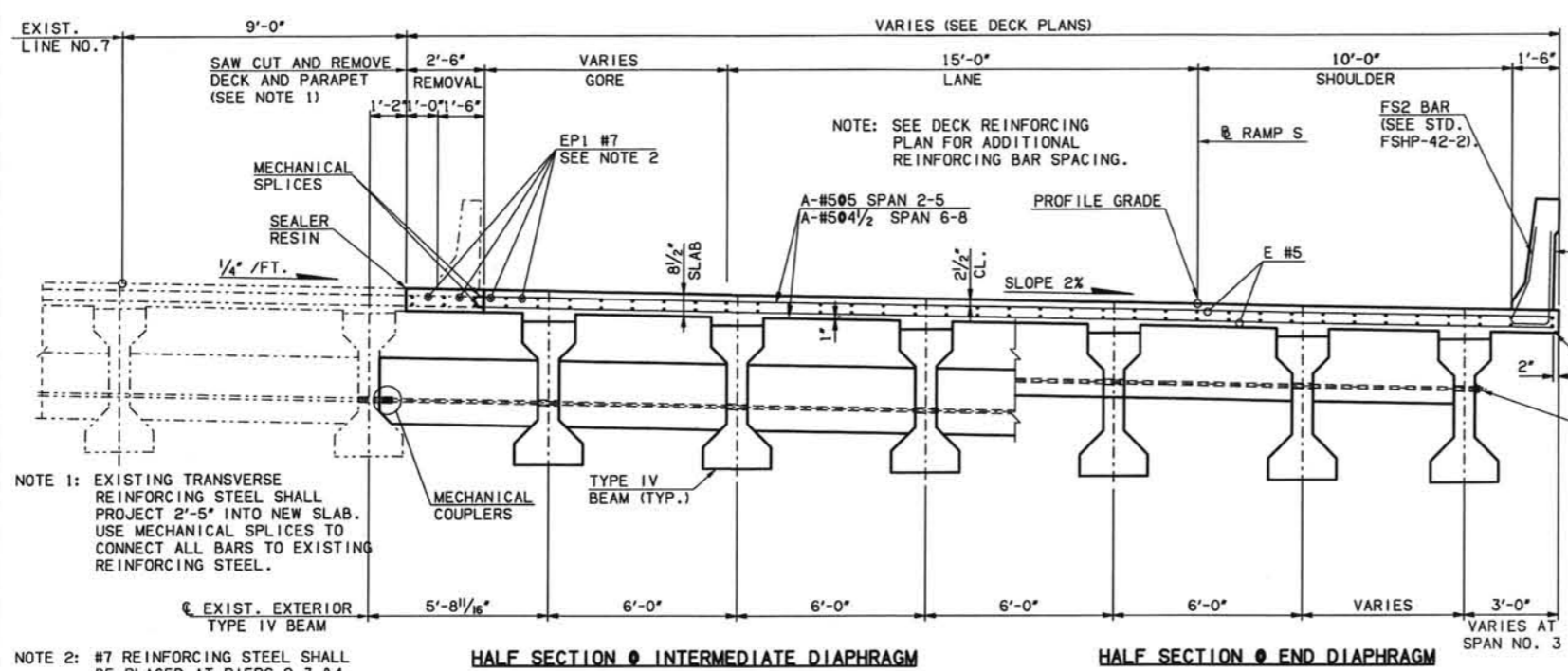


ELEVATION
SCALE HORIZ. 1" = 30'
VERT. 1" = 30'

| | | | | |
|----------|------|------|----------------------------|-----------------|
| Design | SOT | 1/05 | I-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RWS | 1/05 | I-40 RELOCATION BRIDGE I-G | |
| Checked | SOT | 9/12 | GENERAL PLAN AND ELEVATION | |
| Approved | SRJ | 9/12 | © STA. 288+80.92 P.C. BEAM | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 453 |

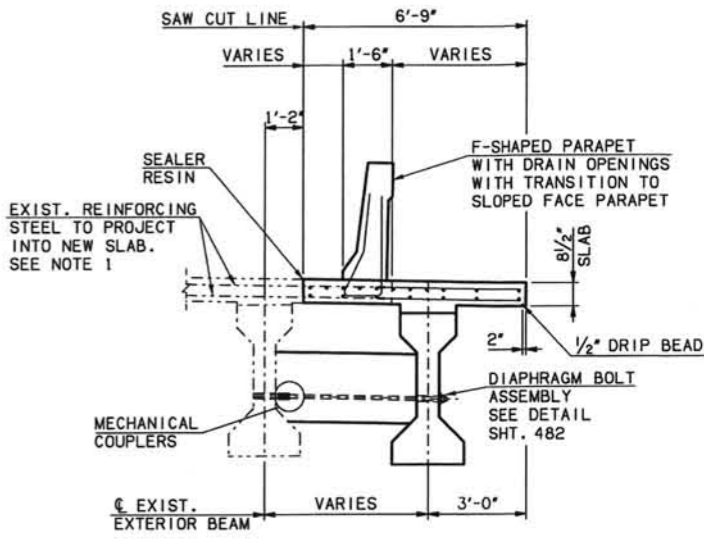
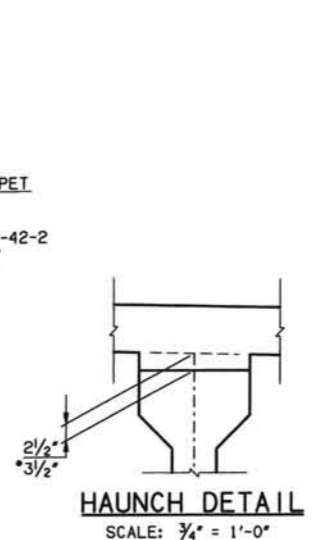
| DDOT DIVISION | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------|-------|-------------|-------------|-----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 454 | 559 |

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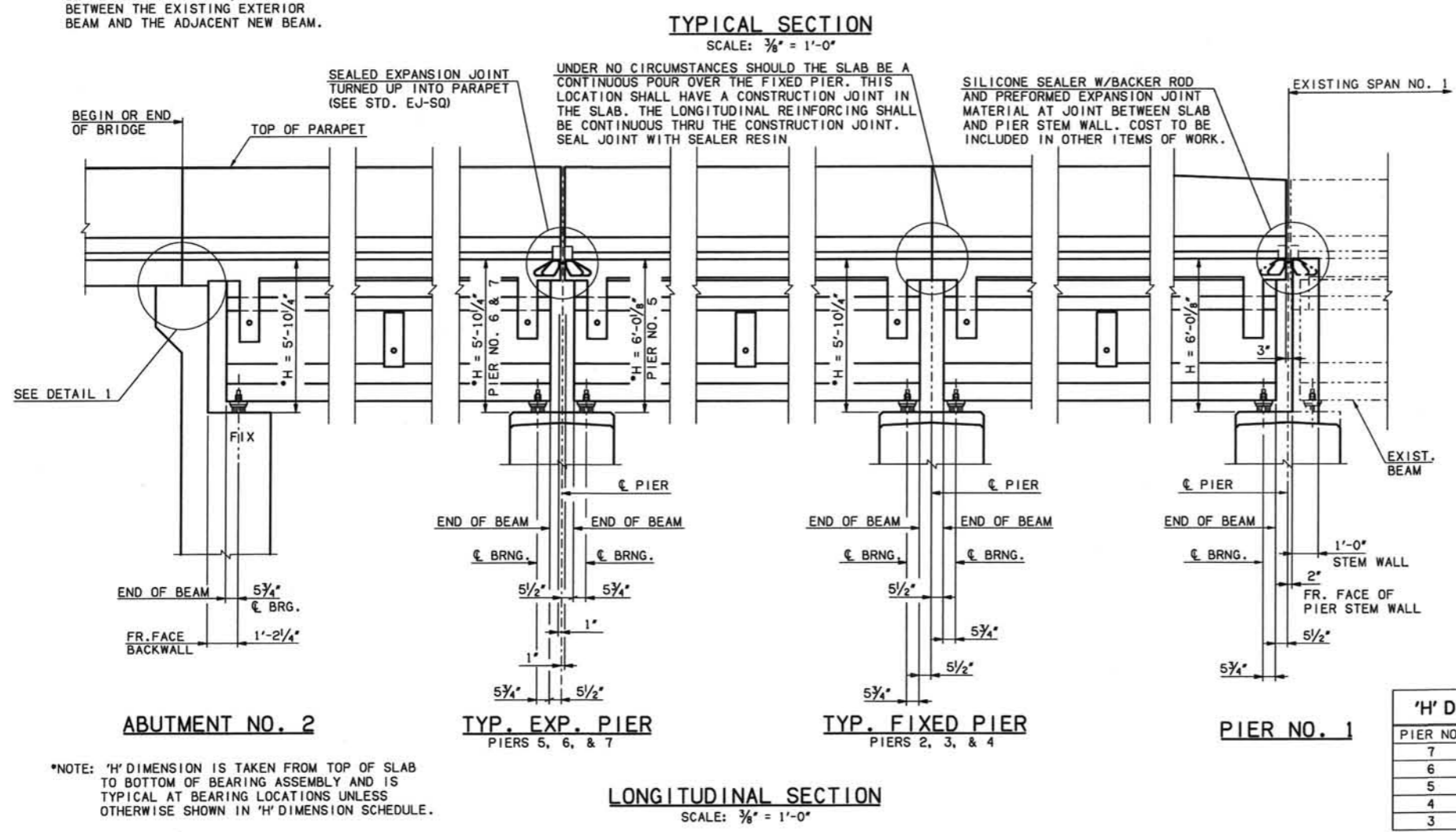


NOTE 1: EXISTING TRANSVERSE REINFORCING STEEL SHALL PROJECT 2'-5" INTO NEW SLAB. USE MECHANICAL SPLICES TO CONNECT ALL BARS TO EXISTING REINFORCING STEEL.

NOTE 2: #7 REINFORCING STEEL SHALL BE PLACED AT PIERS 2, 3 & 4 BETWEEN THE EXISTING EXTERIOR BEAM AND THE ADJACENT NEW BEAM.



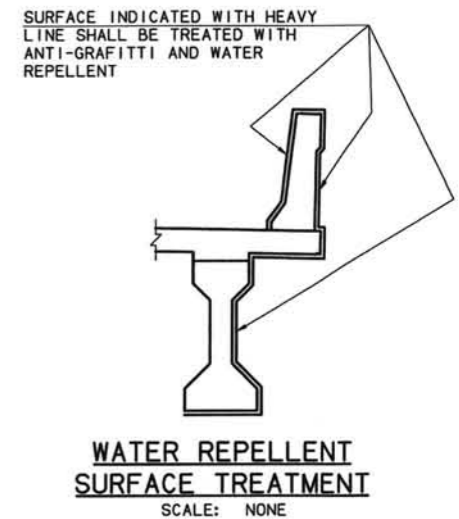
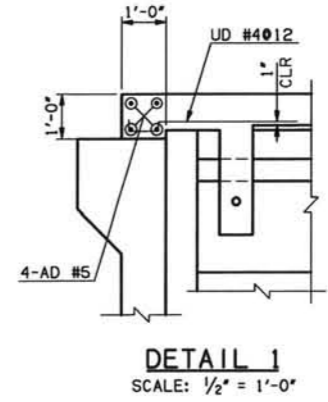
* NOTE: 3/2" HAUNCH AT 5 BEAM BEARING LOCATIONS. SEE FRAMING PLAN FOR LOCATIONS.



*NOTE: 'H' DIMENSION IS TAKEN FROM TOP OF SLAB TO BOTTOM OF BEARING ASSEMBLY AND IS TYPICAL AT BEARING LOCATIONS UNLESS OTHERWISE SHOWN IN 'H' DIMENSION SCHEDULE.

'H' DIMENSION SCHEDULE

| PIER NO. | BEAM NO. | DIMENSION |
|----------|----------|------------|
| 7 | E8 | 5'-11 1/4" |
| 6 | D7 | 5'-11 1/4" |
| 5 | C6 | 6'-1 1/8" |
| 4 | B5 | 5'-11 1/4" |
| 3 | A4 | 5'-11 1/4" |



| SUPERSTRUCTURE QUANTITIES | | |
|--------------------------------------|------|--------|
| ITEM | UNIT | TOTAL |
| PRESTRESSED CONCRETE BEAMS (TYPE IV) | LF | 2180.8 |
| SAW-CUT GROOVING | SY | 1529.8 |
| EXPANSION DEVICE | LF | 293.30 |
| SEALED EXPANSION JOINT | LF | 82.85 |
| 42" F-SHAPED PARAPET | LF | 701.8 |
| STRUCTURAL STEEL | LB | 5090 |
| SPECIAL CONCRETE FINISH | SY | 1614 |
| CLASS AA CONCRETE | CY | 451.4 |
| MECHANICAL SPLICES | EA | 3530 |
| EPOXY COATED REINFORCING STEEL | LB | 116840 |
| WATER REPELLANT (VISUALLY INSPECTED) | SY | 1614 |
| SEALER CRACK PREPARATION | LF | 740 |
| SEALER RESIN | GAL | 6 |

| | | | | |
|----------|------|------|----------------------------|-----------------|
| Design | SB | 9/06 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 9/06 | 1-40 RELOCATION BRIDGE 1-G | |
| Checked | SOT | 9/12 | TYPICAL SECTION | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 454 |

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| BOOK NUMBER | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|-------------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 455 | 559 |

| SUMMARY OF QUANTITIES - BRIDGE I-G | | | | | | |
|--|------|----------|-------|----------------|----------|--------|
| DESCRIPTION | UNIT | ABUTMENT | PIER | SUPERSTRUCTURE | APPROACH | TOTAL |
| SUBSTRUCTURE EXCAVATION COMMON | CY | 435 | | | | 435 |
| CLSM BACKFILL | CY | 801.5 | | | | 801.5 |
| PRESTRESSED CONCRETE BEAMS (TYPE IV) | LF | | | 2180.8 | | 2180.8 |
| APPROACH SLAB | SY | | | | 495.4 | 495.4 |
| SAW-CUT GROOVING | SY | | | 1529.8 | 478.8 | 2008.6 |
| EXPANSION DEVICE | LF | | | 293.30 | | 293.3 |
| SEALED EXPANSION JOINT | LF | | | 82.85 | | 82.85 |
| 42" F-SHAPED PARAPET | LF | | | 701.8 | 100.2 | 802 |
| STRUCTURAL STEEL | LB | | | 5090 | | 5090 |
| STAINLESS STEEL FIXED BEARING ASSEMBLY | EA | 6 | 19 | | | 25 |
| STAINLESS STEEL EXPANSION BEARING ASSEMBLY | EA | | 19 | | | 19 |
| SPECIAL CONCRETE FINISH | SY | 485 | 605 | 1614 | 91 | 2795 |
| CLASS AA CONCRETE | CY | | | 451.4 | | 451.4 |
| CLASS A CONCRETE | CY | 257.1 | 215.5 | | | 472.6 |
| MECHANICAL SPLICES | EA | | | 3530 | | 3530 |
| REINFORCING STEEL | LB | | 26510 | | | 26510 |
| EPOXY COATED REINFORCING STEEL | LB | 27650 | 24600 | 116840 | | 169090 |
| WATER REPELLANT (VISUALLY INSPECTED) | SY | 485 | 605 | 1614 | 91 | 2795 |
| DRILLED SHAFTS 66" DIAMETER | LF | | 520 | | | 520 |
| DRILLED SHAFTS 72" DIAMETER | LF | 228 | | | | 228 |
| DRILLED SHAFTS 84" DIAMETER | LF | 204 | | | | 204 |
| CROSSHOLE SONIC LOGGING | EA | 1 | 5 | | | 6 |
| CSL ACCESS TUBES | LF | 1491 | 3300 | | | 4791 |
| SEALER CRACK PREPARATION | LF | | | 740.0 | | 740 |
| SEALER RESIN | GAL | | | 6 | | 6 |
| 6" PERFORATED PIPE UNDERDRAIN ROUND | LF | 57 | | | | 57 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 20 | | | | 20 |
| REMOVAL OF BRIDGE ITEMS | LSUM | | | | | 1 |

EXISTING BRIDGE PLANS

ORIGINAL BRIDGE WAS CONSTRUCTED UNDER PROJECT NO. 1-35-3(107) 126 05084(05) PLANS ARE AVAILABLE FROM:

REPRODUCTION DEPARTMENT
OKLAHOMA DEPARTMENT OF TRANSPORTATION
200 N.E. 21ST ST.
OKLAHOMA CITY, OK. 73136

CLSM BACKFILL

THE CLSM BACKFILL MUST BE PLACED IN FOUR LIFTS OF EQUAL HEIGHTS.

SEQUENCE OF CONSTRUCTION

THE BRIDGE CONTRACTOR SHALL COORDINATE SEQUENCING WITH ROADWAY CONTRACTOR.

STAY-IN-PLACE FORMS

STEEL STAY-IN-PLACE (SIP) FORMS MAY BE USED IF THE MAXIMUM THICKNESS OF 8" IS OBTAINED BY MEASURING FROM THE TOP PORTION OF THE STEEL CORRUGATION. NO ADDITIONAL CONCRETE WEIGHT OF THE DECK SLAB IS PERMITTED. FOR ADDITIONAL INFORMATION CONCERNING THE USE OF "STAY-IN-PLACE FORMS", SEE SECTION 502 OF THE STANDARD SPECIFICATIONS.

ANCHORAGE ASSEMBLIES

THE CONTRACTOR SHALL HAVE THE OPTION OF METHODS BY WHICH THE ANCHOR BOLTS ARE CONNECTED TO THE SUBSTRUCTURE AS SHOWN ON THE PLANS. ANCHORAGES SHALL BE OF THE SIZE SHOWN ON THE PLANS AND SHALL BE CONNECTED IN ONE OF THE FOLLOWING MANNERS:

- (1) SELF-MIXING INJECTION TYPE ANCHORAGE SYSTEMS SUCH AS HILTI FASTENING SYSTEMS, UNITEX PRO-POXY 300 FAST, OR AN APPROVED EQUAL.
- (2) ENCAPSULATED TYPE ANCHOR SYSTEMS SUCH AS RAWPLUG CO. CHEM-STUD, HILTI ENCAPSULATED, OR AN APPROVED EQUAL.

ANCHORAGES SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS FOR THE SYSTEM USED. ALL COSTS OF ANCHORAGE ASSEMBLIES INCLUDING LABOR, MATERIALS, TOOLS, DRILLING, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

PERFORATED PIPE UNDERDRAIN ROUND

ALL COST OF PIPE UNDERDRAIN COVER MATERIAL, BOTH FINE AND COARSE, EQUIPMENT, AND LABOR NEEDED FOR INSTALLATION SHALL BE INCLUDED IN THE UNIT PRICE BID PER LIN. FT. OF "6" PERFORATED PIPE UNDERDRAIN ROUND". INSTALLATION SHALL BE AS SHOWN IN PLAN DETAILS AND ON STANDARD PUD-3.

NON-PERFORATED PIPE UNDERDRAIN RND.

ALL COST OF TRENCH EXCAVATION, STANDARD BEDDING MATERIAL, BACKFILL, EQUIPMENT, AND LABOR NEEDED FOR INSTALLATION SHALL BE INCLUDED IN THE UNIT PRICE BID PER LIN. FT. OF "6" NON-PERF. PIPE UNDERDRAIN RND.". INSTALLATION SHALL BE AS SHOWN IN THE PLAN DETAILS AND STANDARD PUD-3.

SPECIFICATIONS

COMPLY WITH THE REQUIREMENTS OF THE 2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

DECK HAUNCHES

PLAN QUANTITY FOR CLASS AA CONCRETE INCLUDES 30.1 CY FOR HAUNCHES OVER GIRDERS AND END DIAPHRAGMS. THIS QUANTITY IS CALCULATED ASSUMING A 2 1/2" OR 3 1/2" HAUNCH THE FULL LENGTH OF THE GIRDERS, AS SHOWN ON THE PLANS. THE FINAL HAUNCH HEIGHTS WILL BE SET AFTER ERECTION OF GIRDERS AND DIAPHRAGMS TO PROVIDE FOR DEAD LOAD DEFLECTION AND GRADE ADJUSTMENT.

STRUCTURAL STEEL

ALL STRUCTURAL STEEL SHALL CONFORM TO AASHTO M270 (ASTM A709) GRADE 50, UNLESS SHOWN OR NOTED OTHERWISE.

CONCRETE FINISHING

FINISH THE EXPOSED CONCRETE SURFACES IN ACCORDANCE WITH SHEETS 16A AND 16B. SEE SURFACE TREATMENT DETAILS, SHEETS 496 AND 496A.

VERIFICATION OF EXISTING CONDITIONS

ALL DIMENSIONS OF THE EXISTING BRIDGE COMPONENTS SHOWN ON THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS NECESSARY TO CONNECT THE NEW MATERIAL AND SHALL BE SOLELY RESPONSIBLE FOR THE ACCURACY THEREOF.

HORIZONTAL DIMENSIONS HAVE BEEN CALCULATED USING EXISTING PLANS AND "RAMP S". VERTICAL CONTROL FOR PEDESTAL AND OTHER ELEVATIONS HAVE BEEN CALCULATED FROM SURVEYED ELEVATIONS AT EACH JOINT AT THE INSIDE OF FACE OF PARAPET, USING THE 2% CROSS SLOPE. THE PROFILE GRADE FOR "RAMP S" WAS NOT USED.

VERIFICATION OF EXISTING CONDITIONS (CONT.)

USC & GS VERTICAL DATUM ON EXISTING BRIDGE DESIGN PLANS MAY NOT BE THE SAME AS DATUM UTILIZED ON CURRENT SURVEY. CONTRACTOR SHALL VERIFY ANY DATA UTILIZED FROM OTHER SOURCES, AND FIELD VERIFY AND ADJUST THE PLAN ELEVATIONS AS REQUIRED PRIOR TO FINALIZING THE SHOP DRAWINGS FOR P.C. BEAMS AND PRIOR TO PLACING THE CONCRETE COLUMNS AND BRIDGE SEATS. FINAL ELEVATIONS OF ALL COMPONENTS SHALL BE ESTABLISHED BY EXTENDING THE EXISTING DECK FROM THE BREAK LINE, AS SHOWN ON THE TYPICAL SECTION, AT A 2% CROSS-SLOPE. ADJUSTMENTS MAY BE MADE THROUGH THE HAUNCH AND/OR BY ADJUSTING THE PEDESTAL ELEVATIONS.

BIDDERS SHALL FULLY INFORM THEMSELVES OF THE NATURE OF THE WORK AND CONDITIONS UNDER WHICH IT WILL BE PERFORMED. THE CONTRACTOR SHALL ADOPT METHODS CONSISTENT WITH GOOD CONSTRUCTION PRACTICE AND SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT DAMAGE TO THE EXISTING BRIDGE STRUCTURE OR ROADWAY. DAMAGE DUE TO THE CONTRACTOR'S NEGLIGENCE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE, TO THE SATISFACTION OF THE ENGINEER.

TEMPORARY RETAINING STRUCTURE AND TEMPORARY WATER CONTROL SYSTEM

CONTRACTOR SHALL DETERMINE NEED AND EXTENTS FOR TEMPORARY RETAINING STRUCTURE AND TEMPORARY WATER CONTROL SYSTEM. INCLUDE ALL COSTS ASSOCIATED WITH INSTALLATION AND REMOVAL OF THE TEMPORARY RETAINING STRUCTURE AND TEMPORARY WATER CONTROL SYSTEM, INCLUDING ENGINEERING SERVICES, IN THE PRICE BID FOR RELEVANT EXCAVATION PAY ITEMS. ALL DOCUMENTS, INCLUDING BUT NOT LIMITED TO, DESIGN CALCULATIONS, MATERIAL LISTS, AND DRAWINGS, SHALL BE SIGNED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED BY THE STATE OF OKLAHOMA.

SPECIAL CONCRETE FINISH AND WATER REPELLANT

SPECIAL CONCRETE FINISH CONSISTS OF COLOR STAIN AND NON-SACRIFICIAL ANTI-GRAFFITI TREATMENT. APPLY COLOR STAIN, WATER REPELLANT AND NON-SACRIFICIAL ANTI-GRAFFITI TREATMENT TO THE FOLLOWING NEW AND EXISTING SURFACES OF THE BRIDGE:

- (A) ALL EXPOSED FACES OF THE F-SHAPED PARAPETS ON THE WEST SIDE OF THE BRIDGE DECK AND APPROACH SLABS.
- (B) THE TOP AND OUTSIDE EDGES OF THE DECK OUTSIDE OF PARAPETS, THE UNDERSIDE OF THE CANTILEVER PORTION OF THE WEST SIDE OF THE BRIDGE DECK AND THE EXTERIOR AND BOTTOM OF WEST EXTERIOR BEAM.
- (C) THE FRONT FACE OF THE NORTH ABUTMENT BACKWALL, TOP OF BRIDGE SEAT AND ALL SURFACES OF THE PEDESTALS AT ABUTMENT NO. 2 (NORTH ABUTMENT), THE ENTIRE FRONT FACE OF THE NEW BRIDGE SEAT, THE EXPOSED FRONT FACE OF THE EXISTING BRIDGE SEAT, ALL EXPOSED SURFACES OF THE NEW WINGS TO A DISTANCE OF 8 INCHES BELOW THE FINISHED GROUND LINE, AND ALL EXPOSED SURFACES OF THE EXISTING WINGS.
- (D) ALL EXPOSED SURFACES OF THE NEW PIERS, INCLUDING THE PEDESTALS, TO THE TOP OF DRILLED SHAFTS.

THE TOP AND ROADWAY FACES OF THE PARAPETS USE OKLAHOMA GREY OR APPROVED COLOR. USE OKLAHOMA TAN OR APPROVED COLOR ON ALL OTHER STAINED SURFACES. THE ORIGINAL PLANS INDICATE THAT THE EXISTING CONCRETE SURFACES WERE ORIGINALLY STAINED WITH OKLAHOMA TAN, FEDERAL COLOR NO. 33531 AND OKLAHOMA GREY, FEDERAL COLOR NO. 36586. COLOR SHALL BE ADJUSTED AS NECESSARY TO PROVIDE A CONSISTENT APPEARANCE WITH THE ADJACENT AND EXISTING STRUCTURES IN THE INTERCHANGE. SEE SHEETS 16A AND 16B.

STAINLESS STEEL FIXED BEARING ASSEMBLIES

PROVIDE AND INSTALL FIXED BEARING ASSEMBLIES OF THE SIZE, SHAPE AND AT LOCATIONS AS DETAILED IN THE PLANS. THERE IS AN ESTIMATED TOTAL WEIGHT OF 4,690 LBS. OF STRUCTURAL STEEL FOR THE FIXED BEARING ASSEMBLIES. INCLUDE ALL COSTS ASSOCIATED WITH PROVIDING AND INSTALLING THE FIXED ELASTOMERIC PADS, ANCHOR PLATES, CONTACT ANGLES, ANCHOR BOLTS, NUTS AND WASHERS, INCLUDING ALL MATERIAL, LABOR, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS SHOWN ON THE PLANS, IN THE PRICE BID PER EACH OF "STAINLESS STEEL FIXED BEARING ASSEMBLY".

NOTES CONTINUED ON SHEET 456.

| | | | | |
|----------|------|-------|------------------------------|-----------------|
| Design | OKC | 10/08 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 12/06 | 1-40 RELOCATION BRIDGE I-G | |
| Checked | SOT | 9/12 | GENERAL NOTES AND | |
| Approved | SRJ | 9/12 | SUMMARY OF QUANTITIES | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 455 |

4/24/2013

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| DOT DESIGN | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|------------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 456 | 559 |

GENERAL NOTES BRIDGE I-G (CONTINUED)

STAINLESS STEEL EXPANSION BEARING ASSEMBLIES

PROVIDE AND INSTALL EXPANSION BEARING ASSEMBLIES OF THE SIZE, SHAPE AND AT LOCATIONS AS DETAILED IN THE PLANS. THERE IS AN ESTIMATED TOTAL WEIGHT OF 3,630 LBS. OF STRUCTURAL STEEL FOR THE STAINLESS STEEL EXPANSION BEARING ASSEMBLIES. INCLUDE ALL COSTS ASSOCIATED WITH PROVIDING AND INSTALLING THE EXPANSION ELASTOMERIC PADS, ANCHOR PLATES, CONTACT ANGLES, ANCHOR BOLTS, NUTS AND WASHERS, INCLUDING ALL MATERIAL, LABOR, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS SHOWN ON THE PLANS, IN THE PRICE BID PER EACH OF "STAINLESS STEEL EXPANSION BEARING ASSEMBLY".

SEALING BRIDGE DECK CONSTRUCTION JOINTS

THE JOINT ALONG THE DECK REPLACEMENT AREAS SHALL BE SEALED USING SEALER RESIN IN ACCORDANCE WITH SECTION 523 OF THE STANDARD SPECIFICATIONS.

SEALED EXPANSION JOINT

EXTEND THE EXISTING SEJ TYPE EXPANSION JOINTS FOR THE NEW BRIDGE DECK WIDTHS AS SHOWN IN THE PLANS. CONTRACTOR SHALL EXAMINE EXISTING RECEPTORS AND USE SAME RECEPTOR FOR NEW SEJ. NEW EXPANSION JOINT RECEPTORS SHALL BE WELDED TO THE EXISTING WITH A PENETRATION WELD AND GROUND SMOOTH IN ACCORDANCE WITH SECTION 506 OF THE STANDARD SPECIFICATIONS. THE NEOPRENE GLANDS SHALL BE FIELD SPLICED AND EXTENDED TO THE LIMITS OF THE NEW CLEAR ROADWAYS AND PROJECT INTO THE PARAPETS. FIELD SPLICING OF NEOPRENE GLANDS SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS. A MANUFACTURER'S REPRESENTATIVE SHALL BE PRESENT DURING THE NEOPRENE GLAND FIELD SPLICING OPERATIONS. INSTALLATION OF THE NEW SEALED EXPANSION JOINTS SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND THE MANUFACTURER'S SPECIFICATIONS. ALL COST FOR EXTENDING AND INSTALLING NEW JOINTS, MATERIALS, LABOR, TOOLS AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT FOR 'SEALED EXPANSION JOINT '.

EXPANSION DEVICE

THIS ITEM CONSISTS OF REMOVING AND REPLACING ANY DAMAGED NEOPRENE GLANDS IN THE EXISTING SEALED EXPANSION JOINTS. REMOVE AND REPLACE ONLY THE GLANDS IDENTIFIED BY THE ENGINEER.

MECHANICAL SPLICES

INCLUDES MECHANICAL SPLICE DEVICES USED TO SPLICE REBARS EXTENDING FROM THE EXISTING DECK WITH NEW REBARS.

DRILLED SHAFT CASING

DOUBLE CASING METHOD OF CONSTRUCTION WILL NOT BE ALLOWED.

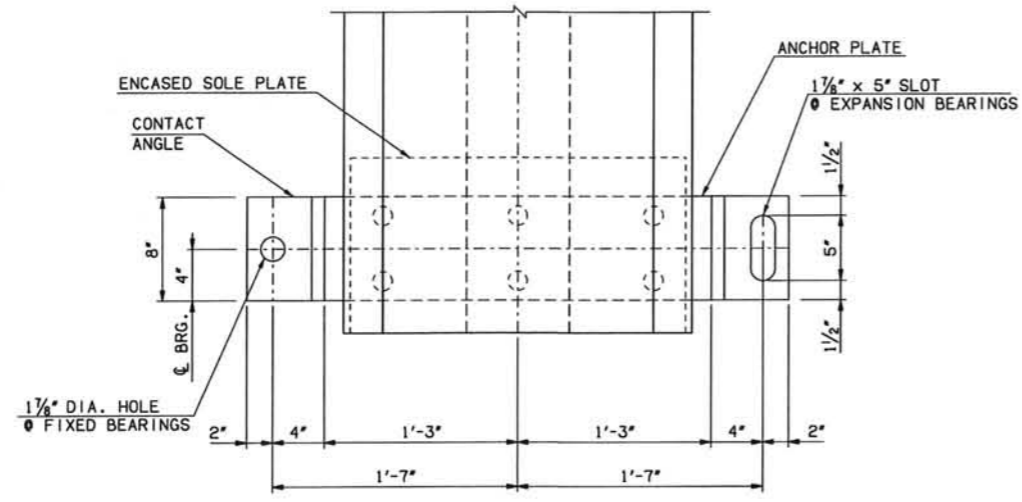
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|----------|------|-------|----------------------------|-----------------|
| Design | OKC | 10/09 | I-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 12/06 | I-40 RELOCATION BRIDGE I-G | |
| Checked | SOT | 9/12 | GENERAL NOTES AND | |
| Approved | SRJ | 9/12 | SUMMARY OF QUANTITIES | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 456 |

4/24/2013

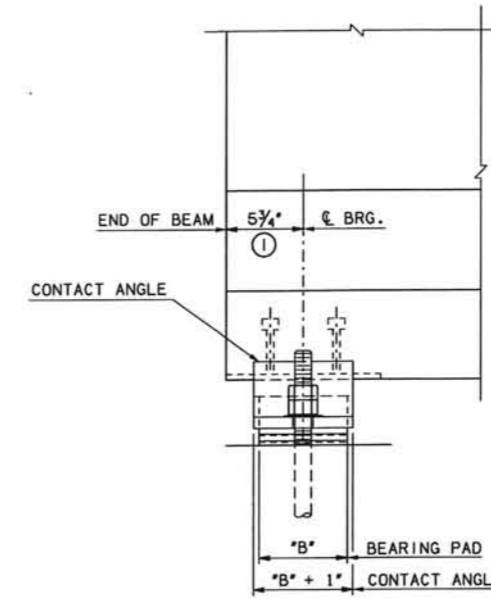
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| | | | | | | | | |
|-------------|----------------|-----------|---------------|-------|-------------|-------------|-----------|--------------|
| DESCRIPTION | CHANGE IN PLAN | DATE | BOOF DIVISION | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | | | 4 | OKLA | 17428(59) | 12 | 476 | 559 |
| DESCRIPTION | | REVISIONS | DATE | | | | | |

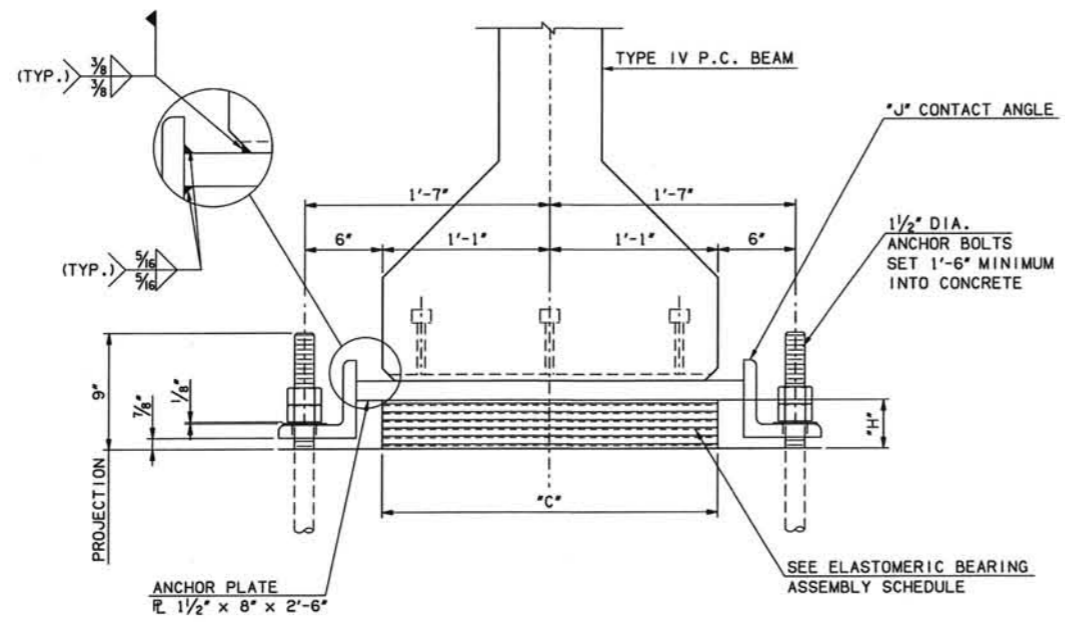


PLAN



SIDE VIEW

1 ANCHOR BOLTS SHALL BE CENTERED IN SLOTS DURING SETTING OF BEAM.



ELEVATION

NOTES:

1. ALL BEARING PADS SHALL BE BONDED TO ANCHOR PLATES IN ACCORDANCE WITH SUBSECTION 733.06(B) OF THE STANDARD SPECIFICATIONS.
2. DURING FIELD WELDING OF ANCHOR PLATE TO ENCASED PLATE THE TEMPERATURE ON THE BOTTOM FACE OF ANCHOR PLATE SHALL BE MAINTAINED BELOW 350° F. TEMPERATURE CRAYONS SHOULD BE USED TO MONITOR THE STEEL TEMPERATURE DURING WELDING.
3. PROVIDE STRUCTURAL STEEL FOR ANCHOR PLATES AND BUILT-UP CONTACT ANGLES IN ACCORDANCE WITH ASTM A240 (AUSTENITIC STAINLESS STEEL, TYPE 316, CHARPY V-NOTCH TESTING NOT REQUIRED). FOR ANCHOR BOLTS, PROVIDE CONTINUOUSLY THREADED BARS IN ACCORDANCE WITH ASTM A320, CLASS 2, GRADE B8M (AUSTENITIC STAINLESS STEEL, TYPE 316, CHARPY V-NOTCH TESTING NOT REQUIRED). USE AUSTENITIC STAINLESS STEEL NUTS AND WASHERS CONFORMING TO ASTM A194, GRADE 8M AND ASTM A320, RESPECTIVELY. PERFORM ALL WELDING CONSISTENT WITH PROCEDURES FOR STAINLESS STEEL.
4. PROVIDE AND INSTALL BEARING ASSEMBLIES OF THE SIZE, SHAPE AND LOCATION AS DETAILED IN THE PLANS. ALL COST FOR ANCHOR PLATES, ELASTOMERIC PADS, CONTACT ANGLES, ANCHOR BOLTS, NUTS, WASHERS, LABOR, MATERIALS, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "STAINLESS STEEL FIXED BEARING ASSEMBLY" AND "STAINLESS STEEL EXPANSION BEARING ASSEMBLY".

| | | | | |
|----------|------|------|----------------------------|-----------------|
| Design | OKC | 9/09 | I-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 9/09 | I-40 RELOCATION BRIDGE I-G | |
| Checked | | 9/12 | BEARING DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 476 |

4/24/2013

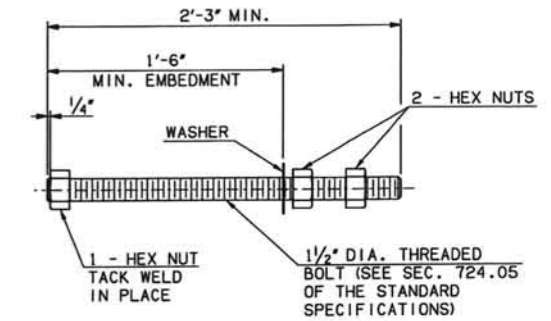
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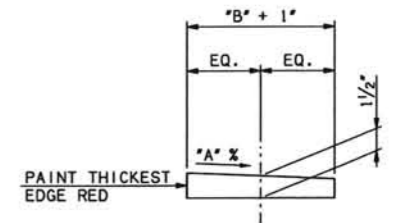
| SOOT DIVISION | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|---------------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 477 | 559 |

BEARING ASSEMBLY SCHEDULE

| LOCATION | GIRDER | *A* BEVEL SLOPE % | *B* LENGTH OF PAD | *C* WIDTH OF PAD | *D* NUMBER OF INTERIOR LAYERS | *E* HEIGHT OF INTERIOR LAYERS (IN) | *F* NUMBER OF 1/8" LAMINATES | *G* HEIGHT OF EXTERIOR LAYER (IN) | *H* TOTAL HEIGHT OF PADS (IN) | *J* CONTACT ANGLE | FIXITY |
|-----------------|--------|-------------------|-------------------|------------------|-------------------------------|------------------------------------|------------------------------|-----------------------------------|-------------------------------|-------------------|--------|
| SPAN 8, ABUT. 2 | A8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 8, ABUT. 2 | B8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 8, ABUT. 2 | C8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 8, ABUT. 2 | D8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 8, ABUT. 2 | E8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 8, ABUT. 2 | F8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 8, PIER 7 | A8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 8, PIER 7 | B8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 8, PIER 7 | C8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 8, PIER 7 | D8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 8, PIER 7 | E8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 8, PIER 7 | F8 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 7, PIER 7 | A7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 7, PIER 7 | B7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 7, PIER 7 | C7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 7, PIER 7 | D7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 7, PIER 7 | E7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 7, PIER 6 | A7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 7, PIER 6 | B7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 7, PIER 6 | C7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 7, PIER 6 | D7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 7, PIER 6 | E7 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | EXP |
| SPAN 6, PIER 6 | A6 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 6, PIER 6 | B6 | 1.12% | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 6, PIER 6 | C6 | 1.05% | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 6, PIER 6 | D6 | 1.04% | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 6, PIER 5 | A6 | | 7 | 26 | 8 | 1/2" | 9 | 1/4" | 5.625 | L 8X6X1X0'-8" | EXP |
| SPAN 6, PIER 5 | B6 | 1.12% | 7 | 26 | 8 | 1/2" | 9 | 1/4" | 5.625 | L 8X6X1X0'-8" | EXP |
| SPAN 6, PIER 5 | C6 | 1.05% | 7 | 26 | 8 | 1/2" | 9 | 1/4" | 5.625 | L 8X6X1X0'-8" | EXP |
| SPAN 6, PIER 5 | D6 | 1.04% | 7 | 26 | 8 | 1/2" | 9 | 1/4" | 5.625 | L 8X6X1X0'-8" | EXP |
| SPAN 5, PIER 5 | A5 | | 7 | 26 | 8 | 1/2" | 9 | 1/4" | 5.625 | L 8X6X1X0'-8" | EXP |
| SPAN 5, PIER 5 | B5 | | 7 | 26 | 8 | 1/2" | 9 | 1/4" | 5.625 | L 8X6X1X0'-8" | EXP |
| SPAN 5, PIER 5 | C5 | | 7 | 26 | 8 | 1/2" | 9 | 1/4" | 5.625 | L 8X6X1X0'-8" | EXP |
| SPAN 5, PIER 4 | A5 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 5, PIER 4 | B5 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 5, PIER 4 | C5 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 4, PIER 4 | A4 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 4, PIER 4 | B4 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 4, PIER 3 | A4 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 4, PIER 3 | B4 | | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 3, PIER 3 | A3 | 1.04% | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 3, PIER 2 | A3 | 1.04% | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 2, PIER 2 | A2 | 1.07% | 7 | 26 | 5 | 1/2" | 6 | 1/4" | 3.75 | L 6x6X1X0'-8" | FIX |
| SPAN 2, PIER 1 | A2 | 1.07% | 7 | 26 | 8 | 1/2" | 9 | 1/4" | 5.625 | L 8X6X1X0'-8" | EXP |

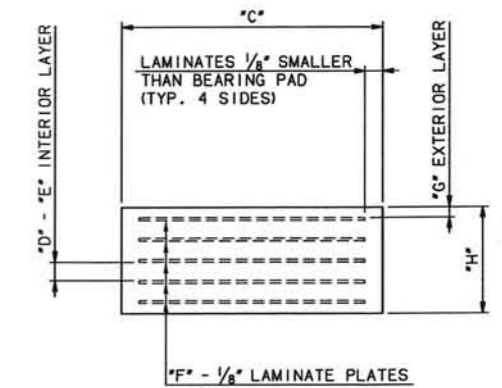


DETAIL OF ANCHOR BOLT



BEVELED ANCHOR PLATE DETAIL

BEVELED ANCHOR PLATE IS REQUIRED WHEN ANGLE BETWEEN UNDERSIDE OF GIRDER AND HORIZONTAL EXCEEDS 1%. ORIENT BEVEL SLOPE TO MATCH PROFILE GRADE AT PIER OR ABUTMENT.



BEARING PAD ELEVATION

(60 DUROMETER HARDNESS)
INCLUDE ALL COSTS OF BEARING PADS IN PRICE BID FOR FIXED OR STAINLESS STEEL EXPANSION BEARING ASSEMBLY

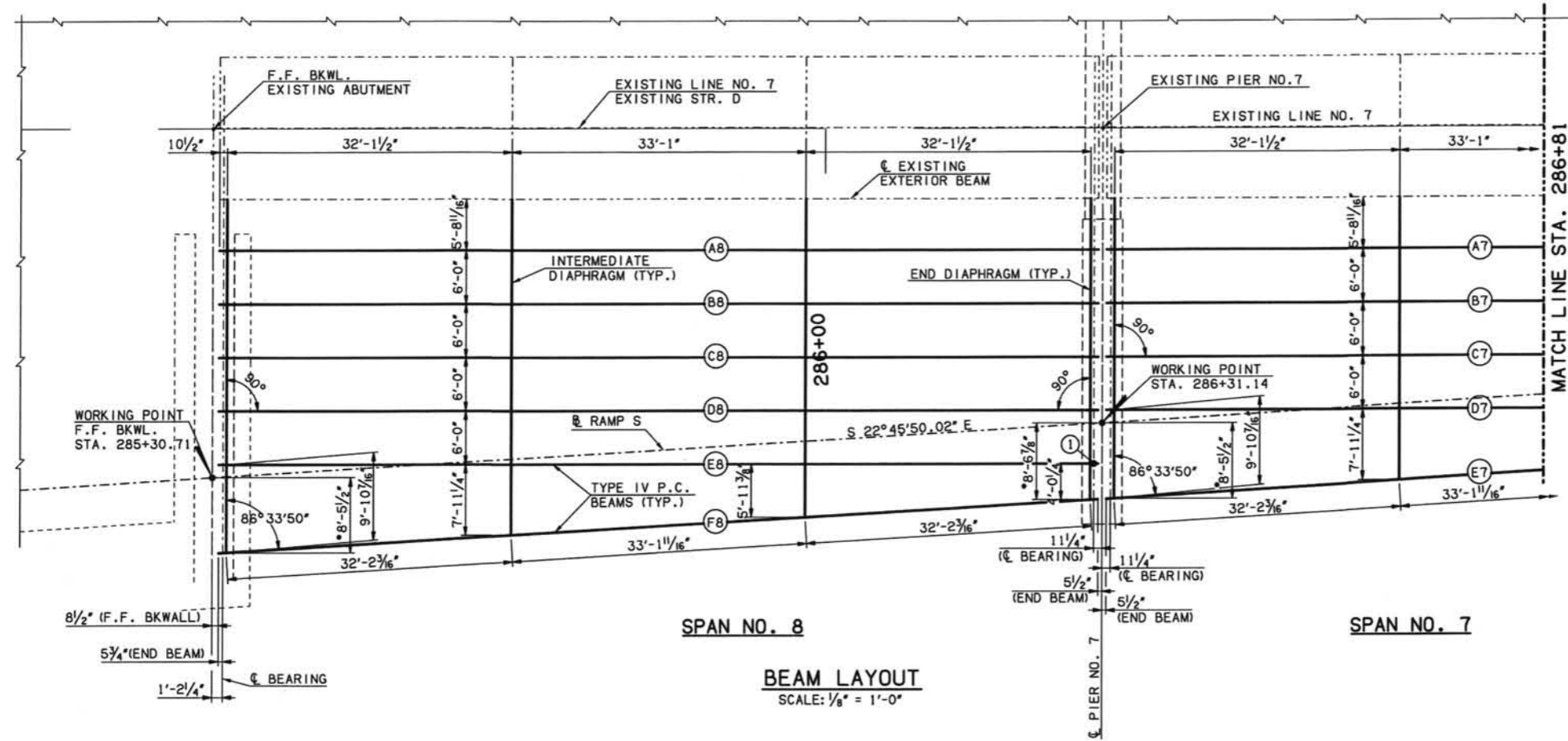
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|----------|------|-------|----------------------------|-----------------|
| Design | OKC | 11/08 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | JLM | 9/09 | 1-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | BEARING DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 477 |

| FOOT DESIGN | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|-------------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 478 | 559 |

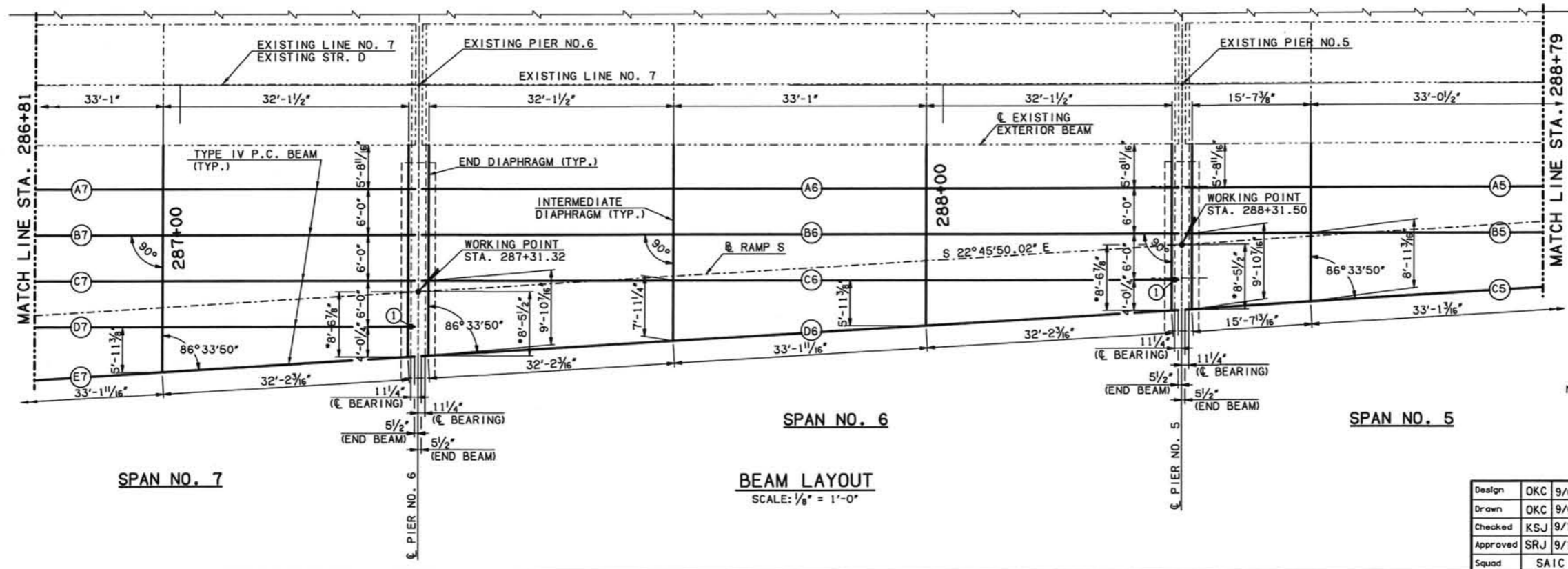
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BEAM LAYOUT
SCALE: 1/8" = 1'-0"



BEAM LAYOUT
SCALE: 1/8" = 1'-0"

① DENOTES 3/2" HAUNCH AT BEAM BEARING.

NOTE: DIMENSIONS NOTED THUS (*) ARE TAKEN FROM THE WORKING POINT TO THE CENTERLINE OF THE EXTERIOR BEAM BEARING.

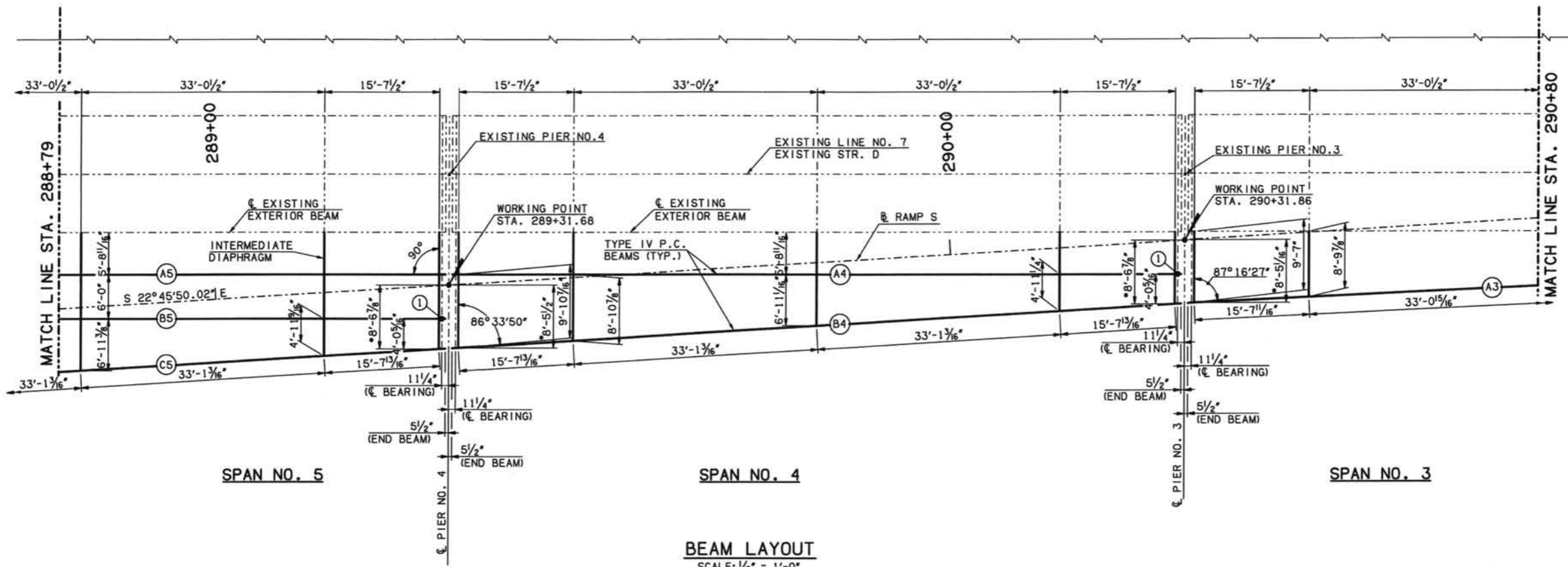
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| Design | OKC | 9/09 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 9/09 | 1-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | FRAMING PLAN | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 478 |

| BOOK | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 479 | 559 |

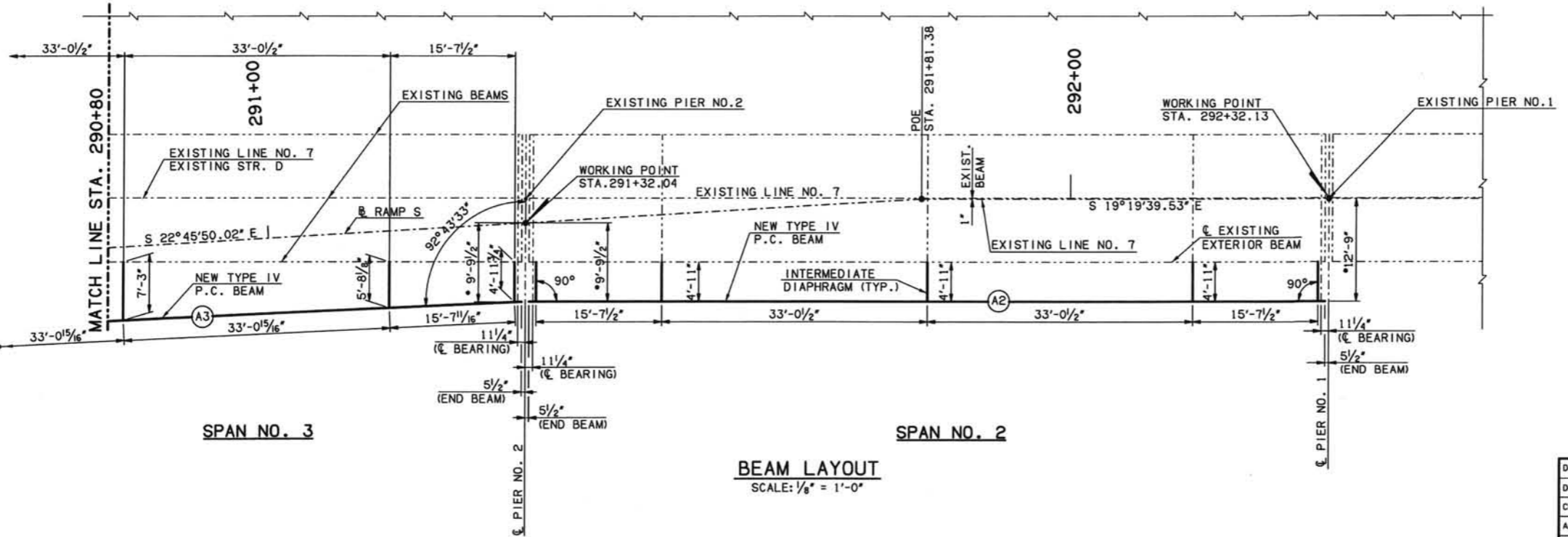
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BEAM LAYOUT
SCALE: 1/8" = 1'-0"



BEAM LAYOUT
SCALE: 1/8" = 1'-0"

① DENOTES 3/2" HAUNCH AT BEAM BEARING.

NOTE: DIMENSIONS NOTED THUS (*) ARE TAKEN FROM THE WORKING POINT TO THE CENTERLINE OF THE EXTERIOR BEAM BEARING.



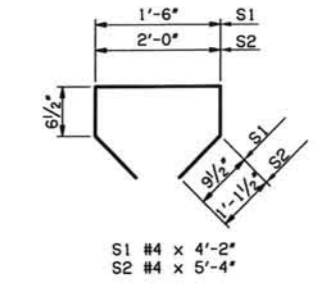
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|----------|------|------|----------------------------|-----------------|
| Design | OKC | 9/09 | I-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 9/09 | I-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | FRAMING PLAN | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 479 |

4/24/2013

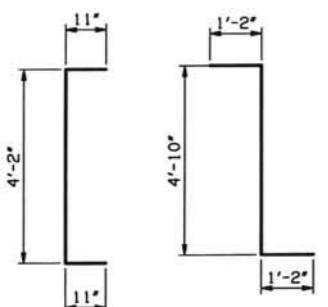
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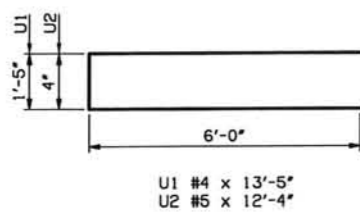
| DESCRIPTION | CHANGE IN PLAN | DATE | DDOT DIVISION | STATE | J.P. PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|-------------|----------------|------|---------------|-------|----------------|-------------|-----------|--------------|
| | | | 4 | OKLA | 17428(59) | 12 | 480 | 559 |
| DESCRIPTION | REVISIONS | DATE | | | | | | |



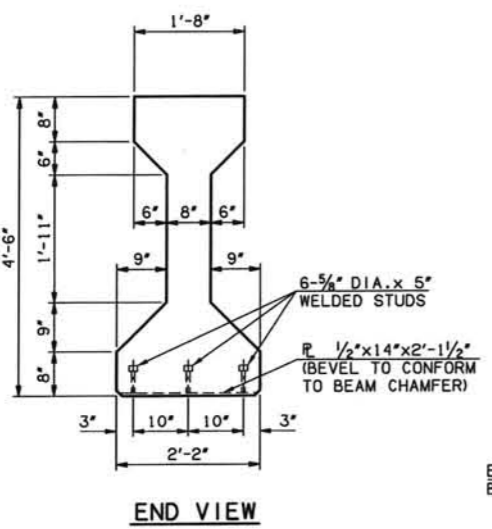
S1 #4 x 4'-2"
S2 #4 x 5'-4"



C #5 x 6'-0"
Z #5 x 7'-2" (EPOXY COATED)



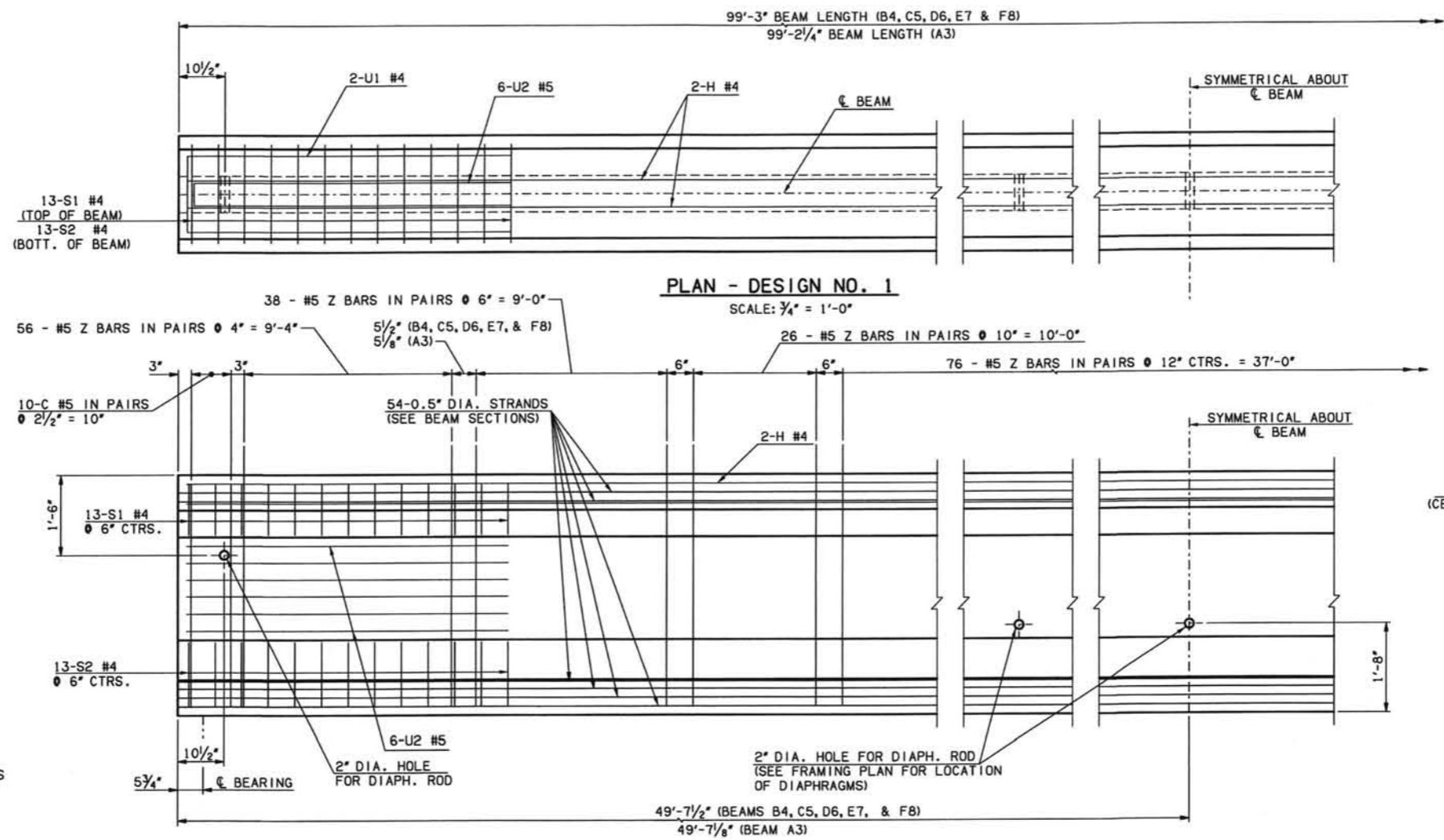
U1 #4 x 13'-5"
U2 #5 x 12'-4"
NOTE: ALL BAR BEND DIMENSIONS ARE OUT TO OUT.



END VIEW

ENCASED SOLE PLATE DETAIL
SCALE: 3/4" = 1'-0"

NOTE: ENCASED SOLE PLATE LOCATED AT EACH END OF BEAM.



PLAN - DESIGN NO. 1
SCALE: 3/4" = 1'-0"

13-S1 #4 (TOP OF BEAM)
13-S2 #4 (BOT. OF BEAM)

99'-3" BEAM LENGTH (B4, C5, D6, E7 & F8)
99'-2 1/4" BEAM LENGTH (A3)

2-U1 #4
6-U2 #5
2-H #4

38 - #5 Z BARS IN PAIRS @ 6" = 9'-0"
56 - #5 Z BARS IN PAIRS @ 4" = 9'-4"
10-C #5 IN PAIRS @ 2 1/2" = 10"
54-0.5" DIA. STRANDS (SEE BEAM SECTIONS)
26 - #5 Z BARS IN PAIRS @ 10" = 10'-0"
76 - #5 Z BARS IN PAIRS @ 12" CTRS. = 37'-0"

5 1/2" (B4, C5, D6, E7, & F8)
5 1/8" (A3)

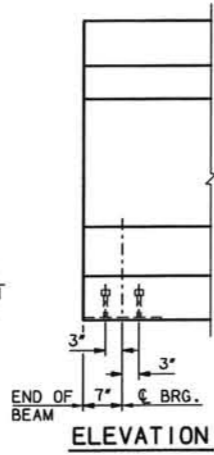
13-S1 #4 @ 6" CTRS.
13-S2 #4 @ 6" CTRS.

6-U2 #5
2-H #4

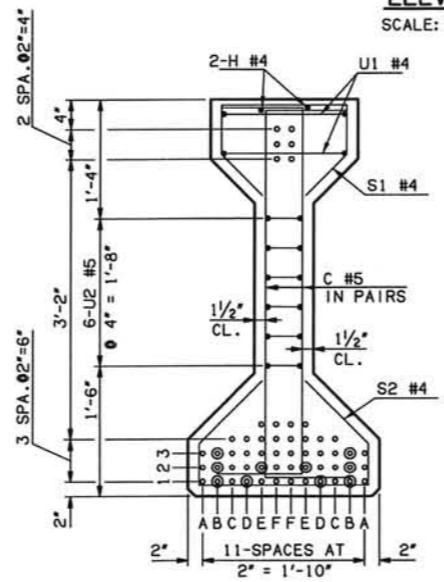
2" DIA. HOLE FOR DIAPH. ROD (SEE FRAMING PLAN FOR LOCATION OF DIAPHRAGMS)

49'-7 1/2" (BEAMS B4, C5, D6, E7, & F8)
49'-7 1/8" (BEAM A3)

ELEVATION
SCALE: 3/4" = 1'-0"

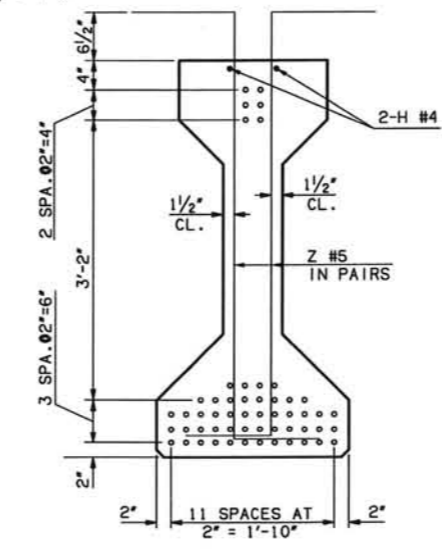


ELEVATION



END SECTION
SCALE: 1" = 1'-0"

BEAM SECTIONS
(54-0.5" DIA. STRANDS)

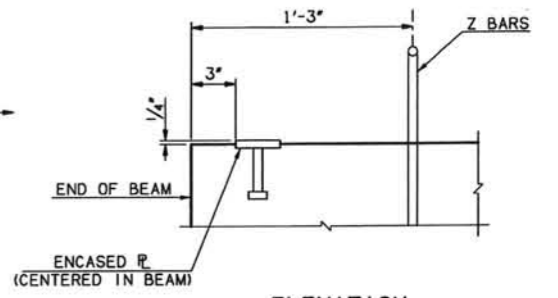


CL SECTION
SCALE: 1" = 1'-0"

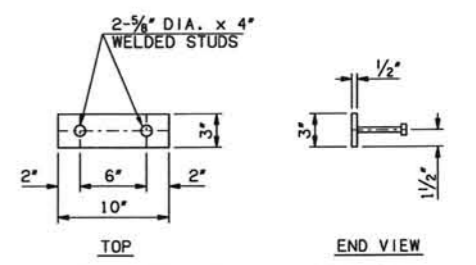
| DEBONDED PAIR | DEBONDED LENGTH |
|---------------|-----------------|
| B1 | 6'-0" |
| D1 | 10'-0" |
| B2 | 4'-0" |
| E2 | 4'-0" |
| B3 | 2'-0" |

NOTE: CIRCLED STRANDS DENOTE DEBONDED STRANDS

PRESTRESSED CONCRETE BEAM NOTES
 COMPRESSIVE STRENGTH:
 THE REQUIRED COMPRESSIVE STRENGTH OF THE CONCRETE IS 6,000 P.S.I. AT TRANSFER OF PRESTRESS AND 8,000 P.S.I. AT 28 DAYS.
 STRAND TYPE:
 THE REQUIRED STRAND TYPE IS LOW-RELAXATION. USE STRAND HAVING A NOMINAL DIAMETER OF 0.5" WITH ULTIMATE TENSILE STRENGTH OF 270 K.S.I.
 LFD OPERATING RATING - HS 44 (FROM EXISTING PLANS)

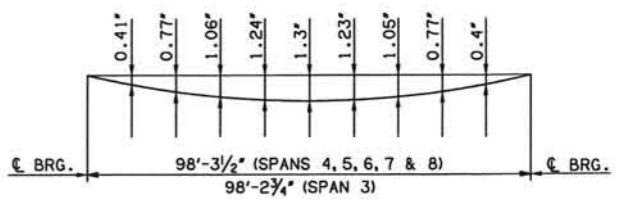


ELEVATION



ENCASED PLATE DETAILS

NOTE: ENCASED BEAM PLATE LOCATED AT EXPANSION END OF BEAM.



DEAD LOAD DEFLECTION DIAGRAM

NOTE: THE DEAD LOAD DEFLECTION SHOWN ABOVE AT THE TENTH POINTS ARE THE INITIAL DEFLECTIONS DUE TO DECK SLAB + DIAPHRAGMS + 5 P.S.F. DECK FORM ALLOWANCE + CONCRETE PARAPET. IT DOES NOT INCLUDE THE BEAM WEIGHT OR FUTURE WEARING SURFACE.

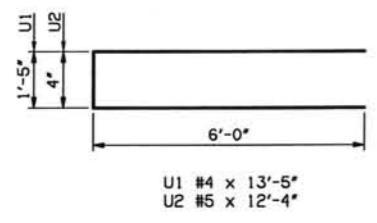
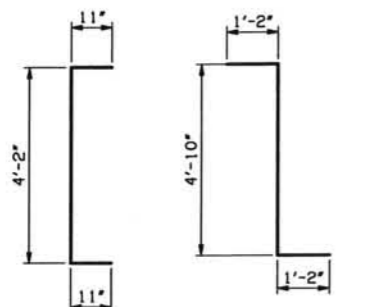
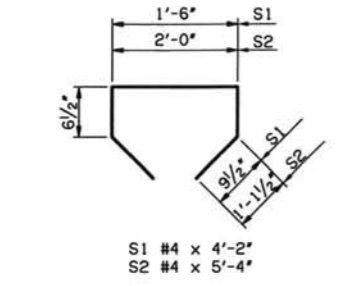
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|----------|------|------|----------------------------|-----------------|
| Design | OKC | 9/09 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 9/09 | 1-40 RELOCATION BRIDGE 1-G | |
| Checked | NO | 9/12 | P.C. BEAM DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 480 |

4/24/2013

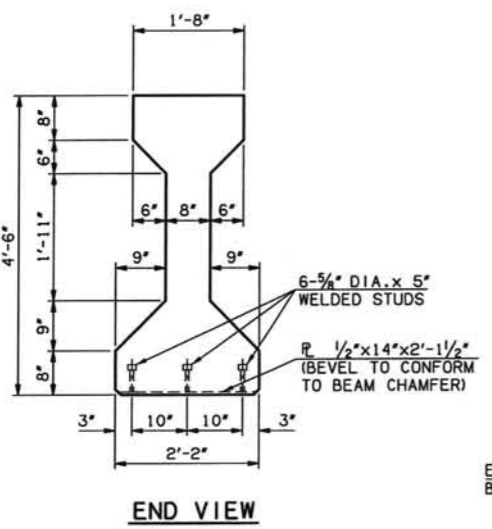
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| DESCRIPTION | CHANGE IN PLAN | DATE | ROOT DIVISION | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|-------------|----------------|------|---------------|-------|-------------|-------------|----------|--------------|
| | | | 4 | OKLA | 17428(59) | 12 | 481 | 559 |
| DESCRIPTION | REVISIONS | DATE | | | | | | |

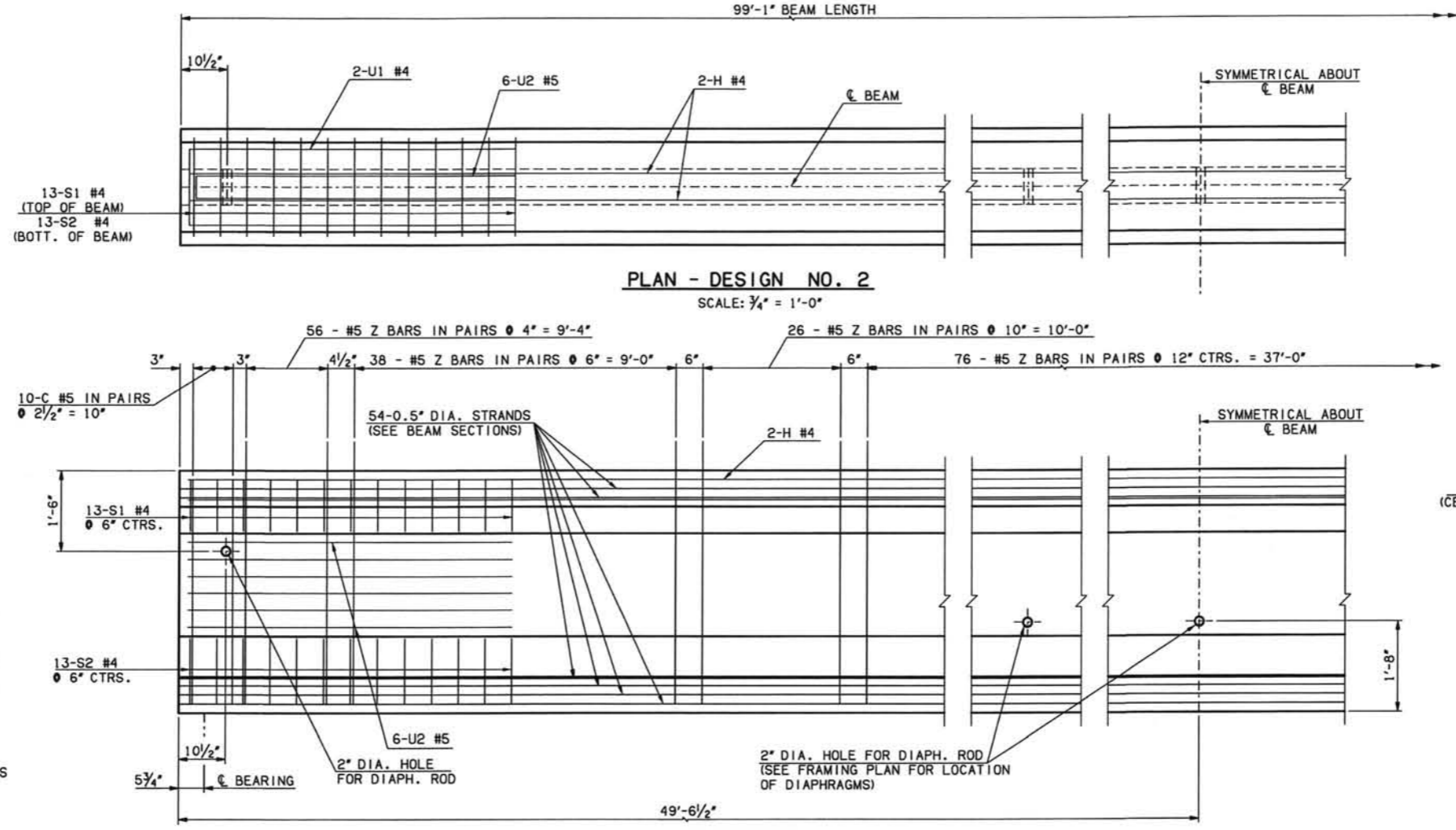


NOTE: ALL BAR BEND DIMENSIONS ARE OUT TO OUT.

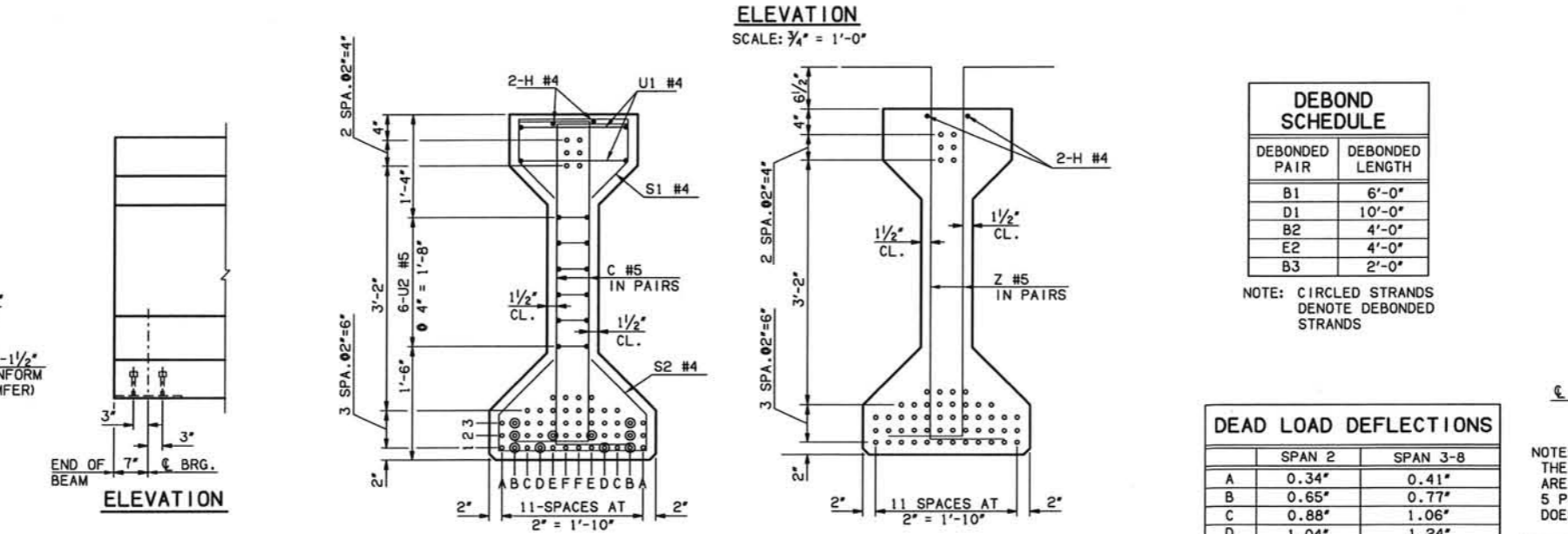


ENCASED SOLE PLATE DETAIL
SCALE: 3/4" = 1'-0"

NOTE: ENCASED SOLE PLATE LOCATED AT EACH END OF BEAM.



PLAN - DESIGN NO. 2
SCALE: 3/4" = 1'-0"



ELEVATION
SCALE: 3/4" = 1'-0"

END SECTION
SCALE: 1" = 1'-0"

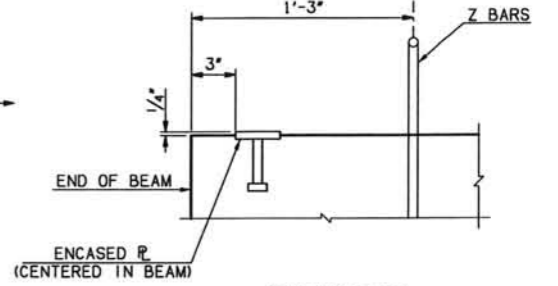
BEAM SECTIONS
(54-0.5" DIA. STRANDS)

| DEBOND SCHEDULE | |
|-----------------|-----------------|
| DEBONDED PAIR | DEBONDED LENGTH |
| B1 | 6'-0" |
| D1 | 10'-0" |
| B2 | 4'-0" |
| E2 | 4'-0" |
| B3 | 2'-0" |

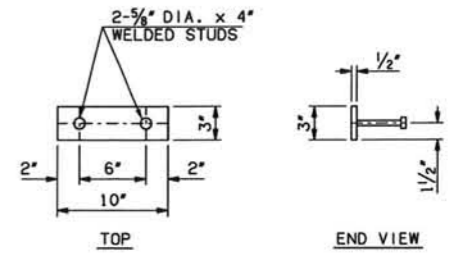
NOTE: CIRCLED STRANDS DENOTE DEBONDED STRANDS

| DEAD LOAD DEFLECTIONS | | |
|-----------------------|--------|----------|
| | SPAN 2 | SPAN 3-8 |
| A | 0.34" | 0.41" |
| B | 0.65" | 0.77" |
| C | 0.88" | 1.06" |
| D | 1.04" | 1.24" |
| E | 1.09" | 1.3" |
| F | 1.04" | 1.23" |
| G | 0.88" | 1.05" |
| H | 0.65" | 0.77" |
| I | 0.34" | 0.4" |

PRESTRESSED CONCRETE BEAM NOTES
 COMPRESSIVE STRENGTH:
 THE REQUIRED COMPRESSIVE STRENGTH OF THE CONCRETE IS 6,000 P.S.I. AT TRANSFER OF PRESTRESS AND 8,000 P.S.I. AT 28 DAYS.
 STRAND TYPE:
 THE REQUIRED STRAND TYPE IS LOW-RELAXATION. USE STRAND HAVING A NOMINAL DIAMETER OF 0.5" WITH ULTIMATE TENSILE STRENGTH OF 270 K.S.I.
 LFD OPERATING RATING - HS 44 (FROM EXISTING PLANS)



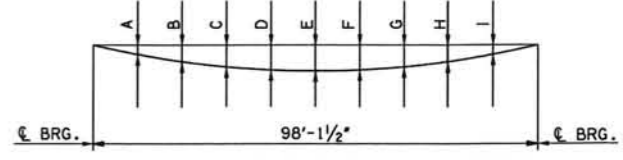
ELEVATION



ENCASED PLATE DETAILS

NOTE: ENCASED BEAM PLATE LOCATED AT EXPANSION END OF BEAM.

| BEAM DESIGN SCHEDULE | |
|----------------------|--------------------|
| SPAN NO. | BEAM NO. |
| 8 | A8, B8, C8, D8, E8 |
| 7 | A7, B7, C7, D7 |
| 6 | A6, B6, C6 |
| 5 | A5, B5 |
| 4 | A4 |
| 2 | A2 |



DEAD LOAD DEFLECTION DIAGRAM

NOTE: THE DEAD LOAD DEFLECTION SHOWN ABOVE AT THE TENTH POINTS ARE THE INITIAL DEFLECTIONS DUE TO DECK SLAB + DIAPHRAGMS + 5 P.S.F. DECK FORM ALLOWANCE + CONCRETE PARAPET. IT DOES NOT INCLUDE THE BEAM WEIGHT OR FUTURE WEARING SURFACE.

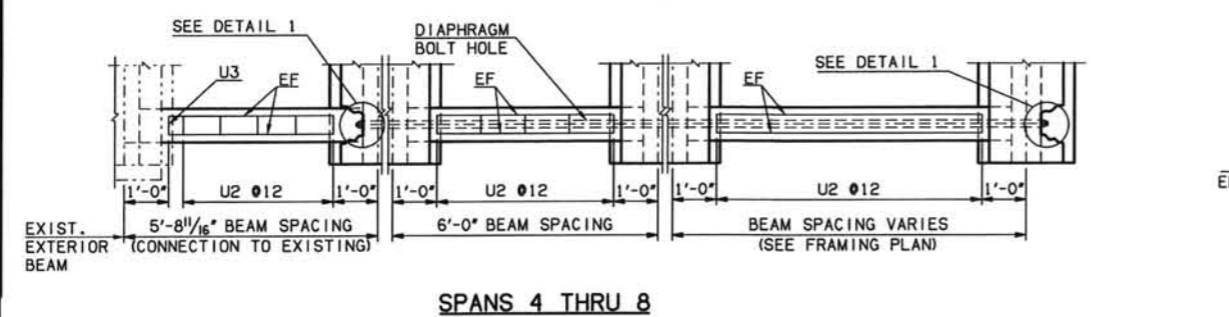
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|----------|------|------|----------------------------|-----------------|
| Design | OKC | 9/09 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 9/09 | 1-40 RELOCATION BRIDGE 1-G | |
| Checked | NO | 9/12 | P.C. BEAM DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 481 |

| DDOT DIVISION | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|---------------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 482 | 559 |

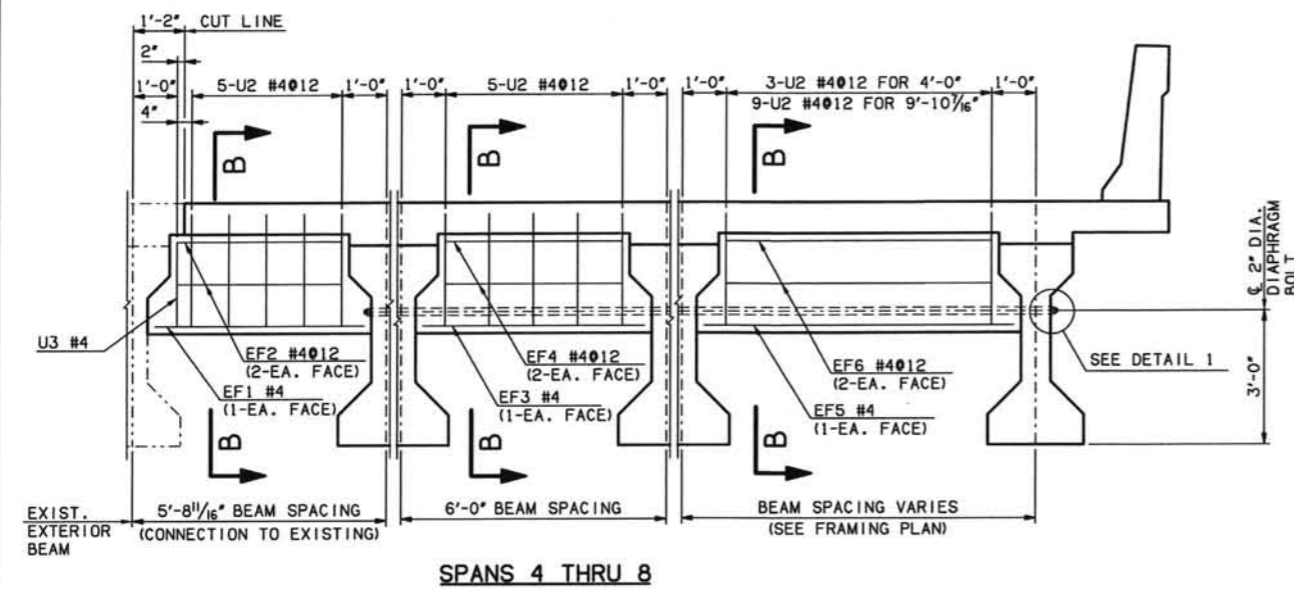
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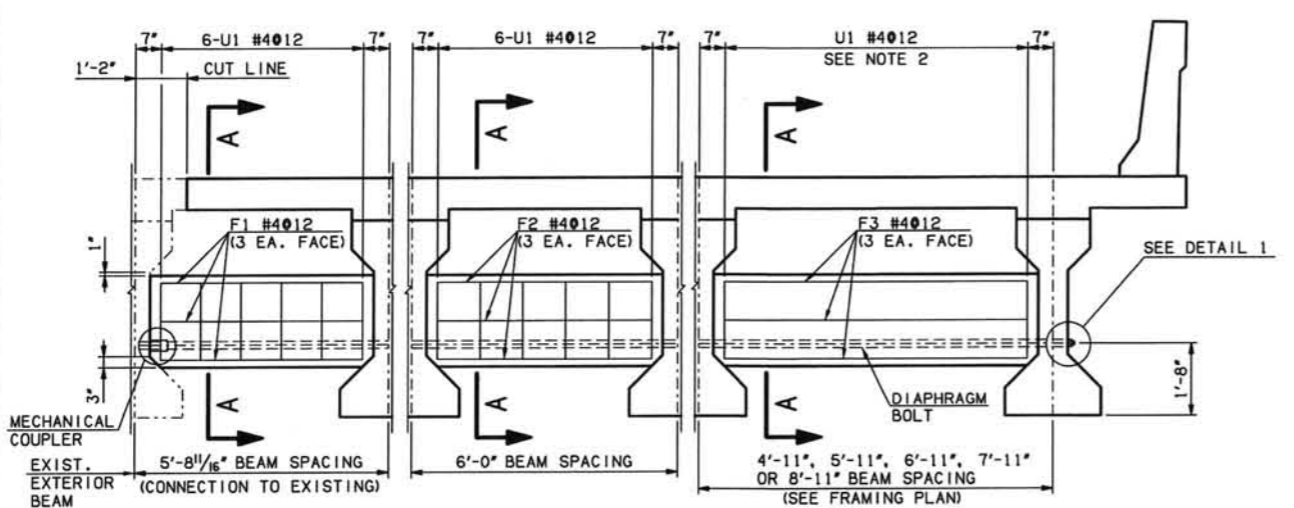
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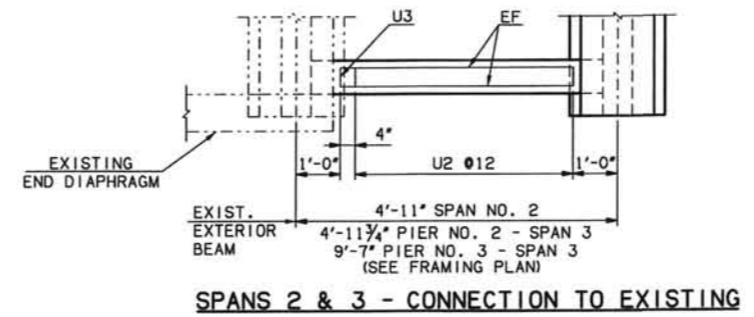
END DIAPHRAGM - PLAN
SCALE: 1/2" = 1'-0"



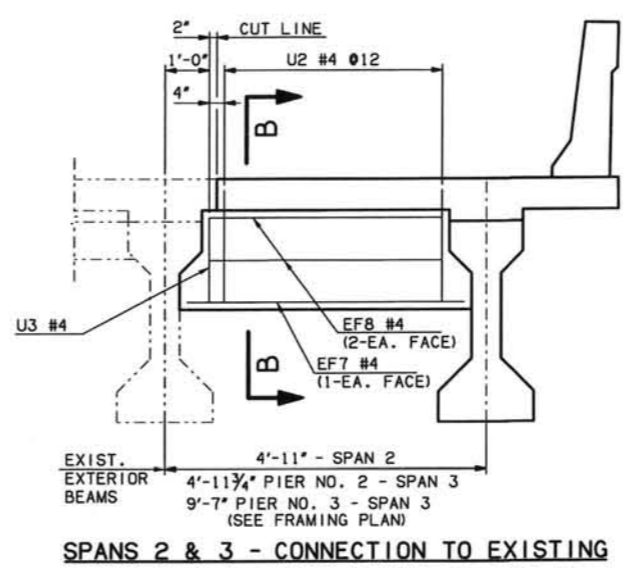
END DIAPHRAGM - ELEVATION
SCALE: 1/2" = 1'-0"



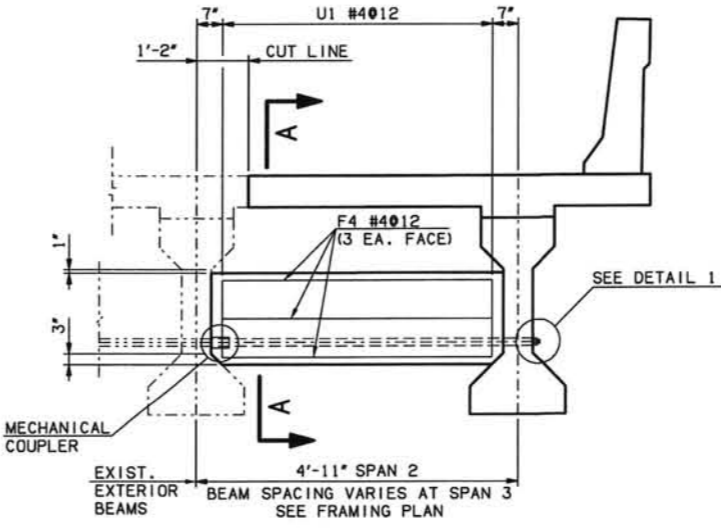
INTERMEDIATE DIAPHRAGM - ELEVATION
SCALE: 1/2" = 1'-0"



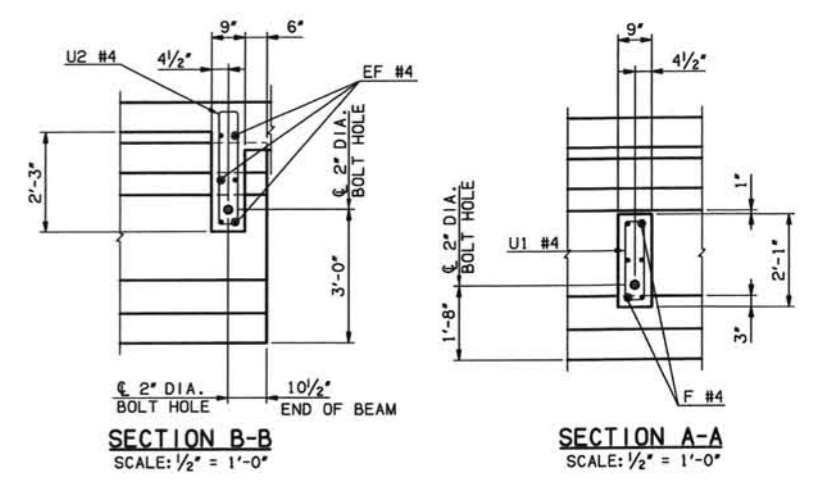
SPANS 2 & 3 - CONNECTION TO EXISTING



SPANS 2 & 3 - CONNECTION TO EXISTING

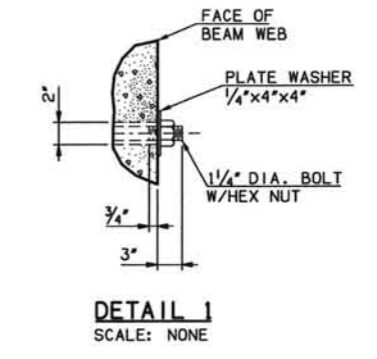


SPANS 2 & 3 CONNECTION TO EXISTING

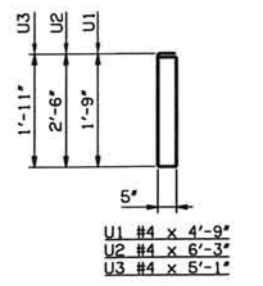


SECTION B-B
SCALE: 1/2" = 1'-0"

SECTION A-A
SCALE: 1/2" = 1'-0"



DETAIL 1
SCALE: NONE



NOTES:

- MECHANICAL COUPLERS: CONTRACTOR SHALL PROVIDE MECHANICAL COUPLERS ALONG THE EXISTING EXTERIOR BEAM LINE AT THE INTERMEDIATE DIAPHRAGM LOCATIONS TO COUPLE THE EXISTING DIAPHRAGM BOLTS WITH THE NEW DIAPHRAGM BOLTS OF THE NEW INTERMEDIATE DIAPHRAGMS. MECHANICAL COUPLERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO INSTALLATION. ALL COST OF MECHANICAL COUPLERS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.
- 5-U1 FOR 4'-11"; 6-U1 FOR 5'-11"; 7-U1 FOR 6'-11"; 8-U1 FOR 7'-11"; 9-U1 FOR 8'-11" DIAPHRAGMS.

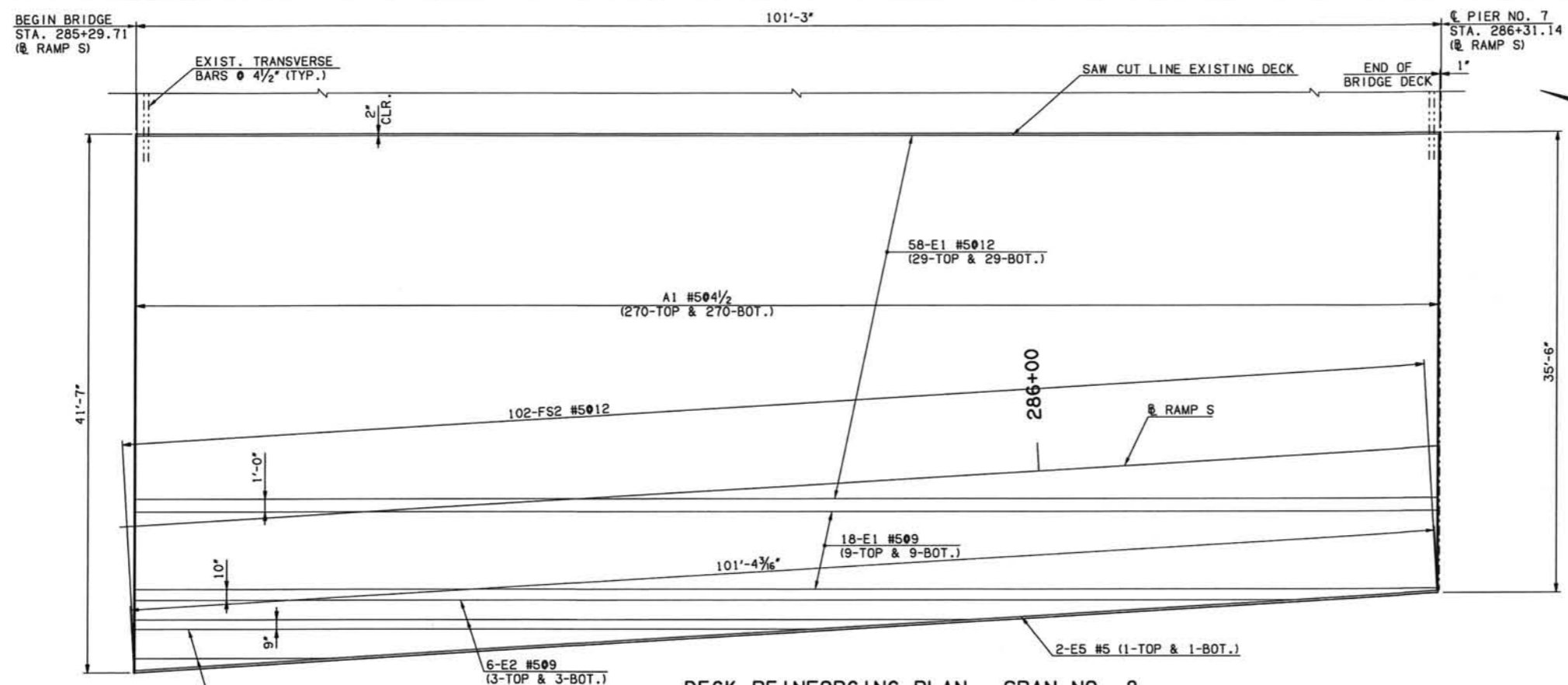
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|----------|------|------|----------------------------|-----------------|
| Design | OKC | 9/09 | I-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 9/09 | I-40 RELOCATION BRIDGE I-G | |
| Checked | SOT | 9/12 | DIAPHRAGM DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 482 |

4/24/2013

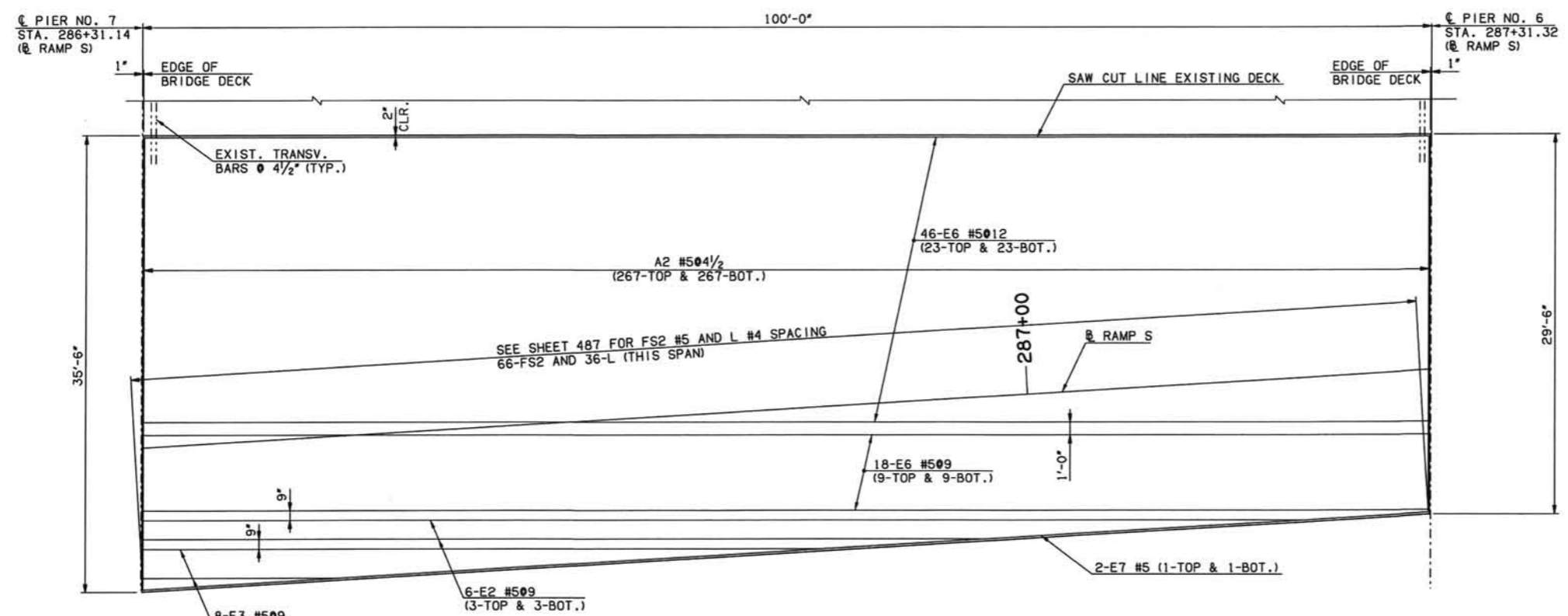
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| BOOK DESIGN | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|-------------|-------|-------------|-------------|-----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 483 | 559 |



DECK REINFORCING PLAN - SPAN NO. 8
SCALE: 3/16" = 1'-0"



DECK REINFORCING PLAN - SPAN NO. 7
SCALE: 3/16" = 1'-0"

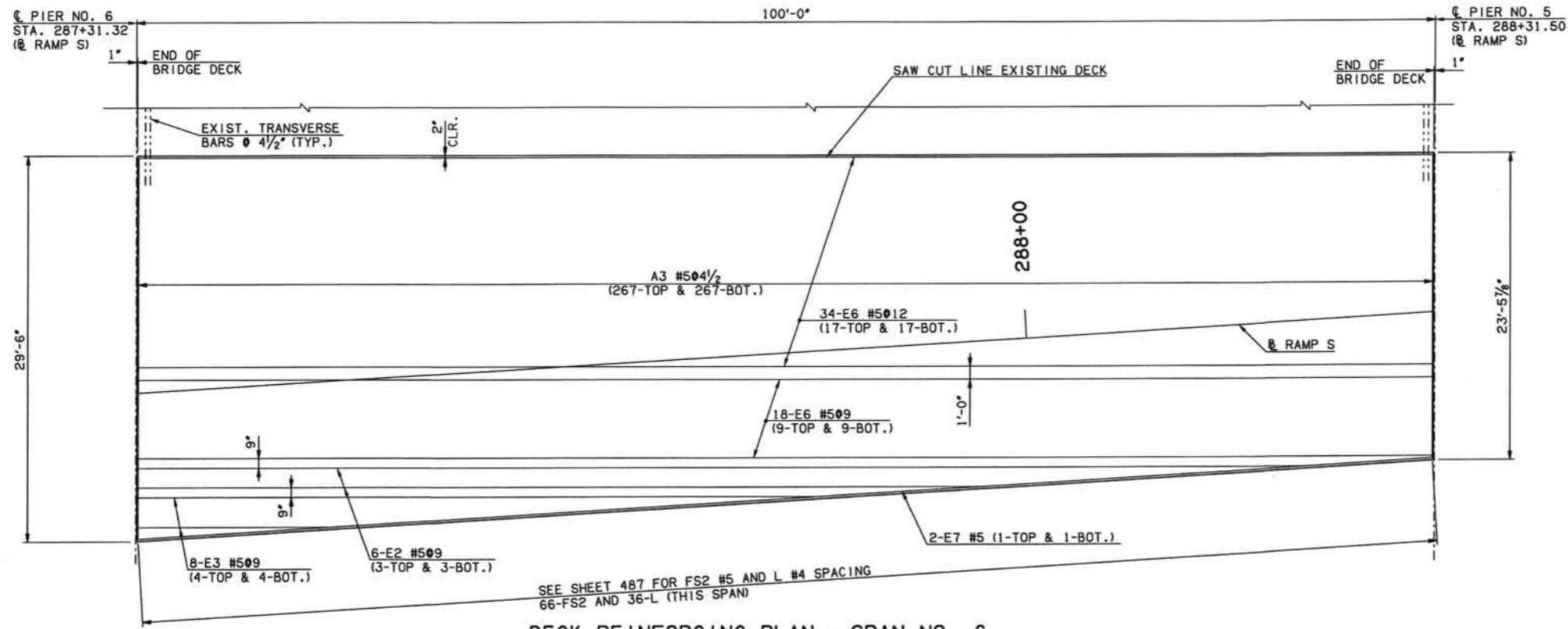
SUPERSTRUCTURE BAR LIST

| EPOXY COATED REINFORCING | | | | | |
|--------------------------|------|------|------|--------------|------------------|
| MARK | SIZE | QTY. | FORM | LENGTH | REMARKS |
| ⑤ A1 | #5 | 540 | STR. | 36'-1" AVG. | 33'-0" TO 39'-1" |
| ⑤ A2 | #5 | 534 | STR. | 30'-0" AVG. | 27'-0" TO 33'-0" |
| ⑤ A3 | #5 | 534 | STR. | 24'-0" AVG. | 21'-0" TO 27'-0" |
| ⑤ A4 | #5 | 480 | STR. | 18'-0" AVG. | 15'-0" TO 21'-0" |
| ⑤ A5 | #5 | 480 | STR. | 12'-0" AVG. | 9'-0" TO 15'-0" |
| ⑤ A6 | #5 | 378 | STR. | 6'-8" AVG. | 4'-3" TO 9'-0" |
| ⑤ A7 | #5 | 584 | STR. | 4'-3" | |
| AD | #5 | 4 | STR. | 41'-3" | |
| ① E1 | #5 | 76 | STR. | 103'-7" | |
| ① E2 | #5 | 30 | STR. | 80'-2" AVG. | 67'-8" TO 92'-8" |
| ① E3 | #5 | 40 | STR. | 33'-10" AVG. | 15'-1" TO 52'-7" |
| ① E4 | #5 | 10 | STR. | 40'-6" AVG. | 21'-9" TO 59'-2" |
| ① E5 | #5 | 2 | STR. | 103'-9" | |
| ① E6 | #5 | 214 | STR. | 102'-3" | |
| ① E7 | #5 | 8 | STR. | 102'-5" | |
| ① E8 | #5 | 2 | STR. | 74'-3" | |
| ① E9 | #5 | 2 | STR. | 81'-4" | |
| ① E10 | #5 | 2 | STR. | 21'-0" | |
| EP1 | #7 | 12 | STR. | 36'-0" | |
| ② FS2 | #5 | 507 | BNT. | 7'-4" | SEE STD. FSHP-42 |
| L | #4 | 208 | BNT. | 1'-3" | SEE STD. FSHP-42 |
| UD | #4 | 42 | BNT. | 3'-9" | |
| EF1 | #4 | 20 | STR. | 4'-8" | |
| EF2 | #4 | 40 | STR. | 3'-8" | |
| EF3 | #4 | 40 | STR. | 5'-0" | |
| EF4 | #4 | 80 | STR. | 4'-0" | |
| EF5 | #4 | 20 | STR. | 8'-10" | |
| ⑥ EF6 | #4 | 40 | STR. | 7'-10" | |
| ⑦ EF7 | #4 | 8 | STR. | 8'-7" | |
| ⑧ EF8 | #4 | 16 | STR. | 7'-7" | |
| ⑨ F1 | #4 | 72 | STR. | 4'-6" | |
| F2 | #4 | 126 | STR. | 4'-10" | |
| ③ F3 | #4 | 72 | STR. | 7'-9" | |
| ④ F4 | #4 | 36 | STR. | 7'-8" | |
| U1 | #4 | 319 | BNT. | 4'-9" | |
| U2 | #4 | 232 | BNT. | 6'-3" | |
| U3 | #4 | 14 | BNT. | 5'-1" | |

- ① INCLUDES 1 - 2'-7" LAP
- ② FIELD BEND FS2 BARS AS NEEDED FOR SLOPED FACE PARAPET.
- ③ FIELD TRIM TO:
3'-9" FOR 4'-11" LONG DIAPHRAGM
4'-9" FOR 5'-11" LONG DIAPHRAGM
5'-9" FOR 6'-11" LONG DIAPHRAGM
6'-9" FOR 7'-11" LONG DIAPHRAGM
- ④ FIELD TRIM TO:
3'-9" FOR 4'-11" LONG DIAPHRAGM
4'-6" FOR 5'-8" LONG DIAPHRAGM
6'-1" FOR 7'-3" LONG DIAPHRAGM
- ⑤ BAR LIST ASSUMES BARS WILL BUTT ENDS IN THE COUPLERS. IF BARS ARE TO BE OVERLAPPED, INCLUDE THE COST OF ADDITIONAL REINFORCING IN THE PRICE BID FOR MECHANICAL SPLICES.
- ⑥ FIELD TRIM TO:
3'-0" FOR 4'-0" LONG DIAPHRAGM
- ⑦ FIELD TRIM TO:
2'-0" FOR 4'-0" LONG DIAPHRAGM
- ⑧ FIELD TRIM TO:
3'-11" FOR 4'-11" LONG DIAPHRAGM
- ⑨ FIELD TRIM TO:
2'-11" FOR 4'-11" LONG DIAPHRAGM

| | | | | |
|----------|------|------|-------------------------------|-----------------|
| Design | OKC | 2/12 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RAH | 2/12 | 1-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 483 |

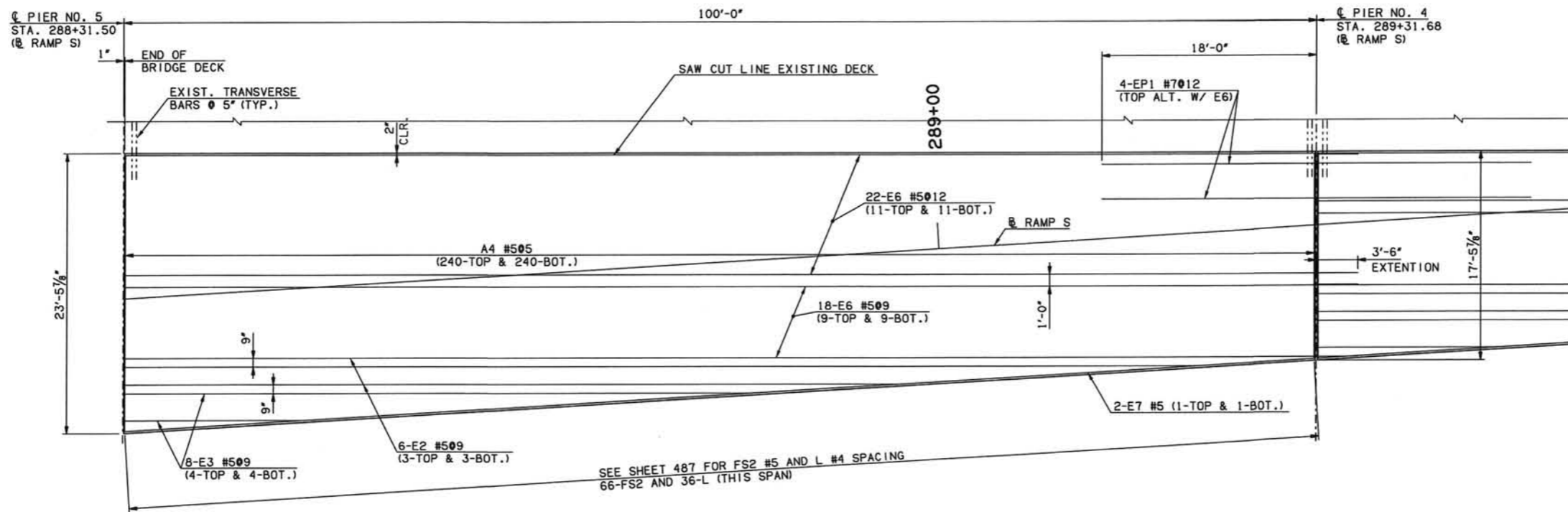
| DDOT DIVISION | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|---------------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 484 | 559 |



DECK REINFORCING PLAN - SPAN NO. 6
SCALE: 3/16" = 1'-0"

BRIDGE DECK NOTES:

- BRIDGE DECK SHALL BE CLASS AA CONCRETE WITH MAXIMUM 1" DIA. AGGREGATE.
- ALL CONSTRUCTION SHALL COMPLY WITH THE REQUIREMENTS OF THE 2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.
- SURFACE OF ROADWAY SHALL SLOPE 2.0% FROM PROFILE GRADE LINE.
- ALL EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER UNLESS OTHERWISE NOTED.
- TRANSVERSE REINFORCING STEEL IN THE BRIDGE DECK SHALL BEGIN AND END A MAXIMUM OF 2" FROM THE END OF CONCRETE.
- LONGITUDINAL REINFORCING STEEL SHALL BEGIN AND END 1" FROM END OF SLAB.
- ALL LONGITUDINAL TOP REINFORCING STEEL IN THE DECK SHALL BE SUPPORTED ON APPROVED CONTINUOUS EPOXY COATED METAL HIGH CHAIRS (HMC) SPACED AT 4'-0" MAXIMUM AND BOTTOM LAYER OF REINFORCING STEEL SHALL BE SUPPORTED ON APPROVED EPOXY COATED METAL SLAB BOLSTERS (SBL) SPACED AT 4'-0" MAXIMUM.
- ALL TIE WIRES USED TO TIE THE REINFORCING STEEL IN THE DECK SHALL BE PLASTIC COATED.



DECK REINFORCING PLAN - SPAN NO. 5
SCALE: 3/16" = 1'-0"

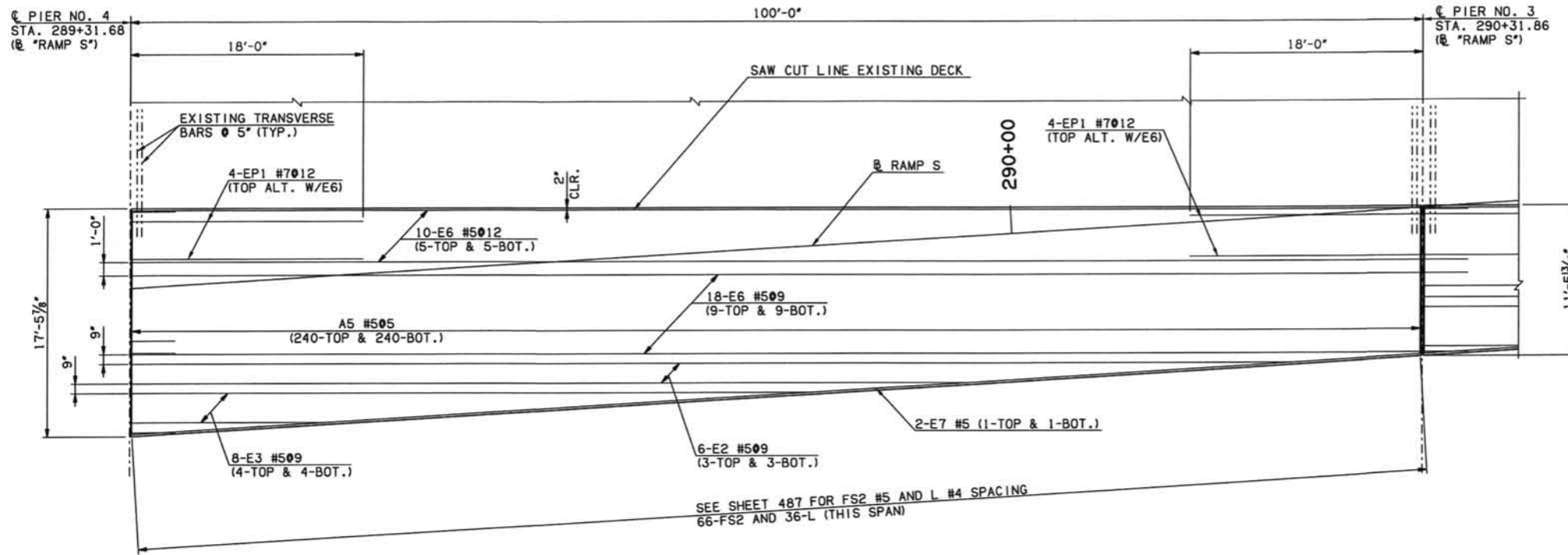
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|----------|------|------|----------------------------|-----------------|
| Design | OKC | 2/12 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RAH | 2/12 | 1-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 484 |

4/24/2013

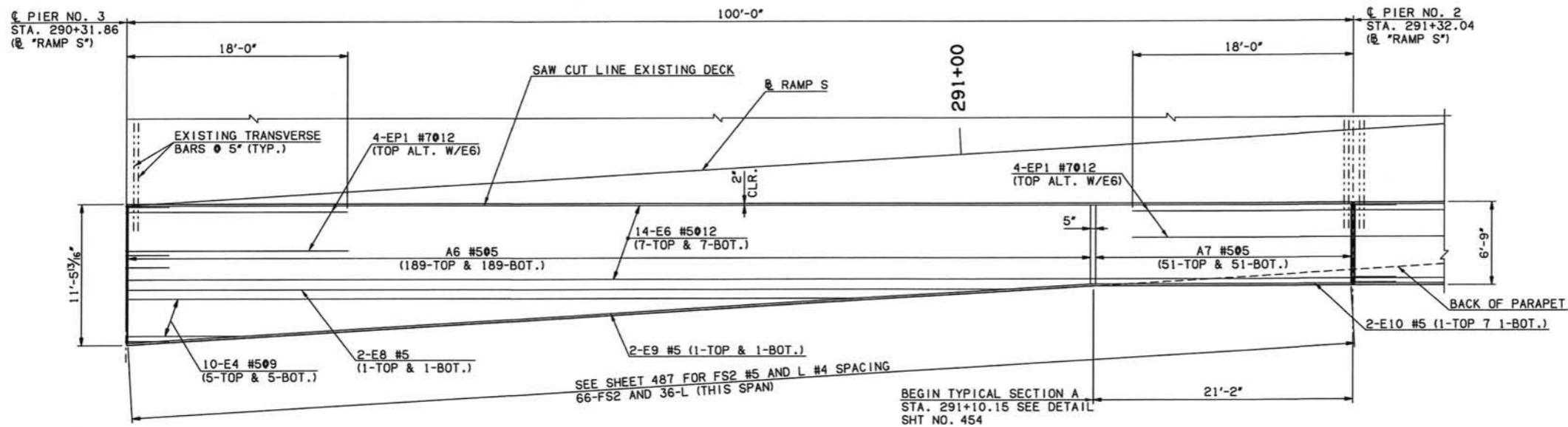
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| BOOTH DIVISION | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|----------------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 485 | 559 |



DECK REINFORCING PLAN - SPAN NO. 4
SCALE: 3/16" = 1'-0"



DECK REINFORCING PLAN - SPAN NO. 3
SCALE: 3/16" = 1'-0"

NOTE:
FS2 BARS ARE PLACED ON THE SAME ALIGNMENT AS THE PARAPET TO THE NORTH, TO THE PI AT STA. 291+81.38. IT RUNS PARALLEL TO THE EDGE OF SLAB FROM THE PI TO THE TIE-IN AT PIER NO. 1.

| | | | | |
|----------|------|------|-------------------------------|-----------------|
| Design | OKC | 2/12 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RAH | 2/12 | 1-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 485 |

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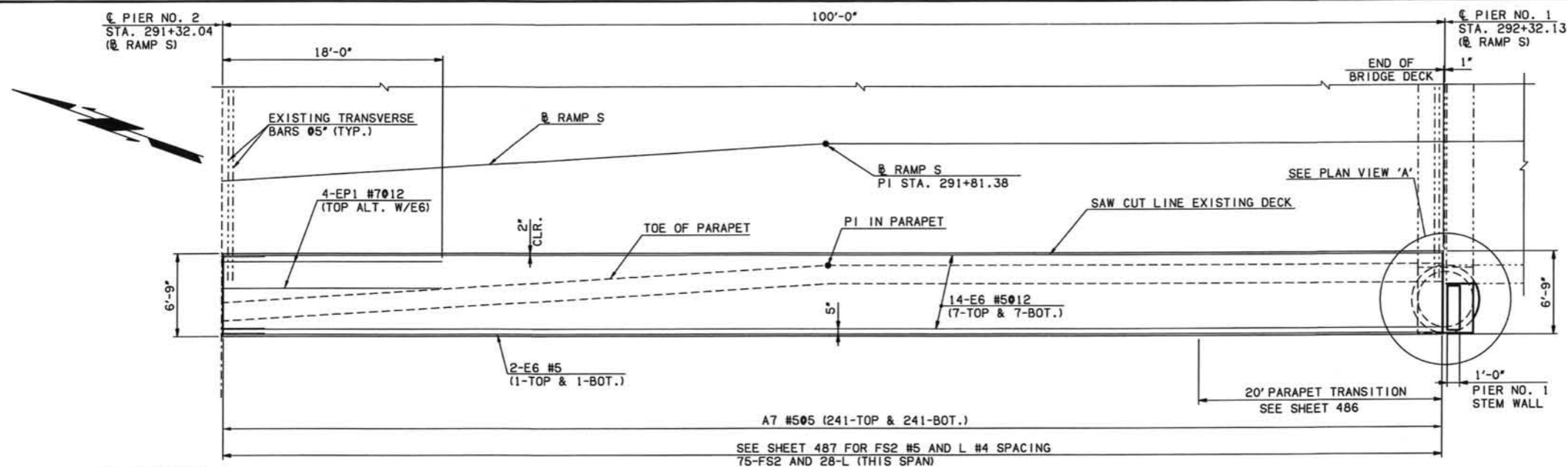
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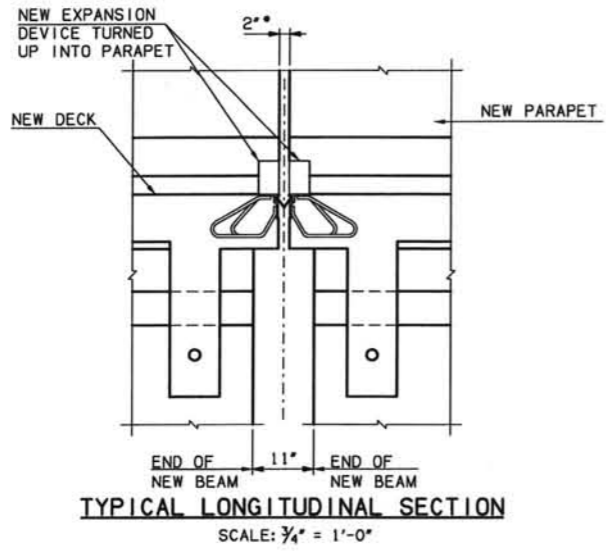
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| DOBT | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 174281591 | 12 | 486 | 559 |

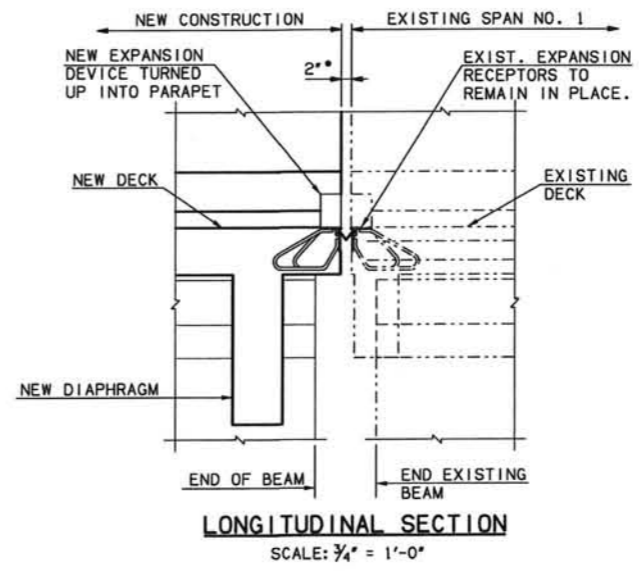


DECK REINFORCING PLAN - SPAN NO. 2
SCALE: 3/16" = 1'-0"

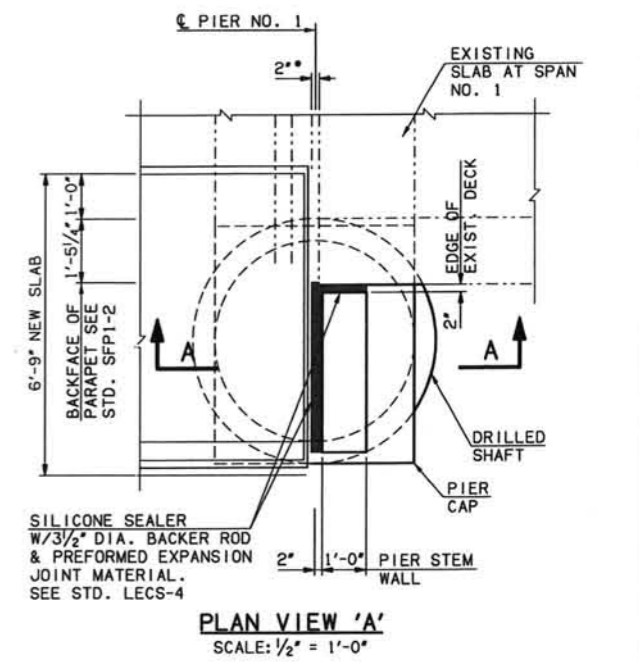
*** NOTE:**
A 2" JOINT OPENING IS SHOWN FOR PLAN PURPOSES. CONTRACTOR SHALL MATCH EXISTING JOINT OPENINGS AT THE TIME OF EXPANSION JOINT INSTALLATION.



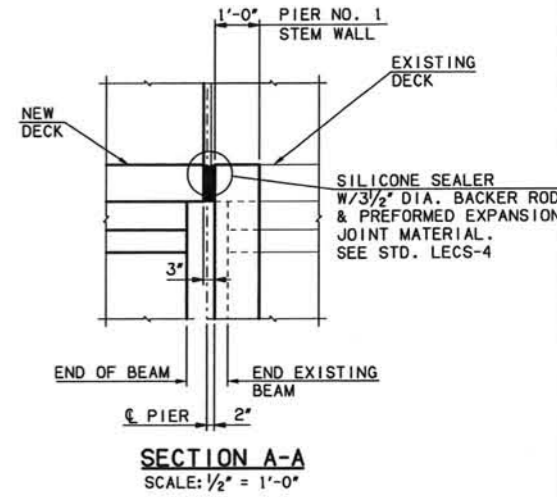
TYPICAL LONGITUDINAL SECTION
SCALE: 3/4" = 1'-0"



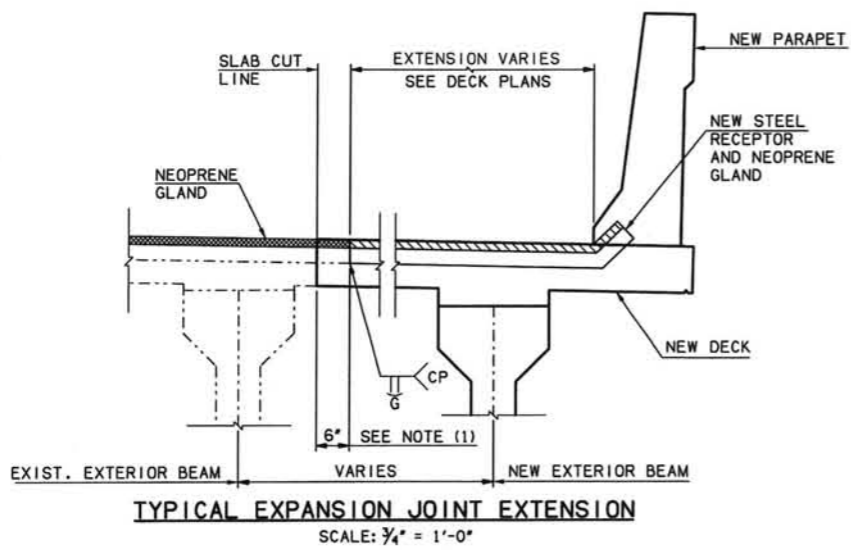
LONGITUDINAL SECTION
SCALE: 3/4" = 1'-0"



PLAN VIEW 'A'
SCALE: 1/2" = 1'-0"

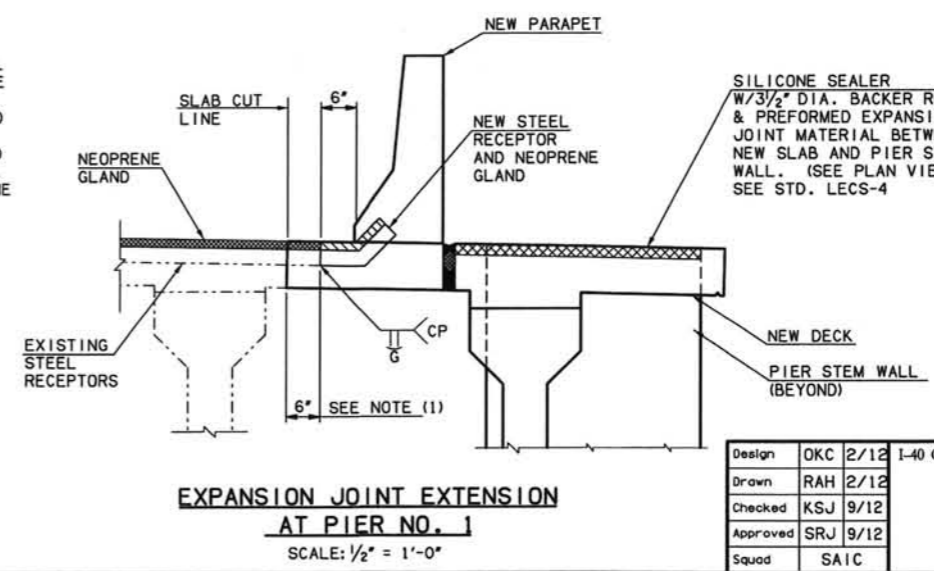


SECTION A-A
SCALE: 1/2" = 1'-0"



TYPICAL EXPANSION JOINT EXTENSION
SCALE: 3/4" = 1'-0"

NOTES:
EXISTING EXPANSION JOINTS SHALL BE EXTENDED FOR NEW BRIDGE DECK WIDTHS. THE EXISTING STEEL RECEPTORS SHALL BE CUT TO PROJECT 6" BEYOND THE BRIDGE SLAB CUT LINE. NEW RECEPTORS SHALL BE WELDED TO THE EXISTING WITH A PENETRATION WELD AND GROUND SMOOTH AS SHOWN ON THE PLANS AND IN COMPLIANCE WITH SECTION 506 OF THE STANDARD SPECIFICATIONS. EXISTING NEOPRENE GLANDS SHALL BE FIELD SPLICED TO EXTEND TO THE LIMITS OF THE NEW CLEAR ROADWAY WIDTHS AND PROJECT INTO THE NEW PARAPETS. FOR ADDITIONAL DETAILS SEE STD. EJ-SQ AND EJ-DTL.



EXPANSION JOINT EXTENSION AT PIER NO. 1
SCALE: 1/2" = 1'-0"

| | | | | |
|----------|------|------|-------------------------------|-----------------|
| Design | OKC | 2/12 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | RAH | 2/12 | 1-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | SUPERSTRUCTURE DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | | |

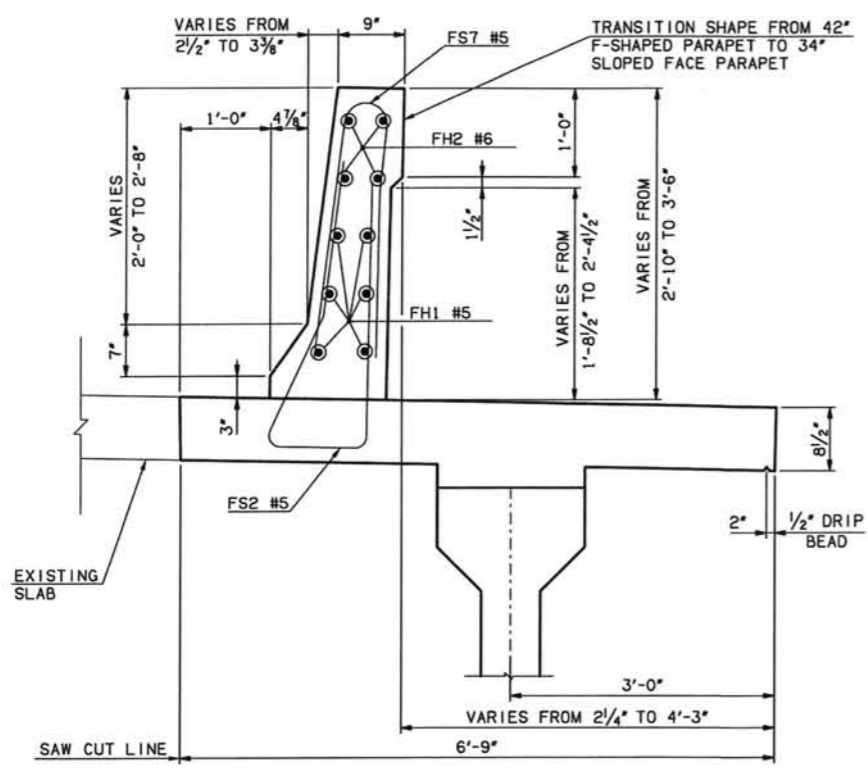
Job Piece No. 174281591 Sheet No. 486

4/24/2013

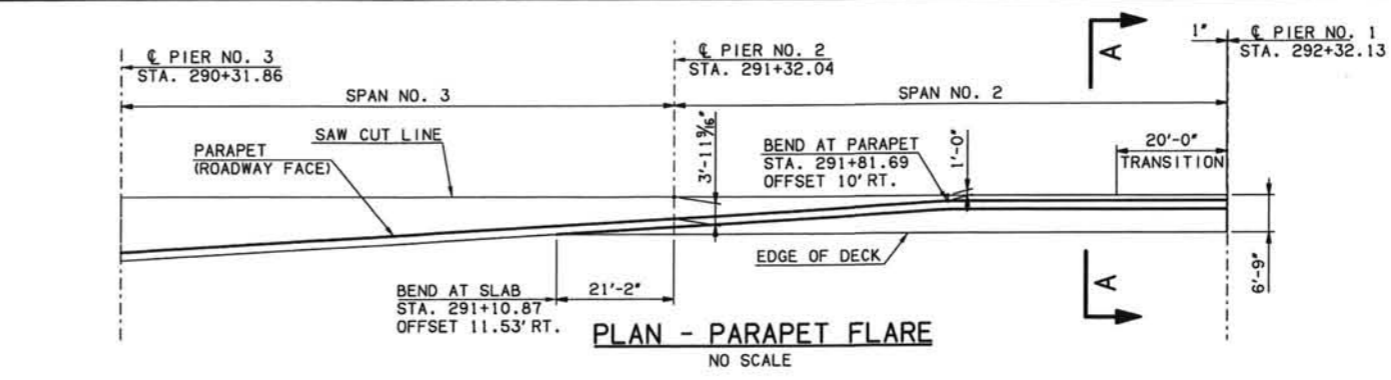
2:09:51 PM

D:\ustah\g\405045\000\20.cod\dgn\BRIDGE I-G\CT2S6s01-8.dgn

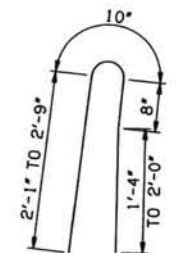
| BOOK DIVISION | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|---------------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 487 | 559 |



SECTION A-A
SCALE: 1" = 1'-0"

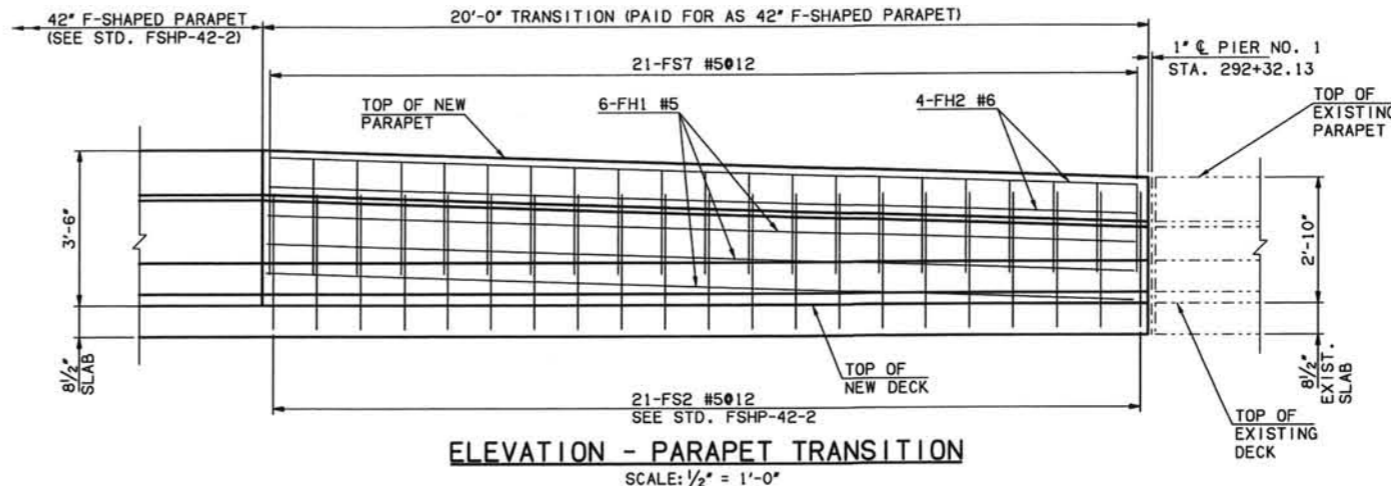


PLAN - PARAPET FLARE
NO SCALE

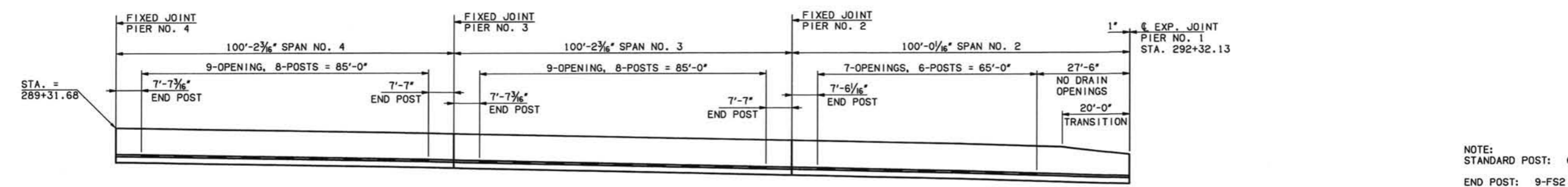
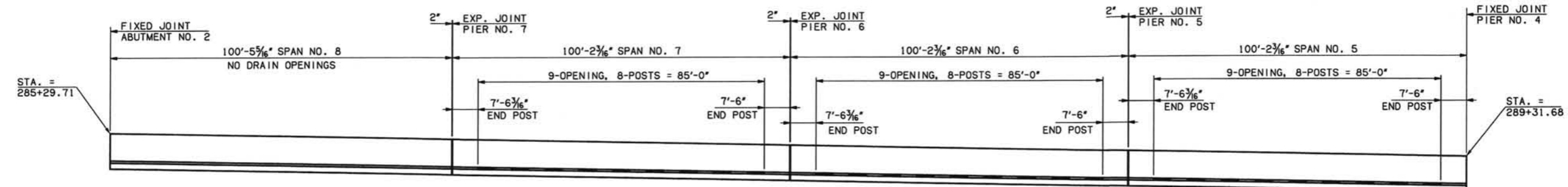


FS7 #5 x 5'-7" AVG.

NOTE: FH1, FH2 & FS7 REINFORCING BARS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT OF 42" F-SHAPED PARAPET.



ELEVATION - PARAPET TRANSITION
SCALE: 1/2" = 1'-0"



PARAPET DRAIN OPENING LAYOUT
SCALE: NOT TO SCALE

NOTE:
STANDARD POST: 6-FS2 #5012
4-L #4 - 2 AT EACH OPENING
END POST: 9-FS2 #50 EQ. SPA.
2-L #4 AT EACH OPENING

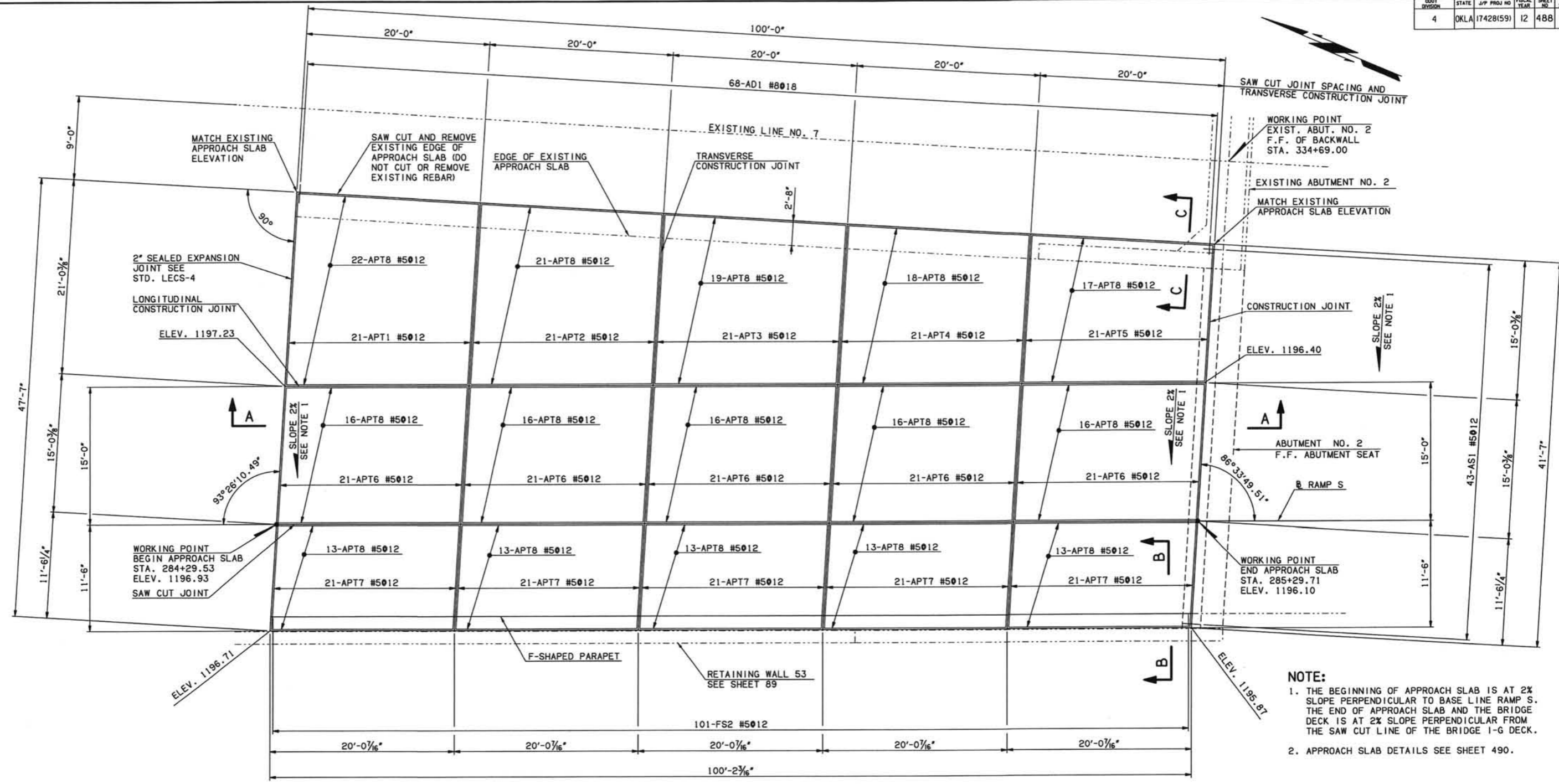
| | | | |
|----------|------|------|--|
| Design | OKC | 9/09 | I-40 CROSSTOWN OKLAHOMA COUNTY I-40 RELOCATION BRIDGE I-G PARAPET DETAILS Job Piece No. 17428(59) Sheet No. 487 |
| Drawn | OKC | 9/09 | |
| Checked | KSJ | 9/12 | |
| Approved | SRJ | 9/12 | |
| Squad | SAIC | | |

| ROOT PROJECT | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|--------------|-------|-------------|-------------|-----------|--------------|
| 4 | OKLA | 17428(59) | 12 | 488 | 559 |

4/24/2013

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PLAN - APPROACH SLAB NO. 2 TOP REINFORCING
SCALE: 3/16" = 1'-0"

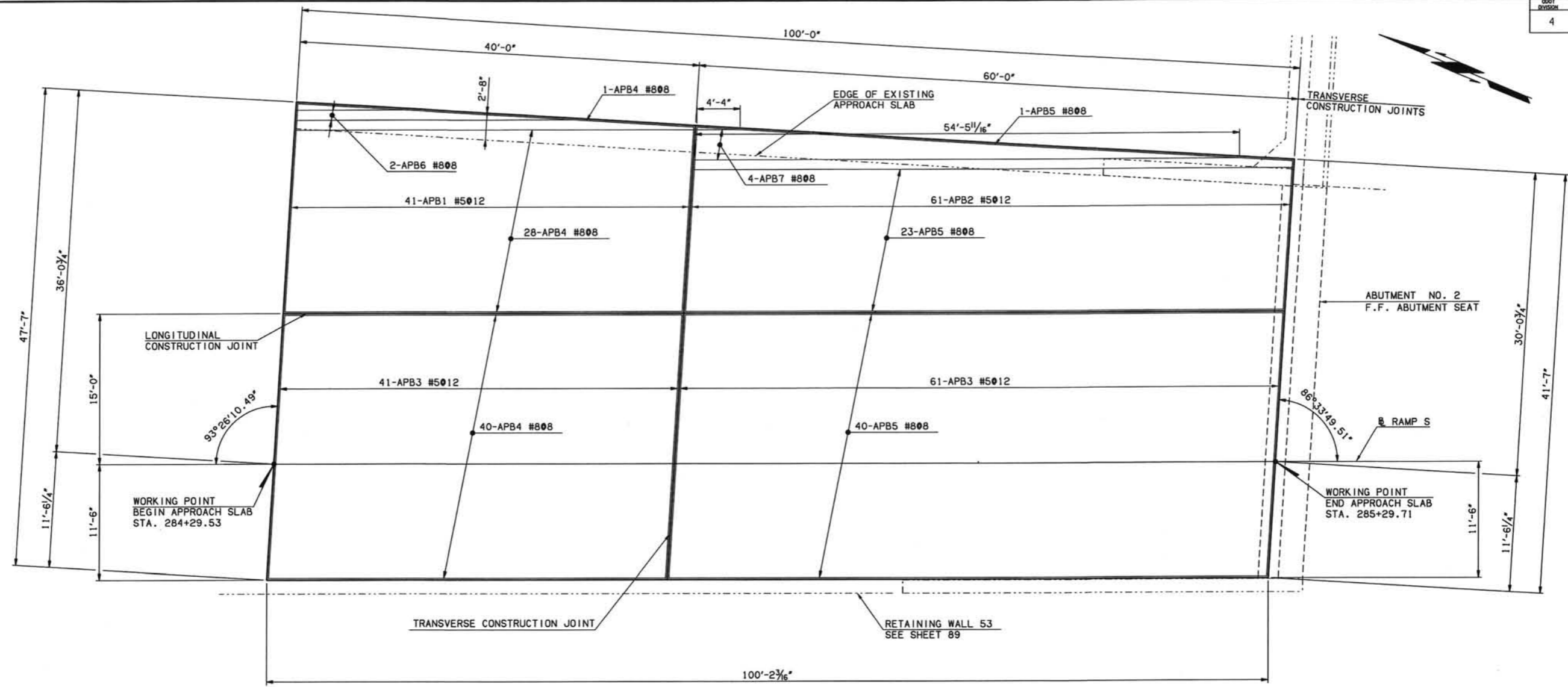
NOTE:
1. THE BEGINNING OF APPROACH SLAB IS AT 2% SLOPE PERPENDICULAR TO BASE LINE RAMP S. THE END OF APPROACH SLAB AND THE BRIDGE DECK IS AT 2% SLOPE PERPENDICULAR FROM THE SAW CUT LINE OF THE BRIDGE I-G DECK.
2. APPROACH SLAB DETAILS SEE SHEET 490.

| | | | | |
|----------|------|------|------------------------------|-----------------|
| Design | OKC | 9/09 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 9/09 | 1-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | APPROACH SLAB DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 488 |

4/24/2013

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| APPROACH SLAB BAR LIST (ONE REQUIRED) | | | | | |
|--|------|------|------|-------------|--------------------|
| EPOXY COATED REINFORCING | | | | | |
| MARK | SIZE | QTY. | FORM | LENGTH | REMARKS |
| APT1 | #5 | 21 | STR. | 20'-0" AVG | 19'-6" TO 20'-8" |
| APT2 | #5 | 21 | STR. | 18'-11" AVG | 18'-4" TO 19'-6" |
| APT3 | #5 | 21 | STR. | 17'-9" AVG | 17'-1" TO 18'-4" |
| APT4 | #5 | 21 | STR. | 16'-6" AVG | 15'-11" TO 17'-1" |
| APT5 | #5 | 21 | STR. | 15'-4" AVG | 14'-9" TO 15'-11" |
| APT6 | #5 | 105 | STR. | 14'-8" | |
| APT7 | #5 | 105 | STR. | 11'-2" | |
| APT8 | #5 | 242 | STR. | 19'-8" | |
| APB1 | #5 | 41 | STR. | 19'-6" AVG | 18'-4" TO 20'-8" |
| APB2 | #5 | 61 | STR. | 16'-7" AVG | 14'-9" TO 18'-4" |
| APB3 | #5 | 102 | STR. | 26'-3" | |
| APB4 | #8 | 69 | STR. | 39'-9" | |
| APB5 | #8 | 64 | STR. | 59'-9" | |
| APB6 | #8 | 2 | STR. | 19'-4" AVG | 11'-0" TO 27'-8" |
| APB7 | #8 | 4 | STR. | 29'-5" AVG | 4'-4" TO 54'-5" |
| AD1 | #8 | 68 | STR. | 2'-0" | SMOOTH DOWEL |
| AS1 | #5 | 43 | BNT. | 4'-9" | |
| FS2 | #5 | 101 | BNT. | 7'-4" | SEE STD. FSHP-42-2 |

PLAN - APPROACH SLAB NO. 2 BOTTOM REINFORCING
SCALE: 3/16" = 1'-0"

| APPROACH SLAB QUANTITIES | | | |
|--------------------------------------|------|--------|-------|
| ITEM | UNIT | SLAB 1 | TOTAL |
| 1 APPROACH SLAB | SY | 495.4 | 495.4 |
| SAW-CUT GROOVING | SY | 478.8 | 478.8 |
| 42" F-SHAPED PARAPET | LF | 100.2 | 100.2 |
| SPECIAL CONCRETE FINISH | SY | 91 | 91 |
| WATER REPELLANT (VISUALLY INSPECTED) | SY | 91 | 91 |

- NOTES:**
- THE DEPARTMENT CONSIDERS THE COST OF CLASS AA CONCRETE, REINFORCING STEEL (INCLUDING FS2 BARS), BACKER ROD, RAPID CURE JOINT SEALANT, POLYSTYRENE AND POLYETHYLENE SHEETING TO BE INCLUDED IN THE CONTRACT UNIT PRICE OF APPROACH SLAB.

THERE IS AN ESTIMATED 165.12 C.Y. OF CLASS AA CONCRETE AND AN ESTIMATED 33712 LB. OF EPOXY COATED REINFORCING STEEL IN EACH APPROACH SLAB.
 - APPROACH SLAB DETAILS SEE SHEET 490.
 - PARAPETS FOR THE GORE ARE INSTALLED WITH ROADWAY PAY ITEMS.

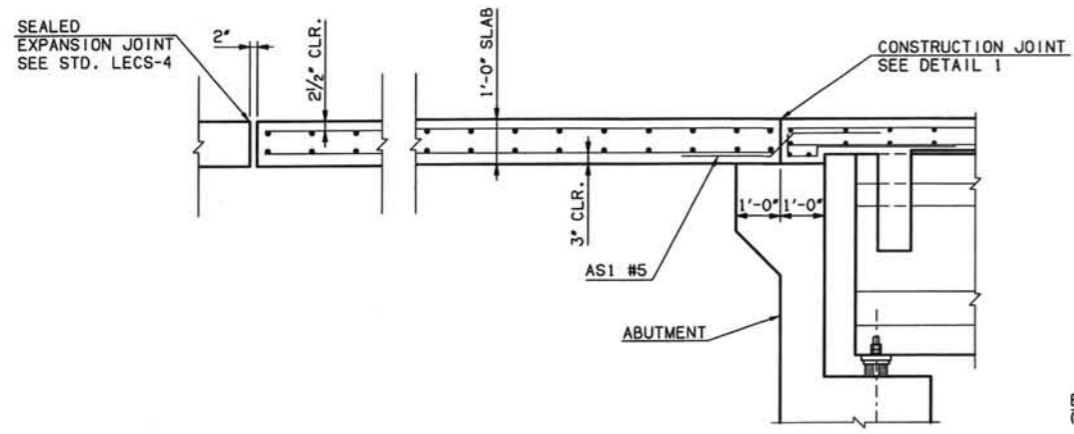
| | | | | |
|----------|------|------|------------------------------|-----------------|
| Design | DKC | 9/09 | I-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | SVH | 9/09 | I-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | APPROACH SLAB DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428159 | Sheet No. 489 |

| DOT DISTRICT | STATE | J/P PROJ NO | FISCAL YEAR | SHEET NO | TOTAL SHEETS |
|--------------|-------|-------------|-------------|----------|--------------|
| 4 | OKLA | 17428159 | 12 | 490 | 559 |

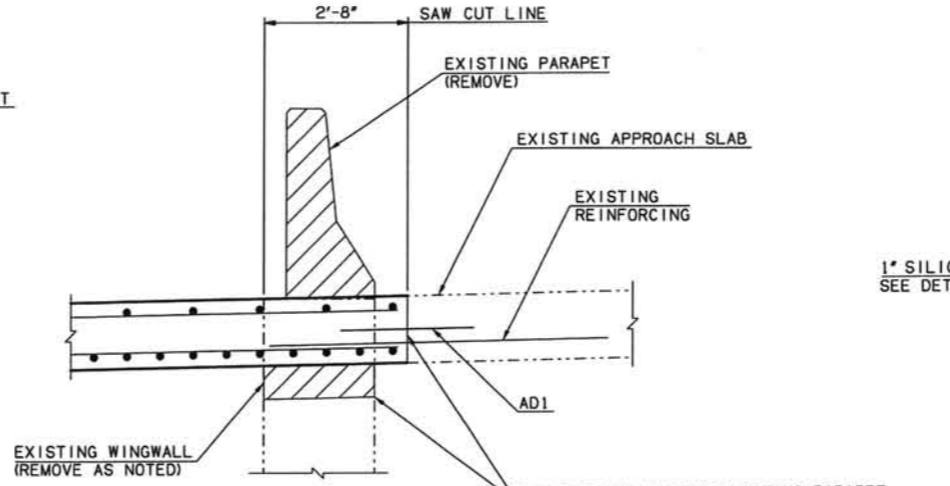
4/24/2013

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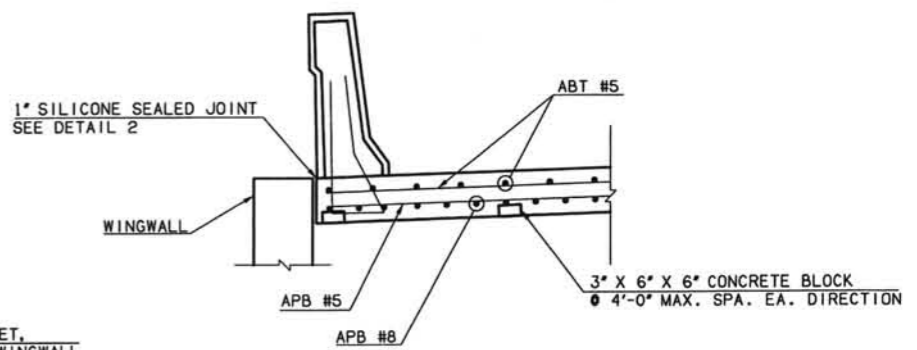
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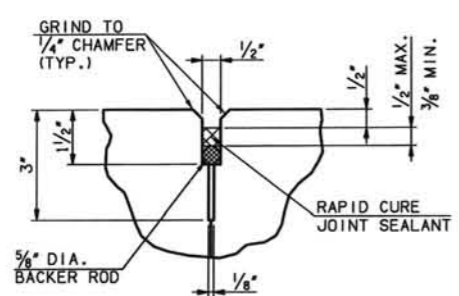
SECTION A-A
SCALE: 1/2" = 1'-0"



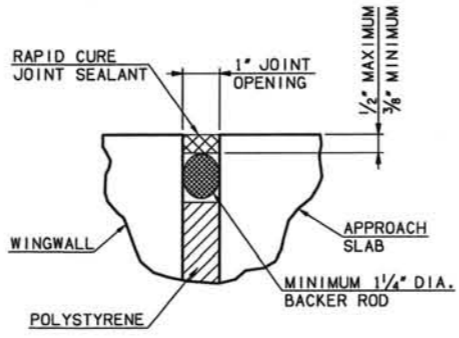
SECTION C-C
SCALE: 3/4" = 1'-0"



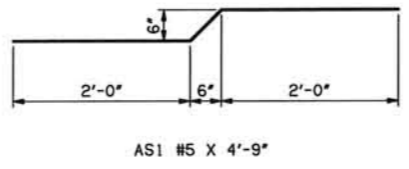
SECTION B-B
SCALE: 1/2" = 1'-0"



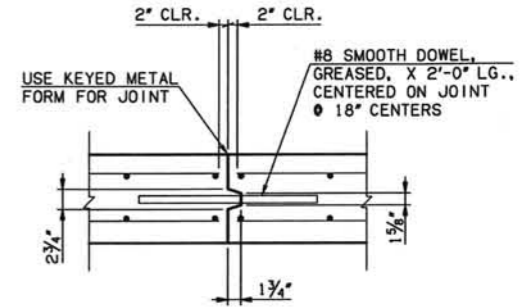
DETAIL 1
SCALE: NONE



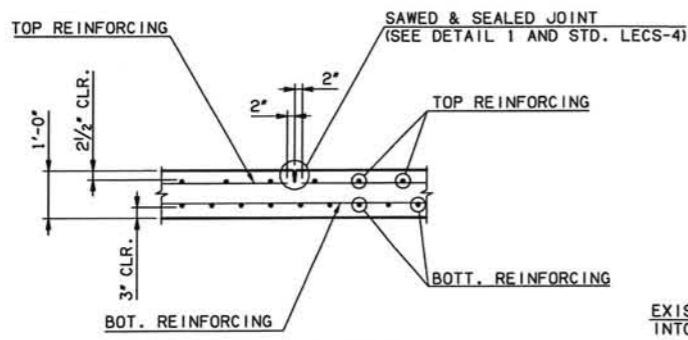
DETAIL 2
SCALE: NONE



AS1 #5 X 4'-9"

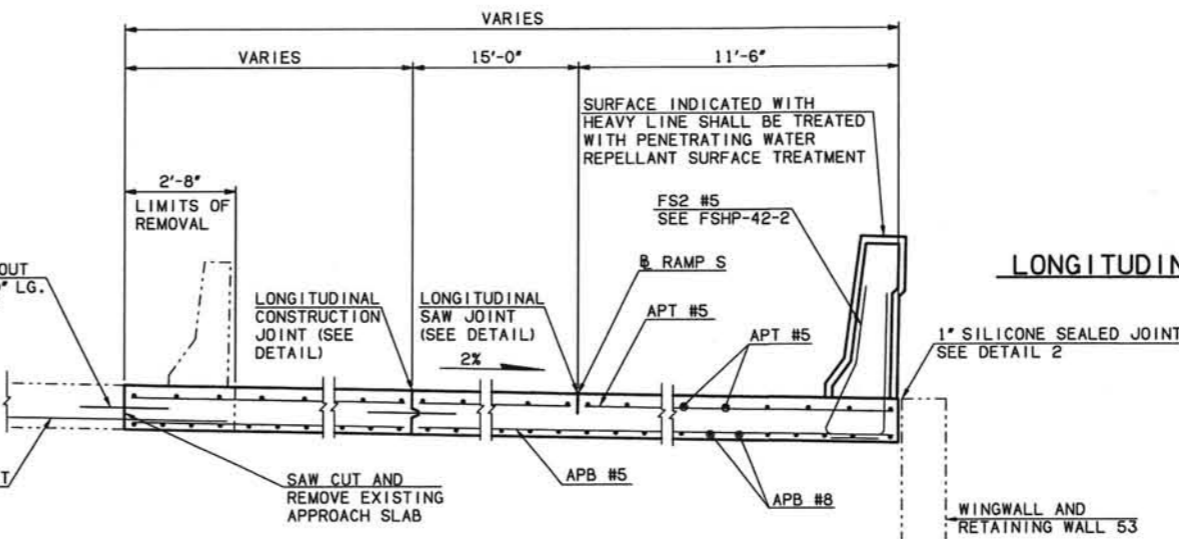


LONGITUDINAL & TRANSVERSE CONSTRUCTION JOINT
SCALE: 3/4" = 1'-0"



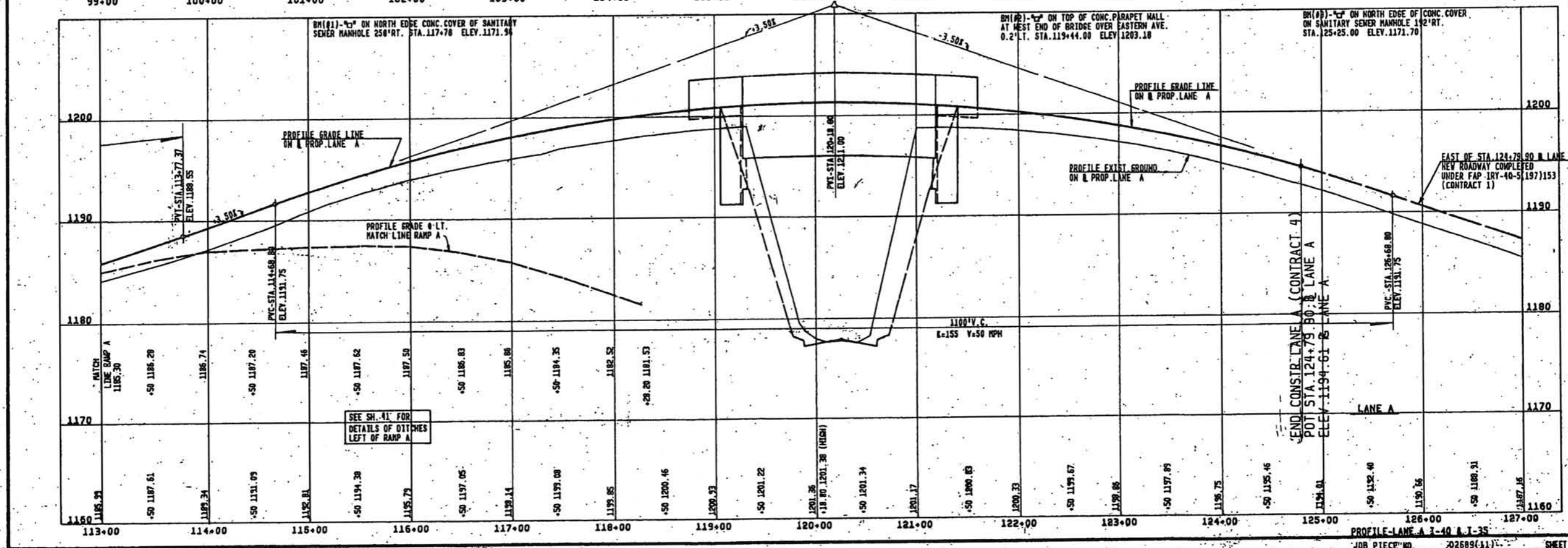
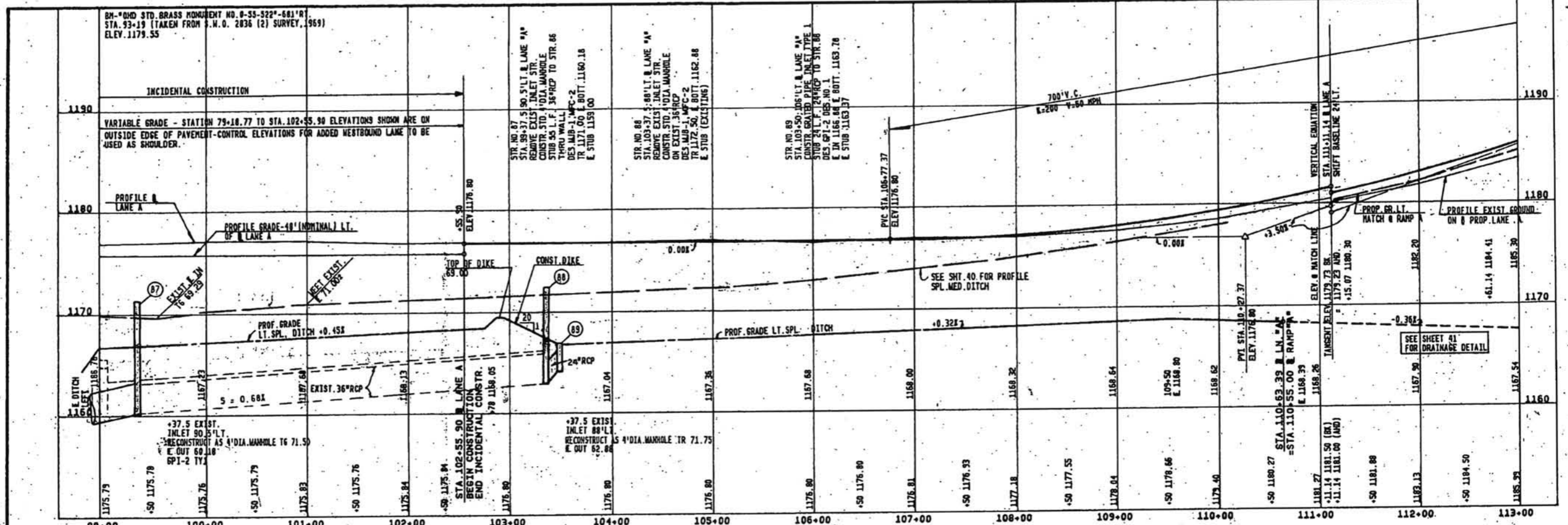
SAW JOINT DETAIL
SCALE: NONE

DRILL AND EPOXY GROUT
AD1 #8 DOWEL X 2'-0" LG.
CENTERED ON JOINTS
Ø 18" CENTERS



TYPICAL SECTION
SCALE: 1/2" = 1'-0"

| | | | | |
|----------|------|------|------------------------------|-----------------|
| Design | OKC | 9/09 | 1-40 CROSSTOWN | OKLAHOMA COUNTY |
| Drawn | OKC | 9/09 | 1-40 RELOCATION BRIDGE I-G | |
| Checked | KSJ | 9/12 | APPROACH SLAB DETAILS | |
| Approved | SRJ | 9/12 | | |
| Squad | SAIC | | Job Piece No. 17428(59) | Sheet No. 490 |



BN-040 STD. BRASS MONUMENT NO. 8-55-522-681 RT.
 STA. 93+19 (TAKEN FROM S.W.O. 2836 (2) SURVEY, 1969)
 ELEV. 1179.55

INCIDENTAL CONSTRUCTION

VARIABLE GRADE - STATION 79+18.77 TO STA. 102+55.90 ELEVATIONS SHOWN ARE ON OUTSIDE EDGE OF PAVEMENT - CONTROL ELEVATIONS FOR ADDED WESTBOUND LANE TO BE USED AS SHOULDER.

PROFILE GRADE LANE A

PROFILE GRADE - 10' (MINIMAL) LT. OF LANE A

EXIST. 18\"/>

EXIST. 36\"/>

TOP OF DIKE 69.00

CONST. DIKE

PROF. GRADE LT. SPL. DITCH +0.45%

EXIST. 36\"/>

PROF. GRADE LT. SPL. DITCH +0.32%

+37.5 EXIST. INLET 90\"/>

+37.5 EXIST. INLET 88\"/>

STA. 99+00

STA. 100+00

STA. 101+00

STA. 102+00

STA. 103+00

STA. 104+00

STA. 105+00

STA. 106+00

STA. 107+00

STA. 108+00

STA. 109+00

STA. 110+00

STA. 111+00

STA. 112+00

STA. 113+00

BN(11)-1\"/>

BN(12)-1\"/>

BN(13)-1\"/>

MATCH LINE RAMP A

PROFILE GRADE LINE ON & PROP. LANE A

PROFILE GRADE 0\"/>

PROFILE GRADE LINE ON & PROP. LANE A

PROFILE EXIST. GROUND ON & PROP. LANE A

EAST OF STA. 124+79.90 & LANE A NEW ROADWAY COMPLETED UNDER FAP-187-40-5 (197)153 (CONTRACT 1)

SEE SH. 41 FOR DETAILS OF DITCHES LEFT OF RAMP A

END CONSTR. LANE A (CONTRACT 4)
 POT. STA. 124+79.90 & LANE A
 ELEV. 1194-61 & LANE A

STA. 113+00

STA. 114+00

STA. 115+00

STA. 116+00

STA. 117+00

STA. 118+00

STA. 119+00

STA. 120+00

STA. 121+00

STA. 122+00

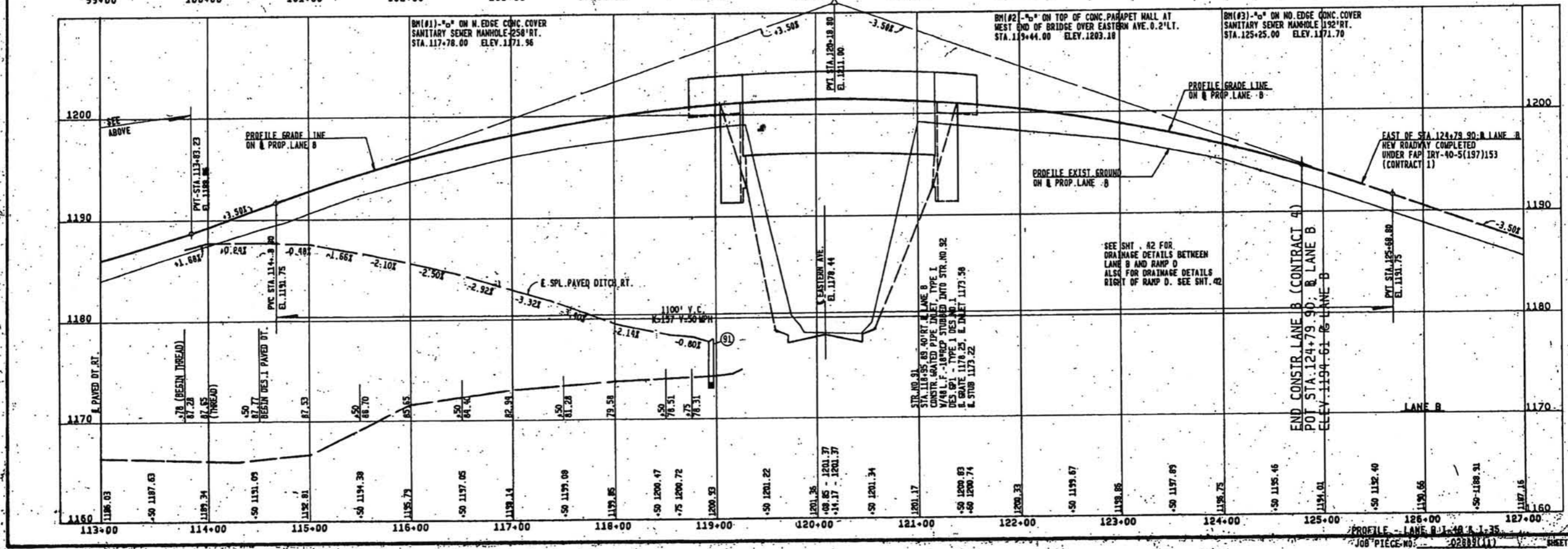
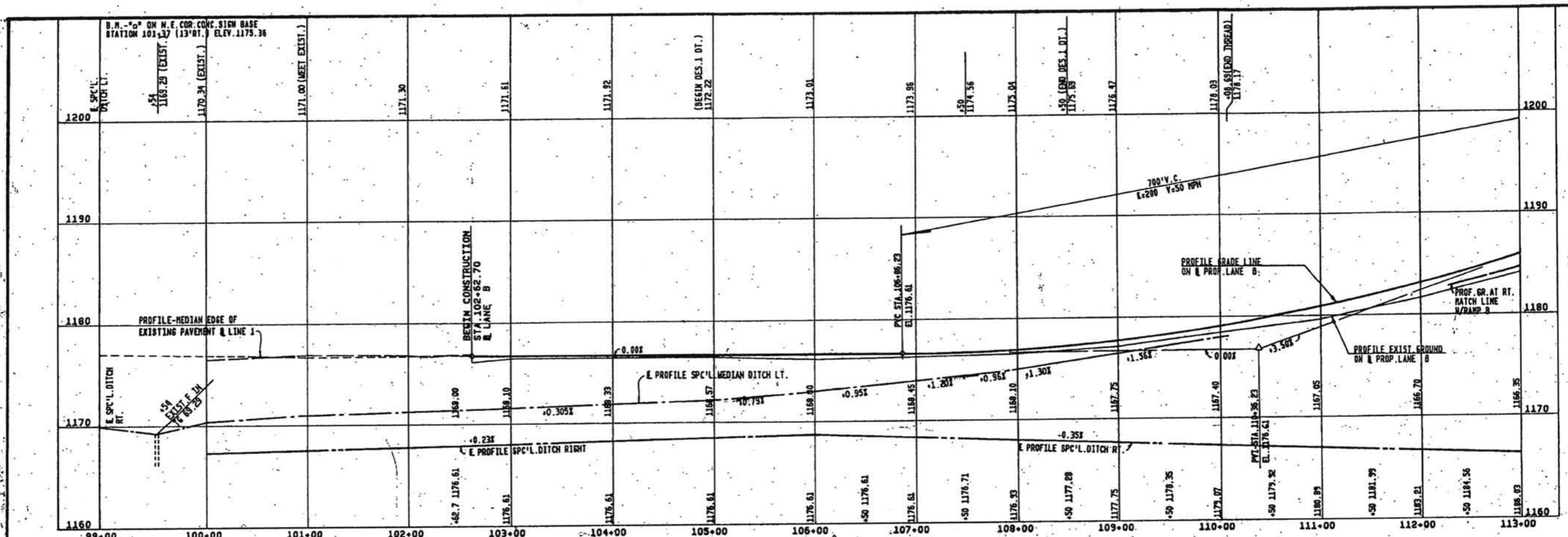
STA. 123+00

STA. 124+00

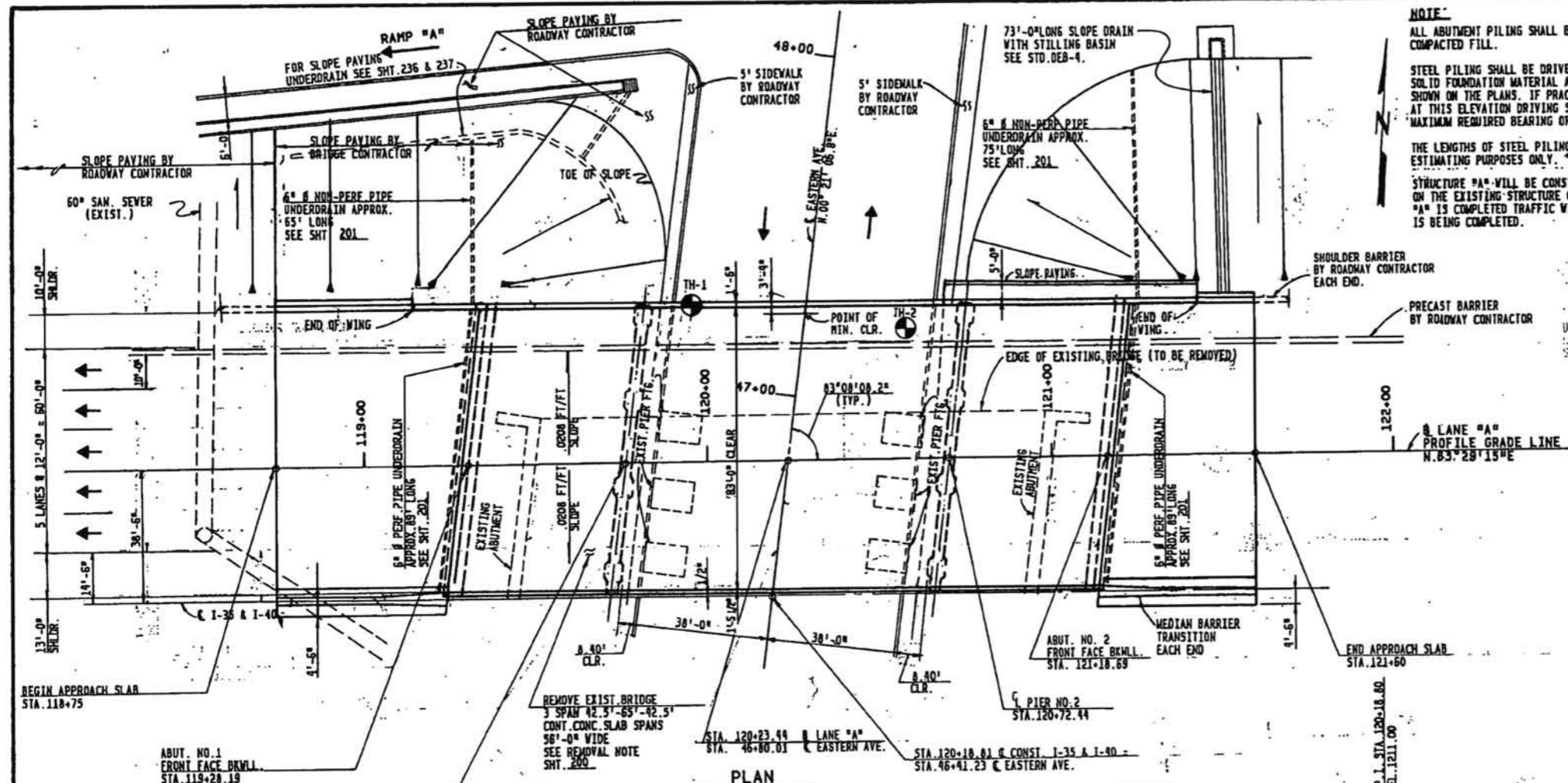
STA. 125+00

STA. 126+00

STA. 127+00



DP.
 36.4 65.5
 21.8 21.66
 65.57 39.47



NOTE:
 ALL ABUTMENT PILING SHALL BE DRIVEN THROUGH THE COMPACTED FILL.
 STEEL PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN ON THE PLANS. IF PRACTICAL REFUSAL IS NOT OBTAINED AT THIS ELEVATION DRIVING SHALL CONTINUE UNTIL THE MAXIMUM REQUIRED BEARING OF 100 TONS IS OBTAINED.
 THE LENGTHS OF STEEL PILING SHOWN ON THE PLANS ARE FOR ESTIMATING PURPOSES ONLY.
 STRUCTURE "A" WILL BE CONSTRUCTED WHILE THE TRAFFIC IS ON THE EXISTING STRUCTURE ON LANE "B". WHEN STRUCTURE "A" IS COMPLETED TRAFFIC WILL USE IT WHILE STRUCTURE "B" IS BEING COMPLETED.

DESIGN DATA

DESIGN AASHTO SPECIFICATIONS, 1992 EDITION
 LOADING HS20-44 & PPM-20-4
 20 LB. PER SQ. FT. FUTURE WEARING SURFACE.
 OPERATING RATING = HS-36.4

| | LFD | WSD |
|------------------------------|-----------------|-----------------|
| PIERS | Ft = 60,000 PSI | Fs = 24,000 PSI |
| REIN. STEEL (GR. 60) | Ft = 4,000 PSI | Fs = 20,000 PSI |
| STRUCTURAL STEEL (A-36) | Ft = 3,000 PSI | Fs = 1,350 PSI |
| CLASS AA CONC. | | Fc = 1,000 PSI |
| CLASS A CONC. | | Fc = 5,000 PSI |
| P.C. BEAM (SHORTER THAN 90') | | Fci = 3,750 PSI |
| P.C. BEAM (LONGER THAN 90') | | Fci = 8,000 PSI |
| P.C. BEAM (LONGER THAN 90') | | Fci = 6,000 PSI |

FOUNDATION PRESSURE

ABUTMENTS
 GROUP 1

PIERS
 GROUP 1
 MAX (GROUP V2)

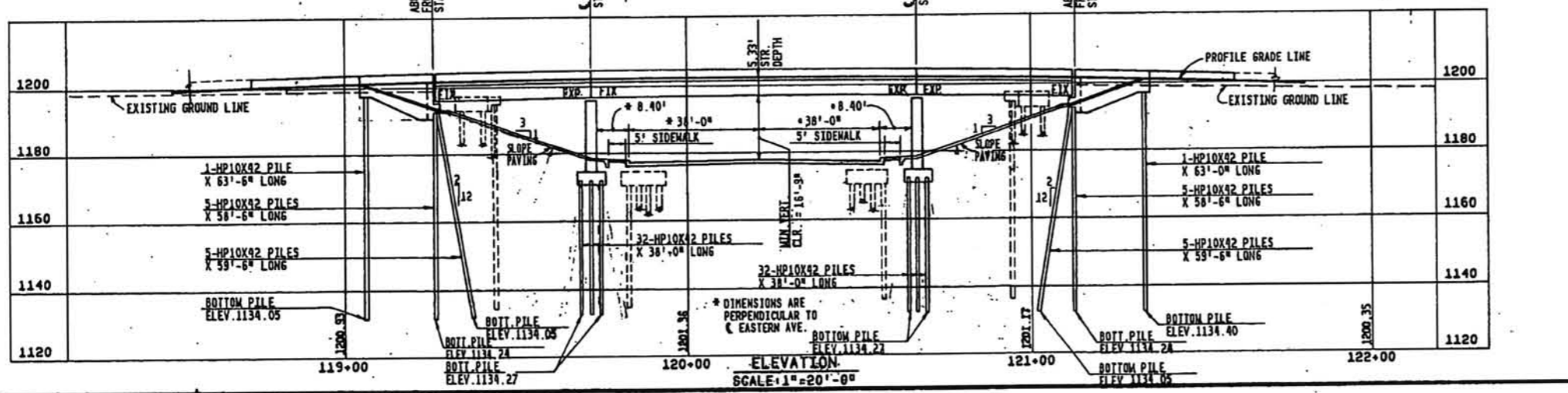
SUMMARY OF QUANTITIES

| ITEM | UNIT | ABUTS. | PIERS | SUPERSTR. | TOTAL |
|------------------------------------|-------|---------|---------|-----------|---------|
| SELECT BORROW | C.Y. | 753.0 | | | 753.0 |
| APPROACH SLABS | S.Y. | | | | 1,134.0 |
| SUBSTR. EXCAV. COMMON | C.Y. | 123.2 | 108.8 | | 232.0 |
| PRESTRESSED CONC. BEAMS (TYPE II) | L.F. | | | 822.6 | 822.6 |
| PRESTRESSED CONC. BEAMS (TYPE IV) | L.F. | | | 1,068.0 | 1,068.0 |
| CLASS AA CONC. | C.Y. | | | 496.6 | 496.6 |
| CONCRETE PARAPET | L.F. | 44.0 | | 382.4 | 569.4 |
| STRUCTURAL STEEL A36 | LB. | 800 | 1,600 | 10,920 | 13,320 |
| CLASS A CONCRETE | C.Y. | 174.4 | 321.2 | | 495.6 |
| REINFORCING STEEL (GRADE 60) | LB. | 15,680 | 34,800 | | 50,480 |
| EPOXY COATED REIN. STEEL (GR. 60) | LB. | | 17,260 | 86,780 | 104,040 |
| STEEL PILING (HP 10 X 42) | L.F. | 1,306.5 | 2,432.0 | | 3,738.5 |
| PENETR. WATER REPELL. SURF. TRTMT. | S.Y. | | 593 | 700 | 1,293 |
| 6" PERFORATED PIPE UNDERDRAIN RND. | L.F. | | | | 178.0 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | L.F. | | | | 140.0 |
| PIPE UNDERDRAIN COVER MATERIAL | C.Y. | | | | 340.0 |
| TRENCH EXCAVATION | C.Y. | | | | 26.0 |
| STANDARD BEDDING MATERIAL | C.Y. | | | | 17.9 |
| REMOVAL OF EXIST. BRIDGE STR. | L.SUM | | | | 1 |
| SEALED EXPANSION JOINT | L.F. | | | 171.2 | 171.2 |

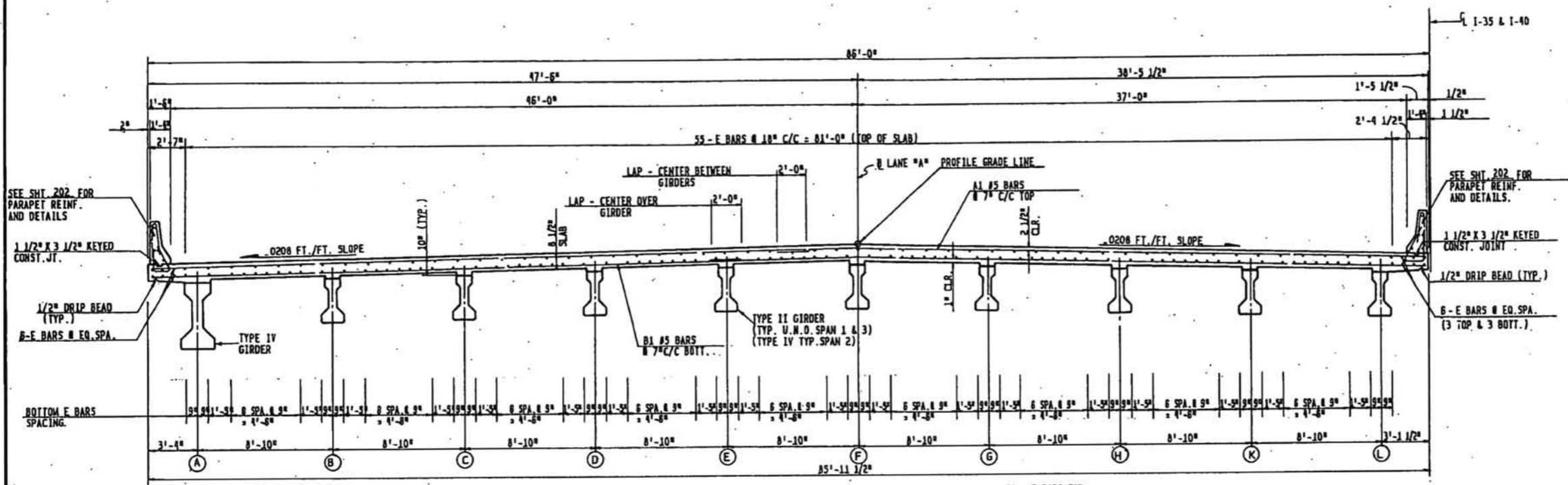
SHEET INDEX

| SHEET NO. | TITLE |
|------------|-----------------------------------|
| 30 | SUMMARY OF QUANTITIES - BRIDGE |
| 200 & 200A | GENERAL NOTES - BRIDGE |
| 201 | MISCELLANEOUS DETAILS - BRIDGE |
| 202 | MISCELLANEOUS DETAILS - BRIDGE |
| 203 | GENERAL PLAN & ELEVATION |
| 203A | FOUNDATION REPORT |
| 204 | BRIDGE STAKING DIAGRAM |
| 205 | ABUTMENT DETAILS (1 OF 3) ABUT. 1 |
| 206 | ABUTMENT DETAILS (2 OF 3) ABUT. 2 |
| 207 | ABUTMENT DETAILS (3 OF 3) BOTH |
| 208 | PIER DETAILS (1 OF 4) |
| 209 | PIER DETAILS (2 OF 4) |
| 210 | PIER DETAILS (3 OF 4) |
| 211 | PIER DETAILS (4 OF 4) |
| 212 | TYPICAL SECTION |
| 213 | CONCRETE DECK PLAN |
| 214 | SIDER LAYOUT |
| 215 | DIAPHRAGM DETAILS |
| 216 | PRESTRESSED CONC. BEAMS TYPE IV |
| 217 | PRESTRESSED CONC. BEAMS TYPE II |
| 218 | BEARING DETAILS |
| 219 | APPROACH SLAB DETAILS |
| 236 | SLOPE PAVING DETAILS (1 OF 2) |
| 237 | SLOPE PAVING DETAILS (2 OF 2) |
| 238 | SEALED EXP. JOINT (1 OF 2) |
| 239 | SEALED EXP. JOINT (2 OF 2) |

VERTICAL CURVE DATA



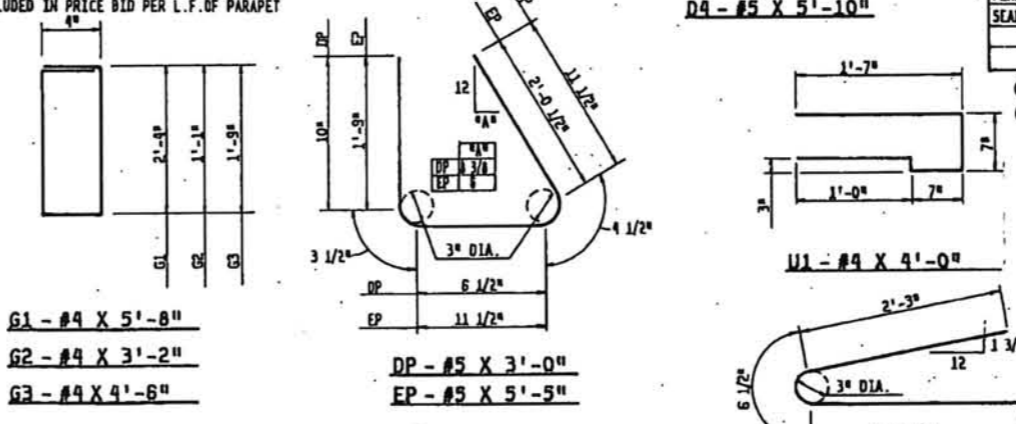
| | | | |
|----------|--|---|---|
| DESIGN | | BRIDGE STRUCTURE "A" GENERAL PLAN & ELEVATION LANE "A" (I-35 & I-40) OVER N. EASTERN AVE. WB | 5515 0671 NX PROJECT NO. J.P. 02689(17) SHEET NO. 201 |
| DRAWN | | | |
| CHECKED | | | |
| APPROVED | | | |



TYPICAL SECTION
(SPAN 1 & 3 SHOWN)
(SPAN 2 ALL TYPE IV GIRDERS)

| BAR LIST SUPERSTRUCTURE (ALL BARS EPOXY COATED) | | | | | | | | | | | | | | |
|---|-----|------|------|--------------|--------|-----|------|------|---------------|--------|-----|------|-------|---------------|
| SPAN 1 | | | | | SPAN 2 | | | | | SPAN 3 | | | | |
| MARK | NO. | SIZE | FORM | LENGTH | MARK | NO. | SIZE | FORM | LENGTH | MARK | NO. | SIZE | FORM | LENGTH |
| A1 | 62 | #5 | STR. | 87'-8" | A1 | 151 | #5 | STR. | 87'-8" | A1 | 63 | #5 | STR. | 87'-8" |
| A0 | 4 | #4 | STR. | 86'-9" | A0 | 4 | #4 | STR. | 86'-9" | A0 | 4 | #4 | STR. | 86'-9" |
| B1 | 62 | #5 | STR. | 87'-8" | B1 | 151 | #5 | STR. | 87'-8" | B1 | 63 | #5 | STR. | 87'-8" |
| C1 | 24 | #5 | STR. | 33'-3"(AVG.) | C3 | 44 | #5 | STR. | 32'-8"(AVG.) | C3 | 22 | #5 | STR. | 32'-8"(AVG.) |
| C2 | 10 | #5 | STR. | 76'-6"(AVG.) | C4 | 20 | #5 | STR. | 73'-6"(AVG.) | C4 | 10 | #5 | STR. | 73'-6"(AVG.) |
| C3 | 22 | #5 | STR. | 32'-8"(AVG.) | C5 | 22 | #5 | STR. | 33'-2"(AVG.) | C5 | 22 | #5 | STR. | 33'-2"(AVG.) |
| C4 | 10 | #5 | STR. | 73'-6"(AVG.) | C6 | 10 | #5 | STR. | 73'-11"(AVG.) | C6 | 10 | #5 | STR. | 73'-11"(AVG.) |
| D1 | 2 | #5 | BNT. | 6'-6" | E5 | 150 | #4 | STR. | 88'-8" | D2 | 2 | #5 | BNT. | 6'-7" |
| D2 | 2 | #5 | BNT. | 6'-7" | EP | 198 | #5 | BNT. | 5'-5" | DP | 40 | #5 | BNT. | 3'-0" |
| DP | 40 | #5 | BNT. | 3'-0" | G1 | 144 | #4 | BNT. | 5'-8" | E6 | 6 | #4 | STR. | 46'-9" |
| E1 | 4 | #4 | STR. | 47'-2" | G3 | 72 | #4 | BNT. | 4'-6" | E7 | 138 | #4 | STR. | 47'-0" |
| E2 | 140 | #4 | STR. | 47'-0" | H2 | 38 | #7 | STR. | 7'-11" | G8 | 2 | #4 | STR. | 47'-1" |
| E3 | 2 | #4 | STR. | 46'-11" | H3 | 38 | #4 | STR. | 7'-3" | E9 | 4 | #4 | STR. | 46'-1" |
| E4 | 4 | #4 | STR. | 45'-10" | H4 | 38 | #4 | STR. | 6'-11" | EP | 53 | #5 | BNT. | 5'-5" |
| EP | 54 | #5 | BNT. | 5'-5" | PH2 | 12 | #4 | STR. | 99'-8" | G1 | 144 | #4 | BNT. | 5'-8" |
| G1 | 144 | #4 | BNT. | 5'-8" | PRI | 198 | #5 | BNT. | 5'-0" | G2 | 72 | #4 | BNT. | 3'-2" |
| G2 | 72 | #4 | BNT. | 3'-2" | PH3L | 6 | #4 | STR. | 46'-1" | H1 | 36 | #4 | STR. | 7'-7" |
| H1 | 36 | #4 | STR. | 7'-7" | PH3R | 6 | #4 | STR. | 46'-9" | H2 | 36 | #7 | STR. | 7'-11" |
| H2 | 36 | #7 | STR. | 7'-11" | PH4R | 6 | #4 | STR. | 47'-3" | H3 | 36 | #4 | STR. | 7'-3" |
| H3 | 36 | #4 | STR. | 7'-3" | PRI | 93 | #5 | BNT. | 5'-0" | PH3L | 6 | #4 | STR. | 46'-1" |
| PH1L | 6 | #4 | STR. | 45'-10" | UL | 85 | #4 | BNT. | 4'-0" | PH3R | 6 | #4 | STR. | 46'-9" |
| PH1R | 6 | #4 | STR. | 47'-3" | | | | | PRI | 93 | #5 | BNT. | 5'-0" | |
| PRI | 94 | #5 | BNT. | 5'-0" | | | | | UL | 85 | #4 | BNT. | 4'-0" | |
| UL | 85 | #4 | BNT. | 4'-0" | | | | | | | | | | |

- ① INCLUDES 2'-0" FOR LAP SPLICE, STAGGER
- ② VARIES: 6'-7" TO 59'-11"
- ③ VARIES: 66'-10" TO 86'-2"
- ④ VARIES: 8'-5" TO 56'-11"
- ⑤ VARIES: 63'-10" TO 83'-2"
- ⑥ VARIES: 8'-11" TO 57'-5"
- ⑦ VARIES: 64'-3" TO 83'-7"
- ⑧ IN END DIAPHRAGMS
- ⑨ IN INTERMEDIATE DIAPHRAGMS
- ⑩ HALF IN EACH PARAPET
- ⑪ INCLUDED IN PRICE BID PER L.F. OF PARAPET



BAR BENDING DIAGRAMS
(ALL DIMENSIONS ARE OUT TO OUT)

| | |
|--------|----|
| 3'-6" | D1 |
| 3'-0" | D2 |
| 3'-2" | D3 |
| 2'-10" | D4 |

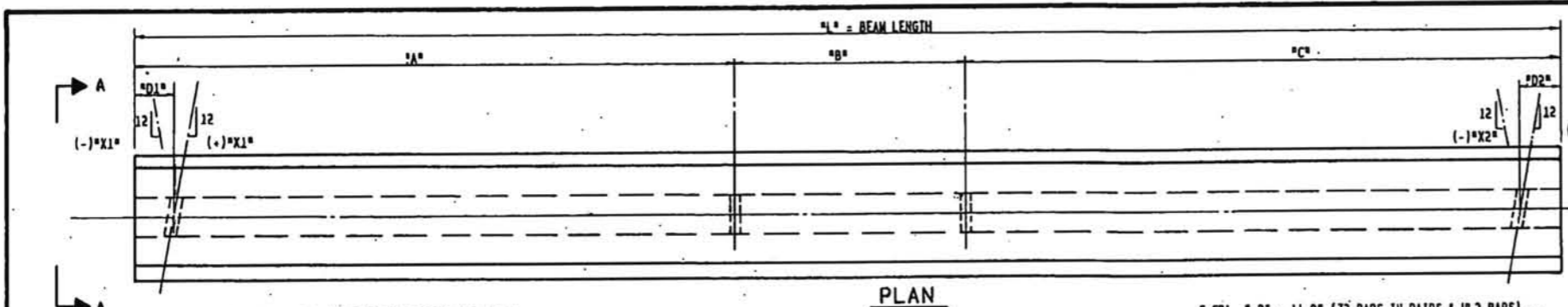
- D1 - #5 X 6'-6"
- D2 - #5 X 6'-0"
- D3 - #5 X 6'-2"
- D4 - #5 X 5'-10"

| SUPERSTRUCTURE QUANTITIES - BRIDGE "A" | | | | | |
|--|------|---------|--------|--------|----------|
| ITEM | UNIT | AMOUNT | | | TOTAL |
| | | SPAN 1 | SPAN 2 | SPAN 3 | |
| PRESTRESSED CONC. BEAMS (TYPE II) | L.F. | 411.30 | — | 411.30 | 822.60 |
| PRESTRESSED CONC. BEAMS (TYPE IV) | L.F. | 45.70 | 976.56 | 45.70 | 1,067.96 |
| CLASS AA CONCRETE | C.Y. | 124.6 | 247.4 | 124.6 | 496.6 |
| CONCRETE PARAPET | L.F. | 93.49 | 195.67 | 93.18 | 382.34 |
| STRUCTURAL STEEL (A36) | LB. | 3,360 | 4,200 | 3,360 | 10,920 |
| EPOXY COATED REINF. STEEL (GR. 60) | LB. | 304,630 | 43,460 | 21,690 | 86,780 |
| PENETRAT. WATER REPEL. TRTMT. | S.Y. | 166 | 369 | 165 | 700 |
| SEALED EXPANSION JOINT | L.F. | 85.6 | — | 85.6 | 171.2 |

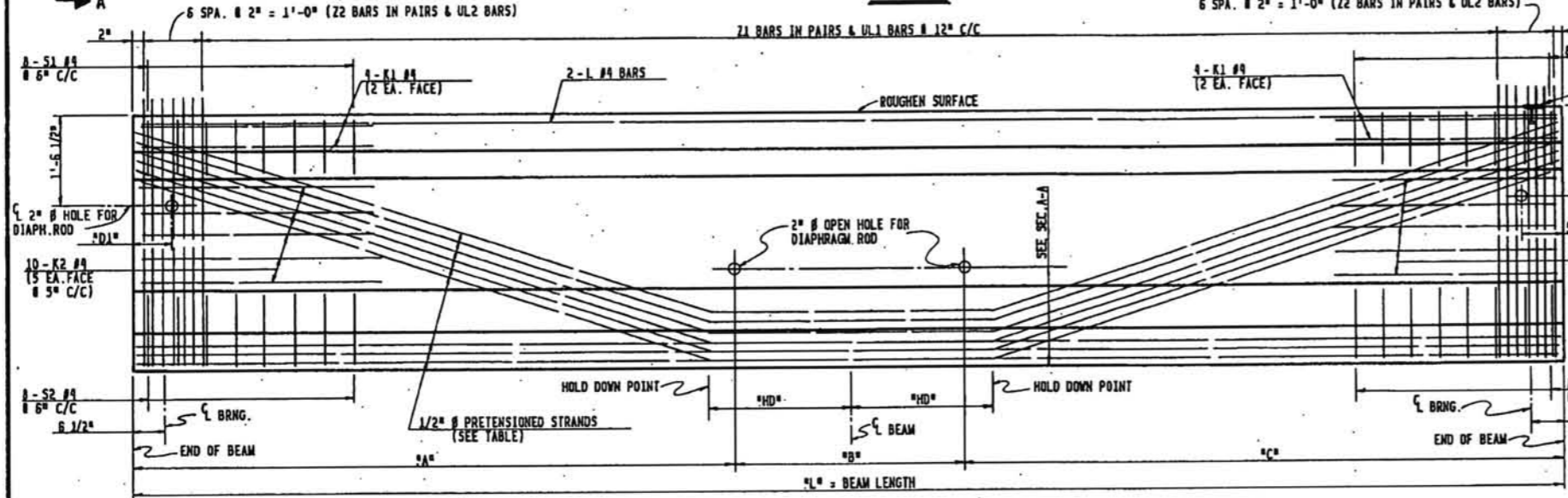
- ① INCLUDES 16.0 C.Y. FOR HAUNCHES
- ② INCLUDES SOLE PLATES, DIAPHRAGM RODS & FITTINGS.

- NOTES:**
- SEE SHT. 201 FOR GENERAL NOTES.
 - SEE SHT. 202 FOR PARAPET DETAILS.
 - FOR DIAPHRAGMS SEE SHT. 215.
 - FOR DECK JOINT DETAILS SEE SHT. 218.
 - ALL BARS THIS SHEET TO BE EPOXY COATED.

| | | |
|------------------------|------------|--|
| DESIGN | JW | BRIDGE "A" |
| DRAWN | JW | |
| CHECKED | WV | |
| APPROVED | SJG | |
| SWAD | G/K ENGRS. | |
| TYPICAL SECTION | | PROJECT NO. J.P. 02689(11) SHEET NO. 212 |

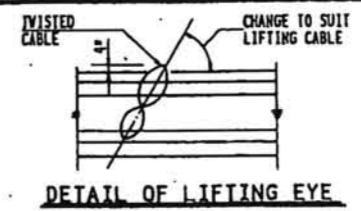


PLAN

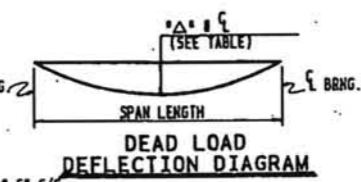


ELEVATION

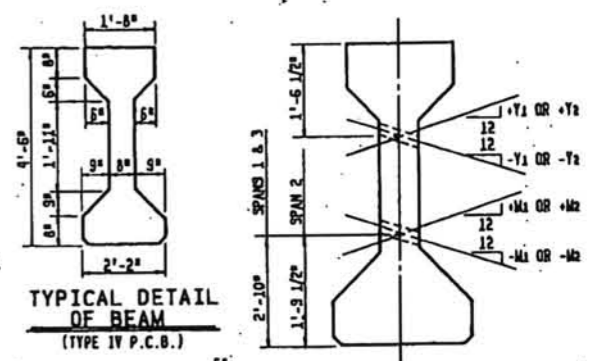
NOTE:
ENCASE PLATE FOR EXPANSION JOINT REQUIRED AT:
RIGHT HAND END BEAM A1
BOTH ENDS BEAMS A2 THRU L2
LEFT HAND END BEAM A3



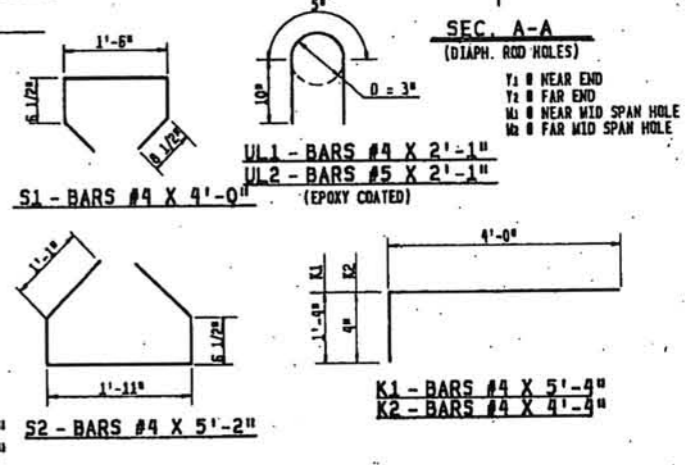
DETAIL OF LIFTING EYE



DEAD LOAD DEFLECTION DIAGRAM



TYPICAL DETAIL OF BEAM (TYPE IV P.C.B.)



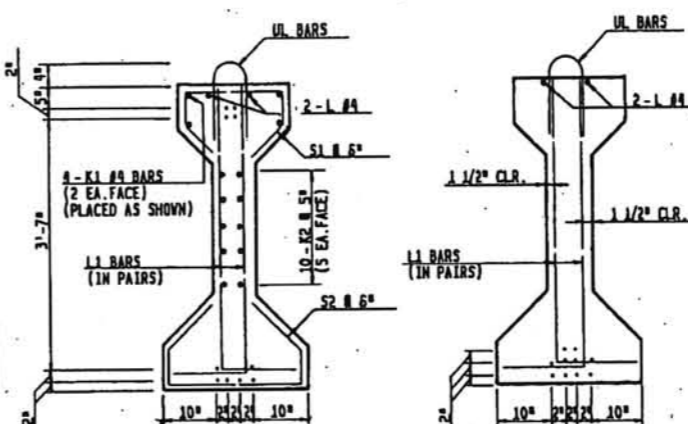
BAR BENDING DIAGRAMS

(ALL DIMENSIONS ARE OUT TO OUT)

| SPAN NO. | BEAM NO. | NO. OF STRANDS | D.L. DEF. Δ | BEAM DIMENSIONS | | | | END DIAPH. DIMENSIONS | | | | DIAPH. CROSS SLOPE | | | | |
|----------|----------|----------------|-------------|-----------------|--------------|-----------|-------------|-----------------------|------------|--------|------------|--------------------|------|------|------|------|
| | | | | L | A | B | C | HD | D1 | X1 | D2 | X2 | Y1 | M1 | M2 | Y2 |
| 1 | A1 | 10 | 1/8" | 45'-8 3/8" | 22'-3 13/16" | -0- | 23'-4 9/16" | 4'-6" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | +1/4 | +1/4 | - | +1/4 |
| | A2 | 46 | 2 1/16" | 97'-7 7/8" | 48'-3 9/16" | -0- | 49'-4 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | +1/4 | +1/4 | - | +1/4 |
| | B2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | +1/4 | +1/4 | +1/4 | +1/4 |
| 2 | C2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | +1/4 | +1/4 | +1/4 | +1/4 |
| | D2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | +1/4 | +1/4 | +1/4 | +1/4 |
| | E2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | +1/4 | +1/4 | +1/4 | +1/4 |
| | F2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | 0 | 0 | 0 | 0 |
| | G2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | -1/4 | -1/4 | -1/4 | -1/4 |
| | H2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | -1/4 | -1/4 | -1/4 | -1/4 |
| 3 | K2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | -1/4 | -1/4 | -1/4 | -1/4 |
| | L2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | -1/4 | -1/4 | -1/4 | -1/4 |
| | A3 | 10 | 1/8" | 45'-8 3/8" | 22'-3 13/16" | -0- | 23'-4 9/16" | 4'-6" | 1'-0 9/16" | 1 7/16 | 1'-0 9/16" | 1 7/16 | +1/4 | +1/4 | - | +1/4 |

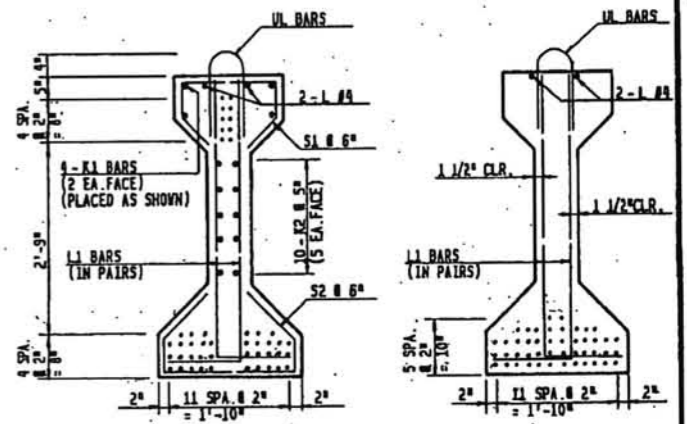
SPAN 1 & 3
USE CONC. F'c = 5,000 PSI; Fct = 3,750 PSI

SPAN 2
USE CONC. F'c = 8,000 PSI; Fct = 6,000 PSI



END VIEW WITH REINFORCING (YE = 20.80°)
SECTION (YM = 3.60°)

10 - 1/2" Ø STRANDS

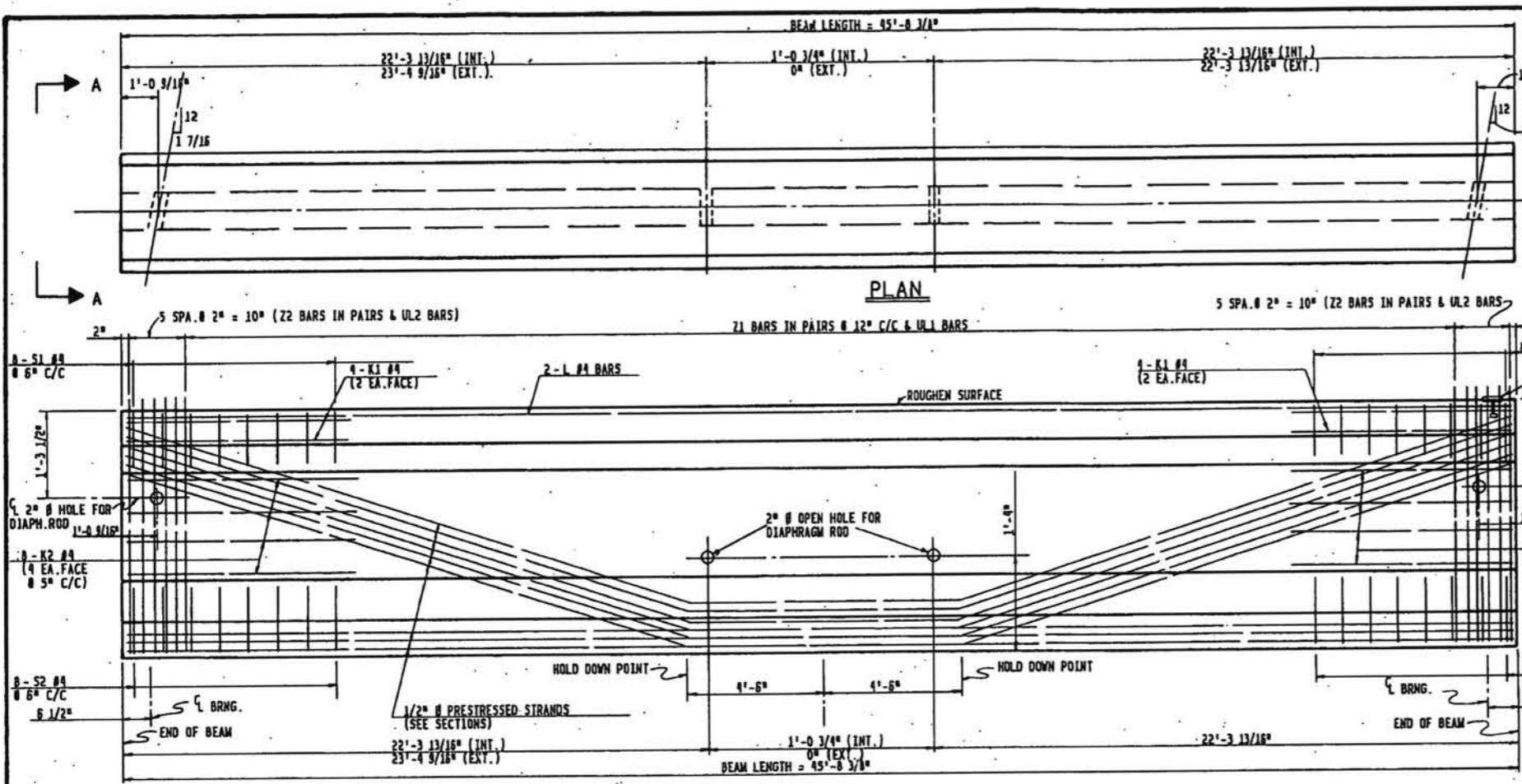


END VIEW WITH REINFORCING (YE = 13.43°)
SECTION (YM = 4.95°)

46 - 1/2" Ø STRANDS

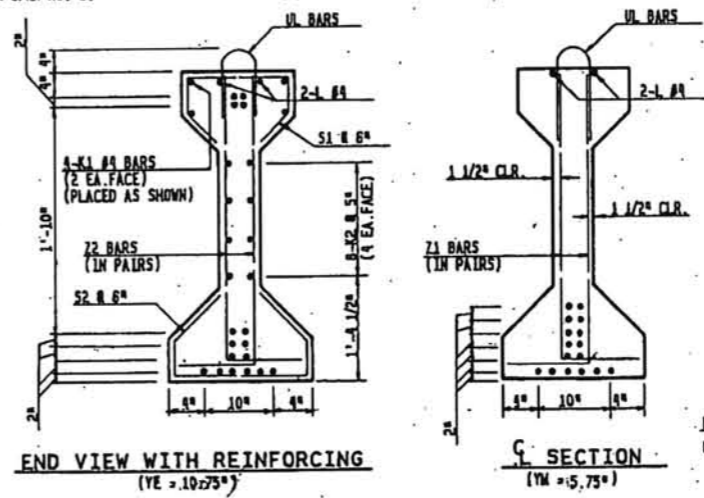
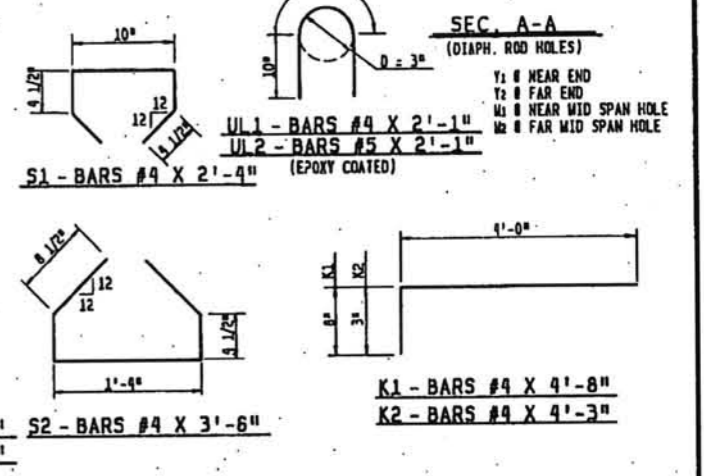
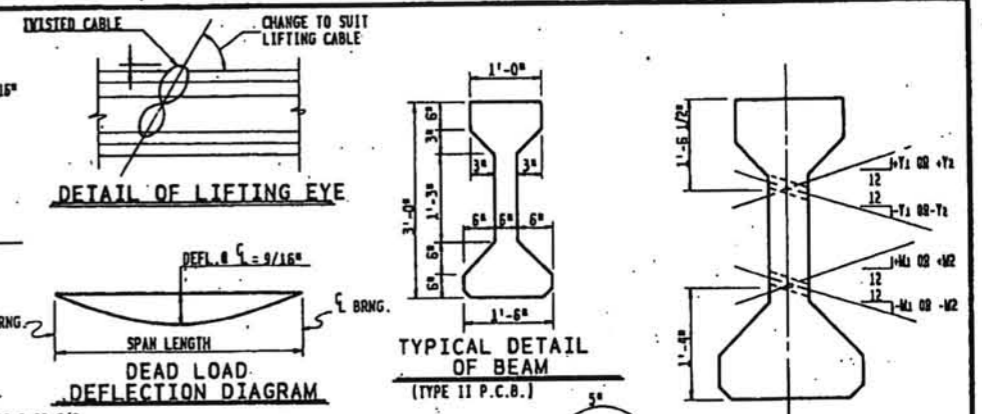
NOTES:
1. FOR ENCASED SOLE PLATE DETAILS SEE TYPICAL ANCHOR BOLT SETTING SHY. 202
2. ALL STRANDS TYPE 270K 7 WIRE UNCOATED LOW RELAXATION STEEL.

| | | |
|----------|------------|---|
| DESIGN | EDJ | BRIDGE "A" PRESTRESSED CONC. BEAMS (1 OF 2) TYPE IV |
| DRAWN | JHG | |
| CHECKED | WVW | |
| APPROVED | SJO | |
| SQUAD | G/K ENGRS. | PROJECT NO. J.P. 02689(11) SHEET NO. 216 |



ELEVATION
16 INTERIOR GIRDERS REQ'D
2 EXTERIOR GIRDERS REQ'D

NOTE:
ENCASED PLATE FOR EXPANSION JOINT REQUIRED AT:
RIGHT HAND END OF BEAMS IN SPAN NO. 1
LEFT HAND END OF BEAMS IN SPAN NO. 3.



NOTE:
USE CONC. $F'_c = 5,000$ PSI; $F_{ct} = 3,750$ PSI THIS SPAN.

16 - 1/2" Ø STRANDS

NOTES:
1. FOR ENCASED SOLE PLATE DETAILS SEE TYPICAL ANCHOR BOLT SETTING SHIT. 202.
2. ALL STRANDS TYPE 270K 7 WIRE UNCOATED LOW RELAXATION STEEL.

| | | |
|----------|------------|--|
| DESIGN | JM | BRIDGE "A" PRESTRESSED CONC. BEAMS TYPE II (2 OF 2) PROJECT NO. J.P. 02689(11) SHEET NO. 217 |
| DRAWN | JM | |
| CHECKED | JM | |
| APPROVED | JM | |
| SWAD | G/K ENGRS. | |

op. INV
36.4 45.5
3.8 7.6
25.57 39.47

NOTE:
ALL ABUTMENT PILING SHALL BE DRIVEN THROUGH THE COMPACTED FILL.

STEEL PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN ON THE PLANS. IF PRACTICAL REFUSAL IS NOT OBTAINED AT THIS ELEVATION, DRIVING SHALL CONTINUE UNTIL THE MAXIMUM REQUIRED BEARING OF 100 TONS IS OBTAINED.

THE LENGTHS OF STEEL PILING SHOWN ON THE PLANS ARE FOR ESTIMATING PURPOSES ONLY.

DESIGN DATA

DESIGN AASHTO SPECIFICATIONS, 1992 EDITION
LOADING HS20-44 & PM-20-4
20 LB. PER SQ. FT. FUTURE WEARING SURFACE
OPERATING RATING = HS-36.4

| PIERS | LFD | WSD |
|------------------------------|-----------------------------|-----------------------------|
| | | |
| REINF. STEEL (GR. 60) | F _y = 60,000 PSI | F _s = 24,000 PSI |
| STRUCTURAL STEEL (A-36) | | F _s = 20,000 PSI |
| CLASS A CONC. | F' _c = 4,000 PSI | F _c = 1,350 PSI |
| CLASS AA CONC. | F' _c = 3,000 PSI | F _c = 1,000 PSI |
| P.C. BEAM (SHORTER THAN 90') | | F _c = 5,000 PSI |
| P.C. BEAM (LONGER THAN 90') | | F _c = 3,750 PSI |
| P.C. BEAM (LONGER THAN 90') | | F _c = 6,000 PSI |

FOUNDATION PRESSURE

ABUTMENTS

GROUP I

PIER

GROUP I
MAX (GROUP VI)

SUMMARY OF QUANTITIES

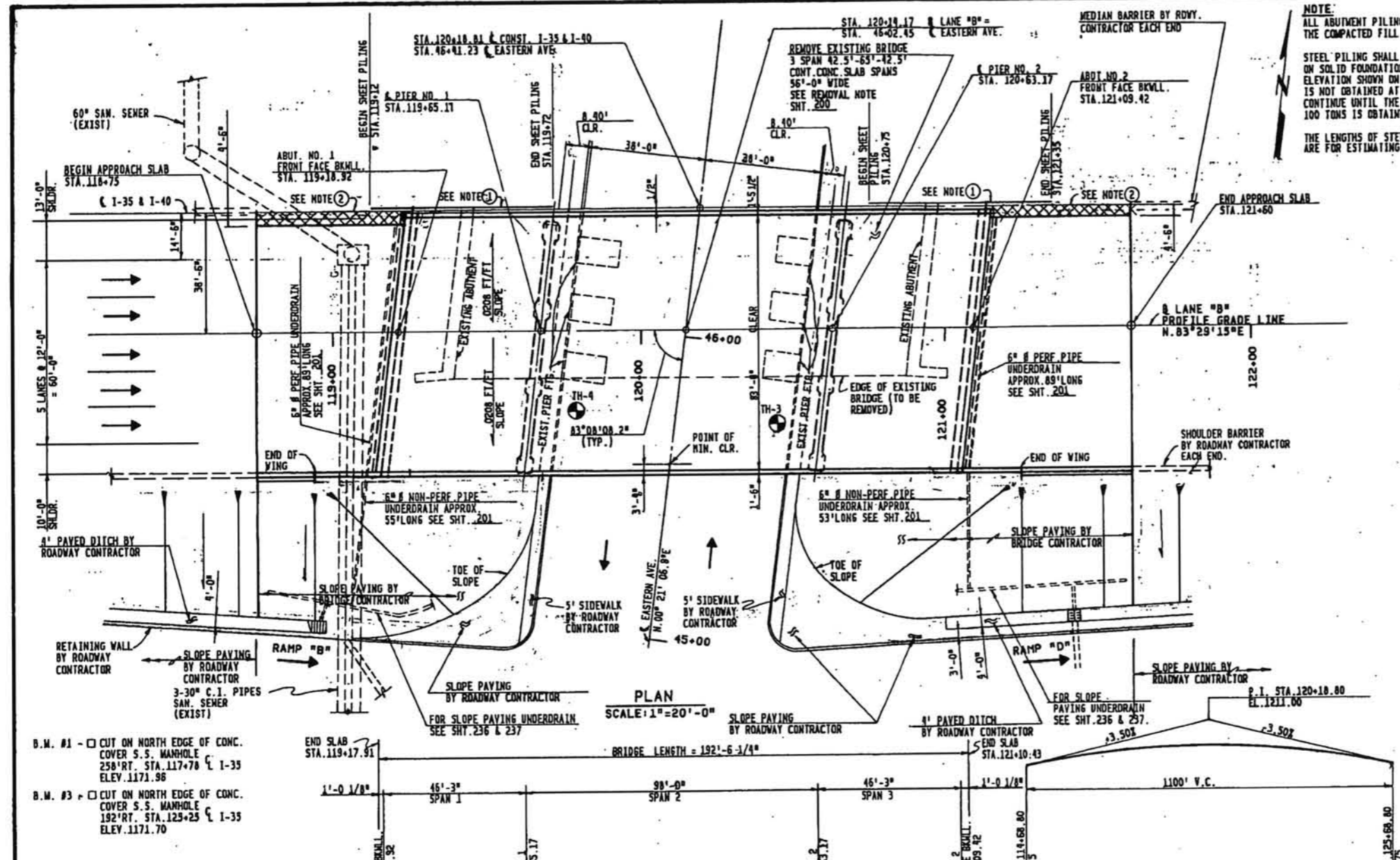
| ITEM | UNIT | ABUTS. | PIERS | SUPERSTR. | TOTAL |
|--------------------------------------|-------|--------|--------|-----------|---------|
| SELECT BORROW | C.Y. | 753.0 | | | 753.0 |
| APPROACH SLABS | S.Y. | | | | 1,021.4 |
| SUBSTR. EXCAV. COMMON | C.Y. | 123.2 | 108.8 | | 232.0 |
| PRESTRESSED CONC. BEAMS (TYPE II) | L.F. | | | 822.6 | 822.6 |
| PRESTRESSED CONC. BEAMS (TYPE IV) | L.F. | | | 1,068.0 | 1,068.0 |
| CLASS AA CONCRETE | C.Y. | | | 496.6 | 496.6 |
| CONCRETE PARAPET | L.F. | 44.0 | | 382.4 | 426.4 |
| STRUCTURAL STEEL A36 | LB. | 800 | 1,600 | 10,920 | 13,320 |
| CLASS A CONCRETE | C.Y. | 174.4 | 324.2 | | 498.6 |
| REINFORCING STEEL (GRADE 60) | LB. | 15,680 | 34,800 | | 50,480 |
| EPOXY COATED REINF. STEEL (GR. 60) | LB. | 17,260 | 86,720 | | 104,040 |
| STEEL PILING (HP 10 X 42) | L.F. | 1,328 | 2,496 | | 3,824.0 |
| PENETRAT. WATER REPELL. SURF. TRTMT. | S.Y. | | 593 | 700 | 1,293 |
| 6" PERFORATED PIPE UNDERDRAIN RND. | L.F. | | | | 875 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | L.F. | | | | 201 |
| PIPE UNDERDRAIN COVER MATERIAL | C.Y. | | | | 925 |
| TRENCH EXCAVATION | C.Y. | | | | 29.1 |
| STANDARD BEDDING MATERIAL | C.Y. | | | | 20.0 |
| REMOVAL OF EXIST. BRIDGE STR. | L.SUM | | | | 1 |
| 4" CONCRETE SLOPEWALL | S.Y. | | | | 3,795.1 |
| SEALED EXP. JOINT | L.F. | | | 171.2 | 171.2 |
| TEMPORARY EARTH RETAINAGE | L.SUM | | | | 1 |

SHEET INDEX

| SHEET NO. | TITLE |
|------------|-----------------------------------|
| 30 | SUMMARY QUANTITIES - BRIDGE |
| 200 & 200A | GENERAL NOTES - BRIDGE |
| 201 | MISCELLANEOUS DETAILS - BRIDGE |
| 202 | MISCELLANEOUS DETAILS - BRIDGE |
| 220 | GENERAL PLAN & ELEVATION |
| 220A | FOUNDATION REPORT |
| 221 | BRIDGE STAKING DIAGRAM |
| 222 | ABUTMENT DETAILS (1 OF 3) ABUT. 1 |
| 223 | ABUTMENT DETAILS (2 OF 3) ABUT. 2 |
| 224 | ABUTMENT DETAILS (3 OF 3) BOTH |
| 225 | PIER DETAILS (1 OF 4) |
| 226 | PIER DETAILS (2 OF 4) |
| 227 | PIER DETAILS (3 OF 4) |
| 228 | PIER DETAILS (4 OF 4) |
| 229 | TYPICAL SECTION |
| 230 | CONCRETE DECK PLAN |
| 231 | GIRDER LAYOUT |
| 232 | PRESTRESSED CONC. BEAMS TYPE IV |
| 233 | PRESTRESSED CONC. BEAMS TYPE II |
| 234 | BEARING DETAILS |
| 235 | APPROACH SLAB DETAILS |
| 236 | SLOPE PAVING DETAILS (1 OF 2) |
| 237 | SLOPE PAVING DETAILS (2 OF 2) |
| 238 | SEALED EXP. JOINT (1 OF 2) |
| 239 | SEALED EXP. JOINT (2 OF 2) |

NOTE ①
120 L.F. (60' EA. END) SHEET PILING FOR TEMP. EARTH RETAINAGE STRUCTURE. SEE "TEMPORARY EARTH RETAINAGE STRUCTURE" NOTE SHT. 200.

NOTE ②
TEMPORARY ASPHALT FILL EACH END. SEE "TEMPORARY ASPHALT FILL" NOTE SHT. 200.



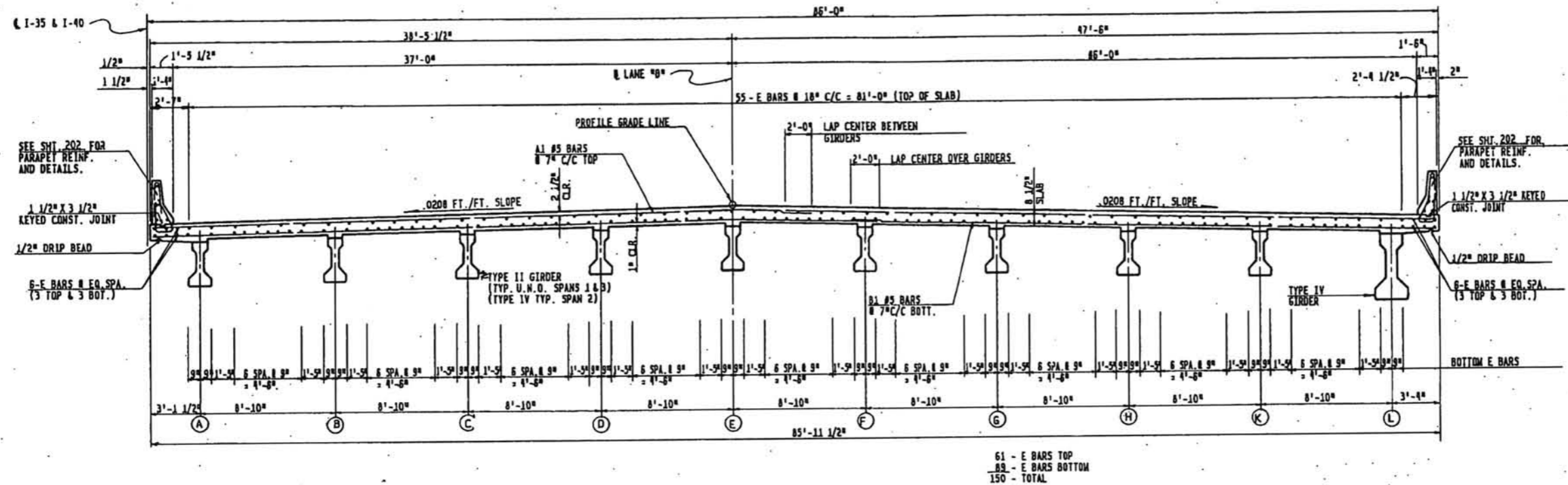
VERTICAL CURVE DATA

| ELEVATION | 1200 | 1180 | 1160 | 1140 | 1120 |
|-------------------------------------|------|------|------|------|------|
| EXISTING GROUND LINE | | | | | |
| PROFILE GRADE LINE | | | | | |
| EXISTING GROUND LINE | | | | | |
| 1 - HP10X42 PILE X 64'-0" LONG | | | | | |
| 5 - HP10X42 PILES X 59'-6" LONG | | | | | |
| 5 - HP10X42 PILES X 60'-6" LONG | | | | | |
| 3-30" C.I. PIPES SAN. SEWER (EXIST) | | | | | |
| BOTTOM PILE ELEV. 1133.40 | | | | | |
| BOTTOM PILE ELEV. 1133.24 | | | | | |
| BOTTOM PILE ELEV. 1133.06 | | | | | |
| BOTTOM PILE ELEV. 1133.27 | | | | | |
| BOTTOM PILE ELEV. 1133.27 | | | | | |
| BOTTOM PILE ELEV. 1133.06 | | | | | |
| BOTTOM PILE ELEV. 1133.24 | | | | | |

BRIDGE STRUCTURE "B" GENERAL PLAN & ELEVATION LANE "B" (I-35 & I-40) OVER N. EASTERN AVE.

PROJECT NO. J.P. 002689(11) SHEET NO. 220

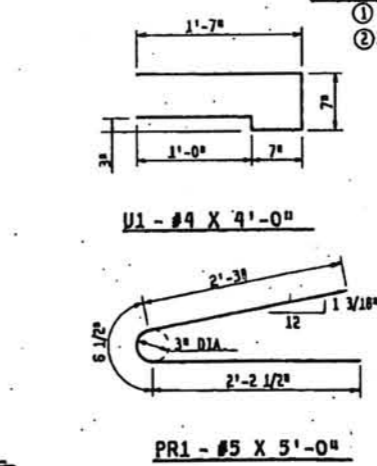
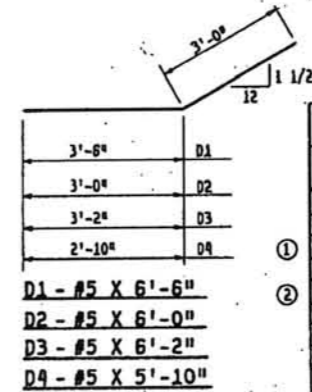
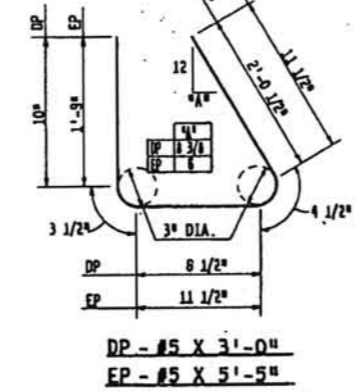
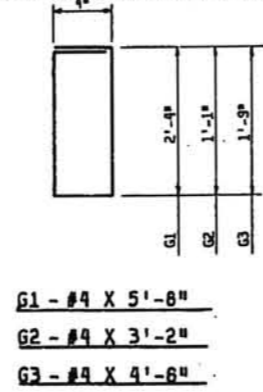
DESIGN: 6/K ENGRS.



TYPICAL SECTION
(SPANS 1 & 3 SHOWN)
(SPAN 2 ALL TYPE IV GIRDERS)

| BAR LIST SUPERSTRUCTURE (ALL BARS ARE EPOXY COATED) | | | | | | | | | | | | | | |
|---|-----|------|------|----------------|------|-----|------|--------|---------------|------|-----|------|---------|---------------|
| SPAN 1 | | | | SPAN 2 | | | | SPAN 3 | | | | | | |
| MARK | NO. | SIZE | FORM | LENGTH | MARK | NO. | SIZE | FORM | LENGTH | MARK | NO. | SIZE | FORM | LENGTH |
| A1 | 63 | #5 | STR. | 87'-8" | A1 | 151 | #5 | STR. | 87'-8" | A1 | 62 | #5 | STR. | 87'-8" |
| AD | 4 | #4 | STR. | 86'-8" | B1 | 151 | #5 | STR. | 87'-8" | AD | 4 | #4 | STR. | 86'-8" |
| B1 | 63 | #5 | STR. | 87'-8" | C3 | 44 | #5 | STR. | 32'-8" (AVG.) | B1 | 62 | #5 | STR. | 87'-8" |
| C3 | 22 | #5 | STR. | 32'-8" (AVG.) | C4 | 20 | #5 | STR. | 73'-8" (AVG.) | C3 | 24 | #5 | STR. | 33'-3" (AVG.) |
| C4 | 10 | #5 | STR. | 73'-8" (AVG.) | D2 | 8 | #5 | BNT. | 8'-7" | C4 | 10 | #5 | STR. | 73'-8" (AVG.) |
| C5 | 22 | #5 | STR. | 33'-2" (AVG.) | E5 | 150 | #4 | STR. | 99'-8" | C5 | 22 | #5 | STR. | 32'-8" (AVG.) |
| C6 | 10 | #5 | STR. | 73'-11" (AVG.) | EP | 198 | #5 | BNT. | 5'-5" | C6 | 10 | #5 | STR. | 73'-8" (AVG.) |
| D2 | 2 | #5 | BNT. | 6'-7" | G1 | 144 | #4 | BNT. | 5'-8" | D2 | 2 | #5 | BNT. | 6'-7" |
| DP | 40 | #5 | BNT. | 3'-0" | G2 | 72 | #4 | BNT. | 4'-6" | DP | 40 | #5 | BNT. | 3'-0" |
| E1 | 4 | #4 | STR. | 46'-1" | H2 | 36 | #7 | STR. | 7'-11" | E1 | 4 | #4 | STR. | 45'-10" |
| E2 | 2 | #4 | STR. | 47'-1" | H3 | 36 | #4 | STR. | 7'-3" | E2 | 2 | #4 | STR. | 46'-11" |
| E3 | 138 | #4 | STR. | 47'-0" | H4 | 36 | #4 | STR. | 8'-11" | E3 | 140 | #4 | STR. | 47'-0" |
| E4 | 6 | #4 | STR. | 46'-9" | PH2 | 12 | #4 | STR. | 99'-8" | E4 | 4 | #4 | STR. | 47'-2" |
| EP | 53 | #5 | BNT. | 5'-5" | PH3 | 6 | #4 | STR. | 46'-9" | EP | 54 | #5 | BNT. | 5'-5" |
| G1 | 144 | #4 | BNT. | 5'-8" | PH3R | 6 | #4 | STR. | 46'-1" | G1 | 144 | #4 | BNT. | 5'-8" |
| G2 | 72 | #4 | BNT. | 3'-2" | PR1 | 93 | #5 | BNT. | 9'-0" | G2 | 72 | #4 | BNT. | 3'-2" |
| H1 | 36 | #4 | STR. | 7'-7" | U1 | 85 | #4 | BNT. | 4'-0" | H1 | 36 | #4 | STR. | 7'-7" |
| H2 | 36 | #7 | STR. | 7'-11" | | | | | H2 | 36 | #7 | STR. | 7'-11" | |
| H3 | 36 | #4 | STR. | 7'-3" | | | | | H3 | 36 | #4 | STR. | 7'-3" | |
| PH3L | 6 | #4 | STR. | 46'-9" | | | | | PH3L | 6 | #4 | STR. | 47'-3" | |
| PH3R | 6 | #4 | STR. | 46'-1" | | | | | PH3R | 6 | #4 | STR. | 45'-10" | |
| PR1 | 93 | #5 | BNT. | 9'-0" | | | | | PR1 | 94 | #5 | BNT. | 9'-0" | |
| U1 | 85 | #4 | BNT. | 4'-0" | | | | | U1 | 85 | #4 | BNT. | 4'-0" | |

- ① INCLUDES 2'-0" FOR LAP SPLICE, STAGGERED
- ② VARIES: 6'-7" TO 59'-11"
- ③ VARIES: 66'-10" TO 86'-2"
- ④ VARIES: 8'-5" TO 56'-11"
- ⑤ VARIES: 63'-10" TO 83'-2"
- ⑥ VARIES: 8'-11" TO 57'-5"
- ⑦ VARIES: 64'-3" TO 83'-7"
- ⑧ IN END DIAPHRAGMS
- ⑨ IN INTERMEDIATE DIAPHRAGMS
- ⑩ HALF IN EACH PARAPET
- ⑪ INCLUDED IN PRICE BID PER L.F. OF PARAPET



| SUPERSTRUCTURE QUANTITIES - BRIDGE "B" | | | | | |
|--|------|--------|--------|--------|----------|
| ITEM | UNIT | AMOUNT | | | |
| | | SPAN 1 | SPAN 2 | SPAN 3 | TOTAL |
| PRESTRESSED CONC. BEAMS (TYPE II) | L.F. | 411.30 | — | 411.30 | 822.6 |
| PRESTRESSED CONC. BEAMS (TYPE IV) | L.F. | 45.70 | 976.56 | 45.70 | 1,067.96 |
| CLASS AA CONCRETE | C.Y. | 124.6 | 247.4 | 124.6 | 496.6 |
| CONCRETE PARAPET | L.F. | 93.18 | 195.67 | 93.49 | 382.34 |
| ① STRUCTURAL STEEL (A36) | LB. | 3,360 | 4,200 | 3,360 | 10,920 |
| ② EPOXY COATED REINF. STEEL (GR. 60) | LB. | 21,690 | 43,460 | 21,630 | 86,780 |
| PENETRAT. WATER REPEL. TRTMT. | S.Y. | 165 | 369 | 166 | 700 |
| SEALED EXPANSION JOINT | L.F. | 85.6 | — | 85.6 | 171.2 |

- ① INCLUDES 16 C.Y. FOR HAUNCHES
- ② INCLUDES SOLE PLATES, DIAPHRAGMS RODS & FITTINGS.

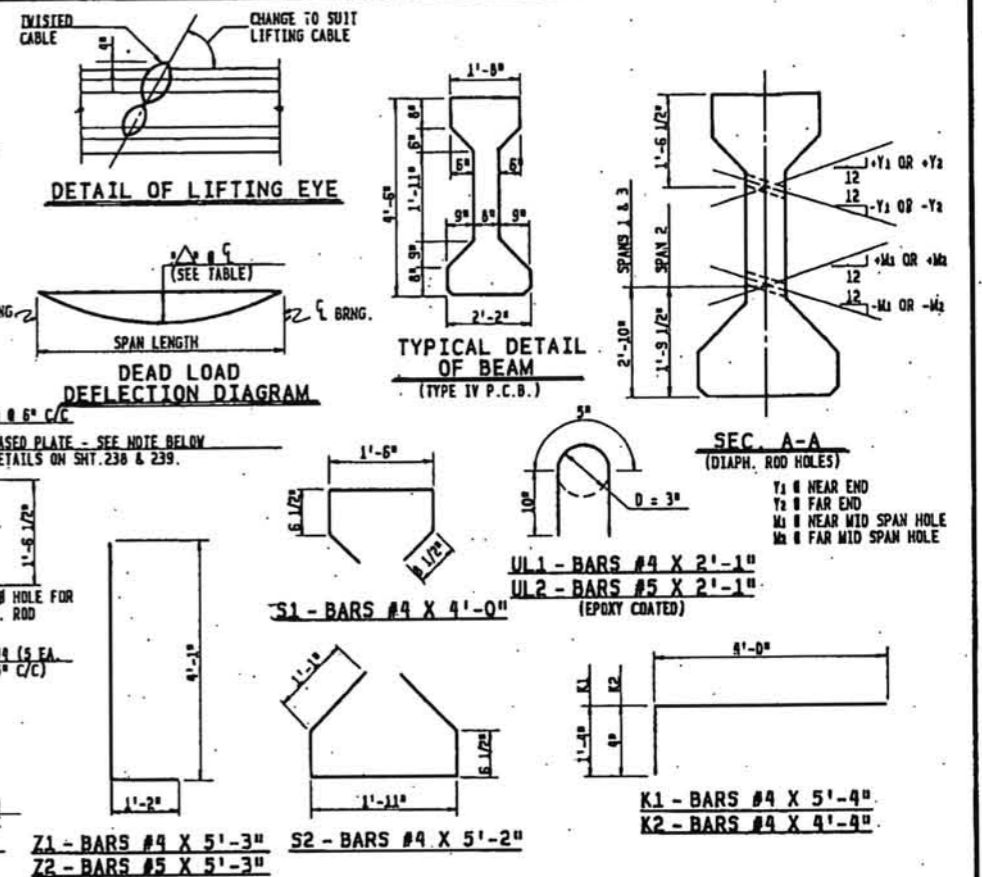
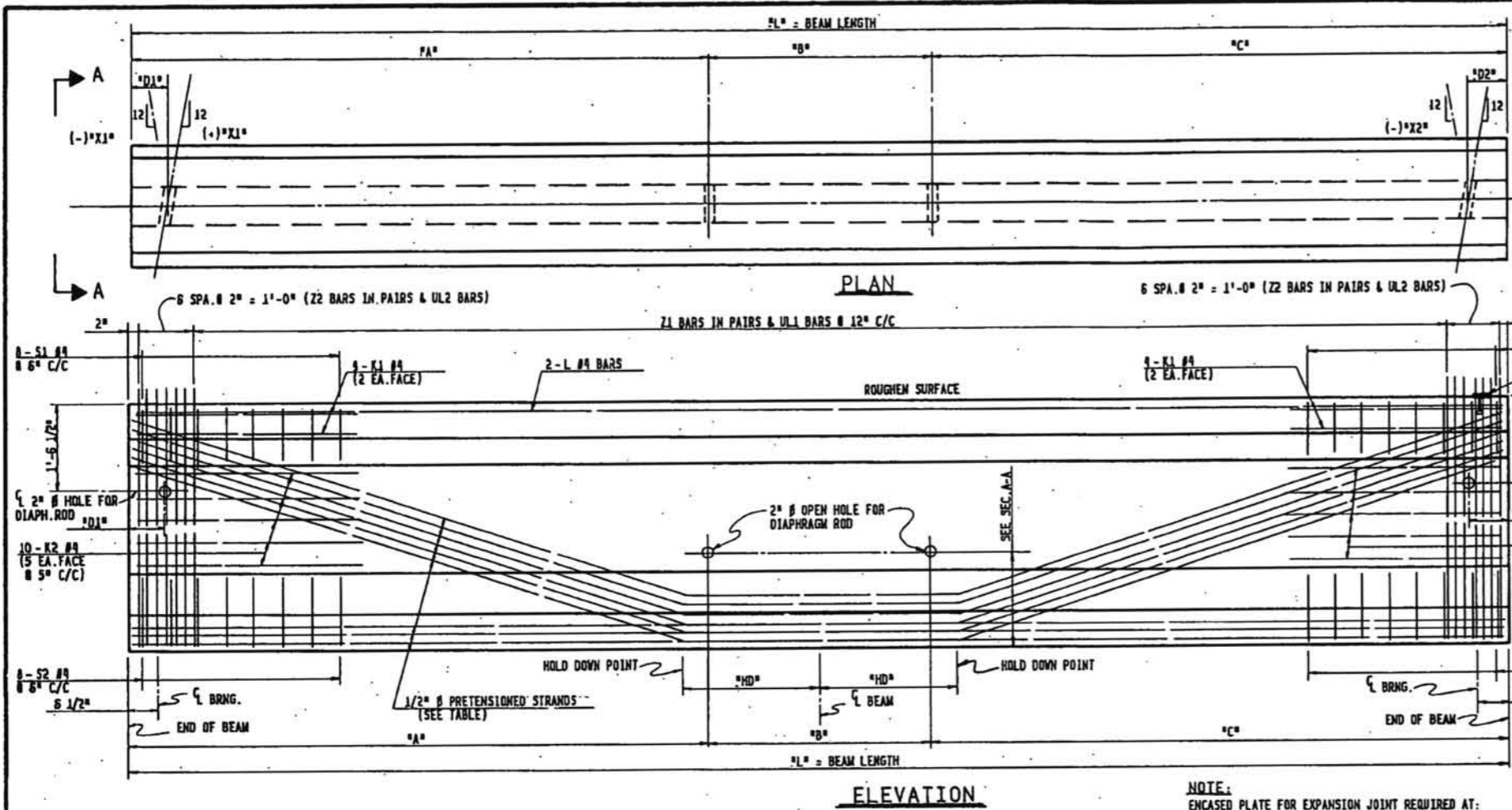
- NOTES:
- SEE SHT. 201 FOR GENERAL NOTES.
 - SEE SHT. 202 FOR PARAPET DETAILS.
 - FOR DIAPHRAGMS SEE SHT. 215.
 - FOR DECK JOINT DETAILS SEE SHT. 234.
 - ALL BARS THIS SHEET TO BE EPOXY COATED.

| | | |
|----------|------------|------------|
| DESIGN | JW | BRIDGE "B" |
| DRAWN | JSV | |
| CHECKED | WV | |
| APPROVED | SAE | |
| SEAL | G/K ENGRS. | |

TYPICAL SECTION

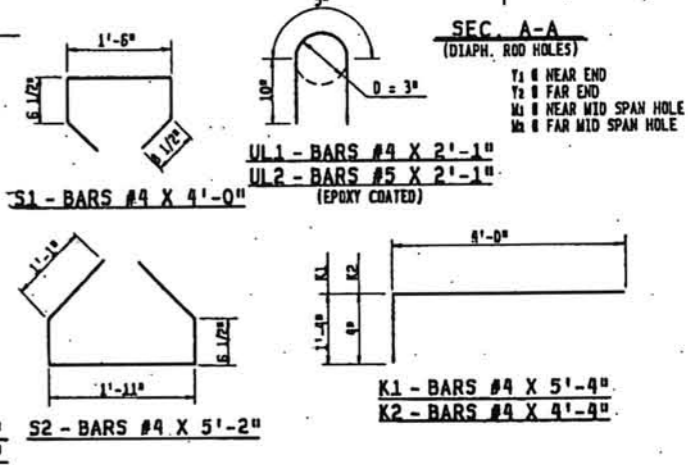
PROJECT NO. J.P. 02689(11) SHEET NO. 229

BAR BENDING DIAGRAMS
(ALL DIMENSIONS ARE OUT TO OUT)



NOTE:
ENCASED PLATE FOR EXPANSION JOINT REQUIRED AT:
RIGHT HAND END BEAM L1
BOTH ENDS BEAMS A2 THRU L2
LEFT HAND END BEAM L3

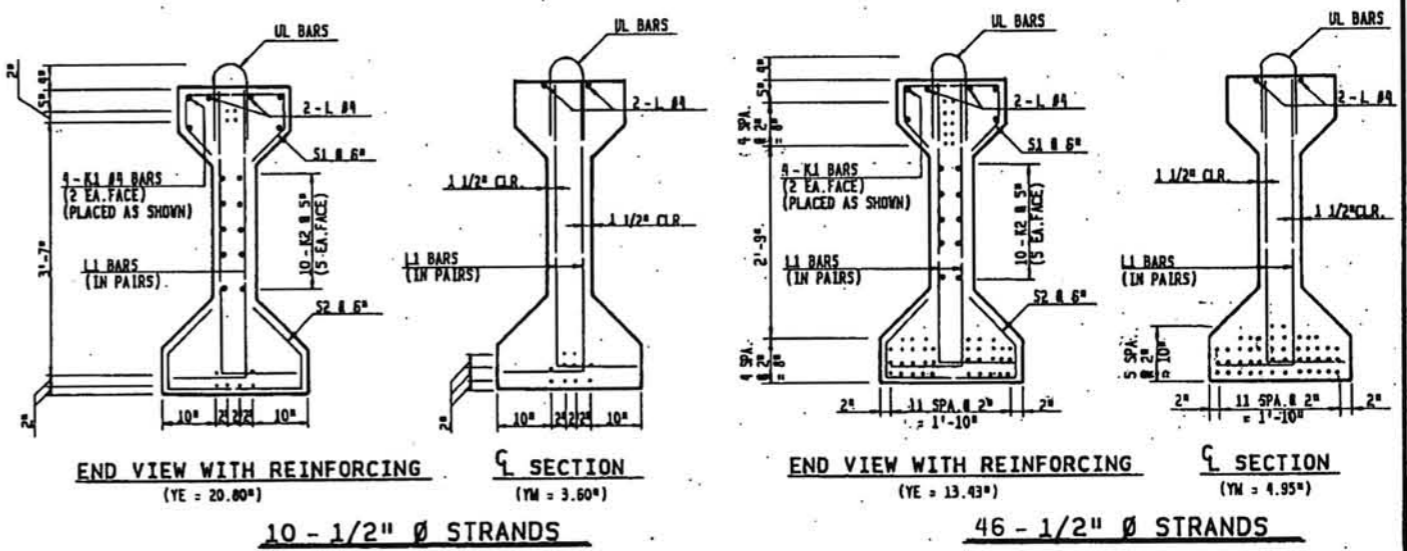
BAR BENDING DIAGRAMS
(ALL DIMENSIONS ARE OUT TO OUT)



| SPAN NO. | BEAM NO. | NO. OF STRANDS | D.L. DEFL. | BEAM DIMENSIONS | | | | END DIAPH. DIMENSIONS | | | | DIAPH. CROSS SLOPE | | | | |
|----------|----------|----------------|------------|-----------------|-------------|-----------|--------------|-----------------------|------------|---------|------------|--------------------|------|------|------|------|
| | | | | L | A | B | C | HD | D1 | X1 | D2 | X2 | Y1 | M1 | M2 | Y2 |
| 1 | L1 | 10 | 1/8" | 45'-8 3/8" | 23'-4 9/16" | -0- | 22'-3 13/16" | 4'-6" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | -1/4 | -1/4 | - | 1/4 |
| 2 | A2 | 46 | 2 1/16" | 97'-7 7/8" | 48'-3 9/16" | -0- | 49'-4 5/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | -1/4 | -1/4 | - | -1/4 |
| | B2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | -1/4 | -1/4 | 1/4 | -1/4 |
| | C2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | -1/4 | -1/4 | -1/4 | -1/4 |
| | D2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | -1/4 | -1/4 | -1/4 | -1/4 |
| | E2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | 0 | 0 | 0 | 0 |
| | F2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | +1/4 | +1/4 | +1/4 | +1/4 |
| 2 | G2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | +1/4 | +1/4 | +1/4 | +1/4 |
| | H2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | +1/4 | +1/4 | +1/4 | +1/4 |
| | K2 | 46 | 2 3/16" | 97'-7 7/8" | 48'-3 9/16" | 1'-0 3/4" | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | +1/4 | +1/4 | +1/4 | +1/4 |
| | L2 | 46 | 2 1/16" | 97'-7 7/8" | 49'-4 5/16" | -0- | 48'-3 9/16" | 10'-0" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | +1/4 | +1/4 | - | +1/4 |
| 3 | L3 | 10 | 1/8" | 45'-8 3/8" | 23'-4 9/16" | -0- | 22'-3 13/16" | 4'-6" | 1'-0 9/16" | 1 7/16" | 1'-0 9/16" | 1 7/16" | -1/4 | -1/4 | - | -1/4 |

SPAN 1 & 3
USE CONC. F'c = 5,000 PSI; Fcl = 3,750 PSI

SPAN 2
USE CONC. F'c = 8,000 PSI; Fcl = 6,000 PSI

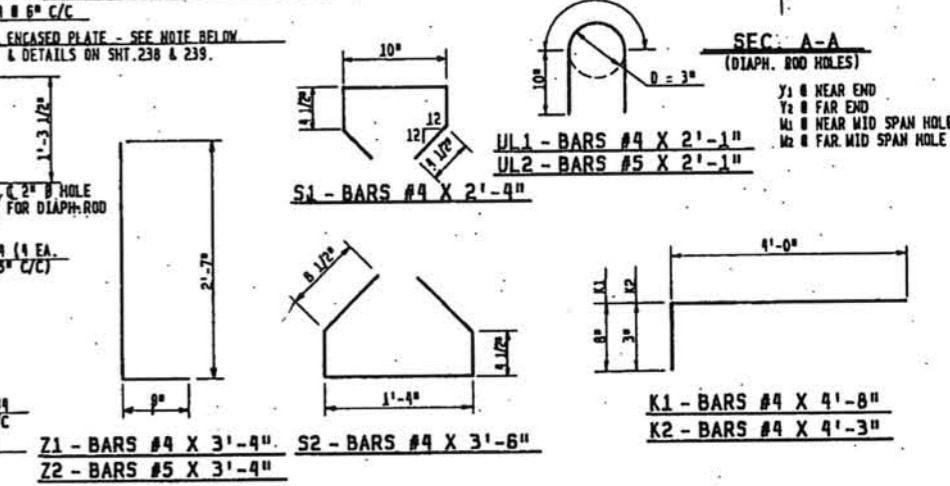
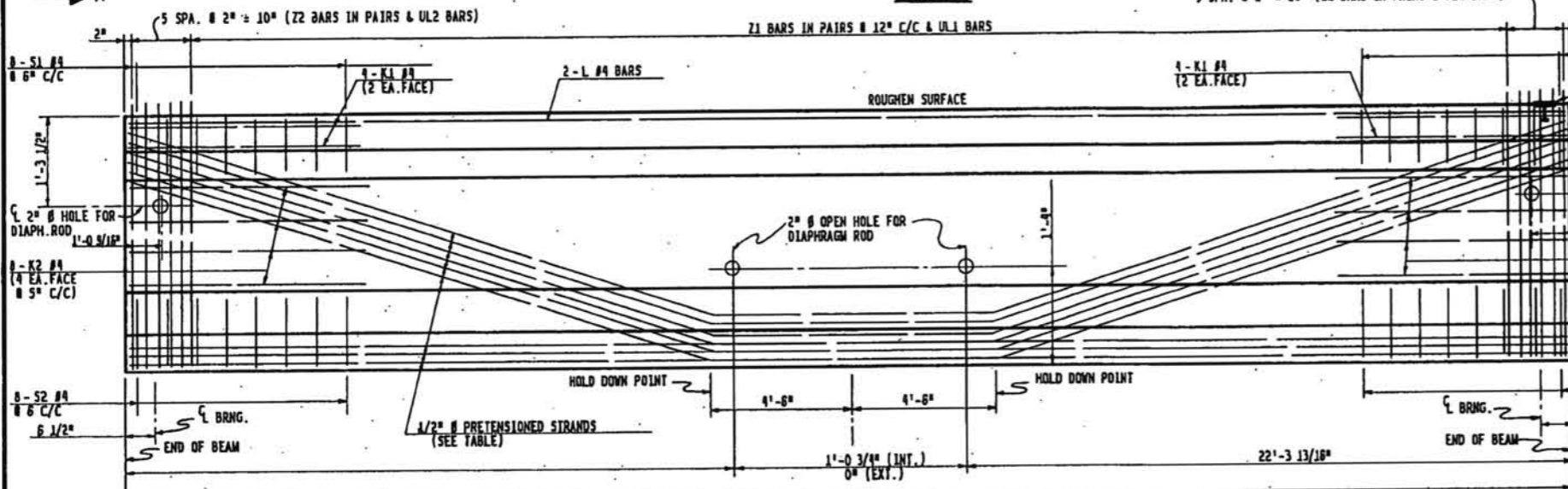
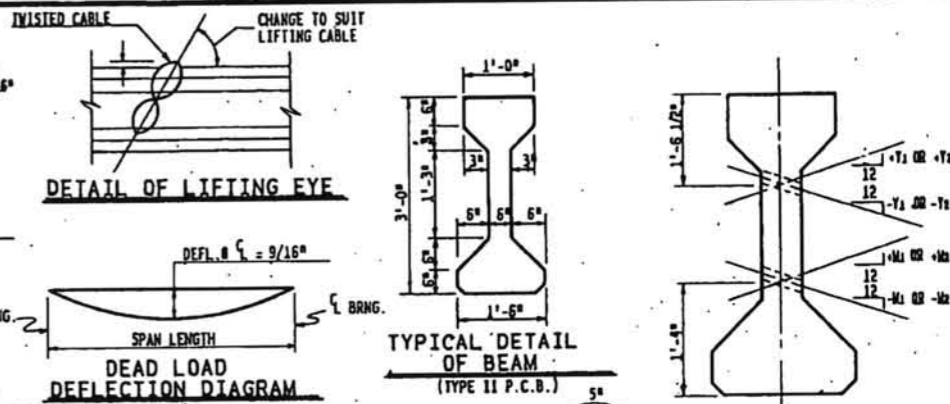
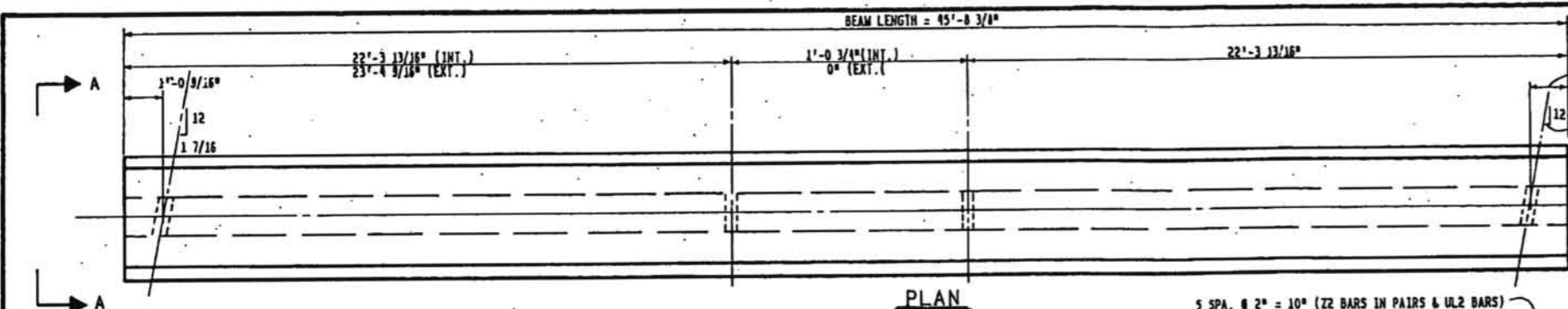


NOTES:
1. FOR ENCASED SOLE PLATE DETAILS SEE TYPICAL ANCHOR BOLT SETTING SHIT. 202.
2. ALL STRANDS TYPE 270K 7 WIRE UNCOATED LOW RELAXATION STEEL.

| | | |
|----------|------------|--------------|
| DESIGN | EDT | BRIDGE #B-22 |
| DRAWN | JAN | |
| CHECKED | WVV | |
| APPROVED | SAJ | |
| SWAD | G/K ENGRS. | |

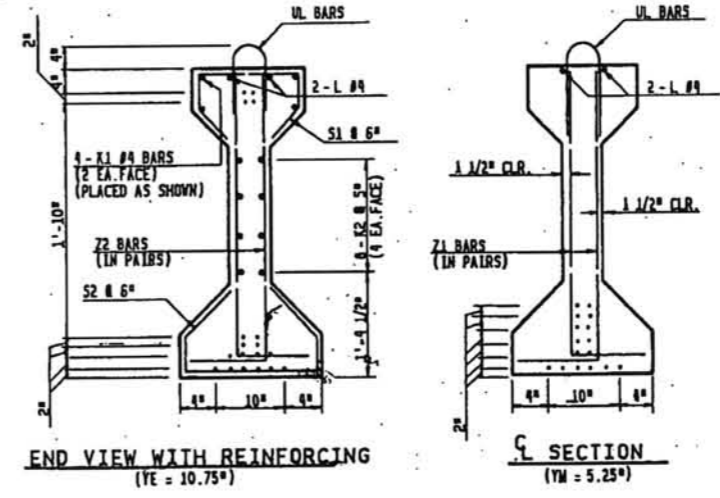
PRESTRESSED CONC. BEAMS (1 OF 2)
TYPE IV

PROJECT NO. J.P. 02689(11) SHEET NO. 232



ELEVATION
 16 INTERIOR GIRDERS REQUIRED
 2 EXTERIOR GIRDERS REQUIRED

NOTE:
 ENCASED PLATE FOR EXPANSION JOINT REQUIRED AT:
 RIGHT HAND END OF BEAMS IN SPAN NO. 1
 LEFT HAND END OF BEAMS IN SPAN NO. 3



16 - 1/2" Ø STRANDS

NOTES:
 1. FOR ENCASED SOLE PLATE DETAILS SEE TYPICAL ANCHOR BOLT SETTING SHIT. 202
 2. ALL STRANDS TYPE 270K 7 WIRE UNCOATED LOW RELAXATION STEEL.

NOTE:
 CONCRETE F'c = 5,000 PSI; Fct = 3,750 PSI THIS SHEET.

| | | |
|----------|------------|----------------------------|
| DESIGN | JW | BRIDGE "B" |
| DRAWN | JSW | |
| CHECKED | WV | |
| APPROVED | SJS | |
| SCALE | G/K ENGRS. | |
| | | PROJECT NO. J.P. 02609(11) |
| | | SHEET NO. 233 |

**PRESTRESSED CONC. BEAMS
 TYPE II (2 OF 2)**

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

SECTION 03 – PROPERTY IDENTIFICATION

3.1 Property Ownership. The following properties were identified as being within the study area:

| Map Parcel # | Parcel # | Ownership | Acres | Land Use | Relo Y or N | Exhibit Page # |
|--------------|------------|---|--------------|----------------|-------------|----------------|
| 1 | R142971015 | Teague Business Properties LLC ATTN: Frances Ann Teague Waggoner 8125 S. I-35 Service Rd. Oklahoma City, OK 73149 | 7.2904 | Comm. | N | 1-2 |
| 2 | R142975105 | Teague Business Properties LLC ATTN: Frances Ann Teague Waggoner 8125 S. I-35 Service Rd. Oklahoma City, OK 73149 | 1.8200 | Res. | N | 3-4 |
| 3 | R146463005 | Magellan Pipeline Company LLC Attn: Property Tax P.O. Box 22186 Tulsa, OK 74121 | 0.1528 | Public Service | N | 5-6 |
| 4 | R142975005 | Magellan Pipeline Company LLC Attn: Property Tax P.O. Box 22186 Tulsa, OK 74121 | 1.3800 | Public Service | N | 7-8 |
| 5 | R142971055 | Waggoner Family Properties Inc. P.O. Box 1516 Billings, MT 59103 | 16.412 5 | Industria l | N | 9-10 |
| 6 | R142976010 | State of Oklahoma Dept. of Transportation | 0.5897 | Exempt | N | 11-12 |
| 7 | R142971045 | Waggoner Family Properties II Inc. P.O. Box 1516 Billings, MT 59103 | 1.1949 | Comm. | N | 13-14 |
| 8 | R131721925 | St. Louis & Santa Fe RR Co. 503 Frisco Bldg St. Louis, MO 63104 | 12.145 6 | Public Service | N | 15 |
| 9 | R131722000 | Native American Cultural Center and Museum (UC) P.O. Box 26980 Oklahoma City, OK 73126 | 155.49 00 | Exempt | N | 16-17 |
| 10 | R168611055 | City of Oklahoma City | 29.050 0 | Exempt | N | 18 |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| | | | | | | |
|----|------------|--|--------|----------------|---|-------|
| 11 | R131721395 | Pumpco LLC P.O. Box 892230 Oklahoma City, OK 73189 | 1.2154 | Industria I | N | 19-20 |
| 12 | R131721377 | Jacob Company Inc. 727 SW 23 rd St. Oklahoma City, OK 73109 | 1.8850 | Industria I | N | 21-22 |
| 13 | R027881500 | Standard Steel Company P.O. Box 302 Oklahoma City, OK 73101 | 2.3640 | Comm. | N | 23-24 |
| 14 | R027857200 | Standard Steel Company P.O. Box 302 Oklahoma City, OK 73101 | 1.5496 | Industria I | Y | 25-26 |
| 15 | R027883000 | Standard Steel Company 1400 E. Reno Ave. Oklahoma City, OK 73117 | 2.6126 | Industria I | N | 27-28 |
| 16 | R027884500 | Standard Steel Company P.O. Box 302 Oklahoma City, OK 73101 | 1.0938 | Comm. | N | 29-30 |
| 17 | R027886000 | Standard Iron & Metal Company Inc. P.O. Box 302 Oklahoma City, OK 73101 | 3.9680 | Industria I | N | 31-32 |
| 18 | R142970295 | Lewis, Charles W & Elizabeth A Living Trust 1600 E. Reno Ave. Oklahoma City, OK 73117 | 7.6292 | Comm. | Y | 33-34 |
| 19 | R142970286 | Brightlight Hospitality Inc. 1750 E. Reno Ave. Oklahoma City, OK 73117 | 1.4949 | Comm. | Y | 35-36 |
| 20 | R142970283 | Brightlight Hospitality Inc. 1750 E. Reno Ave. Oklahoma City, OK 73117 | 1.9135 | Comm. | Y | 37-38 |
| 21 | R142970280 | Noor Hotel LLC 1800 E. Reno Oklahoma City, OK 73117 | 2.9527 | Comm. | Y | 39-40 |
| 22 | R142970285 | MLK/1948 Reno LLC 3200 Canyon Rd. Oklahoma City, OK 73120 | 0.9626 | Comm. | Y | 41-42 |

3.1.02 Potential Business and Residential Relocations.

Residential/Single Family = 0

Business/Comm. = 6

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
OKLAHOMA COUNTY
J/P 30444(04)

- P.12 Jacob Company Inc., Storage Warehouse
Building size = 15,000sf

- P.14 Standard Steel Company, Industrial Light Manufacturing
Building size = 20,300sf

- P.18 Standard Steel Company
Automotive Center
Building size = 5,040sf
Service Garage
Building size = 8,550sf

- P.20 Brightlight Hospitality Inc., Econo Lodge Inns and Suites Near Bricktown
Building size = 38,582sf

- P.21 Noor Hotel LLC, Quality Inn
Building size = 46,194sf

- P.22 MLK/1948 Reno LLC, Greyhound, Restaurant
Building size = 6,516sf

- 3.2 Indian Ownership. There are no Indian Ownerships within the study area.

- 3.3 Tribal Ownership. There is one parcel within the study area.

P.9 Native American Cultural Center and Museum (UC), American Indian Cultural Center and Museum (UC)

- 3.4 Identify any Military Properties within the study area. There are no Military Properties located in project area.

- 3.5 Oklahoma Turnpike Authority (OTA) Properties. There were no OTA properties identified within the study area.

- 3.6 Public Parks and Recreational Areas. According to www.nps.gov there were no public parks or recreational areas identified within the study area.

- 3.7 Identify any Wildlife and Waterfowl Refuges. There are no wildlife or waterfowl refuges in the study area.

- 3.8 Identify any cemeteries. There were no cemeteries identified within the study area.

- 3.9 Airports. There were no airports located in the study area.

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
OKLAHOMA COUNTY
J/P 30444(04)

- 3.10 Wetland Restoration Program (WRP) sites. According to the National Resources Conservation Service there are no WRP sites within the project area.

NRCS

Natural Resources Conservation Services

USDA

4850 N Lincoln Blvd, Suite B

OKC, OK 73105

Tuesday, January 14, 2014

Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



[Guest Book](#)
[TXD Levies](#)
[Glossary](#)
[Map/GIS Search](#)
[New Search](#)

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:23:24 AM

Account #: R142971015 **Type:** Commercial **Physical Address:** 1101 SE 15TH ST
Business Name: OIL STORAGE YARD **Map Parcel:** OKLAHOMA CITY
Owner Name1: TEAGUE BUSINESS PROPERTIES LLC **1/4 Section #:** 1607
Owner Name2: ATTN FRANCES ANN TEAGUE WAGGONER **Parent Account:**
Mailing Address: 8125 S I 35 SERVICE RD **Taxing District:** TXD 353
City, St. & Zip: OKLAHOMA CITY, OK 731492904 **School System:** Crooked Oak #53
of Bldgs: 0 **Acres:** 7.2904

| | | | | | |
|---------------------------------------|----------------------------|---------------|--------------------------------------|----------------------------------|----------|
| Personal Property | Property Value Information | | Value History | Lot Width: 0 | Depth: 0 |
| Cross reference for Personal Property | 2013 | 2014 | Sketch | Views | |
| Market Value | 80,000 | June 2014 | | | |
| Taxable Market | 80,000 | June 2014 | | | |
| Gross Assessed | 8,800 | June 2014 | | *All Photos may not be Available | |
| Exemptions | - 0 | - 0 | | | |
| Net Assessed | 8,800 | June 2014 | | | |
| Tax Rate | 117.70 | November 2014 | | | |
| Land Value | | 80,000 | | | |
| 2013 Tax Savings | See details | \$0 | Click to View taxes on this property | | |

Section: 2 Township: 11N Range: 3W QTR: SW QTR QTR:

Subdivision/Legal Description: UNPLTD PT SEC 02 11N 3W Block: 000 Lot: 000 [Subdivision Sales](#)
 PT SW4 SEC 2 11N 3W BEG 967.5FT W & 50FT N OF SE/C SW4 TH N533.7FT W 570.5FT S252.7FT W46.5FT S281FT E TO BEG

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 4/4/2000 | 110,161 | 110,161 | 0 | 12,118 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|----------|---------------------------|-----------------------------|--|
| 1/1/2002 | > | 16 | 8475-1078 | TEAGUE BILLY M ZELLA M | TEAGUE BUSINESS PROPERTIES LLC AT TN FRANCES ANN TEAGUE WAGGONER |
| 1/11/1999 | > | HIST DOC | 7495-213 | YOUNT BILLIE JO | TEAGUE BILLY M & ZELLA M |
| 9/16/1996 | > | HIST DOC | 6950-332 | YOUNT DAYMOND D & BILLIE JO | YOUNT BILLIE JO |
| 9/1/1986 | > | HIST DOC | 5525-1879 | MABREY JAMES L | YOUNT DAYMOND D & BILLIE JO |

| | | | |
|----------|---|-----|------|
| 2/1/1982 | > | DOC | 1808 |
|----------|---|-----|------|

Account Status/Adjustments/Exemptions

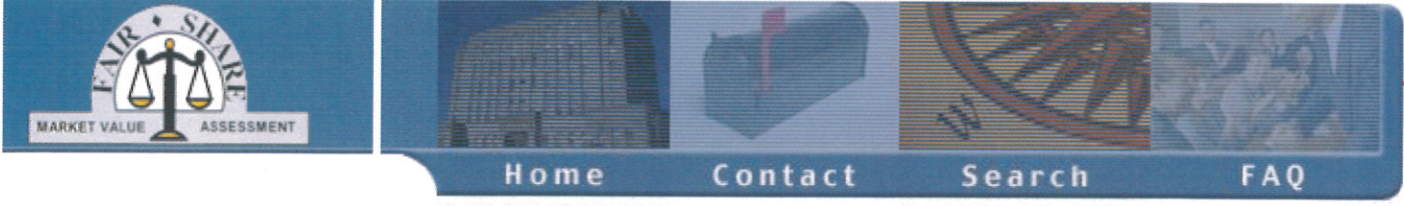
| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2001 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



[Guest Book](#)
[TXD Levies](#)
[Glossary](#)
[Map/GIS Search](#)
[New Search](#)

Oklahoma County Assessor Property Display

Screen Produced 1/14/2014 10:53:42 AM

Account #: **R142975105** Type: **Residential**

Physical Address:

Business Name:

[Map Parcel](#)

Owner Name1: TEAGUE BUSINESS PROPERTIES LLC

1/4 Section #: 1607

Owner Name2: ATTN FRANCES ANN TEAGUE WAGGONER

Parent Account:

Mailing Address: 8125 S I 35 SERVICE RD

Taxing District: [TXD 353](#)

City, St. & Zip: OKLAHOMA CITY, OK 731492904

School System: Crooked Oak #53

of Bldgs: 0

Acres: 1.8200

| | | | | | |
|---------------------------------------|----------------------------|---------------|--|--|----------|
| Personal Property | Property Value Information | | Value History | Lot Width: 0 | Depth: 0 |
| Cross reference for Personal Property | 2013 | 2014 | Sketch | Views | |
| Market Value | 13,059 | June 2014 | | 2014 2013 2012 2011 | |
| Taxable Market | 13,059 | June 2014 | | 2010 2009 2008 2007 | |
| Gross Assessed | 1,436 | June 2014 | | *All Photos may not be Available | |
| Exemptions | -0 | -0 | | | |
| Net Assessed | 1,436 | June 2014 | | | |
| Tax Rate | 117.70 | November 2014 | | | |
| Land Value | | 13,059 | | | |
| 2013 Tax Savings | See details | \$0 | Click to View taxes on this property | | |

Section: 2 Township: 11N Range: 3W QTR: SW QTR QTR:

Subdivision/Legal Description:

[UNPLTD PT SEC 02 11N 3W](#) Block: 000 Lot: 000 [Subdivision Sales](#)

PT SW4 SEC 2 11N 3W BEG 50FT N & 755.41FT W OF SE/C SW4 TH ON A CURVE TO THE LEFT 56.06FT NWLY464.18FT NWLY11.19FT W 67.71FT S468FT SE35.28FT E157.09FT TO BEG

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 4/9/2010 | 13,059 | 13,059 | 0 | 1,436 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|----------|----------------------|--------------------------|---------------------------|------------------------|---|
| 1/1/2002 | > | 16 | 8475-1078 | TEAGUE BILLY M ZELLA M | TEAGUE BUSINESS PROPERTIES LLC ATTN FRANCES ANN TEAGUE WAGGONER |
| 8/1/1984 | > | HIST DOC | 5220-1544 | | TEAGUE BILLY M |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 1999 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



[Guest Book](#)
 [TXD Levies](#)
 [Glossary](#)
 [Map/GIS Search](#)
 [New Search](#)

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 10:54:04 AM

Account #: R146463005 **Type:** Public Service

Physical Address:

Business Name:

[Map Parcel](#)

Owner Name1: MAGELLAN PIPELINE COMPANY LLC

1/4 Section #: 1607

Owner Name2:

Parent Account:

Mailing Address: ATTN PROPERTY TAX

Taxing District: TXD 353

PO BOX 22186

School System: Crooked Oak #53

City, St. & Zip: TULSA, OK 741212186

of Bldgs: 0

Acres: .1528

Lot Width: 0

Depth: 0

| Personal Property | Property Value Information | | Value History | Lot Width: 0 | Depth: 0 |
|---------------------------------------|----------------------------|---------------|---------------|---|----------|
| Cross reference for Personal Property | 2013 | 2014 | Sketch | Views | |
| Market Value | 0 | June 2014 | | 2014 2013 2012 2011 | |
| Taxable Market | 0 | June 2014 | | 2010 2009 2008 2007 | |
| Gross Assessed | 0 | June 2014 | | *All Photos may not be Available | |
| Exemptions | -0 | -0 | | | |
| Net Assessed | 0 | June 2014 | | | |
| Tax Rate | 117.70 | November 2014 | | | |
| Land Value | 0 | | | | |

2013 Tax Savings See details \$0

[Click to View taxes on this property](#)

Section: 2 Township: 11N Range: 3W QTR: SW QTR QTR:

Subdivision/Legal Description: [JAMES SUB ADDITION](#) Block: 003 Lot: 000 [Subdivision Sales](#)
 THAT PT OF LOTS 23 24 & 25 W OF I-35 PUBLIC SERVICE

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 4/6/1987 | 0 | 0 | 0 | 0 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|----------|-----------|----------------------------|-------------------------------|
| 1/27/2004 | > | 16 | 9187-798 | WILLIAMS PIPE LINE COMPANY | MAGELLAN PIPELINE COMPANY LLC |
| 7/1/1984 | > | HIST DOC | 5200-1678 | WILLIAMS PIPE LINE COMPANY | WILLIAMS PIPE LINE COMPANY |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2000 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display

Screen Produced 1/14/2014 10:59:55 AM

Account #: R142975005 Type: Public Service

Physical Address:

Business Name:

Map Parcel

Owner Name1: MAGELLAN PIPELINE COMPANY LLC

1/4 Section #: 1607

Owner Name2:

Parent Account:

Mailing Address: ATTN PROPERTY TAX
PO BOX 22186

Taxing District: TXD 353

City, St. & Zip: TULSA, OK 741212186

School System: Crooked Oak #53

of Bldgs: 0

Acres: 1.3800

| | | | | | |
|---------------------------------------|----------------------------|---------------|--------------------------------------|--|----------|
| Personal Property | Property Value Information | | Value History | Lot Width: 0 | Depth: 0 |
| Cross reference for Personal Property | 2013 | 2014 | Sketch | Views | |
| Market Value | 0 | June 2014 | | 2014 2013 2012 2011 2010 2009 2008 2007 *All Photos may not be Available | |
| Taxable Market | 0 | June 2014 | | | |
| Gross Assessed | 0 | June 2014 | | | |
| Exemptions | -0 | -0 | | | |
| Net Assessed | 0 | June 2014 | | | |
| Tax Rate | 117.70 | November 2014 | If Available | | |
| Land Value | | 0 | | | |
| 2013 Tax Savings | See details | \$0 | Click to View taxes on this property | | |

Section: 2 Township: 11N Range: 3W QTR: SW QTR QTR:

Subdivision/Legal Description:

[UNPLTD PT SEC 02 11N 3W](#) Block: 000 Lot: 000 [Subdivision Sales](#)
 PT SW4 SEC 2 11N 3W BEG 967.5FT W & 583FT N OF SE/C SW4 TH N781.3FT TO S R/W OF SL&SF RR TH NWLY ALONG SD R/W TO COMPTON PARK ADDN TH S863.3FT E570FT TO BEG EX 1.38ACRS FOR HWY PUBLIC SERVICE

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information.

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 4/6/1987 | 0 | 0 | 0 | 0 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|--------------------------|---------------------------|----------------------------|-------------------------------|
| 1/27/2004 | > | 16 | 9187-798 | WILLIAMS PIPE LINE COMPANY | MAGELLAN PIPELINE COMPANY LLC |
| 7/1/1984 | > | HIST DOC | 5200-1678 | | WILLIAMS PIPE LINE COMPANY |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2000 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 10:54:24 AM

Account #: R142971055 **Type:** Industrial **Physical Address:**
Business Name: INDUSTRIAL VACANT LAND **Map Parcel**
Owner Name1: WAGGONER FAMILY PROPERTIES INC **1/4 Section #:** 1607
Owner Name2: 5220 MIDLAND RD **Parent Account:**
Mailing Address: PO BOX 1516 **Taxing District:** TXD 253
City, St. & Zip: BILLINGS, MT 591031516 **School System:** Crooked Oak #53
of Bldgs: 0 **Acres:** 16.4125

| | | | | | |
|---------------------------------------|----------------------------|---------------|---------------|--|----------|
| Personal Property | Property Value Information | | Value History | Lot Width: 0 | Depth: 0 |
| Cross reference for Personal Property | 2013 | 2014 | Sketch | Views | |
| Market Value | 169,850 | June 2014 | | 2014 2013 2012 2011 2010 2009 2008 2007 *All Photos may not be Available | |
| <u>Taxable Market</u> | 169,850 | June 2014 | | | |
| Gross Assessed | 18,683 | June 2014 | | | |
| Exemptions | - 0 | - 0 | | | |
| Net Assessed | 18,683 | June 2014 | | | |
| Tax Rate | 117.92 | November 2014 | If Available | | |
| Land Value | 169,850 | | | | |

2013 Tax Savings See details \$0 [Click to View taxes on this property](#)

Section: 2 Township: 11N Range: 3W QTR: SW QTR QTR:

Subdivision/Legal Description: [UNPLTD PT SEC 02 11N 3W](#) Block: 000 Lot: 000 [Subdivision Sales](#)
 PT SW4 SEC 2 11N 3W BEG 50FT N OF SE/C SW4 TH N1271.05FT TOS R/W OF RR TH WLY ON A CURVE 503.66FT NWLY260.12FT SLY33.47FT WLY25FT SELY398.44FT SELY319.15FT SELY408.46FT E339.42FTS187FT E100FT TO BEG CONT 714929.22 SQ FT OR 16.4125ACRS MORE OR LESS SUBJ TO ESMTS OF RECORD

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 4/19/1996 | 0 | 0 | 0 | 0 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|------------|------|--------------------------|---------------------------|--------------------------|--------------------------------|
| 10/11/1995 | > | HIST DOC | 6803-408 | HORSESHOE PROPERTIES INC | WAGGONER FAMILY PROPERTIES INC |
| 11/16/1993 | > | HIST DOC | 6517-137 | HORSESHOE PROPERTIES INC | HORSESHOE PROPERTIES INC |
| 3/25/1992 | > | HIST DOC | 6271-1965 | GUARANTY BANK & TRUST CO | HORSESHOE PROPERTIES INC |
| 12/13/1990 | > | HIST DOC | 6122-765 | OWEN ROBERT C | GUARANTY BANK & TRUST CO |
| 4/1/1983 | > | HIST DOC | 4990-545 | | OWEN ROBERT C |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 1999 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display

Screen Produced 1/14/2014 10:54:38 AM

Account #: R142976010 Type: Exempt

Physical Address:

Business Name:

Map Parcel

Owner Name1: STATE OF OKLAHOMA
Owner Name2: DEPT OF TRANSPORTATION

1/4 Section #: 1607

Parent Account:

Mailing Address: 0

Taxing District: TXD 253

School System: Crooked Oak #53

City, St. & Zip: ,

of Bldgs: 0 Acres: .5897

Lot Width: 0 Depth: 0

Table with columns for Personal Property, Property Value Information (2013, 2014), Value History, Sketch, and Views (2014-2007).

2013 Tax Savings See details \$0 Click to View taxes on this property

Section: 2 Township: 11N Range: 3W QTR: SW QTR QTR:

Subdivision/Legal Description:

UNPLTD PT SEC 02 11N 3W Block: 000 Lot: 000
PT SW4 SEC 2 11N 3W BEG AT A POINT OF INTERSECTION OF E R/WLINE INTERSTATE HWY & S LINE ST LOUIS & SF R/W SD PT BEING 1351.26FT N & 892.60FT W OF SE/C SW 4 TH SELY 485.33FT NLY398.47FT NELY25FT NLY33.48FT TO S LINE RR R/W TH NWLY150.84FT TO BEG PLUS A TR BEG 438.92FT W & 231.88FT N OF SE/C SW 4 TH NWLY404.66FT SELY408.56FT W 11.42FT TO BEG & N17FT OF S50FT E100FT SW 4 & N17FT OF S50FT W 62.50FT E162.50FT SE4 SE4 SW 4 & EX A TR BEG 438.92FT W & 231.88FT N OF SE/C SW 4 TH SELY211.61FT NWLY 203.92FT W 11.42FT TO BEG

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

Table with columns: Notice Date, Market Value, Taxable Market, Adjustments/Exemptions, Net Assessed Value.

Sales Documents/Deed History

Table with columns: Date, View, Type, Book-Page, Price, Grantor, Grantee.

No Sales Documents returned.

Non Sales Documents/Deed History

Table with columns: Date, View, Type, Book-Page, Grantor, Grantee.

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2000 | |

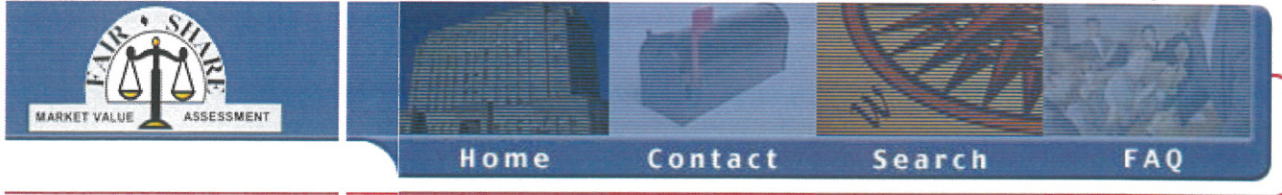
Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.

Monday, January 27, 2014

Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Oklahoma County Assessor Property Display Screen Produced 1/27/2014 10:19:16 AM

Account #: R142971045 **Type:** Commercial **Physical Address:** 1301 SE 15TH ST
Business Name: COMMERCIAL VACANT LAND OKLAHOMA CITY
Owner Name1: WAGGONER FAMILY PROPERTIES II INC **1/4 Section #:** 1607
Owner Name2: **Parent Account:**
Mailing Address: PO BOX 1516 **Taxing District:**
City, St. & Zip: BILLINGS, MT 591031516 **School System:** Crooked Oak #53
of Bldgs: 0 **Acres:** 1.1949

| Personal Property | Property Value Information | | Value History | Lot Width: 0 | Depth: 0 |
|---------------------------------------|----------------------------|---------------|---|---|----------|
| Cross reference for Personal Property | 2013 | 2014 | Sketch | Views | |
| Market Value | 202,950 | June 2014 | | <input type="button" value="2014"/> <input type="button" value="2013"/> <input type="button" value="2012"/> <input type="button" value="2011"/> | |
| Taxable Market | 202,950 | June 2014 | | <input type="button" value="2010"/> <input type="button" value="2009"/> <input type="button" value="2008"/> <input type="button" value="2007"/> | |
| Gross Assessed | 22,324 | June 2014 | | *All Photos may not be Available | |
| Exemptions | - 0 | - 0 | | | |
| Net Assessed | 22,324 | June 2014 | | | |
| Tax Rate | 117.92 | November 2014 | If Available | | |
| Land Value | | 266,000 | | | |
| 2013 Tax Savings | See details | \$0 | <input type="button" value="Click to View taxes on this property"/> | | |

Section: 2 Township: 11N Range: 3W QTR: SW QTR QTR:

Subdivision/Legal Description: UNPLTD PT SEC 02 11N 3W Block: 000 Lot: 000
 PT SW4 SEC 2 11N 3W BEG 50FT N & 100FT W OF SE/C SW4 TH W218.2FT TO HWY TH RIGHT ON CURVE NW22.01FT NW203.92FT E339.42FT S183.06FT TO BEG CONT 1.21ACRS MORE OR LESS

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 4/6/1987 | 0 | 0 | 0 | 0 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|-----------|------|------|-----------|-----------|--|-----------------------------------|
| 7/23/2013 | > | DE | 12326-686 | \$265,000 | GREENFIELD ENVIROMENTAL MULTISTATE TRUST LLC | WAGGONER FAMILY PROPERTIES II INC |

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|----------|------------|---|---|
| 7/15/2013 | > | 16 | 12326-694 | TRONOX INCORPORATED FKA KERR MCGEE REFINING | TRONOX WORLDWIDE PTY LTD |
| 6/28/2013 | > | 16 | 12326-698 | TRONOX WORLDWIDE PTY LTD | GREENFIELD ENIRONMENTAL MULTISTATE TRUST LL TRS |
| 2/14/2011 | > | 16 | 11756-1340 | TRONOX WORLDWIDE LLC | GREENFIELD ENVIROMENTAL MULTISTATE TRUST LLC |
| 2/14/2011 | > | 13 | 11756-1320 | TRIPLE S REFINING CORP FKA KERR MCGEE | TRONOX WORLDWIDE LLC |
| 11/1/1985 | > | HIST DOC | 5391-304 | | KERR-MCGEE CORPORATION |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
|----------------------------------|----------------|-----------------|

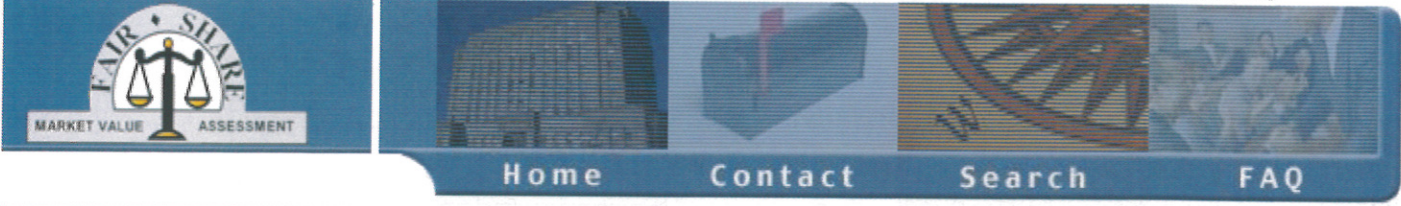
No Adjustment/Exemption records returned.

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:00:35 AM

Account #: R131721925 Type: Public Service

Physical Address: 1008 S HIGH AVE

Business Name:

Map Parcel OKLAHOMA CITY

Owner Name1: ST LOUIS & SANTA FE RR CO

1/4 Section #: 1607

Owner Name2:

Parent Account:

Mailing Address: 503 FRISCO BLDG

Taxing District: TXD 300

City, St. & Zip: ST LOUIS, MI

School System: Oklahoma City #89

of Bldgs: 0 Acres: 12.1456

| | | | | | |
|---------------------------------------|----------------------------|---------------|---------------|----------------------------------|----------|
| Personal Property | Property Value Information | | Value History | Lot Width: 0 | Depth: 0 |
| Cross reference for Personal Property | 2013 | 2014 | Sketch | Views | |
| Market Value | 0 | June 2014 | | 2014 2013 2012 2011 | |
| Taxable Market | 0 | June 2014 | | 2010 2009 2008 2007 | |
| Gross Assessed | 0 | June 2014 | | *All Photos may not be Available | |
| Exemptions | -0 | -0 | | | |
| Net Assessed | 0 | June 2014 | | | |
| Tax Rate | 115.48 | November 2014 | If Available | | |
| Land Value | | 0 | | | |

2013 Tax Savings See details \$0 Click to View taxes on this property

Section: 2 Township: 11N Range: 3W QTR: SW QTR QTR:

Subdivision/Legal Description: UNPLTD PT SEC 02 11N 3W Block: 000 Lot: 000 [Subdivision Sales](#)

PT OF SW4 SEC 2 11N 3W BEG AT NW/C OF SW4 TH E791.6FT S982.4FT NWLY 812.8FT N867.8FT TO BEG LESS BEG 154FT E OF NW/C OF SW4 E180FT S463FT W304FT N180FT E124FT N283FT TO BEG PUBLICSERVICE

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 4/6/1987 | 0 | 0 | 0 | 0 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|------------|------|----------|-----------|---------|---------------------------|
| 11/11/1911 | > | HIST DOC | 0001-0000 | | ST LOUIS & SANTA FE RR CO |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2000 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.

The Schemmer Associates, Inc.

Exhibit 3

I-35: ADD ADDITIONAL RAMPS FOR I-35 NB AND SB AT

Mar 2014

Page 15

THE I-40/I-35 JCT

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:00:09 AM

Account #: R131722000 **Type:** Exempt **Physical Address:** 659 AMERICAN INDIAN BLVD

Business Name: AMERICAN INDIAN CULTURAL CENTER AND MUSEUM (UC) **Map Parcel:** OKLAHOMA CITY

Owner Name1: NATIVE AMERICAN CULTURAL **1/4 Section #:** 1605

Owner Name2: AND EDUCATIONAL AUTHORITY **Parent Account:** R168611055

Mailing Address: PO BOX 26980 **Taxing District:** TXD 053

City, St. & Zip: OKLAHOMA CITY, OK 73126 **School System:** Crooked Oak #53

of Bldgs: 3 **Acres:** 155.4900

| | | | | | |
|---------------------------------------|----------------------------|---------------|---------------|---|--------|
| Personal Property | Property Value Information | | Value History | Lot Width: | Depth: |
| Cross reference for Personal Property | 2013 | 2014 | | Sketch | Views |
| Market Value | 0 | June 2014 | | 2014 2013 2012 2011 2010 2009 2008 2007 <small>*All Photos may not be Available</small> | |
| <u>Taxable Market</u> | 0 | June 2014 | | | |
| Gross Assessed | 0 | June 2014 | | | |
| Exemptions | -0 | -0 | | | |
| Net Assessed | 0 | June 2014 | | | |
| Tax Rate | 101.93 | November 2014 | | | |
| Land Value | | 0 | If Available | | |

2013 Tax Savings See details \$0 Click to View taxes on this property

Section: 2 Township: 11N Range: 3W QTR: NE QTR QTR:

Subdivision/Legal Description: UNPLTD PT SEC 02 11N 3W Block: 000 Lot: 000 [Subdivision Sales](#)

PT OF SEC 2 11N 3W BEG 603.47FT S & 150FT W OF NE/C NE4 SEC 2 TH S58.79FT SW4164.88FT RIGHT ON CURVE NW90.12FT RIGHT ON CURVE NE256.41FT NE352.81FT RIGHT ON CURVE NE201.49FT NE1092.13FT S25.87FT NE821.76FT NW50FT NE625.41FT NE827.72FT NE254.23FT TO BEG CONT 29.05ACRS MORE OR LESS & A TR BEG 1291.19FT S & 150FT W OF NE/C NE4 TH S805.81FT E117FT S630.00FT TO A POINT ON N R/W LINE OF RR TH SW ALONG N R/W LINE 270.21FT RIGHT ON CURVE 740.26FT SW 1619.74FT RIGHT ON A CURVE 879.39FT NELY 25FT NW 272.55FT NW 731.32FT NW 1082.15FT NE3874.33FT S110FT E150FT TO BEG CONT 155.49ACRS MORE OR LESS

[Click for sales of similar properties](#) [View the 10 newest sales](#)

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
|-------------|--------------|----------------|------------------------|--------------------|

No N.O.V. record returned.

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|------|-----------|-------------------|--|
| 5/12/2005 | > | OT | 9706-1254 | CITY OF OKLA CITY | NATIVE AMERICAN CULTURAL AND EDUCATIONAL AUTHORITY |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
|----------------------------------|----------------|-----------------|

No Adjustment/Exemption records returned.

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|-------------------------|-----------------|------------|------------|------|---------|
| Bldg #1 | Office Building | Commercial | 2009 | 1 | 1 |
| Bldg #2 | Museum | Commercial | 2009 | 1 | 1 |
| Bldg #3 | Museum | Commercial | 2009 | 1 | 1 |

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:22:53 AM

Account #: R168611055 Type: Exempt

Physical Address:

Business Name:

Map Parcel

Owner Name1: CITY OF OKLA CITY

1/4 Section #: 1605

Owner Name2:

Parent Account:

Mailing Address: 0

Taxing District: TXD 053

City, St. & Zip:

School System: Crooked Oak #53

of Bldgs: 0

Acres: 29.0500

Lot Width: 0

Depth: 0

| Personal Property | Property Value Information | | Value History |
|---------------------------------------|----------------------------|---------------|----------------------------------|
| Cross reference for Personal Property | 2013 | 2014 | Sketch |
| Market Value | 0 | June 2014 | |
| Taxable Market | 0 | June 2014 | |
| Gross Assessed | 0 | June 2014 | |
| Exemptions | -0 | -0 | |
| Net Assessed | 0 | June 2014 | |
| Tax Rate | 101.93 | November 2014 | |
| Land Value | | 0 | *All Photos may not be Available |

2013 Tax Savings See details \$0

Click to View taxes on this property

Section: 2 Township: 11N Range: 3W QTR: NE QTR QTR:

Subdivision/Legal Description:

GREELEY TOWNSHIP Block: 000 Lot: 000
PT OF NE4 SEC 2 11N 3W THAT PT OF NE4 S OF I40 PLUS GOV LOT16 IN SE4 SEC 2 11N 3W EXEMPT

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 4/6/1987 | 0 | 0 | 0 | 0 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|------------|------|----------|-----------|---------|-------------------|
| 11/11/1911 | > | HIST DOC | 0001-0000 | | CITY OF OKLA CITY |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2000 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.



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 [New Search](#)

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:02:26 AM

Account #: R131721395 **Type:** Industrial **Physical Address:** 1124 E RENO AVE
Business Name: BAKER HUGHES [Map Parcel](#) OKLAHOMA CITY
Owner Name1: PUMPCO LLC **1/4 Section #:** 1608
Owner Name2: **Parent Account:**
Mailing Address: PO BOX 892230 **Taxing District:** TXD 200
City, St. & Zip: OKLAHOMA CITY, OK 731892230 **School System:** Oklahoma City #89
of Bldgs: 2 **Acres:** 1.2154

| Personal Property | Property Value Information | | Value History | Sketch | Views |
|-----------------------|----------------------------|---------------|---------------|--------|-------|
| | 2013 | 2014 | | | |
| Market Value | 585,375 | June 2014 | | | |
| <u>Taxable Market</u> | 504,155 | June 2014 | | | |
| Gross Assessed | 55,456 | June 2014 | | | |
| Exemptions | - 0 | - 0 | | | |
| Net Assessed | 55,456 | June 2014 | | | |
| Tax Rate | 115.70 | November 2014 | | | |
| Land Value | | 44,473 | | | |

[2013 Tax Savings](#) See details \$1,034 [Click to View taxes on this property](#)

Section: 2 Township: 11N Range: 3W QTR: NW QTR QTR:

Subdivision/Legal Description: [UNPLTD PT SEC 02 11N 3W](#) Block: 000 Lot: 000 [Subdivision Sales](#)
 PT NW4 SEC 2 11N 3W BEG 882.45FT E NW/C LOT 14 BLK 1 RENO AVE ADDN TH E165.45FT S320FT W165.45FT N320FT TO BEG

[Click for sales of similar properties](#) [View the 10 newest sales](#)

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/8/2013 | 585,375 | 504,155 | 0 | 55,456 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------------|------|--------------------|--------------------------|-----------|--------------|------------|
| 10/31/2003 | > | DE | 9100-863 | \$225,000 | HARRELL MIKE | PUMPCO INC |

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|--------------------------|---------------------------|------------|--------------|
| 9/22/2006 | > | 13 | 10921-743 | PUMPCO INC | PUMPCO LLC |
| 3/22/1993 | > | HIST DOC | 6408-1872 | ALLEN E P | HARRELL MIKE |
| 1/1/1982 | > | HIST DOC | 4842-393 | | ALLEN E P |

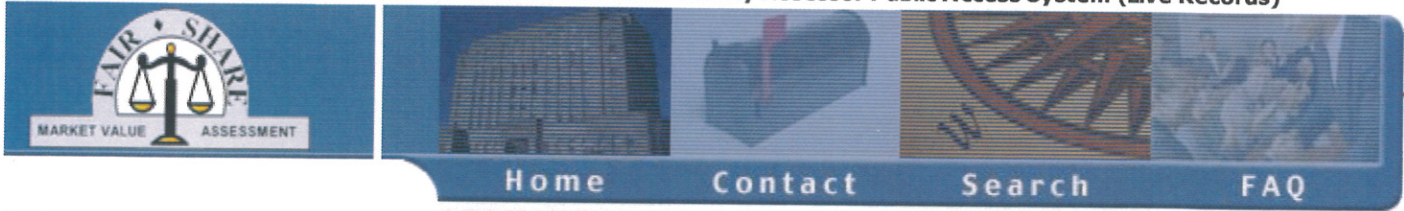
Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2005 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|-------------------------|---------------------------|------------|------------|--------|---------|
| Bldg #1 | Indust Lght Manufacturing | Industrial | 2005 | 10,500 | 1 |
| Bldg #2 | Indust Lght Manufacturing | Industrial | 2011 | 3,000 | 1 |

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



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Oklahoma County Assessor Property Display

Screen Produced 1/14/2014 11:02:38 AM

Account #: **R131721377** Type: **Industrial**

Physical Address: 1250 E RENO AVE

Business Name: TUFF SHED

Map Parcel OKLAHOMA CITY

Owner Name1: JACOB COMPANY INC

1/4 Section #: 1608

Owner Name2:

Parent Account:

Mailing Address: 727 SW 23RD ST

Taxing District: TXD 200

City, St. & Zip: OKLAHOMA CITY, OK 731091705

School System: Oklahoma City #89

of Bldgs: 1 Acres: 1.8850

Lot Width: 0 Depth: 0

| Personal Property | Property Value Information | | Value History | Sketch | Views |
|---------------------------------------|----------------------------|---------------|---------------|--------|-------|
| | 2013 | 2014 | | | |
| Cross reference for Personal Property | | | | | |
| Market Value | 487,500 | June 2014 | | | |
| <u>Taxable Market</u> | 385,597 | June 2014 | | | |
| Gross Assessed | 42,415 | June 2014 | | | |
| Exemptions | - 0 | - 0 | | | |
| Net Assessed | 42,415 | June 2014 | | | |
| Tax Rate | 115.70 | November 2014 | | | |
| Land Value | | 60,760 | | | |

2013 Tax Savings See details \$1,297

Click to View taxes on this property

Section: 2 Township: 11N Range: 3W QTR: NW QTR QTR:

Subdivision/Legal Description: [UNPLTD PT SEC 02 11N 3W](#) Block: 000 Lot: 000 [Subdivision Sales](#)

PT NW 4 SEC 2 11N 3W BEG NW/C FORMERLY LOT 14 BLK 1 DAUGHERTYS FACTORY POINT BEING 1080.7FT E & 33FT S OF NW/C NW4 TH S320FT TO SW/C VAC LOT 21 BLK 1 DAUGHERTYS FACTORY TH E350FT N170FT W180FT N150FT W170FT TO BEG EX N17FT CONT 1.88ACRS MORE OR LESS

Click for sales of similar properties View the 10 newest sales

Click for sales of similar properties View the 10 newest sales

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/8/2013 | 487,500 | 385,597 | 0 | 42,415 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|------------|------|----------|-----------|------------------------------|------------------------------|
| 9/22/2006 | > | 13 | 10921-743 | PUMPCO INC | PUMPCO LLC |
| 10/18/1995 | > | HIST DOC | 6805-1809 | MYERS WILLIAM S JR REV TRUST | JACOB COMPANY LLC |
| 6/11/1992 | > | HIST DOC | 6301-1470 | SCHAAPVELD KENNETH L | MYERS WILLIAM S JR REV TRUST |
| 6/1/1985 | > | HIST DOC | 5337-1064 | | SCHAAPVELD KENNETH L |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 1999 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|-------------------------|-------------------|------------|------------|--------|---------|
| Bldg #1 | Storage Warehouse | Industrial | 1985 | 4,250 | 1 |
| Bldg #1 | Storage Warehouse | Industrial | 1980 | 15,000 | 1 |

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:03:06 AM

Account #: R027881500 Type: Commercial Physical Address: 1300 E RENO AVE
Business Name: OK TRUCK SALES NATIONAL CONTRACTORS EXCHANGE
Owner Name1: STANDARD STEEL COMPANY
Owner Name2:
Mailing Address: PO BOX 302
City, St. & Zip: OKLAHOMA CITY, OK 731010302

Map Parcel OKLAHOMA CITY
1/4 Section #: 1608
Parent Account:
Taxing District: TXD 200
School System: Oklahoma City #89
of Bldgs: 1 Acres: 2.3640

Lot Width: 0 Depth: 0

Table with columns for Personal Property, Property Value Information, Value History, Sketch, and Views. Includes rows for Market Value, Taxable Market, Gross Assessed, Exemptions, Net Assessed, Tax Rate, and Land Value.

2013 Tax Savings See details \$763 Click to View taxes on this property

Section: 2 Township: 11N Range: 3W QTR: NW QTR QTR:

Subdivision/Legal Description: ROSEDALE SUB ADD Block: 000 Lot: 000
N380FT OF BLK 2 EX A TRI TR BEG 320FT S OF NW/C BLK 2 & EX A TR BEG 412.46FT S & 904.19FT W OF NE/C OF NW4 SEC 2 11N 3WTH W 199.99FT NW 68.55FT E258.57FT S35FT TO BEG

Click for sales of similar properties View the 10 newest sales

Last Mailed Notice of Value (N.O.V.) Information

Table with columns: Notice Date, Market Value, Taxable Market, Adjustments/Exemptions, Net Assessed Value. Row: 3/8/2013, 165,161, 105,260, 0, 11,577

Sales Documents/Deed History

Table with columns: Date, View, Type, Book-Page, Price, Grantor, Grantee

No Sales Documents returned.

Non Sales Documents/Deed History

Table with columns: Date, View, Type, Book-Page, Grantor, Grantee. Rows for 1/15/1992 and 8/1/1975.

Account Status/Adjustments/Exemptions

Table with columns: Status/Adjustment/Exemption Type, Effective Year, Assessed Amount. Row: 5% Capped Account, 1999

Click on building number to access detailed information:

Table with columns: Bldg #, Built Ac, Type, Year Built, SQFT, Stories

1/14/14

| Bldg # | Description | Type | Year Built | Qty | Status |
|---------|-----------------------------|------------|------------|-----|--------|
| Bldg #1 | Miscellaneous Imps CM *CODE | Commercial | 1980 | 1 | 1 |



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Oklahoma County Assessor Property Display

Screen Produced 1/14/2014 11:13:33 AM

Account #: R027857200 **Type:** Industrial
Business Name: STANDARD IRON
Owner Name1: STANDARD STEEL COMPANY
Owner Name2:
Mailing Address: PO BOX 302
City, St. & Zip: OKLAHOMA CITY, OK 731010302

Physical Address: 1340 E RENO AVE
 OKLAHOMA CITY
1/4 Section #: 1608
Parent Account:
Taxing District:
School System: Oklahoma City #89
of Bldgs: 1 **Acres:** 1.5496

| Personal Property | Property Value Information | | Value History |
|--------------------------------|----------------------------|---------------|---------------|
| | 2013 | 2014 | |
| Market Value | 471,500 | June 2014 | |
| Taxable Market | 195,894 | June 2014 | |
| Gross Assessed | 21,547 | June 2014 | |
| Exemptions | -0 | -0 | |
| Net Assessed | 21,547 | June 2014 | |
| Tax Rate | 115.70 | November 2014 | |
| Land Value | | 54,650 | |

Lot Width: 0 **Depth:** 0
 Sketch

Click Here to View PT

*All Photos may not be Available

See details \$3,508

Section: 2 Township: 11N Range: 3W QTR: NW QTR QTR:

Subdivision/Legal Description: [DAUGHERTYS FACTORY SUB](#) Block: 010 Lot: 000
 LOTS 1 THRU 7 EX N17FT & LOTS 12 THRU 22

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/8/2013 | 471,500 | 195,894 | 0 | 21,547 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|-------------------------------------|--------------------------|---------------------------|------------------------------|------------------------------|
| 4/20/1994 | <input type="button" value=">"/> | HIST DOC | 6591-2310 | STANDARD STEEL COMPANY | STANDARD STEEL COMPANY |
| 1/15/1992 | <input type="button" value=">"/> | HIST DOC | 6247-64 | STANDARD IRON & METAL CO INC | STANDARD STEEL COMPANY |
| 8/1/1975 | <input type="button" value=">"/> | HIST DOC | 4240-1058 | | STANDARD IRON & METAL CO INC |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 1999 | |

Click on building number to access detailed information:

| Bldg # | Build Ac | Turn | Year Built | SOFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

| Bldg # | Description | Type | Year Built | SQFT | Stories |
|---------|---------------------------|------------|------------|--------|---------|
| Bldg #1 | Indust Lght Manufacturing | Industrial | 1979 | 20,300 | 1 |

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:13:13 AM

Account #: R027883000 **Type:** Industrial **Physical Address:** 1400 E RENO AVE
Business Name: STANDARD STEEL **Map Parcel:** OKLAHOMA CITY
Owner Name1: STANDARD STEEL COMPANY **1/4 Section #:** 1608
Owner Name2: **Parent Account:**
Mailing Address: 1400 E RENO AVE **Taxing District:** TXD 200
School System: Oklahoma City #89
City, St. & Zip: OKLAHOMA CITY, OK 731171812 **# of Bldgs:** 1 **Acres:** 2.6126

| | | | | | |
|---------------------------------------|----------------------------|---------------|---------------|----------------------------------|----------|
| Personal Property | Property Value Information | | Value History | Lot Width: 0 | Depth: 0 |
| Cross reference for Personal Property | 2013 | 2014 | Sketch | Views | |
| Market Value | 1,010,500 | June 2014 | | 2014 2013 2012 2011 | |
| Taxable Market | 403,269 | June 2014 | | 2010 2009 2008 2007 | |
| Gross Assessed | 44,359 | June 2014 | | *All Photos may not be Available | |
| Exemptions | -0 | -0 | | | |
| Net Assessed | 44,359 | June 2014 | | | |
| Tax Rate | 115.70 | November 2014 | | | |
| Land Value | | 84,200 | If Available | | |

2013 Tax Savings See details \$7,728 [Click to View taxes on this property](#)

Section: 2 Township: 11N Range: 3W QTR: NW QTR QTR:

Subdivision/Legal Description: [ROSEDALE SUB ADD](#) Block: 004 Lot: 000 [Subdivision Sales](#)
 N380FT OF BLK 4 EX A TR BEG 380FT S OF NE/C BLK 4 TH W28.67FT NE31.60FT S13.59FT TO BEG

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[Click for sales of similar properties](#) [View the 10 newest sales](#)

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/8/2013 | 1,010,500 | 403,269 | 0 | 44,359 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|------------|------|--------------------------|---------------------------|------------------|------------------------|
| 4/23/1996 | > | HIST DOC | 6884-752 | SKALOVSKY ALBERT | STANDARD STEEL COMPANY |
| 11/22/1995 | > | HIST DOC | 6820-2288 | SKALOVSKY SAM | SKALOVSKY ALBERT |
| 11/11/1911 | > | HIST DOC | 0001-0000 | | SKALOVSKY SAM |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
|----------------------------------|----------------|-----------------|

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|-------------------------|---------------------------|------------|------------|--------|---------|
| Bldg #1 | Indust Lght Manufacturing | Industrial | 1979 | 20,300 | 1 |
| Bldg #1 | Indust Lght Manufacturing | Industrial | 1967 | 15,562 | 1 |
| Bldg #1 | Indust Lght Manufacturing | Industrial | 1967 | 17,980 | 1 |



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[New Search](#)

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:13:56 AM

Account #: R027884500 **Type:** Commercial
Business Name: COMMERCIAL VACANT LAND
Owner Name1: STANDARD STEEL COMPANY
Owner Name2:
Mailing Address: PO BOX 302
City, St. & Zip: OKLAHOMA CITY, OK 731010302

Physical Address:

1/4 Section #: 1608
Parent Account:
Taxing District:
School System: Oklahoma City #89
of Bldgs: 0 **Acres:** 1.0938

| | | | | | |
|---|----------------------------|---------------|--|---|--|
| <input type="button" value="Personal Property"/> Cross reference for Personal Property | Property Value Information | | <input type="button" value="Value History"/> | Lot Width: 0 Depth: 0 Sketch Views | |
| | 2013 | 2014 | | | |
| Market Value | 119,113 | June 2014 | <input type="button" value="If Available"/> | <input type="button" value="2014"/> <input type="button" value="2013"/> <input type="button" value="2012"/> <input type="button" value="2011"/> | |
| Taxable Market | 45,293 | June 2014 | | <input type="button" value="2010"/> <input type="button" value="2009"/> <input type="button" value="2008"/> <input type="button" value="2007"/> | |
| Gross Assessed | 4,982 | June 2014 | | *All Photos may not be Available | |
| Exemptions | -0 | -0 | | | |
| Net Assessed | 4,982 | June 2014 | | | |
| Tax Rate | 115.70 | November 2014 | | | |
| Land Value | | 119,113 | | | |

See details \$940

Section: 2 Township: 11N Range: 3W QTR: NW QTR QTR:

Subdivision/Legal Description: [ROSEDALE SUB ADD](#) Block: 000 Lot: 000
 N380FT OF W/2 BLK 5 EX BEG SE/C SD TR TH W135.59FT N13.59FTNE40.39FT E98.86FT S30.95FT TO BEG

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/8/2013 | 119,113 | 45,293 | 0 | 4,982 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|----------|------|--------------------|----------------------------|-----------|-------------------------|------------------------|
| 4/4/2006 | > | DE | 10068-1520 | \$140,000 | BRITTIAN ROBERT MICHAEL | STANDARD STEEL COMPANY |

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|------------|------|--------------------------|---------------------------|-----------------------|-------------------------|
| 1/15/1993 | > | HIST DOC | 6386-199 | BRITTIAN LLOYD EUGENE | BRITTIAN ROBERT MICHAEL |
| 12/18/1992 | > | HIST DOC | 6375-1692 | BRITTIAN LLOYD EUGENE | BRITTIAN LLOYD EUGENE |
| 7/1/1984 | > | HIST DOC | 5202-292 | | BRITTIAN LLOYD EUGENE |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2008 | |

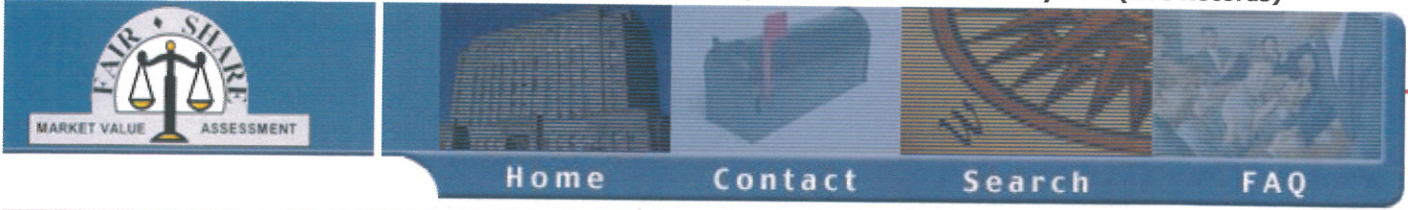
Click on building number to access detailed information:

| Rld# | Ruilt Ac | Tvne | Year Built | SOFT | Stories |
|------|----------|------|------------|------|---------|
|------|----------|------|------------|------|---------|

| PARCELS | TYPE | ACREAGE | EXEMPT | STATUS |
|---------|------|---------|--------|--------|
|---------|------|---------|--------|--------|

No Building Detail record returned.

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



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[New Search](#)

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:14:08 AM

Account #: R027886000 **Type:** Industrial **Physical Address:** 1418 E RENO AVE
Business Name: ADS **Map Parcel:** OKLAHOMA CITY
Owner Name1: STANDARD IRON & METAL COMPANY INC **1/4 Section #:** 1608
Owner Name2: **Parent Account:**
Mailing Address: PO BOX 302 **Taxing District:** TXD 200
City, St. & Zip: OKLAHOMA CITY, OK 731010302 **School System:** Oklahoma City #89
of Bldgs: 1 **Acres:** 3.9680

| | | | | | | |
|--|----------------------------|---------------|-------------------------------|--|-------------------------|--|
| Personal Property Cross reference for Personal Property | Property Value Information | | Value History | Lot Width: 0 Depth: 0 | Sketch | Views |
| | 2013 | 2014 | | | | |
| Market Value | 191,424 | June 2014 | | | If Available | 2014 2013 2012 2011 |
| Taxable Market | 114,584 | June 2014 | | 2010 2009 2008 2007 | | |
| Gross Assessed | 12,604 | June 2014 | | | | |
| Exemptions | -0 | -0 | | | | |
| Net Assessed | 12,604 | June 2014 | | | | |
| Tax Rate | 115.70 | November 2014 | | | | |
| Land Value | | 38,000 | | | | |
| 2013 Tax Savings | See details | \$978 | | | | Click to View taxes on this property |

Section: 2 Township: 11N Range: 3W QTR: NW QTR QTR:

Subdivision/Legal Description: [ROSEDALE SUB ADD](#) Block: 000 Lot: 000 [Subdivision Sales](#)
 N380FT OF E/2 BLK 5 & ALL BLK 9 & PT BLK 10 LYING W OF A LINE DESCRIBED AS BEG 100FT E OF NW/C BLK 10 TH IN A SLY DIRECTION TO A POINT ON S LINE BLK 10 WHICH IS 164FT E OF SW/C BLK 10 PLUS PT OF VACATED STREET BEG NE/C BLK 5 TH S380FT E57FT N380FT W57FT TO BEG EX A TR BEG 412.46FT S OF NE/C NW4 SEC2 11N 3W TH W168.59FT N30.95FT E195.59FT S30.95FT W27FT TO BEG

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[View the 10 newest sales](#)

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/8/2013 | 191,424 | 114,584 | 0 | 12,604 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|------|------|------|-----------|-------|---------|---------|
|------|------|------|-----------|-------|---------|---------|

No Sales Documents returned.

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|--------------------------|---------------------------|------------------------|------------------------------|
| 4/20/1994 | > | HIST DOC | 6591-2310 | STANDARD STEEL COMPANY | STANDARD STEEL COMPANY |
| 2/17/1977 | > | HIST DOC | 4347-366 | | STANDARD IRON & METAL CO INC |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
|----------------------------------|----------------|-----------------|

5% Capped Account

1999

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|-------------------------|-------------------|------------|------------|-------|---------|
| Bldg #1 | Storage Warehouse | Industrial | 1930 | 8,000 | 1 |

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:19:17 AM

Account #: R142970295 Type: Commercial Physical Address: 1600 E RENO AVE
Business Name: LEWIS RV SALES Map Parcel OKLAHOMA CITY
Owner Name1: LEWIS CHARLES W & ELIZABETH A TRS 1/4 Section #: 1605
Owner Name2: LEWIS CHARLES W & ELIZABETH A LIVING TR Parent Account:
Mailing Address: 1600 E RENO AVE Taxing District: TXD 253
City, St. & Zip: OKLAHOMA CITY, OK 731173404 School System: Crooked Oak #53
of Bldgs: 2 Acres: 7.6292

Table with columns for Personal Property, Property Value Information (2013, 2014), Value History, Sketch, and Views. Includes market value, taxable market, gross assessed, exemptions, net assessed, tax rate, and land value.

Section: 2 Township: 11N Range: 3W QTR: NE QTR QTR:
Subdivision/Legal Description: UNPLTD PT SEC 02 11N 3W Block: 000 Lot: 000
PT NE4 SEC 2 11N 3W BEG AT A POINT 1349.90FT W OF NE/C OF NE4 TH W925.6FT S33FT SELY 411.3FT E565.50FT NELY 180.90FT N385.61FT TO BEG SUBJ TO ESMTS & R/WS OF RECORD

Last Mailed Notice of Value (N.O.V.) Information

Table with columns: Notice Date, Market Value, Taxable Market, Adjustments/Exemptions, Net Assessed Value. Row: 3/17/2010, 739,568, 739,568, 0, 81,352

Sales Documents/Deed History
Table with columns: Date, View, Type, Book-Page, Price, Grantor, Grantee

No Sales Documents returned.

Table with columns: Date, View, Type, Book-Page, Grantor, Grantee. Rows include sales from 1999, 1995, and 1977.

Table with columns: Status/Adjustment/Exemption Type, Effective Year, Assessed Amount. Row: 5% Capped Account, 1999

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|-------------------------|-------------------|------------|------------|-------|---------|
| Bldg #1 | Automotive Center | Commercial | 1972 | 5,040 | 1 |
| Bldg #2 | Service Garage | Commercial | 2001 | 8,550 | 1 |

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:19:33 AM

Account #: R142970286 **Type:** Commercial **Physical Address:** 1700 E RENO

Business Name: COMMERCIAL VACANT LAND/ PARKING FOR ECONO LODGE **Map Parcel:** OKLAHOMA CITY

Owner Name1: BRIGHTLIGHT HOSPITALITY INC **1/4 Section #:** 1605

Owner Name2: **Parent Account:** R142970280

Mailing Address: 1750 E RENO AVE **Taxing District:** TXD 253

City, St. & Zip: OKLAHOMA CITY, OK 731173400 **School System:** Crooked Oak #53

of Bldgs: 0 **Acres:** 1.4949

| Personal Property | Property Value Information | | Value History | Lot Width: | Depth: |
|---------------------------------------|----------------------------|---------------|--|---|--------|
| Cross reference for Personal Property | 2013 | 2014 | | Sketch | Views |
| Market Value | 107,450 | June 2014 | | 2014 2013 2012 2011 2010 2009 2008 2007 <i>*All Photos may not be Available</i> | |
| Taxable Market | 107,450 | June 2014 | | | |
| Gross Assessed | 11,819 | June 2014 | | | |
| Exemptions | - 0 | - 0 | | | |
| Net Assessed | 11,819 | June 2014 | | | |
| Tax Rate | 117.92 | November 2014 | | | |
| Land Value | | 107,450 | If Available | | |
| 2013 Tax Savings | See details | \$0 | Click to View taxes on this property | | |

Section: 2 Township: 11N Range: 3W QTR: NE QTR QTR:

Subdivision/Legal Description: UNPLTD PT SEC 02 11N 3W Block: 000 Lot: 000 [Subdivision Sales](#)

PT NE4 SEC 2 11N 3W BEG 1141.85FT W OF NE/C NE4 TH W 208.04FT S 375.92FT E 88.80FT NE 119.8FT N 366.06FT TO BEG CONT 1.79ACRS MORE OR LESS

No Comparative Sales records returned.

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/17/2010 | 107,450 | 107,450 | 0 | 11,819 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|----------|------|------|------------|-------------|------------------------|-----------------------------|
| 6/1/2009 | > | DEMP | 11107-1531 | \$2,750,000 | DHA OKLAHOMA HOTEL LLC | BRIGHTLIGHT HOSPITALITY INC |

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|------|-----------|------------------------|------------------------|
| 9/13/2004 | > | 16 | 9458-1114 | DHA OKLAHOMA HOTEL LLC | DHA OKLAHOMA HOTEL LLC |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2011 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|--------|----------|------|------------|------|---------|
|--------|----------|------|------------|------|---------|

No Building Detail record returned.

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



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[Glossary](#)
[Map/GIS Search](#)
[New Search](#)

Oklahoma County Assessor Property Display

Screen Produced 1/14/2014 11:19:44 AM

Account #: R142970283 **Type:** Commercial
Business Name: ECONO LODGE
Owner Name1: BRIGHTLIGHT HOSPITALITY INC
Owner Name2:
Mailing Address: 1750 E RENO AVE
City, St. & Zip: OKLAHOMA CITY, OK 731173400

Physical Address: 1750 E RENO
[Map Parcel](#) OKLAHOMA CITY
1/4 Section #: 1605
Parent Account: R142970280
Taxing District: [TXD 253](#)
School System: Crooked Oak #53
of Bldgs: 1 **Acres:** 1.9135

| | | | | | | | | | | | |
|--|--|---------------|-------------------------------|------|--|----------------------------------|------|------|------|------|------|
| Personal Property Cross reference for Personal Property | Property Value Information | | Value History | | Lot Width: 0 Depth: 0 Sketch | Views | | | | | |
| | 2013 | 2014 | 2013 | 2014 | | 2014 | 2013 | 2012 | 2011 | 2010 | 2009 |
| Market Value | 1,650,000 | June 2014 | | | If Available | *All Photos may not be Available | | | | | |
| Taxable Market | 1,650,000 | June 2014 | | | | | | | | | |
| Gross Assessed | 181,499 | June 2014 | | | | | | | | | |
| Exemptions | - 0 | - 0 | | | | | | | | | |
| Net Assessed | 181,499 | June 2014 | | | | | | | | | |
| Tax Rate | 117.92 | November 2014 | | | | | | | | | |
| Land Value | | 137,528 | | | | | | | | | |

[2013 Tax Savings](#) See details \$0

[Click to View taxes on this property](#)

Section: 2 Township: 11N Range: 3W QTR: NE QTR QTR:

Subdivision/Legal Description:
[UNPLTD PT SEC 02 11N 3W](#) Block: 000 Lot: 000 [Subdivision Sales](#)
 PT NE4 SEC 2 11N 3W BEG 853.85FT W OF NE/C NE4 TH W 288FT S366.06FT NE208.94FT NE82.32FT N333.58FT TO BEG CONT 2.33ACRS MORE OR LESS

[Click for sales of similar properties](#) [View the 10 newest sales](#)

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/17/2010 | 2,337,528 | 2,337,528 | 0 | 257,128 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|----------|------|------|------------|-------------|------------------------|-----------------------------|
| 6/1/2009 | > | DEMP | 11107-1531 | \$2,750,000 | DHA OKLAHOMA HOTEL LLC | BRIGHTLIGHT HOSPITALITY INC |

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|------|-----------|------------------------|------------------------|
| 9/13/2004 | > | 16 | 9458-1116 | DHA OKLAHOMA HOTEL LLC | DHA OKLAHOMA HOTEL LLC |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2011 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|-------------------------|----------|------------|------------|--------|---------|
| Bldg #1 | Motel | Commercial | 1970 | 38,582 | 2 |

21

Tuesday, January 14, 2014 Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



Guest Book TXD Levies Glossary Map/GIS Search New Search

Oklahoma County Assessor Property Display Screen Produced 1/14/2014 11:22:21 AM

Account #: R142970280 **Type:** Commercial
Business Name: QUALITY INN
Owner Name1: NOOR HOTEL LLC
Owner Name2: KCP HOTEL LLC
Mailing Address: GNS 2 LLC
 1800 E RENO
City, St. & Zip: OKLAHOMA CITY, OK 73117

Physical Address: 1800 E RENO AVE
 Map Parcel OKLAHOMA CITY
1/4 Section #: 1605
Parent Account:
Taxing District: TXD 253
School System: Crooked Oak #53
of Bldgs: 1 **Acres:** 2.9527

| Personal Property | Property Value Information | | Value History |
|-------------------|----------------------------|---------------|---------------|
| | 2013 | 2014 | |
| Market Value | 1,200,000 | June 2014 | |
| Taxable Market | 1,200,000 | June 2014 | |
| Gross Assessed | 131,999 | June 2014 | |
| Exemptions | - 0 | - 0 | |
| Net Assessed | 131,999 | June 2014 | |
| Tax Rate | 117.92 | November 2014 | |
| Land Value | | 212,221 | |

Lot Width: 0 Depth: 0

Sketch Views

2014 2013 2012 2011

2010 2009 2008 2007

*All Photos may not be Available

2013 Tax Savings See details \$0

Click to View taxes on this property

Section: 2 Township: 11N Range: 3W QTR: NE QTR QTR:

Subdivision/Legal Description: UNPLTD PT SEC 02 11N 3W Block: 000 Lot: 000 [Subdivision Sales](#)
 PT NE4 SEC 2 11N 3W BEG 300FT W OF NE/C NE4 TH W 553.85FT S333.58FT NE117.25FT NE448.64FT N212.66FT TO BEG CONT 3.51ACRS MORE OR LESS

Click for sales of similar properties View the 10 newest sales

Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/18/2009 | 2,784,755 | 2,784,755 | 0 | 306,322 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|-----------|------|------|---------------------------|-------------|-------------------------------|--|
| 10/2/2008 | > | DE | 10922-401 | \$2,925,000 | SHREE LAKSHMI CORP | NOOR HOTEL LLC KCP HOTEL LLC GNS 2 LLC |
| 4/9/2008 | > | DE | 10780-16 | \$2,700,000 | MMAS HOTELS LLC | SHREE LAKSHMI CORP |
| 3/6/2007 | > | DE | 10409-529 | \$2,550,000 | DHA OKLAHOMA HOTEL LLC | PATEL MANILAL V & MADHUBEN M |
| 4/30/2003 | > | DE | 8900-468 | \$1,950,000 | TRADE WINDS CENTRAL MOTOR INC | DHA OKLAHOMA HOTEL LLC |

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|-----------|------|----------|---------------------------|-------------------------------|-------------------------------|
| 3/6/2007 | > | OT | 10409-531 | PATEL MANILAL V & MADHUBEN M | MMAS HOTELS LLC |
| 9/13/2004 | > | 16 | 9458-1118 | DHA OKLAHOMA HOTEL LLC | DHA OKLAHOMA HOTEL LLC |
| 6/1/1969 | > | HIST DOC | 3875-139 | TRADE WINDS CENTRAL MOTOR INC | TRADE WINDS CENTRAL MOTOR INC |

11/11/1911



[HIST DOC](#)

[0001-0000](#)

TRADE WINDS CENTRAL MOTOR INC

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2010 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|-------------------------|----------|------------|------------|--------|---------|
| Bldg #1 | Motel | Commercial | 1970 | 46,194 | 2 |

Tuesday, January 14, 2014

Leonard Sullivan-Oklahoma County Assessor Public Access System (Live Records)



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[TXD Levies](#)
[Glossary](#)
[Map/GIS Search](#)
[New Search](#)

Oklahoma County Assessor Property Display

Screen Produced 1/14/2014 11:22:34 AM

Account #: **R142970285** Type: **Commercial**

Physical Address: 1948 E RENO AVE

Business Name: **GREYHOUND**

Map Parcel OKLAHOMA CITY

Owner Name1: MLK/1948 RENO LLC 1

1/4 Section #: 1605

Owner Name2: MLK/1948 RENO LLC 2

Parent Account:

Mailing Address: 3200 CANYON RD

Taxing District: **TXD 253**

City, St. & Zip: OKLAHOMA CITY, OK 731205617

School System: Crooked Oak #53

of Bldgs: 1 Acres: .9626

| Personal Property | Property Value Information | | Sketch | Views | |
|---------------------------------------|----------------------------|---------------|--------|----------------------------------|--|
| | 2013 | 2014 | | | |
| Cross reference for Personal Property | | | | | |
| Market Value | 150,002 | June 2014 | | | |
| Taxable Market | 150,002 | June 2014 | | *All Photos may not be Available | |
| Gross Assessed | 16,500 | June 2014 | | | |
| Exemptions | - 0 | - 0 | | | |
| Net Assessed | 16,500 | June 2014 | | | |
| Tax Rate | 117.92 | November 2014 | | | |

Lot Width: 0 Depth: 0

2013 Tax Savings See details \$0

Click to View taxes on this property

Section: 2 Township: 11N Range: 3W QTR: NE QTR QTR:

Subdivision/Legal Description:

[UNPLTD PT SEC 02 11N 3W](#) Block: 000 Lot: 000 [Subdivision Sales](#)

PT NE4 SEC 2 11N 3W BEG 40FT W OF NE/C NE4 TH W260FT S212.53FT NELY194.70FT NELY92.50FT N109.50FT TO BEG EX A TR BEG 50FT W & 50FT S OF NE/C NE4 TH S20FT NWLY28.42FT E20FT TO BEG & EX A TR BEG 69.54FT W & 50FT S OF NE/C NE4 TH SE28.42FT S47.42FT SW25.85FT N58.98FT NW35.52FT E25FT TO BEG SUBJ TO ESMTS OF RECORD

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Last Mailed Notice of Value (N.O.V.) Information

| Notice Date | Market Value | Taxable Market | Adjustments/Exemptions | Net Assessed Value |
|-------------|--------------|----------------|------------------------|--------------------|
| 3/17/2008 | 259,513 | 259,513 | 0 | 28,546 |

Sales Documents/Deed History

| Date | View | Type | Book-Page | Price | Grantor | Grantee |
|-----------|------|------|---------------------------|-----------|---|---|
| 1/12/2011 | > | DE | 11551-960 | \$140,000 | RCB BANK | MLK/1948 RENO LLC 1 MLK/1948 RENO LLC 2 |
| 11/5/2001 | > | DE | 8260-380 | \$217,000 | UNDERWOOD GWENDOLYN ALICE UNDERWOOD GARY DALE | 1948 RENO LLC C/O BRUCE BROWN |

Non Sales Documents/Deed History

| Date | View | Type | Book-Page | Grantor | Grantee |
|------------|------|------|---------------------------|---|--------------------|
| 7/9/2010 | > | OT | 11528-934 | HOME NATIONAL BANK FEDERAL DEPOSIT INSURANCE CORP | RCB BANK |
| 11/13/2008 | > | OT | 10961-870 | 1948 RENO LLC C/O MONTE STRIKER | HOME NATIONAL BANK |

| | | | | | |
|-----------|---|--------------------------|---------------------------|----------------|------------------------------|
| 4/11/1994 | > | HIST DOC | 6587-1491 | UNDERWOOD GARY | UNDERWOOD GWENDOLYN ALICE |
| 3/1/1985 | > | HIST DOC | 5291-426 | | UNDERWOOD GARY |

Account Status/Adjustments/Exemptions

| Status/Adjustment/Exemption Type | Effective Year | Assessed Amount |
|----------------------------------|----------------|-----------------|
| 5% Capped Account | 2013 | |

Click on building number to access detailed information:

| Bldg # | Built As | Type | Year Built | SQFT | Stories |
|-------------------------|------------|------------|------------|-------|---------|
| Bldg #1 | Restaurant | Commercial | 1970 | 6,516 | 1 |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

SECTION 04 - UTILITY INFORMATION

Guy Engineering Services, Inc. conducted the utility data collection for this project. The following utilities were found in the study area:

| COMMUNICATIONS | |
|---|---|
| Type of Utility | Communication |
| Name and Address of Utility Owner | Lightcore / CenturyLink P.O. Box 750 Hennessey, OK 73742 |
| Name and Phone number of Contact Person | Mike Smith 405-853-7164 – Office 405-340-9161 – Cell |
| Utility Product | Communications |
| Utility Attributes (if applicable) | 96 count – Fiber Optic |
| General Location of Utility | This line crosses I-35 at approximate southbound station 215+00 then heads in an east / west direction along the railroad right of way beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| COMMUNICATIONS | |
|---|--|
| Type of Utility | Communication |
| Name and Address of Utility Owner | ODOT Fiber 200 NE 21 st Street, Rm. 3-C1 Oklahoma City, OK 73105 |
| Name and Phone number of Contact Person | Robbie Moore 405-522-4596 Office 405-833-6923 Cell |
| Utility Product | Communications |
| Utility Attributes (if applicable) | 144 count – Fiber Optic |
| General Location of Utility | This line runs north along the west side of the southbound lane of I-35 from approximate station 200+00 to 230+75 (150' Lt.). At this station the line turns east to cross the southbound lane and then turns north again. At approximate station 243+50 this line heads west and crosses the interstate then heads north beyond the project limits. This line splits to the west and crosses the southbound lane of I-35 at approximate station 241+00. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| COMMUNICATIONS | |
|---|---|
| Type of Utility | Communication |
| Name and Address of Utility Owner | ODOT Fiber 200 NE 21 st Street, Rm. 3-C1 Oklahoma City, OK 73105 |
| Name and Phone number of Contact Person | Robbie Moore 405-522-4596 Office 405-833-6923 Cell |
| Utility Product | Communications |
| Utility Attributes (if applicable) | 144 count – Fiber Optic |
| General Location of Utility | This line runs along the south side of the westbound lane of I-40 crossing the southbound entrance ramp at approximate station 251+50. This line continues to approximate station 261+75 (100' Rt.) where this line crosses the eastbound lane and heads east to beyond the project limits. At approximate station 256+00 this line splits off to feed the ODOT dynamic message sign located at approximate station 266+00. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| COMMUNICATIONS | |
|---|--|
| Type of Utility | Communication |
| Name and Address of Utility Owner | ODOT Fiber 200 NE 21 st Street, Rm. 3-C1 Oklahoma City, OK 73105 |
| Name and Phone number of Contact Person | Robbie Moore 405-522-4596 Office 405-833-6923 Cell |
| Utility Product | Communications |
| Utility Attributes (if applicable) | 144 count – Fiber Optic |
| General Location of Utility | This line crosses I-35 at approximate station 295+00 (westbound) then heads north along the west side of N. Eastern Ave. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| ELECTRIC | |
|---|--|
| Type of Utility | Electric |
| Name and Address of Utility Owner | ODOT Fiber 200 NE 21 st Street, Rm. 3-C1 Oklahoma City, OK 73105 |
| Name and Phone number of Contact Person | Robbie Moore 405-522-4596 Office 405-833-6923 Cell |
| Utility Product | Electric – Service to sign |
| Utility Attributes (if applicable) | Unknown Size– Underground Electric |
| General Location of Utility | This line crosses the west bound lane of I-40 then turns to the east and parallels the interstate on the north side approximately 60' north of the centerline. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| COMMUNICATIONS | |
|---|--|
| Type of Utility | Communication |
| Name and Address of Utility Owner | Chickasaw Telephone / Indian Nations 124 West Vinita Ave. Sulphur, OK 73086 |
| Name and Phone number of Contact Person | Cindy Wilson 580-622-2111 Office 580-622-3837 Cell |
| Utility Product | Communications |
| Utility Attributes (if applicable) | 96 count – Fiber Optic |
| General Location of Utility | This line crosses the south bound lane of I-35 at approximate station 215+00 then heads in an east / west direction along the railroad right of way beyond the project limits. (It is located in the same trench as the Lightcore line.) |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| ELECTRIC | |
|---|---|
| Type of Utility | Electric |
| Name and Address of Utility Owner | OG&E P.O. Box 321, MC109 Oklahoma City, OK 73101 |
| Name and Phone number of Contact Person | Rae Ann Lawrence 405-553-5790 Office 405-553-5785 Cell |
| Utility Product | Electric |
| Utility Attributes (if applicable) | 3 wire trans. / 4 wire dist. – Overhead Electric |
| General Location of Utility | This line runs north along the southbound lane of I-35 from approximate station 200+75 to station 204+50 (125' Lt.). At this point the line turns east and continues beyond the project limits crossing the interstate at approximate station 204+00. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| ELECTRIC | |
|---|--|
| Type of Utility | Electric |
| Name and Address of Utility Owner | OG&E P.O. Box 321, MC109 Oklahoma City, OK 73101 |
| Name and Phone number of Contact Person | Rae Ann Lawrence 405-553-5790 Office 405-553-5785 Cell |
| Utility Product | Electric |
| Utility Attributes (if applicable) | 4 wire 3 phase – Overhead Electric |
| General Location of Utility | This line continues north from the crossing located at 204+50 to approximate station 208+50 where the line turns west and continues beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| ELECTRIC | |
|---|--|
| Type of Utility | Electric |
| Name and Address of Utility Owner | OG&E P.O. Box 321, MC109 Oklahoma City, OK 73101 |
| Name and Phone number of Contact Person | Rae Ann Lawrence 405-553-5790 Office 405-553-5785 Cell |
| Utility Product | Electric |
| Utility Attributes (if applicable) | 4 wire 3 phase – Overhead Electric |
| General Location of Utility | This line runs north along the north bound lane of I-35 from station 115+50 to 124+00 located approximately 150' east of the centerline then heads East beyond the limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| ELECTRIC | |
|---|---|
| Type of Utility | Electric |
| Name and Address of Utility Owner | OG&E P.O. Box 321, MC109 Oklahoma City, OK 73101 |
| Name and Phone number of Contact Person | Rae Ann Lawrence 405-553-5790 Office 405-553-5785 Cell |
| Utility Product | Electric |
| Utility Attributes (if applicable) | 6 wire transmission – Overhead Electric |
| General Location of Utility | This line crosses I-35 at approximate Northbound station 138+75 then leaves the project limits in an east / west direction. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| ELECTRIC | |
|---|---|
| Type of Utility | Electric |
| Name and Address of Utility Owner | OG&E P.O. Box 321, MC109 Oklahoma City, OK 73101 |
| Name and Phone number of Contact Person | Rae Ann Lawrence 405-553-5790 Office 405-553-5785 Cell |
| Utility Product | Electric |
| Utility Attributes (if applicable) | 3 wire transmission – Overhead Electric |
| General Location of Utility | This line crosses the westbound lane of I-40 at approximate station 273+75 then heads in a north / south direction beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| GAS | |
|---|--|
| Type of Utility | Gas |
| Name and Address of Utility Owner | Oklahoma Natural Gas 4901 N. Santa Fe Oklahoma City, OK 73118 |
| Name and Phone number of Contact Person | Tommy Brown 405-556-6411 Office 405-323-9045 |
| Utility Product | Gas |
| Utility Attributes (if applicable) | Unknown Size – Gas Line |
| General Location of Utility | This line runs along the north side of the west bound lane of I-40 located approximately 50' north of the centerline from 251+50 to 273+50. At this station the line jogs to the north and continues east beyond the project boundary at 100' north of the centerline. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| GAS | |
|---|---|
| Type of Utility | Gas |
| Name and Address of Utility Owner | Magellan Midstream One Williams Center, OTC-9 Tulsa, OK 74172 |
| Name and Phone number of Contact Person | Kyle Detwiler 918-574-7873 Office 918-521-4174 Cell |
| Utility Product | Gas |
| Utility Attributes (if applicable) | 8" steel – Gas Line |
| General Location of Utility | This line crosses I-35 at approximate Southbound station 216+25 then heads in an east / west direction along the North side of the railroad right of way beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| GAS | |
|---|--|
| Type of Utility | Gas |
| Name and Address of Utility Owner | Magellan Midstream One Williams Center, OTC-9 Tulsa, OK 74172 |
| Name and Phone number of Contact Person | Kyle Detwiler 918-574-7873 Office 918-521-4174 Cell |
| Utility Product | Gas |
| Utility Attributes (if applicable) | 6" steel – Gas Line |
| General Location of Utility | This line runs north along the north bound lane of I-35 from approximate 116+50 to 131+25 (175' Rt.) where this line turns to the west and crosses the interstate. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| GAS | |
|---|---|
| Type of Utility | Gas |
| Name and Address of Utility Owner | Superior Pipeline 7130 S. Lewis Ave. Suite 510 Tulsa, OK 74136 |
| Name and Phone number of Contact Person | Gary Cook 918-477-5607 Office |
| Utility Product | Gas |
| Utility Attributes (if applicable) | 10" steel – Gas Line |
| General Location of Utility | This line crosses I-35 at approximate Northbound station 131+50 then heads in an east / west direction beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| GAS | |
|---|---|
| Type of Utility | Gas |
| Name and Address of Utility Owner | Phillips 66 94569 Harmon Road Medford, OK 73759 |
| Name and Phone number of Contact Person | Gary Bowen 580-395-2363 X281 Office 580-763-2572 Cell |
| Utility Product | Jet Fuel |
| Utility Attributes (if applicable) | 4" steel – Fuel Line |
| General Location of Utility | This line runs north along the north bound lane of I-35 from approximate 116+50 to 131+25 (185' Rt.) where this line turns to the west and crosses the interstate, extending beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| PETROLEUM | |
|---|--|
| Type of Utility | PETROLEUM |
| Name and Address of Utility Owner | Plains Pipeline 740306 S. 3510 Rd. Cushing, OK 74023 |
| Name and Phone number of Contact Person | Daniel Y. So 918-223-0205 Office 405-200-9806 Cell |
| Utility Product | PETROLEUM |
| Utility Attributes (if applicable) | 6" steel – Petroleum Line |
| General Location of Utility | This line crosses I-35 at approximate Northbound station 131+50 and extends in an east / west direction beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| GAS | |
|---|--|
| Type of Utility | Gas |
| Name and Address of Utility Owner | DCP Midstream 2445 NW 164 th Edmond, OK 73013 |
| Name and Phone number of Contact Person | Jeff Childers 405-605-3800 Office |
| Utility Product | Gas |
| Utility Attributes (if applicable) | 10" steel – Gas Line |
| General Location of Utility | This line crosses I-35 at approximate Southbound station 220+25 and extends in an east / west direction beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| GAS | |
|---|--|
| Type of Utility | Gas |
| Name and Address of Utility Owner | DCP Midstream 2445 NW 164 th Edmond, OK 73013 |
| Name and Phone number of Contact Person | Jeff Childers 405-605-3800 Office |
| Utility Product | Gas |
| Utility Attributes (if applicable) | 12" steel – Gas Line |
| General Location of Utility | This line runs north along the east side of I-35 and East along the south side of I-40 approximately 195' east / south of the interstate. At approximate Eastbound station 161+50 this line turns north and continues beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| WATER | |
|---|--|
| Type of Utility | Water |
| Name and Address of Utility Owner | The City of Oklahoma City 420 W. Main, Suite 500 Oklahoma City, OK 73102 |
| Name and Phone number of Contact Person | Nathan Madenwald 405-297-2068 Office 405-625-7245 |
| Utility Product | Water |
| Utility Attributes (if applicable) | Unknown Sized Main – Water Line |
| General Location of Utility | This line crosses I-35 at approximate station 100+50 then heads in an east / west direction beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| SANITARY | |
|---|--|
| Type of Utility | Sanitary |
| Name and Address of Utility Owner | The City of Oklahoma City 420 W. Main, Suite 500 Oklahoma City, OK 73102 |
| Name and Phone number of Contact Person | Nathan Madenwald 405-297-2068 Office 405-625-7245 |
| Utility Product | Sanitary |
| Utility Attributes (if applicable) | 54" – Sanitary Sewer |
| General Location of Utility | This line crosses I-35 at approximate Southbound station 218+25 and extends east / west beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| SANITARY | |
|---|--|
| Type of Utility | Sanitary |
| Name and Address of Utility Owner | The City of Oklahoma City 420 W. Main, Suite 500 Oklahoma City, OK 73102 |
| Name and Phone number of Contact Person | Nathan Madenwald 405-297-2068 Office 405-625-7245 |
| Utility Product | Sanitary |
| Utility Attributes (if applicable) | 42" – Sanitary Sewer |
| General Location of Utility | This line crosses I-35 at approximate Southbound station 233+25 and extends east / west beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| SANITARY | |
|---|--|
| Type of Utility | Sanitary |
| Name and Address of Utility Owner | The City of Oklahoma City 420 W. Main, Suite 500 Oklahoma City, OK 73102 |
| Name and Phone number of Contact Person | Nathan Madenwald 405-297-2068 Office 405-625-7245 |
| Utility Product | Sanitary |
| Utility Attributes (if applicable) | 48" – Sanitary Sewer |
| General Location of Utility | This line crosses I-35 at approximate Southbound station 240+25 and extends east / west beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| SANITARY | |
|---|---|
| Type of Utility | Sanitary |
| Name and Address of Utility Owner | The City of Oklahoma City 420 W. Main, Suite 500 Oklahoma City, OK 73102 |
| Name and Phone number of Contact Person | Nathan Madenwald 405-297-2068 Office 405-625-7245 |
| Utility Product | Sanitary |
| Utility Attributes (if applicable) | 24" – Sanitary Sewer |
| General Location of Utility | This line heads south then east along the north side of the west bound lane of I-40 from station 267+25 to 273+75 (75' Lt.) where line ties into the existing crossing. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| SANITARY | |
|---|---|
| Type of Utility | Sanitary |
| Name and Address of Utility Owner | The City of Oklahoma City 420 W. Main, Suite 500 Oklahoma City, OK 73102 |
| Name and Phone number of Contact Person | Nathan Madenwald 405-297-2068 Office 405-625-7245 |
| Utility Product | Sanitary |
| Utility Attributes (if applicable) | 24" – Sanitary Sewer |
| General Location of Utility | This line crosses I-40 at approximate Westbound station 273+75 and extends north / south beyond the project limits. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| SANITARY | |
|---|--|
| Type of Utility | Sanitary |
| Name and Address of Utility Owner | The City of Oklahoma City 420 W. Main, Suite 500 Oklahoma City, OK 73102 |
| Name and Phone number of Contact Person | Nathan Madenwald 405-297-2068 Office 405-625-7245 |
| Utility Product | Sanitary |
| Utility Attributes (if applicable) | 48" – Sanitary Sewer |
| General Location of Utility | This line parallels the south side of the east bound lane of I-40 from approximate station 166+00 to station 187+50 (160' Rt.) |

Trade Fixture (Billboard) Ownership:

I-35 is a regulated route and attached are records of trade fixture (billboard) ownership according to the Right-of-Way & Utilities Division's Outdoor Advertising Branch. Locations are on the plan view map.

Sign

SFN 490005 Twf No 2142 Reg 10895 Adj code CLSA Sign Owner 2205 Land Owner 9969 Land Owner Red Flag N

Hwy 35 Cnty 55 Seg 15 Side W D / U D XHwy 40 Miles 4 Dir S Dist From Row 10

Address 1202 N I-35 City OKLAHOMA CITY Aerial Page 6

Legal Land Desc NW SE SW Section 2 Township 11 TWN Dir N Range 3 Range Dir W Latitude 35.45322956

S/O Name CHANCELLOR MEDIA WHITECO L/O Name MALCOM HALL Longitude -97.48825478

Height 14 Width 48 Total HT 62 S-Type V2 Panels # 2 Illum Y TRI VISION N Led Display N

Zoning Authority Bus- 600ft Y Name

Reg 02/01/1996 Permit Expires 03/31/2014 Erected 01/01/1985 Field Inv 05/27/2011 Removal

Comment

Documents Photos Save And Print Certificate Save And Print Permit CHANGE SFN # Created 03/08/2008 11:15:58

Created By RWP

Sign SFN 4035801 Twf No 3781 Reg 12080 Adj code CLSA Sign Owner 2205 Land Owner 1545 Land Owner Red Flag N
 Hwy 35 Cnty 55 Seg 15 Side W D/U D XHwy 140 Miles 7 Dir S Dist From Row 25
 Address _____ City OKLAHOMA CITY Aerial Page 6
 Legal Land Desc SW NE SW Section 2 Township 11 Range 3 Range Dir W Latitude 35.45379044
 S/O Name CHANCELLOR MEDIA WHITECO L/O Name BURLINGTON NORTHERN SANTA FE Longitude -97.4900667
 Height 14 Width 48 Total HT 60 S-Type V2 Panels # 2 Illum Y TRI VISION N Led Display N
 Zoning _____ Authority OKC (1-3) Bus- 600ft Y Name BNSF RAIL YARD
 Reg 07/18/2000 Permit Expires 09/30/2014 Erected _____ Field Inv 05/27/2011 Removal _____
 Comment _____
 Documents _____ Photos _____ Save And Print Certificate _____ Save And Print Permit _____ CHANGE SFN # _____ Created 03/08/2008 11:16:37
 Created By RWP

Sign | Sign owner | Land Owner | Adjudication | Sign Face hst | Comments

Sign

SFN 499801 Twf No 1892 Reg 10723 Adj code CLSA Sign Owner 1308 Land Owner 9836 Land Owner Red Flag N

Hwy 35 Cnty 55 Seg 15 Side W D/U D XHwy 40 Miles .5 Dir S Dist From Row 25

Address 1515 SOUTH I-35 City OKLAHOMA CITY Aerial Page 6

Legal Land Desc SW SE SW Section 2 Township 11 Range 3 TWN Dir N Range Dir W Latitude 35.45122521

S/O Name TYLER OUTDOOR SIGN COMPANY L/O Name MAC TEAGUE Longitude -97.48873258

Height 14 Width 48 Total HT 40 S-Type V2 Panels # 2 Illum Y TRI VISION N Led Display N

Zoning Authority CITY OF OKC Bus-600ft Y Name

Reg 12/22/1994 Permit Expires 12/31/2014 Erected 12/23/1994 Field Inv 05/27/2011 Removal

Comment

Documents | Photos | Save And Print Certificate | Save And Print Permit | CHANGE SFN # | Created 03/08/2008 11:16:00 | Created By RWP

Sign

SFN 474501 Twf No 915 Reg 9984 Adj code CLSA Sign Owner 2205 Land Owner 14233 Land Owner Red Flag N

Hwy 35 Crty 55 Seg 15 Side W D/U D X-hwy 40 Miles 1 Dir S Dist From Row 25

Address 1201 S I-35 City OKLAHOMA CITY Aerial Page 6

Legal Land Desc SE SW NW Section 2 Township 11 Range 3 TWN Dir N Range Dir W Latitude 35.45704306

S/O Name CHANCELLOR MEDIA WHITECO L/O Name TDI Longitude -97.49198786

Height 14 Width 48 Total HT 50 S-Type V2 Panels # 2 Illum Y TRI VISION N Led Display N

Zoning Authority OKLAHOMA CITY OKLA Bus-600ft Y Name STORAGE LOT

Reg 08/31/1987 Permit Expires 09/30/2015 Erected 07/22/1987 Field Inv 05/27/2011 Removal

Comment

Documents Photos Save And Print Certificate Save And Print Permit CHANGE SFN # Created 03/08/2008 11:15:57 Created By RWP

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
OKLAHOMA COUNTY
J/P 30444(04)

SECTION 05 – ACCIDENT HISTORY

An Accident History Report was provided by ODOT Traffic Engineering Division and is included herein.



Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

Study Map & Totals

Legend

| | |
|--------------------------------------|-----------------|
| ▲ | Fatality |
| ■ | Injury |
| ● | Property Damage |



Remarks:

NONE

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 thru 12-31-2013

| | 2008 | | | | 2009 | | | | 2010 | | | | 2011 | | | | 2012 | | | | 2013* | | | |
|------------|------|-----|----|-----|------|-----|-----|-----|------|-----|-----|-----|------|-----|-----|-----|------|-----|-----|-----|-------|-----|-----|-----|
| | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot |
| Collisions | 1 | 25 | 55 | 81 | 1 | 58 | 101 | 160 | 2 | 64 | 101 | 167 | 1 | 74 | 165 | 240 | 2 | 74 | 130 | 206 | | 55 | 143 | 198 |
| Persons | 1 | 36 | | 37 | 1 | 81 | | 82 | 3 | 98 | | 101 | 3 | 113 | | 116 | 6 | 102 | | 108 | | 88 | | 88 |

* DENOTES A YEAR FOR WHICH DATA MAY BE INCOMPLETE.

| | Study Total | | | |
|------------|-------------|--------|-----------------|-------|
| | Fatal | Injury | Property Damage | Total |
| Collisions | 7 | 350 | 695 | 1052 |
| Persons | 14 | 518 | | 532 |



STUDY TOTALS - BY CITY AND HWY CLASS

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

STUDY TOTALS

| Year | HIGHWAY COLLISIONS | | | |
|--------|--------------------|-----|-----|------|
| | Fat | Inj | PD | Tot |
| 2008 | 1 | 25 | 55 | 81 |
| 2009 | 1 | 58 | 101 | 160 |
| 2010 | 2 | 64 | 101 | 167 |
| 2011 | 1 | 74 | 165 | 240 |
| 2012 | 2 | 74 | 130 | 206 |
| 2013 * | | 55 | 143 | 198 |
| Total: | 7 | 350 | 695 | 1052 |

* DENOTES A YEAR FOR WHICH DATA MAY BE INCOMPLETE.

County: (55) OKLAHOMA

| | HIGHWAY COLLISIONS | | | |
|-----------------|--------------------|-----|-----|------|
| | Fat | Inj | PD | Tot |
| (70) OKLA. CITY | 7 | 350 | 695 | 1052 |



TABULATION OF COLLISIONS

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

Collisions By Type Of Collision

| Type Of Collision | 2008 | | | | 2009 | | | | 2010 | | | | 2011 | | | | 2012 | | | |
|------------------------------|------------|------------|------------|------------|------------|------------|------------|-------------|------------|------------|------------|-------------|------------|------------|-------------|-------------|------------|------------|-------------|-------------|
| | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot |
| Rear-End (front-to-rear) | | 10 | 29 | 39 | | 27 | 51 | 78 | | 30 | 57 | 87 | | 39 | 111 | 150 | | 38 | 70 | 108 |
| Head-On (front-to-front) | | 4 | 2 | 6 | | 1 | 2 | 3 | | 1 | 2 | 4 | | 3 | 1 | 4 | | 1 | 6 | 5 |
| Right Angle | | | 1 | 1 | | | 1 | 1 | | | 1 | 1 | | 1 | 2 | 3 | | | 1 | 2 |
| Angle Turning | | | | | | 2 | 3 | 5 | | 3 | 2 | 5 | | | 1 | 1 | | | | 1 |
| Other Angle | 1 | | 2 | 3 | | 3 | 9 | 12 | | 6 | 8 | 14 | | | 9 | 9 | | | 2 | 10 |
| Sideswipe Same Direction | | 1 | 3 | 4 | | | 7 | 7 | | 3 | 10 | 13 | | 1 | 13 | 14 | | | 1 | 12 |
| Sideswipe Opposite Direction | | | | | | | | | | | | | | | 1 | 1 | | | | 1 |
| Fixed Object | | 5 | 3 | 8 | | 6 | 11 | 17 | | 8 | 8 | 16 | 1 | 8 | 12 | 21 | | | 17 | 8 |
| Pedestrian | | 1 | | 1 | | | | | | 1 | | 1 | | | | | | | 1 | 1 |
| Pedal Cycle | | | | | | | | | | | | | | | | | | | | |
| Animal | | | | | | | | | | | | | | | | | | | | |
| Overturn/Rollover | | 2 | 2 | 4 | 1 | 10 | 1 | 12 | 1 | 5 | 2 | 8 | | 4 | | 4 | 1 | 3 | 4 | 8 |
| Vehicle-Train | | | | | | | | | | | | | | | | | | | | |
| Other Single Vehicle Crash | | | 1 | 1 | | 1 | | 1 | | 1 | | 1 | | | 2 | 2 | | | 1 | 1 |
| Other | | 2 | 12 | 14 | | 8 | 16 | 24 | | 4 | 13 | 17 | | 18 | 13 | 31 | | | 4 | 16 |
| Total | 1 | 25 | 55 | 81 | 1 | 58 | 101 | 160 | 2 | 64 | 101 | 167 | 1 | 74 | 165 | 240 | 2 | 74 | 130 | 206 |
| Percent | 0.1 | 2.4 | 5.2 | 7.7 | 0.1 | 5.5 | 9.6 | 15.2 | 0.2 | 6.1 | 9.6 | 15.9 | 0.1 | 7.0 | 15.7 | 22.8 | 0.2 | 7.0 | 12.4 | 19.6 |

Collisions By Type Of Collision

| Type Of Collision | 2013* | | | | Total | | | | |
|------------------------------|-------|------------|-------------|-------------|------------|-------------|-------------|-------------|------------|
| | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Pct |
| Rear-End (front-to-rear) | | 33 | 82 | 115 | | 177 | 400 | 577 | 54.8 |
| Head-On (front-to-front) | | 3 | 6 | 9 | 2 | 19 | 17 | 38 | 3.6 |
| Right Angle | | 1 | 1 | 2 | | 4 | 7 | 11 | 1.0 |
| Angle Turning | | | 3 | 3 | | 5 | 10 | 15 | 1.4 |
| Other Angle | | 3 | 2 | 5 | 1 | 14 | 40 | 55 | 5.2 |
| Sideswipe Same Direction | | | 13 | 13 | | 6 | 58 | 64 | 6.1 |
| Sideswipe Opposite Direction | | | 3 | 3 | | | 5 | 5 | 0.5 |
| Fixed Object | | 7 | 15 | 22 | 1 | 51 | 57 | 109 | 10.4 |
| Pedestrian | | | | | | 3 | | 3 | 0.3 |
| Pedal Cycle | | | | | | | | | |
| Animal | | | | | | | | | |
| Overturn/Rollover | | 3 | 1 | 4 | 3 | 27 | 10 | 40 | 3.8 |
| Vehicle-Train | | | | | | | | | |
| Other Single Vehicle Crash | | | | | | 3 | 4 | 7 | 0.7 |
| Other | | 5 | 17 | 22 | | 41 | 87 | 128 | 12.2 |
| Total | | 55 | 143 | 198 | 7 | 350 | 695 | 1052 | 100 |
| Percent | | 5.2 | 13.6 | 18.8 | 0.7 | 33.3 | 66.1 | 100 | |



TABULATION OF COLLISIONS

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

Units By Unit Type

| Unit Type | 2008 | | | | 2009 | | | | 2010 | | | | 2011 | | | | 2012 | | | |
|----------------------|------|-----|-----|-----|------|-----|-----|------|------|-----|-----|------|------|-----|------|------|------|-----|------|------|
| | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot |
| Train | | | | | | | | | | | | | | | | | | | | |
| Pedestrian | | 1 | | 1 | | | | | | 1 | | 1 | | | | | | 1 | | 1 |
| Animal | | | | | | | | | | | | | | | | | | | | |
| Pedal Cycle | | | | | | | | | | | | | | | | | | | | |
| Parked Vehicle | | | 2 | 2 | | 2 | | 2 | | 1 | | 1 | | 1 | | 4 | | 4 | | 4 |
| CMV | | 2 | | 2 | | 5 | 9 | 14 | | 8 | 13 | 21 | | 3 | 18 | 21 | 1 | 4 | 21 | 26 |
| Other Single Vehicle | | 8 | 6 | 14 | 1 | 16 | 12 | 29 | 1 | 13 | 9 | 23 | 1 | 12 | 13 | 26 | 1 | 22 | 11 | 34 |
| Other Multi-Vehicle | 2 | 40 | 114 | 156 | | 94 | 191 | 285 | 4 | 112 | 187 | 303 | | 150 | 330 | 480 | 1 | 118 | 246 | 365 |
| Total | 2 | 51 | 122 | 175 | 1 | 117 | 212 | 330 | 5 | 135 | 209 | 349 | 1 | 166 | 361 | 528 | 3 | 149 | 278 | 430 |
| Percent | 0.1 | 2.3 | 5.4 | 7.8 | | 5.2 | 9.5 | 14.7 | 0.2 | 6.0 | 9.3 | 15.6 | | 7.4 | 16.1 | 23.6 | 0.1 | 6.7 | 12.4 | 19.2 |

Units By Unit Type

| Unit Type | 2013* | | | | Total | | | | Pct |
|----------------------|-------|-----|------|------|-------|------|------|------|------|
| | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | |
| Train | | | | | | | | | |
| Pedestrian | | 3 | | 3 | | 6 | | 6 | 0.3 |
| Animal | | | | | | | | | |
| Pedal Cycle | | | | | | | | | |
| Parked Vehicle | | | 1 | 1 | | 8 | 3 | 11 | 0.5 |
| CMV | | 4 | 16 | 20 | 1 | 26 | 77 | 104 | 4.6 |
| Other Single Vehicle | | 10 | 16 | 26 | 4 | 81 | 67 | 152 | 6.8 |
| Other Multi-Vehicle | | 115 | 263 | 378 | 7 | 629 | 1331 | 1967 | 87.8 |
| Total | | 132 | 296 | 428 | 12 | 750 | 1478 | 2240 | 100 |
| Percent | | 5.9 | 13.2 | 19.1 | 0.5 | 33.5 | 66.0 | 100 | |



TABULATION OF COLLISIONS

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

Vehicles By Vehicle Type

| Vehicle Type | 2008 | | | | 2009 | | | | 2010 | | | | 2011 | | | | 2012 | | | |
|-------------------------------------|------------|------------|------------|------------|----------|------------|------------|-------------|------------|------------|------------|-------------|----------|------------|-------------|-------------|------------|------------|-------------|-------------|
| | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot |
| Passenger Vehicle-2 Door | | 5 | 17 | 22 | | 13 | 18 | 31 | 1 | 13 | 20 | 34 | | 15 | 32 | 47 | 1 | 13 | 23 | 37 |
| Passenger Vehicle-4 Door | 1 | 24 | 43 | 68 | | 50 | 75 | 125 | 1 | 44 | 77 | 122 | 1 | 66 | 139 | 206 | | 56 | 117 | 173 |
| Passenger Vehicle-Convertible | | 1 | | 1 | | 1 | 1 | 1 | 1 | 1 | 1 | 2 | | | | | | | 1 | 1 |
| Pickup Truck | | 8 | 29 | 37 | | 21 | 55 | 76 | | 30 | 48 | 78 | | 35 | 90 | 125 | 1 | 26 | 46 | 73 |
| Single-Unit Truck (2 axles) | | | | | | | 1 | 1 | | 1 | 1 | 2 | | | 3 | 3 | | | 1 | 1 |
| Single-Unit Truck (3 or more axles) | | 1 | | 1 | | | 1 | 1 | | | 2 | 2 | | | 4 | 4 | | | 2 | 2 |
| School Bus | | | | | | | | | | | | | | | | | | | | |
| Truck/Trailer | | | | | | | | | | 1 | 1 | 2 | | | | | | | 2 | 2 |
| Truck-Tractor (bobtail) | | | | | | | | | | 1 | 1 | 2 | | | | | | | 1 | 1 |
| Truck-Tractor/Semi-Trailer | | 1 | | 1 | | 5 | 6 | 11 | | 5 | 6 | 11 | | 3 | 17 | 20 | 1 | 4 | 16 | 21 |
| Truck-Tractor/Double | | | | | | | | | | | 1 | 1 | | | | | | | 1 | 1 |
| Truck-Tractor/Triple | | | | | | | | | | | | | | | | | | | | |
| Bus/Large Van (9-15 seats) | | | | | | | | | | | | | | | | | | | | |
| Bus (16+ seats) | | | | | | | | | | | | | | 1 | | 1 | | | | |
| Motorcycle | | 1 | | 1 | | 1 | | 1 | | | | | | 2 | | 2 | | 9 | | 9 |
| Motor Scooter/Moped | | | | | | | | | | | | | | | | | | | | |
| Motor Home | | | 1 | 1 | | | | | | | 1 | 1 | | | 1 | 1 | | | | |
| Farm Machinery | | | | | | | | | | | | | | | | | | | | |
| ATV | | | | | | | | | | | | | | | | | | | | |
| Sport Utility Vehicle (SUV) | 1 | 7 | 28 | 36 | 1 | 21 | 35 | 57 | 1 | 28 | 40 | 69 | | 32 | 54 | 86 | | 31 | 56 | 87 |
| Passenger Van | | 2 | 2 | 4 | | 4 | 13 | 17 | 1 | 4 | 7 | 12 | | 9 | 12 | 21 | | 7 | 8 | 15 |
| Truck More Than 10,000 lbs. | | | | | | | | | | 1 | 1 | 2 | | | | | | | | |
| Van (10,000 lbs. or less) | | | | | | 1 | 2 | 3 | | 3 | 1 | 4 | | 2 | 3 | 5 | | | 3 | 3 |
| Other | | | 2 | 2 | | 1 | 5 | 6 | | 2 | 2 | 4 | | 1 | 6 | 7 | | 2 | 1 | 3 |
| Total | 2 | 50 | 122 | 174 | 1 | 117 | 212 | 330 | 5 | 134 | 209 | 348 | 1 | 166 | 361 | 528 | 3 | 148 | 278 | 429 |
| Percent | 0.1 | 2.2 | 5.5 | 7.8 | | 5.2 | 9.5 | 14.8 | 0.2 | 6.0 | 9.4 | 15.6 | | 7.4 | 16.2 | 23.6 | 0.1 | 6.6 | 12.4 | 19.2 |



TABULATION OF COLLISIONS

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

Vehicles By Vehicle Type

| Vehicle Type | 2013* | | | | Total | | | | |
|-------------------------------------|-------|------------|-------------|-------------|------------|-------------|-------------|-------------|------------|
| | Fat | Inj | PD | Tot | Fat | Inj | PD | Tot | Pct |
| Passenger Vehicle-2 Door | | 19 | 23 | 42 | 2 | 78 | 133 | 213 | 9.5 |
| Passenger Vehicle-4 Door | | 45 | 114 | 159 | 3 | 285 | 565 | 853 | 38.2 |
| Passenger Vehicle-Convertible | | | 3 | 3 | 1 | 2 | 5 | 8 | 0.4 |
| Pickup Truck | | 33 | 64 | 97 | 1 | 153 | 332 | 486 | 21.8 |
| Single-Unit Truck (2 axles) | | 1 | 1 | 2 | | 2 | 7 | 9 | 0.4 |
| Single-Unit Truck (3 or more axles) | | | 1 | 1 | | 1 | 10 | 11 | 0.5 |
| School Bus | | | | | | | | | |
| Truck/Trailer | | | 4 | 4 | | 1 | 7 | 8 | 0.4 |
| Truck-Tractor (bobtail) | | | | | | 1 | 2 | 3 | 0.1 |
| Truck-Tractor/Semi-Trailer | | 3 | 10 | 13 | 1 | 21 | 55 | 77 | 3.4 |
| Truck-Tractor/Double | | | | | | | 2 | 2 | 0.1 |
| Truck-Tractor/Triple | | | | | | | | | |
| Bus/Large Van (9-15 seats) | | | | | | | | | |
| Bus (16+ seats) | | | | | | 1 | | 1 | |
| Motorcycle | | | 1 | 1 | | 13 | 1 | 14 | 0.6 |
| Motor Scooter/Moped | | | | | | | | | |
| Motor Home | | | 1 | 1 | | | 4 | 4 | 0.2 |
| Farm Machinery | | | | | | | | | |
| ATV | | | | | | | | | |
| Sport Utility Vehicle (SUV) | | 21 | 61 | 82 | 3 | 140 | 274 | 417 | 18.7 |
| Passenger Van | | 5 | 8 | 13 | 1 | 31 | 50 | 82 | 3.7 |
| Truck More Than 10,000 lbs. | | | 2 | 2 | | 1 | 3 | 4 | 0.2 |
| Van (10,000 lbs. or less) | | 2 | 2 | 4 | | 8 | 11 | 19 | 0.9 |
| Other | | | 1 | 1 | | 6 | 17 | 23 | 1.0 |
| Total | | 129 | 296 | 425 | 12 | 744 | 1478 | 2234 | 100 |
| Percent | | 5.8 | 13.2 | 19.0 | 0.5 | 33.3 | 66.2 | 100 | |

UNRESTRICTED

C 409



TABULATION OF COLLISIONS

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

Day And Time Of Occurrence Of Collisions

| Day | Hour Of The Day | | | | | | | | | | | | | | | | | | | | | | | | Tot | Pcnt | | | | | | |
|-----------|-------------------------|---|---|---|---|----|--------------|----|---|----|----|----|-----------------------|----|----|----|----|----|---------|---|---|----|----|-----|----------------------|------|--|--|--|--|------|-----|
| | AM | | | | | | | | | | | | PM | | | | | | | | | | | | | | | | | | | |
| | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | | | | | | | | |
| Sunday | 2 | 5 | 3 | 2 | 2 | | 2 | 1 | 3 | 1 | 8 | 6 | 3 | 13 | 3 | 3 | 9 | 3 | 1 | 3 | 2 | | 5 | 7 | 87 | 8.3 | | | | | | |
| Monday | 1 | | 1 | | 1 | 9 | 18 | 8 | 4 | 8 | 6 | 8 | 4 | 4 | 13 | 7 | 20 | 3 | 5 | 5 | 1 | 1 | 1 | 1 | 129 | 12.3 | | | | | | |
| Tuesday | 3 | 4 | | 1 | 2 | 10 | 31 | 27 | 6 | 4 | 1 | 5 | 5 | 15 | 16 | 16 | 19 | 9 | 5 | 2 | 2 | 1 | 2 | 186 | 17.7 | | | | | | | |
| Wednesday | 2 | 2 | 1 | | | 9 | 25 | 32 | 6 | 3 | 2 | 2 | 6 | 11 | 15 | 13 | 26 | 8 | 3 | 1 | | 1 | 1 | 169 | 16.1 | | | | | | | |
| Thursday | 2 | 3 | 1 | | 1 | 10 | 25 | 21 | 3 | 2 | 5 | 3 | 3 | 5 | 5 | 12 | 18 | 8 | 5 | 1 | 3 | 4 | 3 | 143 | 13.6 | | | | | | | |
| Friday | 1 | 3 | | | | 1 | 31 | 16 | 7 | 8 | 9 | 11 | 10 | 15 | 18 | 25 | 39 | 16 | 9 | 3 | 5 | 8 | 2 | 237 | 22.5 | | | | | | | |
| Saturday | 4 | 5 | 1 | 2 | 2 | 1 | 2 | 2 | 3 | 4 | 8 | 6 | 7 | 6 | 7 | 6 | 5 | 2 | 2 | 6 | 6 | 5 | 7 | 101 | 9.6 | | | | | | | |
| | Early Morning - Sunrise | | | | | | Morning Peak | | | | | | Mid Morning/Afternoon | | | | | | PM Peak | | | | | | Evening - Late Night | | | | | | Tot | 100 |
| Total | 97 | | | | | | 273 | | | | | | 294 | | | | | | 267 | | | | | | 121 | | | | | | 1052 | |
| Percent | 9.2 | | | | | | 26.0 | | | | | | 27.9 | | | | | | 25.4 | | | | | | 11.5 | | | | | | 100 | |

Roadway/Lighting

| Roadway Conditions | Lighting Conditions | | | | | Total | Percent |
|----------------------------|---------------------|----------|----------|---------|---------|-------|---------|
| | Daylight | Darkness | Twilight | Lighted | Unknown | | |
| Dry | 707 | 34 | 31 | 156 | | 928 | 88.2 |
| Wet (Water) | 58 | 2 | 6 | 21 | | 87 | 8.3 |
| Ice, Snow, or Slush | 19 | | | 16 | | 35 | 3.3 |
| Mud, Dirt, Gravel, or Sand | | | | | | | |
| Other | 2 | | | | | 2 | 0.2 |
| Total | 786 | 36 | 37 | 193 | | 1052 | 100 |
| Percent | 74.7 | 3.4 | 3.5 | 18.3 | | 100 | |

Weather Conditions

| Weather Conditions | Total | Percent |
|--------------------|-------|---------|
| Clear | 561 | 53.3 |
| Clouds Present | 380 | 36.1 |
| Raining/Fog | 78 | 7.4 |
| Snowing/Sleet/Hail | 30 | 2.9 |
| Other | 3 | 0.3 |
| Total | 1052 | 100 |



TABULATION OF COLLISIONS

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

Drivers By Driver Conditions

| Unsafe/Unlawful | Apparently Normal | | | Alcohol Involved | | | | | | Sleep Suspected | | | Drug Use Indicated | | | Unknown Condition | | | Collision Severity | | | | | | | |
|----------------------|-------------------|------|------|------------------|-----|-----|---------------|-----|-----|-----------------|-----|-----|--------------------|-----|-----|-------------------|-----|-----|--------------------|------|------|-------|------|-----|-----|-----|
| | | | | Ability Impaired | | | Odor Detected | | | | | | | | | | | | | | | | | | | |
| | Fat | Inj | P-D | Fat | Inj | P-D | Fat | Inj | P-D | Fat | Inj | P-D | Fat | Inj | P-D | Fat | Inj | P-D | Fat | Inj | P-D | Total | Pcnt | | | |
| Failed to Yield | | 3 | 9 | | 1 | 1 | | | | | | | | | | | | | | 4 | 10 | 14 | 0.6 | | | |
| Failed to Stop | | 3 | 4 | | | 1 | | | | | | | | | | | | 1 | | 3 | 6 | 9 | 0.4 | | | |
| Failed to Signal | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Improper Turn | | 1 | 4 | | | | | | | | | | | | | | | 1 | | 1 | 5 | 6 | 0.3 | | | |
| Improper Start | | | 1 | | | | | | | | | | | | | | | 1 | | 1 | 1 | 2 | 0.1 | | | |
| Improper Stop | | 2 | | | | | | | | | | | | | | | | | | 2 | | 2 | 0.1 | | | |
| Improper Backing | | | | | | 1 | | | | | | | | | | | | | | | 1 | 1 | | | | |
| Improper Parking | | | 2 | | | | | | | | | | | | | | | | | | 2 | 2 | 0.1 | | | |
| Improper Passing | | | 1 | | | | | | | | | | | | | | | | | | 1 | 1 | | | | |
| Improper Lane Change | | 24 | 105 | | | 2 | | | | | | | | | | | | 5 | 32 | | 29 | 139 | 168 | 7.6 | | |
| Left of Center | | | 1 | | | | | | | | | | | | | | | | | | 1 | 1 | | | | |
| Following Too Close | | 147 | 322 | | 1 | | | | | | | | 2 | | | | 10 | 21 | | 158 | 345 | 503 | 22.6 | | | |
| Unsafe Speed | | 101 | 83 | 1 | 4 | 2 | | 1 | | | 1 | | | | | 1 | 4 | 11 | 2 | 111 | 96 | 209 | 9.4 | | | |
| DWI | | | 1 | 1 | 13 | 12 | | | | | | | | 3 | 1 | | | | | 1 | 16 | 14 | 31 | 1.4 | | |
| Inattention | | 22 | 41 | | 1 | | | | | 1 | 5 | 1 | | | | | | | 4 | 1 | 1 | 32 | 43 | 76 | 3.4 | |
| Negligent Driving | | 3 | 20 | | | | | | | | | | | | | | | | | 3 | | 3 | 23 | 26 | 1.2 | |
| Defective Vehicle | | 6 | 6 | | | | | | | | | | | | | | | | | 1 | | 6 | 7 | 13 | 0.6 | |
| Wrong Way | | | | 1 | | | | | | | | | | | | | | | | | 1 | | 1 | | | |
| No Improper Action | 6 | 345 | 739 | | 4 | 1 | | 1 | 1 | | | | 1 | 1 | | | 8 | 12 | 6 | 359 | 754 | 1119 | 50.3 | | | |
| Other | | 6 | 22 | 1 | | | | | | | | | | | | | | | | 5 | 5 | 1 | 11 | 27 | 39 | 1.8 |
| Total | 6 | 663 | 1361 | 4 | 24 | 20 | | 2 | 1 | 1 | 6 | 1 | | 4 | 4 | 1 | 37 | 88 | 12 | 736 | 1475 | 2223 | 100 | | | |
| Percent | 0.3 | 29.8 | 61.2 | 0.2 | 1.1 | 0.9 | | 0.1 | | | 0.3 | | | 0.2 | 0.2 | | 1.7 | 4.0 | 0.5 | 33.1 | 66.4 | 100 | | | | |

Collisions By Special Feature

| Special Feature | Total | | | |
|-----------------|-------|-----|----|-----|
| | Fat | Inj | PD | Tot |
| Bridge | | 2 | 7 | 9 |
| Work Zone | 1 | 25 | 71 | 97 |
| Cross Median | | 2 | | 2 |
| Train Collision | | | | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date | |
|---------------|------|-----------------|--------|-----------|--------------|------------------------|--------------|--------|---------|--------|--------------|--------|----------------------|-----------------|----------------|---------------|----------|------------|--|
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | AT: I-40, | | 126.66, | | I-40 SB ENT | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | X-RD/RMP GOR | NO | E | E | 2 | | | OTHER | IMP-LN-CHG | DARK | DRY | PDO | 10-29-2009 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | X-RD/RMP GOR | NO | S | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DARK | DRY | P INJ | 12-13-2009 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | RAMP | NO | S | - | 2 | 2 | | REAR-END | UNSAF-SPD | DARK | ICE | I INJ | 01-28-2010 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | RAMP | NO | S | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DARK | DRY | I INJ | 11-13-2010 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | WKZONE RAMP | NO | E | - | 1 | | | F-O SAND-BARRELS | D-W-I | DARK | DRY | PDO | 01-15-2011 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | RAMP | NO | S | - | 1 | | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | DRY | PDO | 05-30-2011 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | X-RD/RMP GOR WKZONE | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-22-2011 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | X-RD/RMP GOR WKZONE | NO | E | E | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 08-24-2011 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | X-RD/RMP GOR WKZONE | NO | E | E | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DARK | DRY | PDO | 08-30-2011 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | RAMP | NO | S | S | 2 | 4 | | REAR-END | INATT | DYLG | DRY | I INJ | 05-15-2012 | |
| 55 | 70 | 15 | 10 | 05.80 | I-40 SB ENT | X-RD/RMP GOR | NO | E | - | 1 | 2 | | ROLLOVER | D-W-I | DARK | DRY | N-I INJ | 01-27-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | AT: I-40, | | 126.67, | | I-40 NB EXIT | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP MRG | NO | W | W | 2 | | | REAR-END | IMP-PARK | DYLG | DRY | PDO | 02-06-2009 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR | NO | N | N | 2 | | | ANGLE-TURNING | UNSAF-SPD | DYLG | DRY | PDO | 03-02-2009 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | M/L RAMP GOR | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 10-02-2009 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | WET | PDO | 04-02-2010 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR | NO | N | - | 1 | 1 | | F-O TRAFF-SIGN | D-W-I | DARK | DRY | P INJ | 05-09-2010 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR | NO | N | - | 1 | | | F-O BARR-CONCRETE | INATT | DARK | DRY | PDO | 11-20-2010 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR WKZONE | NO | E | S | 2 | | | RIGHT-ANGLE | IMP-PASS | DYLG | DRY | PDO | 04-22-2011 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | M/L RAMP GOR | NO | N | - | 1 | | | F-O TRAFF-SIGN | INATT | DYLG | DRY | PDO | 05-09-2011 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | RAMP WKZONE | NO | W | W | 2 | | | SIDESWIPE-OPP | UNSAF-SPD | DARK | DRY | PDO | 05-22-2011 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR WKZONE | NO | N | - | 1 | | | F-O IMPACT-ATTEN | UNSAF-SPD | DYLG | WET | PDO | 05-24-2011 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR | NO | W | - | 1 | | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | DRY | PDO | 06-30-2011 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR | NO | N | - | 1 | 1 | | F-O IMPACT-ATTEN | UNSAF-SPD | DARK | DRY | P INJ | 07-03-2011 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR WKZONE | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 07-11-2011 | |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | RAMP WKZONE | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | DRY | P INJ | 01-24-2012 | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| | | | | | HIGHWAY COLLISIONS | | | | | | | | | | | | | |
|------|------|------|--------|-----------|--------------------|--------------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | RAMP WKZONE | NO | W | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DARK | WET | N-I INJ | 02-10-2012 |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | RAMP WKZONE | NO | E | - | 1 | | | ROLLOVER | NO-IMP-ACT | DYLG | DRY | PDO | 03-18-2012 |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | RAMP WKZONE | NO | W | - | 1 | 1 | | F-O BR-PIER | UNSAF-SPD | DARK | WET | P INJ | 03-19-2012 |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | RAMP | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | SLEEPY | DARK | DRY | P INJ | 09-21-2012 |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | RAMP | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 04-30-2013 |
| 55 | 70 | 15 | 10 | 05.81 | I-40 NB EXIT | X-RD/RMP GOR | NO | W | - | 1 | | | F-O BARR-CONCRETE | NO-IMP-ACT | DYLG | DRY | PDO | 10-18-2013 |

(55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.68

| | | | | | | | | | | | | | | | | | | |
|----|----|----|----|-------|--|--|----|---|---|---|---|--|----------------|-----------|------|-----|-------|------------|
| 55 | 70 | 15 | 10 | 05.82 | | | NO | S | S | 2 | 1 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 10-15-2010 |
| 55 | 70 | 15 | 10 | 05.82 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 03-25-2012 |
| 55 | 70 | 15 | 10 | 05.82 | | | NO | N | N | 3 | 1 | | SIDESWIPE-SAME | UNSAF-SPD | DARK | ICE | I INJ | 12-25-2012 |
| 55 | 70 | 15 | 10 | 05.82 | | | NO | W | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DYLG | DRY | P INJ | 01-20-2013 |
| 55 | 70 | 15 | 10 | 05.82 | | | NO | N | N | 2 | | | SIDESWIPE-OPP | UNSAF-SPD | DYLG | ICE | PDO | 11-24-2013 |
| 55 | 70 | 15 | 10 | 05.82 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 12-19-2013 |

(55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.69

| | | | | | | | | | | | | | | | | | | |
|----|----|----|----|-------|--|--------------|----|---|---|---|---|---|----------|------------|------|-----|-------|------------|
| 55 | 70 | 15 | 10 | 05.83 | | | NO | S | S | 4 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 06-01-2010 |
| 55 | 70 | 15 | 10 | 05.83 | | RAMP/RAMP JC | NO | S | S | 2 | | 5 | HEAD-ON | UNSAF-SPD | DARK | DRY | FAT | 08-10-2012 |
| 55 | 70 | 15 | 10 | 05.83 | | RAMP/RAMP JC | NO | S | S | 3 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 03-20-2013 |
| 55 | 70 | 15 | 10 | 05.83 | | | NO | S | S | 2 | 1 | | REAR-END | IMP-LN-CHG | DYLG | WET | P INJ | 04-02-2013 |
| 55 | 70 | 15 | 10 | 05.83 | | | NO | N | N | 2 | | | HEAD-ON | IMP-LN-CHG | DYLG | DRY | PDO | 10-30-2013 |

(55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.71

| | | | | | | | | | | | | | | | | | | |
|----|----|----|----|-------|--|---------------------|----|---|---|---|---|--|-----------------|------------|------|-----|-------|------------|
| 55 | 70 | 15 | 10 | 05.85 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 09-17-2008 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O TRAFF-SIGN | INATT | DARK | DRY | PDO | 09-27-2008 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O DITCH | UNSAF-SPD | DYLG | ICE | PDO | 01-26-2009 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O TRAFF-SIGN | FOL-CLOSE | DARK | DRY | PDO | 02-15-2009 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O TRAFF-SIGN | UNSAF-SPD | DARK | DRY | PDO | 04-02-2009 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | 1 | | F-O TRAFF-SIGN | INATT | DARK | DRY | P INJ | 04-17-2009 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O GUARDRL-END | UNSAF-SPD | DARK | WET | PDO | 07-04-2009 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O TRAFF-SIGN | UNSAF-SPD | DARK | DRY | PDO | 08-02-2009 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O TRAFF-SIGN | UNSAF-SPD | DARK | WET | PDO | 02-07-2010 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-25-2010 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR WKZONE | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-25-2012 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| | | | | | HIGHWAY COLLISIONS | | | | | | | | | | | | | |
|------|------|------|--------|-----------|--------------------|--------------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 05.85 | | WKZONE | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DAWN | DRY | PDO | 11-15-2012 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O CULVERT | UNSAF-SPD | DARK | DRY | PDO | 01-07-2013 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O TRAFF-SIGN | NO-IMP-ACT | DYLG | DRY | PDO | 02-23-2013 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O TRAFF-SIGN | UNSAF-SPD | DARK | DRY | PDO | 03-18-2013 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | E | 2 | | | OTHER | IMP-LN-CHG | DARK | DRY | PDO | 06-22-2013 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | S | - | 1 | 1 | | F-O TRAFF-SIGN | INATT | DYLG | DRY | N-I INJ | 08-03-2013 |
| 55 | 70 | 15 | 10 | 05.85 | | X-RD LOOP GR | NO | E | - | 1 | | | F-O TRAFF-SIGN | UNSAF-SPD | DARK | DRY | PDO | 12-15-2013 |

(55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.73

| | | | | | | | | | | | | | | | | | | |
|----|----|----|----|-------|--|--------|----|---|---|---|---|--|-------------------|------------|------|-----|---------|------------|
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 10-09-2008 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | - | 1 | | | ROLLOVER | D-W-I | DARK | DRY | PDO | 11-28-2008 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | - | 1 | 1 | | F-O BARR-CONCRETE | NO-IMP-ACT | DARK | DRY | N-I INJ | 11-29-2008 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | I INJ | 12-01-2008 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | - | 1 | | | ROLLOVER | UNSAF-SPD | DARK | WET | PDO | 12-18-2008 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 4 | 2 | | REAR-END | IMP-LN-CHG | DYLG | DRY | I INJ | 03-10-2009 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-15-2009 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | - | 1 | 1 | | ROLLOVER | D-W-I | DARK | DRY | I INJ | 04-18-2009 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 4 | 2 | | OTHER | FOL-CLOSE | DYLG | DRY | P INJ | 05-21-2009 |
| 55 | 70 | 15 | 10 | 05.87 | | WKZONE | NO | N | N | 3 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 07-19-2009 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | - | 1 | 1 | | F-O BARR-CONCRETE | NO-IMP-ACT | DARK | DRY | N-I INJ | 08-08-2009 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | 1 | | ANGLE-OTHER | IMP-LN-CHG | DAWN | WET | P INJ | 01-15-2010 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-31-2010 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 04-11-2010 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 4 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 04-21-2010 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 05-20-2010 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DARK | DRY | PDO | 09-04-2010 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | | | OTHER | OTHER | DYLG | DRY | PDO | 09-23-2010 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | WET | PDO | 11-12-2010 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | | | REAR-END | UNSAF-SPD | DARK | DRY | PDO | 12-03-2010 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-11-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-11-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-11-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | 4 | | REAR-END | FOL-CLOSE | DYLG | WET | P INJ | 04-24-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 05-27-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-05-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 07-25-2011 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 05.87 | | WKZONE | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 08-03-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | WKZONE | NO | N | - | 2 | 1 | | REAR-END | UNSAF-SPD | DYLG | DRY | I INJ | 08-04-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | WET | PDO | 10-27-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | WKZONE | NO | N | N | 3 | 1 | | HEAD-ON | IMP-LN-CHG | DYLG | DRY | N-I INJ | 10-28-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | WKZONE | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-30-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DARK | DRY | P INJ | 12-16-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 12-31-2011 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 3 | | | OTHER | NO-IMP-ACT | DARK | DRY | PDO | 01-05-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 01-05-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | 2 | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | I INJ | 01-09-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-17-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | 1 | | REAR-END | INATT | DYLG | DRY | P INJ | 01-30-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | 2 | | REAR-END | FOL-CLOSE | DAWN | DRY | N-I INJ | 02-01-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-16-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 04-18-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 4 | 3 | | REAR-END | FOL-CLOSE | DYLG | DRY | I INJ | 05-04-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-06-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 07-17-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-27-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | - | 1 | 1 | | F-O IMPACT-ATTEN | INATT | DARK | DRY | P INJ | 09-09-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 10-29-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 11-08-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | ANGLE-OTHER | FOL-CLOSE | DARK | DRY | PDO | 11-21-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | 1 | | REAR-END | INATT | DYLG | DRY | P INJ | 11-25-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-11-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DARK | ICE | I INJ | 12-25-2012 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-14-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 01-22-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-23-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-09-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 04-14-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-03-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | 2 | | OTHER | FOL-CLOSE | DYLG | DRY | N-I INJ | 05-08-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 05-15-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | SIDESWIPE-SAME | NEG-DRVING | DYLG | DRY | PDO | 05-17-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 3 | 5 | | OTHER | UNSAF-SPD | DYLG | DRY | P INJ | 05-17-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | 1 | | REAR-END | D-W-I | DYLG | DRY | P INJ | 07-11-2013 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|----------|--------------------|--------------------------------------|--------|--------|--------|--------|--------|----------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 08-09-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 09-10-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-12-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-16-2013 |
| 55 | 70 | 15 | 10 | 05.87 | | | NO | E | E | 2 | | | SIDESWIPE-OPP | FOL-CLOSE | DYLG | SNOW | PDO | 12-06-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.74 | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.88 | | INCIDENT | NO | S | S | 2 | | | OTHER | NEG-DRVING | DYLG | DRY | PDO | 10-20-2008 |
| 55 | 70 | 15 | 10 | 05.88 | | X-RD/RMP GOR | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 12-11-2008 |
| 55 | 70 | 15 | 10 | 05.88 | | | NO | N | N | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 02-23-2009 |
| 55 | 70 | 15 | 10 | 05.88 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-25-2009 |
| 55 | 70 | 15 | 10 | 05.88 | | RAMP | NO | E | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DARK | WET | P INJ | 12-08-2009 |
| 55 | 70 | 15 | 10 | 05.88 | | RAMP | NO | S | - | 1 | 1 | | F-O RET-WALL | UNSAF-SPD | DARK | WET | P INJ | 02-04-2010 |
| 55 | 70 | 15 | 10 | 05.88 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 05-26-2010 |
| 55 | 70 | 15 | 10 | 05.88 | | RAMP | NO | S | S | 2 | 5 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 02-18-2011 |
| 55 | 70 | 15 | 10 | 05.88 | | WKZONE | NO | W | W | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 10-22-2011 |
| 55 | 70 | 15 | 10 | 05.88 | | RAMP | NO | E | - | 1 | | | F-O BARR-CONCRETE | UNSAF-SPD | DARK | DRY | PDO | 01-07-2012 |
| 55 | 70 | 15 | 10 | 05.88 | | X-RD/RMP MRG | NO | E | E | 2 | 1 | | REAR-END | IMP-LN-CHG | DARK | ICE | N-I INJ | 02-14-2012 |
| 55 | 70 | 15 | 10 | 05.88 | | X-RD/RMP MRG | NO | E | E | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 04-24-2012 |
| 55 | 70 | 15 | 10 | 05.88 | | X-RD/RMP MRG | NO | E | - | 1 | 1 | | OTH-SINGLE-VEH | NO-IMP-ACT | DYLG | DRY | N-I INJ | 05-08-2012 |
| 55 | 70 | 15 | 10 | 05.88 | | X-RD/RMP MRG | NO | W | - | 1 | | | F-O IMPACT-ATTEN | UNSAF-SPD | DYLG | DRY | PDO | 03-08-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: I-40, 126.75, WB I-40/SB I235 BR | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.89 | | WB I-40/SB I235 BR | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-05-2011 |
| 55 | 70 | 15 | 10 | 05.89 | | WB I-40/SB I235 BR | NO | S | S | 3 | 1 | | HEAD-ON | IMP-STOP | DARK | ICE | P INJ | 12-25-2012 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.76 | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.90 | | X-RD/RMP MRG | NO | S | - | 1 | 1 | | F-O RET-WALL | NO-IMP-ACT | DARK | DRY | P INJ | 05-09-2009 |
| 55 | 70 | 15 | 10 | 05.90 | | WKZONE | NO | S | - | 1 | 1 | | F-O OTHER | OTHER | DARK | DRY | N-I INJ | 06-27-2009 |
| 55 | 70 | 15 | 10 | 05.90 | | WKZONE | NO | S | - | 1 | | | F-O OTHER | OTHER | DARK | DRY | PDO | 06-27-2009 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.77 | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | W | W | 2 | 2 | | REAR-END | D-W-I | DARK | DRY | P INJ | 12-24-2008 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 04-10-2009 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DARK | DRY | PDO | 01-08-2010 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 06-22-2010 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | WET | PDO | 09-08-2010 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 10-15-2010 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 11-23-2010 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-24-2010 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | S | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-11-2011 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 04-26-2011 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 05-11-2011 |
| 55 | 70 | 15 | 10 | 05.91 | | WKZONE | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | OTHER | PDO | 11-22-2011 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 11-22-2011 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 3 | 2 | | REAR-END | FOL-CLOSE | DAWN | DRY | N-I INJ | 01-31-2012 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 03-02-2012 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 05-25-2012 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-22-2012 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-14-2012 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-25-2012 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 09-26-2012 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | - | 1 | | | F-O GUARDRL-FACE | D-W-I | DARK | DRY | PDO | 12-13-2012 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 3 | | | SIDESWIPE-SAME | INATT | DYLG | WET | PDO | 02-12-2013 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | W | W | 2 | 1 | | REAR-END | UNSAF-SPD | DARK | DRY | I INJ | 02-16-2013 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-06-2013 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-04-2013 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | E | E | 2 | | | OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 09-11-2013 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 4 | | | REAR-END | FOL-CLOSE | DUSK | DRY | PDO | 09-17-2013 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | WET | PDO | 11-05-2013 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 11-15-2013 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | N | N | 2 | | | SIDESWIPE-SAME | UNSAF-SPD | DYLG | ICE | PDO | 11-22-2013 |
| 55 | 70 | 15 | 10 | 05.91 | | | NO | S | S | 2 | 2 | | REAR-END | UNSAF-SPD | DARK | ICE | N-I INJ | 11-25-2013 |

(55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.79

| | | | | | | | | | | | | | | | | | | |
|----|----|----|----|-------|--|--|----|---|---|---|---|--|----------------|------------|------|-----|-------|------------|
| 55 | 70 | 15 | 10 | 05.93 | | | NO | E | E | 5 | | | OTHER | OTHER | DYLG | ICE | PDO | 12-09-2008 |
| 55 | 70 | 15 | 10 | 05.93 | | | NO | E | - | 2 | | | REAR-END | OTHER | DYLG | ICE | PDO | 12-09-2008 |
| 55 | 70 | 15 | 10 | 05.93 | | | NO | E | E | 3 | | | REAR-END | OTHER | DYLG | DRY | PDO | 05-09-2009 |
| 55 | 70 | 15 | 10 | 05.93 | | | NO | E | - | 1 | | | F-O | D-W-I | DARK | DRY | PDO | 07-03-2009 |
| 55 | 70 | 15 | 10 | 05.93 | | | NO | W | W | 3 | 2 | | BARR-CONCRETE | D-W-I | DARK | WET | P INJ | 01-16-2010 |
| 55 | 70 | 15 | 10 | 05.93 | | | NO | E | E | 3 | | | SIDESWIPE-SAME | D-W-I | DARK | WET | P INJ | 01-16-2010 |
| 55 | 70 | 15 | 10 | 05.93 | | | NO | E | E | 2 | | | OTHER | OTHER | DYLG | WET | PDO | 04-16-2010 |
| 55 | 70 | 15 | 10 | 05.93 | | | NO | E | E | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 05-13-2010 |
| 55 | 70 | 15 | 10 | 05.93 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 06-08-2010 |
| 55 | 70 | 15 | 10 | 05.93 | | | NO | E | E | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 08-19-2013 |

(55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.80

| | | | | | | | | | | | | | | | | | | |
|----|----|----|----|-------|--|--|----|---|---|---|---|---|-------------|-----------|------|-----|---------|------------|
| 55 | 70 | 15 | 10 | 05.94 | | | NO | W | W | 2 | 1 | | ANGLE-OTHER | UNSAF-SPD | DARK | DRY | N-I INJ | 03-28-2010 |
| 55 | 70 | 15 | 10 | 05.94 | | | NO | W | - | 1 | 2 | 2 | ROLLOVER | UNSAF-SPD | DARK | DRY | FAT | 11-25-2010 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|----------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 05.94 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DUSK | WET | PDO | 12-07-2010 |
| 55 | 70 | 15 | 10 | 05.94 | | | NO | E | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DARK | WET | N-I INJ | 12-30-2010 |
| 55 | 70 | 15 | 10 | 05.94 | | | NO | W | W | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 01-22-2011 |
| 55 | 70 | 15 | 10 | 05.94 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | SNOW | PDO | 02-09-2011 |
| 55 | 70 | 15 | 10 | 05.94 | | WKZONE | NO | S | S | 2 | | | REAR-END | UNSAF-SPD | DYLG | SNOW | PDO | 02-09-2011 |
| 55 | 70 | 15 | 10 | 05.94 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 03-10-2011 |
| 55 | 70 | 15 | 10 | 05.94 | | WKZONE | NO | W | W | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DARK | DRY | PDO | 04-02-2012 |
| 55 | 70 | 15 | 10 | 05.94 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 09-24-2012 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.81 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.95 | | | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | DRY | PDO | 02-05-2009 |
| 55 | 70 | 15 | 10 | 05.95 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 05-13-2009 |
| 55 | 70 | 15 | 10 | 05.95 | | | NO | S | S | 2 | | | ANGLE-OTHER | NO-IMP-ACT | DYLG | DRY | PDO | 11-02-2012 |
| 55 | 70 | 15 | 10 | 05.95 | | | NO | W | W | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | WET | P INJ | 03-23-2013 |
| 55 | 70 | 15 | 10 | 05.95 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-10-2013 |
| 55 | 70 | 15 | 10 | 05.95 | | | NO | N | N | 2 | | | OTHER | NO-IMP-ACT | DYLG | DRY | PDO | 07-30-2013 |
| 55 | 70 | 15 | 10 | 05.95 | | | NO | S | S | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 08-09-2013 |
| 55 | 70 | 15 | 10 | 05.95 | | | NO | N | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DARK | DRY | N-I INJ | 12-16-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.82 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.96 | | | NO | N | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DYLG | DRY | P INJ | 12-01-2008 |
| 55 | 70 | 15 | 10 | 05.96 | | | NO | S | S | 2 | | | HEAD-ON | UNSAF-SPD | DYLG | SNOW | PDO | 03-20-2010 |
| 55 | 70 | 15 | 10 | 05.96 | | | NO | E | E | 2 | 3 | | HEAD-ON | UNSAF-SPD | DYLG | ICE | N-I INJ | 11-24-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.84 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.98 | | | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 02-23-2009 |
| 55 | 70 | 15 | 10 | 05.98 | | | NO | N | - | 1 | 2 | | ROLLOVER | UNSAF-SPD | DARK | WET | I INJ | 03-14-2009 |
| 55 | 70 | 15 | 10 | 05.98 | | | NO | E | E | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 03-30-2009 |
| 55 | 70 | 15 | 10 | 05.98 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 07-01-2009 |
| 55 | 70 | 15 | 10 | 05.98 | | | NO | E | E | 2 | | | OTHER | NEG-DRVING | DYLG | DRY | PDO | 08-13-2010 |
| 55 | 70 | 15 | 10 | 05.98 | | WKZONE | NO | E | E | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 10-20-2011 |
| 55 | 70 | 15 | 10 | 05.98 | | | NO | E | E | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 08-19-2013 |
| 55 | 70 | 15 | 10 | 05.98 | | | NO | E | E | 12 | 1 | | REAR-END | UNSAF-SPD | DARK | ICE | I INJ | 12-26-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.85 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 05.99 | | | NO | W | W | 2 | 1 | | REAR-END | UNSAF-SPD | DYLG | DRY | N-I INJ | 08-24-2008 |
| 55 | 70 | 15 | 10 | 05.99 | | | NO | W | W | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 09-30-2011 |
| 55 | 70 | 15 | 10 | 05.99 | | WKZONE | NO | E | E | 2 | 1 | | REAR-END | FOL-CLOSE | DAWN | DRY | N-I INJ | 11-04-2011 |
| 55 | 70 | 15 | 10 | 05.99 | | | NO | E | E | 3 | 2 | | HEAD-ON | D-W-I | DYLG | DRY | I INJ | 05-30-2012 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--|------|------|--------|-----------|------------|----------|--------------|--------|--------|--------|--------|--------|----------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 05.99 | | | NO | W | W | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-23-2013 |
| 55 | 70 | 15 | 10 | 05.99 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-29-2013 |
| 55 | 70 | 15 | 10 | 05.99 | | | NO | E | E | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 08-13-2013 |
| 55 | 70 | 15 | 10 | 05.99 | | | NO | E | - | 1 | | | F-O BARR-CONCRETE | DEF-VEH | DYLG | DRY | PDO | 09-21-2013 |
| 55 | 70 | 15 | 10 | 05.99 | | | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-24-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.86 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 06.00 | | | NO | N | N | 2 | 2 | | REAR-END | INATT | DYLG | DRY | I INJ | 02-20-2009 |
| 55 | 70 | 15 | 10 | 06.00 | | | NO | W | W | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DARK | DRY | PDO | 04-09-2009 |
| 55 | 70 | 15 | 10 | 06.00 | | | NO | S | - | 1 | | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | WET | PDO | 04-27-2009 |
| 55 | 70 | 15 | 10 | 06.00 | | | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | SLEEPY | DARK | DRY | P INJ | 02-07-2010 |
| 55 | 70 | 15 | 10 | 06.00 | | | NO | W | W | 2 | | | REAR-END | D-W-I | DARK | DRY | PDO | 04-03-2010 |
| 55 | 70 | 15 | 10 | 06.00 | | | NO | - | - | 4 | 2 | 1 | HEAD-ON | WRNG-WAY | DARK | WET | FAT | 04-17-2010 |
| 55 | 70 | 15 | 10 | 06.00 | | | NO | W | W | 3 | 1 | | OTHER | FOL-CLOSE | DYLG | DRY | P INJ | 01-19-2011 |
| 55 | 70 | 15 | 10 | 06.00 | | | NO | N | N | 2 | 2 | | HEAD-ON | D-W-I | DARK | DRY | P INJ | 03-27-2011 |
| 55 | 70 | 15 | 10 | 06.00 | | | NO | W | W | 2 | | | REAR-END | IMP-LN-CHG | DYLG | WET | PDO | 04-13-2012 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: I-40, 126.87, I-40 WB OP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 06.01 | I-40 WB OP | BRIDGE | NO | N | - | 1 | | | F-O RET-WALL | NO-IMP-ACT | DYLG | DRY | PDO | 03-06-2010 |
| 55 | 70 | 15 | 10 | 06.01 | I-40 WB OP | BRIDGE | NO | N | - | 1 | | | F-O BARR-CONCRETE | INATT | DARK | DRY | PDO | 05-25-2010 |
| 55 | 70 | 15 | 10 | 06.01 | I-40 WB OP | BRIDGE | NO | E | E | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 06-16-2011 |
| 55 | 70 | 15 | 10 | 06.01 | I-40 WB OP | BRIDGE | NO | N | - | 1 | 1 | | F-O BARR-CONCRETE | D-W-I | DYLG | DRY | I INJ | 07-04-2011 |
| 55 | 70 | 15 | 10 | 06.01 | I-40 WB OP | LOOP | NO | E | - | 1 | 1 | | F-O TRAFF-SIGN | UNSAF-SPD | DYLG | DRY | N-I INJ | 04-29-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.88 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 06.02 | | | NO | N | N | 2 | 1 | | SIDESWIPE-SAME | UNSAF-SPD | DYLG | ICE | P INJ | 03-20-2010 |
| 55 | 70 | 15 | 10 | 06.02 | | | NO | E | E | 2 | 2 | | REAR-END | UNSAF-SPD | DARK | DRY | N-I INJ | 10-12-2010 |
| 55 | 70 | 15 | 10 | 06.02 | | WKZONE | NO | W | W | 4 | 3 | | OTHER | FOL-CLOSE | DARK | DRY | P INJ | 04-16-2011 |
| 55 | 70 | 15 | 10 | 06.02 | | | NO | N | N | 2 | | | REAR-END | INATT | DARK | DRY | PDO | 07-15-2012 |
| 55 | 70 | 15 | 10 | 06.02 | | WKZONE | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 08-07-2012 |
| 55 | 70 | 15 | 10 | 06.02 | | | NO | W | W | 3 | | | OTHER | FOL-CLOSE | DARK | DRY | PDO | 11-19-2012 |
| 55 | 70 | 15 | 10 | 06.02 | | | NO | E | E | 2 | 1 | | REAR-END | INATT | DYLG | DRY | P INJ | 07-24-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.89 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 06.03 | | | NO | N | N | 2 | | | HEAD-ON | UNSAF-SPD | DYLG | ICE | PDO | 12-09-2008 |
| 55 | 70 | 15 | 10 | 06.03 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-29-2010 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|-------------|------------------------|--------------|--------|---------|--------|-------------|--------|----------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 06.03 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-15-2010 |
| 55 | 70 | 15 | 10 | 06.03 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 05-18-2011 |
| 55 | 70 | 15 | 10 | 06.03 | | WKZONE | NO | W | - | 1 | 2 | | F-O BARR-CONCRETE | INATT | DYLG | DRY | I INJ | 07-14-2012 |
| 55 | 70 | 15 | 10 | 06.03 | | | NO | E | E | 2 | | | REAR-END | UNSAF-SPD | DYLG | ICE | PDO | 11-22-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | AT: I-40, | | 126.90, | | I-40 EB ENT | | | | | | | |
| 55 | 70 | 15 | 10 | 06.04 | I-40 EB ENT | M/L RAMP MRG | NO | N | - | 1 | 1 | | PEDESTRIAN | INATT | DYLG | DRY | I INJ | 11-17-2008 |
| 55 | 70 | 15 | 10 | 06.04 | I-40 EB ENT | RAMP | NO | N | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DARK | DRY | P INJ | 01-10-2009 |
| 55 | 70 | 15 | 10 | 06.04 | I-40 EB ENT | RAMP | NO | E | - | 1 | 2 | | ROLLOVER | UNSAF-SPD | DARK | DRY | N-I INJ | 04-25-2009 |
| 55 | 70 | 15 | 10 | 06.04 | I-40 EB ENT | RAMP | NO | S | N | 2 | | | OTHER | DEF-VEH | DYLG | DRY | PDO | 06-30-2009 |
| 55 | 70 | 15 | 10 | 06.04 | I-40 EB ENT | M/L RAMP MRG | NO | E | E | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 04-02-2010 |
| 55 | 70 | 15 | 10 | 06.04 | I-40 EB ENT | M/L RAMP MRG WKZONE | NO | E | - | 1 | | | OTH-SINGLE-VEH | UNSAF-SPD | DYLG | WET | PDO | 12-14-2011 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | AT: 126.91 | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 06.05 | | RAMP | NO | W | W | 2 | 2 | | SIDESWIPE-SAME | NEG-DRVING | DYLG | DRY | N-I INJ | 11-12-2008 |
| 55 | 70 | 15 | 10 | 06.05 | | | NO | S | S | 2 | 2 | | REAR-END | IMP-LN-CHG | DYLG | DRY | P INJ | 12-12-2008 |
| 55 | 70 | 15 | 10 | 06.05 | | | NO | W | W | 3 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 02-12-2009 |
| 55 | 70 | 15 | 10 | 06.05 | | X-RD/RMP MRG | NO | W | W | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 03-29-2009 |
| 55 | 70 | 15 | 10 | 06.05 | | X-RD/RMP MRG | NO | W | - | 1 | | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | DRY | PDO | 01-21-2010 |
| 55 | 70 | 15 | 10 | 06.05 | | X-RD/RMP MRG | NO | W | W | 2 | 1 | | REAR-END | FOL-CLOSE | DUSK | WET | P INJ | 03-08-2010 |
| 55 | 70 | 15 | 10 | 06.05 | | RAMP | NO | W | - | 1 | | | F-O BARR-CONCRETE | UNSAF-SPD | DARK | WET | PDO | 03-19-2010 |
| 55 | 70 | 15 | 10 | 06.05 | | X-RD/RMP GOR | NO | S | S | 2 | 4 | | REAR-END | FOL-CLOSE | DARK | DRY | P INJ | 01-26-2011 |
| 55 | 70 | 15 | 10 | 06.05 | | X-RD/RMP GOR | NO | S | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DARK | ICE | P INJ | 02-03-2011 |
| 55 | 70 | 15 | 10 | 06.05 | | LOOP | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | NO-IMP-ACT | DYLG | DRY | N-I INJ | 02-19-2011 |
| 55 | 70 | 15 | 10 | 06.05 | | TURN LN MRGE | NO | W | W | 2 | | | ANGLE-OTHER | F-YIELD | DYLG | DRY | PDO | 11-03-2011 |
| 55 | 70 | 15 | 10 | 06.05 | | RAMP WKZONE | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | DRY | N-I INJ | 05-05-2012 |
| 55 | 70 | 15 | 10 | 06.05 | | X-RD/RMP GOR | NO | S | S | 3 | 1 | | HEAD-ON | FOL-CLOSE | DYLG | DRY | N-I INJ | 03-06-2013 |
| 55 | 70 | 15 | 10 | 06.05 | | X-RD/RMP MRG | NO | W | W | 3 | 2 | | REAR-END | UNSAF-SPD | DYLG | WET | N-I INJ | 07-26-2013 |
| 55 | 70 | 15 | 10 | 06.05 | | RAMP | NO | W | - | 1 | | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | WET | PDO | 09-19-2013 |
| 55 | 70 | 15 | 10 | 06.05 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-04-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | AT: 126.92 | | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 06.06 | | | NO | W | W | 2 | 1 | | REAR-END | IMP-LN-CHG | DYLG | DRY | P INJ | 12-19-2008 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|------|--------|-----------------|----------|----------|--------------|-----------|--------|--------|--------|------------|----------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 06.06 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 12-20-2008 |
| 55 | 70 | 15 | 10 | 06.06 | | | NO | W | W | 3 | | | REAR-END | OTHER | DYLG | DRY | PDO | 01-09-2009 |
| 55 | 70 | 15 | 10 | 06.06 | | | NO | E | E | 2 | | | OTHER | NEG-DRVING | DYLG | DRY | PDO | 02-19-2009 |
| 55 | 70 | 15 | 10 | 06.06 | | | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-22-2010 |
| 55 | 70 | 15 | 10 | 06.06 | | | NO | E | E | 2 | | | SIDESWIPE-SAME | NO-IMP-ACT | DARK | DRY | PDO | 10-31-2012 |
| 55 | 70 | 15 | 10 | 06.06 | | | NO | E | E | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DAWN | DRY | PDO | 11-05-2012 |
| 55 | 70 | 15 | 10 | 06.06 | | | NO | W | W | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-14-2012 |
| 55 | 70 | 15 | 10 | 06.06 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-06-2013 |
| (55) OKLAHOMA | | | | (70) OKLA. CITY | | | | HWY: I-35 | | | | AT: 126.93 | | | | | | |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-26-2008 |
| 55 | 70 | 15 | 10 | 06.07 | | WKZONE | NO | W | W | 2 | 1 | | HEAD-ON | UNSAF-SPD | DARK | DRY | N-I INJ | 12-05-2008 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 3 | 2 | | HEAD-ON | FOL-CLOSE | DARK | DRY | I INJ | 12-05-2008 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 04-24-2009 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-17-2009 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | E | E | 2 | | | OTHER | NEG-DRVING | DYLG | DRY | PDO | 07-29-2009 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 01-13-2010 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-01-2010 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | E | E | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | DRY | PDO | 03-24-2010 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | E | E | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 06-17-2010 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 3 | | | OTHER | IMP-START | DYLG | DRY | PDO | 08-06-2010 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | N | N | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 09-07-2010 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | S | - | 1 | | | F-O BARR-CONCRETE | NO-IMP-ACT | DARK | DRY | PDO | 10-13-2010 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | - | 1 | 1 | | F-O RET-WALL | UNSAF-SPD | DARK | DRY | P INJ | 12-21-2010 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | E | E | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 09-28-2011 |
| 55 | 70 | 15 | 10 | 06.07 | | WKZONE | NO | E | E | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 10-25-2011 |
| 55 | 70 | 15 | 10 | 06.07 | | WKZONE | NO | N | - | 1 | | | F-O RET-WALL | D-W-I | DARK | DRY | PDO | 11-05-2011 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 03-17-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | WET | N-I INJ | 03-19-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | N | - | 1 | 2 | | F-O BARR-CONCRETE | IMP-LN-CHG | DYLG | DRY | P INJ | 03-20-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | WKZONE | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | DRY | I INJ | 03-24-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | I INJ | 03-30-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 03-31-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-28-2012 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|----------|---------------------|--------------|------------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 06-15-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 3 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 07-18-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 09-03-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 09-08-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | 1 | | REAR-END | IMP-LN-CHG | DYLG | DRY | P INJ | 10-05-2012 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | - | 1 | | | F-O TRAFF-SIGN | UNSAF-SPD | DARK | DRY | PDO | 02-27-2013 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | E | E | 2 | | | HEAD-ON | IMP-LN-CHG | DARK | DRY | PDO | 09-21-2013 |
| 55 | 70 | 15 | 10 | 06.07 | | | NO | W | W | 2 | | | HEAD-ON | IMP-LN-CHG | DYLG | DRY | PDO | 10-07-2013 |
| 55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | | AT: 126.95 | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 06.09 | | | NO | W | W | 2 | | | REAR-END | UNSAF-SPD | DYLG | WET | PDO | 09-15-2011 |
| 55 | 70 | 15 | 10 | 06.09 | | | NO | W | W | 2 | 2 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 06-24-2013 |
| 55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | | AT: 126.96 | | | | | | | | | | |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | E | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | ICE | N-I INJ | 12-09-2008 |
| 55 | 70 | 15 | 10 | 06.10 | | RAMP/RAMP JC | NO | N | - | 1 | | | ROLLOVER | D-W-I | DARK | DRY | PDO | 01-03-2009 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | W | W | 2 | | | OTHER | OTHER | DYLG | DRY | PDO | 01-30-2009 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | E | E | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 04-06-2009 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | E | E | 2 | | | SIDESWIPE-SAME | NEG-DRIVING | DYLG | DRY | PDO | 04-11-2009 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 04-23-2009 |
| 55 | 70 | 15 | 10 | 06.10 | | WKZONE RAMP/RAMP JC | NO | N | N | 2 | | | ANGLE-OTHER | F-YIELD | DARK | DRY | PDO | 07-13-2009 |
| 55 | 70 | 15 | 10 | 06.10 | | WKZONE RAMP/RAMP JC | NO | N | N | 2 | | | REAR-END | F-YIELD | DARK | DRY | PDO | 07-15-2009 |
| 55 | 70 | 15 | 10 | 06.10 | | WKZONE RAMP/RAMP JC | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-09-2009 |
| 55 | 70 | 15 | 10 | 06.10 | | WKZONE | NO | W | W | 2 | 1 | | REAR-END | D-W-I | DARK | DRY | P INJ | 10-15-2009 |
| 55 | 70 | 15 | 10 | 06.10 | | RAMP | NO | W | - | 1 | | | F-O DITCH | D-W-I | DARK | ICE | PDO | 01-31-2010 |
| 55 | 70 | 15 | 10 | 06.10 | | RAMP | NO | W | W | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 07-09-2010 |
| 55 | 70 | 15 | 10 | 06.10 | | WKZONE RAMP | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DUSK | DRY | PDO | 07-19-2010 |
| 55 | 70 | 15 | 10 | 06.10 | | RAMP | NO | N | - | 1 | 2 | | ROLLOVER | UNSAF-SPD | DARK | DRY | I INJ | 08-28-2010 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | W | W | 3 | 3 | | OTHER | FOL-CLOSE | DYLG | DRY | I INJ | 01-13-2011 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-13-2011 |
| 55 | 70 | 15 | 10 | 06.10 | | RAMP WKZONE | NO | N | N | 3 | | | REAR-END | INATT | DYLG | DRY | PDO | 11-30-2011 |
| 55 | 70 | 15 | 10 | 06.10 | | RAMP/RAMP JC | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 04-10-2012 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
|--------------------|----|----|----|-------|----------|--------------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| 55 | 70 | 15 | 10 | 06.10 | | RAMP WKZONE | NO | W | N | 2 | | | OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 05-12-2012 |
| 55 | 70 | 15 | 10 | 06.10 | | RAMP WKZONE | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DARK | DRY | P INJ | 10-03-2012 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | W | W | 3 | | | ANGLE-OTHER | FOL-CLOSE | DYLG | DRY | PDO | 11-19-2012 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | FOL-CLOSE | DARK | DRY | PDO | 12-12-2012 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 02-22-2013 |
| 55 | 70 | 15 | 10 | 06.10 | | X-RD/RMP GOR | NO | W | W | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 04-15-2013 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | W | W | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 07-28-2013 |
| 55 | 70 | 15 | 10 | 06.10 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-28-2013 |
| 55 | 70 | 15 | 10 | 06.10 | | RAMP/RAMP JC | NO | N | N | 2 | | | ANGLE-OTHER | UNSAF-SPD | DYLG | WET | PDO | 09-17-2013 |
| 55 | 70 | 15 | 10 | 06.10 | | X-RD/RMP GOR | NO | W | W | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 09-30-2013 |
| 55 | 70 | 15 | 10 | 06.10 | | X-RD/RMP GOR | NO | W | W | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 10-25-2013 |
| 55 | 70 | 15 | 10 | 06.10 | | RAMP | NO | N | N | 3 | | | REAR-END | NO-IMP-ACT | DARK | ICE | PDO | 12-20-2013 |

| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.97 | | | | | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
|--|----|----|----|-------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | FOL-CLOSE | DYLG | DRY | PDO | 10-23-2008 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 2 | 1 | | REAR-END | IMP-LN-CHG | DYLG | DRY | N-I INJ | 04-22-2010 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 2 | | | ANGLE-OTHER | UNSAF-SPD | DARK | DRY | PDO | 06-06-2010 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 01-05-2011 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-18-2011 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-04-2011 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-04-2011 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 2 | | | OTHER | NO-IMP-ACT | DYLG | DRY | PDO | 02-27-2012 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | - | 1 | 2 | | ROLLOVER | NO-IMP-ACT | DARK | DRY | I INJ | 11-02-2012 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 11-16-2012 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 01-19-2013 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 04-15-2013 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 3 | 4 | | REAR-END | FOL-CLOSE | DYLG | WET | N-I INJ | 07-14-2013 |
| 55 | 70 | 15 | 10 | 06.11 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | NO-IMP-ACT | DYLG | DRY | PDO | 10-23-2013 |

| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: I-40, 126.98, I-40 WB EXIT | | | | | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
|--|----|----|----|-------|--------------|--------------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | RAMP | NO | S | - | 1 | | | OTH-SINGLE-VEH | UNSAF-SPD | DARK | DRY | PDO | 08-07-2008 |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | RAMP | NO | W | W | 2 | | | SIDESWIPE-SAME | D-W-I | DARK | DRY | PDO | 10-03-2008 |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | RAMP | NO | S | S | 3 | | | HEAD-ON | FOL-CLOSE | DARK | DRY | PDO | 11-12-2008 |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | M/L RAMP GOR | NO | W | W | 3 | | | OTHER | FOL-CLOSE | DARK | DRY | PDO | 11-26-2008 |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | M/L RAMP MRG | NO | S | S | 2 | | | REAR-END | OTHER | DYLG | DRY | PDO | 05-21-2010 |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | M/L RAMP GOR | NO | S | - | 1 | | | ROLLOVER | UNSAF-SPD | DARK | DRY | PDO | 09-07-2010 |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | M/L RAMP GOR | NO | W | W | 2 | 1 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 11-28-2010 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|--------------|--------------|--------------|----------------------------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | M/L RAMP GOR | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DARK | WET | PDO | 11-14-2011 |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | RAMP WKZONE | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLGT | WET | PDO | 04-04-2012 |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | M/L RAMP MRG | NO | S | S | 2 | 1 | | HEAD-ON | DEF-VEH | DYLGT | DRY | N-I INJ | 06-15-2012 |
| 55 | 70 | 15 | 10 | 06.12 | I-40 WB EXIT | M/L RAMP MRG | NO | S | S | 2 | 2 | | REAR-END | IMP-LN-CHG | DYLGT | DRY | N-I INJ | 11-13-2013 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.05 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.05 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 09-16-2008 |
| 55 | 70 | 42 | 10 | 00.05 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 09-06-2011 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.10 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.10 | | | NO | N | - | 1 | 1 | | F-O GROUND | UNSAF-SPD | DYLGT | DRY | N-I INJ | 08-25-2008 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.14 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.14 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLGT | WET | PDO | 10-26-2013 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.15 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.15 | | | NO | S | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DARK | DRY | P INJ | 01-25-2011 |
| 55 | 70 | 42 | 10 | 00.15 | | WKZONE | NO | E | - | 1 | | | F-O SAND-BARRELS | NO-IMP-ACT | DARK | DRY | PDO | 08-30-2011 |
| 55 | 70 | 42 | 10 | 00.15 | | | NO | S | S | 2 | | | HEAD-ON | INATT | DYLGT | DRY | PDO | 10-07-2011 |
| 55 | 70 | 42 | 10 | 00.15 | | | NO | S | S | 2 | | | REAR-END | INATT | DARK | DRY | PDO | 11-15-2013 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.19 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.19 | | | NO | W | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DYLGT | DRY | I INJ | 03-25-2012 |
| 55 | 70 | 42 | 10 | 00.19 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 09-26-2013 |
| 55 | 70 | 42 | 10 | 00.19 | | | NO | N | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DYLGT | DRY | P INJ | 11-03-2013 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.22 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.22 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 02-20-2009 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.23 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.23 | | | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DARK | DRY | N-I INJ | 07-15-2012 |
| 55 | 70 | 42 | 10 | 00.23 | | M/L RAMP MRG | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | DRY | PDO | 12-03-2013 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.24 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.24 | | | NO | N | N | 2 | | | ANGLE-OTHER | D-W-I | DYLGT | DRY | PDO | 10-05-2013 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: I-40, 0.25, I-40 OP/EB | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.25 | I-40 OP/EB | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLGT | DRY | PDO | 03-17-2011 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.26 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.26 | | | NO | N | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DYLGT | WET | N-I INJ | 08-18-2012 |
| 55 OKLAHOMA | | (70) OKLA. CITY | | HWY: | | | | AT: 0.27 | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.27 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 09-18-2009 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|---|------|------|--------|-----------|------------|------------------------|--------------|--------|--------|--------|--------|--------|----------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 42 | 10 | 00.27 | | | NO | N | - | 1 | | | F-O BARR-CONCRETE | D-W-I | DARK | DRY | PDO | 12-11-2013 |
| 55 | 70 | 42 | 10 | 00.27 | | | NO | N | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DARK | DRY | N-I INJ | 12-26-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 0.28 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.28 | | WKZONE | NO | N | - | 1 | | | F-O IMPACT-ATTEN | UNSAF-SPD | DARK | WET | PDO | 03-10-2012 |
| 55 | 70 | 42 | 10 | 00.28 | | | NO | N | N | 2 | 1 | | REAR-END | INATT | DYLG | DRY | P INJ | 08-21-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: I-40, 0.29, I-40 UP/WB | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 42 | 10 | 00.29 | I-40 UP/WB | M/L RAMP MRG WKZONE | NO | N | - | 1 | | | F-O IMPACT-ATTEN | IMP-TURN | DARK | DRY | PDO | 06-30-2011 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.00, MM 151 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 09.99 | MM 151 | | NO | W | W | 2 | 4 | | REAR-END | UNSAF-SPD | DYLG | DRY | I INJ | 10-02-2011 |
| 55 | 70 | 69 | 10 | 09.99 | MM 151 | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | WET | PDO | 10-09-2011 |
| 55 | 70 | 69 | 10 | 09.99 | MM 151 | | NO | W | W | 2 | 1 | | OTHER | FOL-CLOSE | DARK | DRY | I INJ | 11-19-2011 |
| 55 | 70 | 69 | 10 | 09.99 | MM 151 | | NO | W | W | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 12-23-2011 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.01 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.00 | | | NO | W | W | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 12-09-2008 |
| 55 | 70 | 69 | 10 | 10.00 | | WKZONE | NO | W | W | 2 | | | OTHER | UNSAF-SPD | DYLG | WET | PDO | 04-24-2011 |
| 55 | 70 | 69 | 10 | 10.00 | | | NO | W | W | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 12-27-2011 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.02 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.01 | | | NO | E | E | 2 | 3 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 11-20-2009 |
| 55 | 70 | 69 | 10 | 10.01 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 10-08-2010 |
| 55 | 70 | 69 | 10 | 10.01 | | | NO | E | E | 2 | | | REAR-END | UNSAF-SPD | DYLG | WET | PDO | 12-03-2011 |
| 55 | 70 | 69 | 10 | 10.01 | | | NO | E | E | 2 | 2 | | HEAD-ON | IMP-TURN | DYLG | DRY | P INJ | 04-26-2012 |
| 55 | 70 | 69 | 10 | 10.01 | | | NO | E | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | DRY | N-I INJ | 10-25-2012 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.03 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | W | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | WET | N-I INJ | 01-28-2009 |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | W | W | 2 | | | REAR-END | DEF-VEH | DYLG | DRY | PDO | 09-09-2009 |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | E | E | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 03-17-2010 |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-01-2010 |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | W | W | 3 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 01-15-2011 |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | W | W | 2 | | | OTHER | NEG-DRVING | DYLG | DRY | PDO | 01-23-2011 |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 03-29-2011 |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-02-2011 |
| 55 | 70 | 69 | 10 | 10.02 | | WKZONE | NO | E | E | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 08-03-2011 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|---|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|----------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 69 | 10 | 10.02 | | WKZONE | NO | E | E | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 09-08-2011 |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-27-2011 |
| 55 | 70 | 69 | 10 | 10.02 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-05-2012 |
| 55 | 70 | 69 | 10 | 10.02 | | WKZONE | NO | E | - | 1 | | 1 | ROLLOVER | NO-IMP-ACT | DYLG | DRY | FAT | 08-10-2012 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.12 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.11 | | | NO | E | E | 4 | 6 | | REAR-END | UNSAF-SPD | DYLG | DRY | I INJ | 05-17-2010 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.13 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | E | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 08-26-2008 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | E | 2 | | | REAR-END | UNSAF-SPD | DYLG | ICE | PDO | 12-09-2008 |
| 55 | 70 | 69 | 10 | 10.12 | | INCIDENT | NO | E | W | 2 | | | HEAD-ON | DEF-VEH | DARK | DRY | PDO | 04-15-2009 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-28-2009 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | E | 3 | 1 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 06-09-2009 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | W | - | 1 | | | F-O TRAFF-SIGN | UNSAF-SPD | DYLG | ICE | PDO | 12-24-2009 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | D-W-I | DUSK | WET | PDO | 02-02-2010 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | E | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 02-19-2010 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | E | 4 | 2 | | HEAD-ON | FOL-CLOSE | DYLG | DRY | P INJ | 05-26-2010 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | E | - | 1 | 1 | | F-O BARR-CONCRETE | SLEEPY | DARK | DRY | N-I INJ | 01-21-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-06-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 05-25-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | W | W | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 05-31-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | W | W | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 06-05-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | DRY | PDO | 06-30-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | E | 5 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 07-18-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-18-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | E | E | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 09-01-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-21-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | W | - | 1 | | | F-O BARR-CONCRETE | INATT | DARK | DRY | PDO | 10-25-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | W | - | 1 | | | F-O BARR-CONCRETE | INATT | DYLG | DRY | PDO | 11-14-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | E | E | 2 | | | REAR-END | NEG-DRVING | DARK | DRY | PDO | 12-09-2011 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | - | 1 | | | F-O BARR-CONCRETE | UNSAF-SPD | DYLG | WET | PDO | 05-11-2012 |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | E | 2 | | | OTHER | NO-IMP-ACT | DYLG | DRY | PDO | 07-22-2012 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | W | - | 1 | | | OTH-SINGLE-VEH | NO-IMP-ACT | DYLG | DRY | PDO | 11-13-2012 |
| 55 | 70 | 69 | 10 | 10.12 | | WKZONE | NO | W | W | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 03-26-2013 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 69 | 10 | 10.12 | | | NO | E | E | 2 | 1 | | REAR-END | IMP-LN-CHG | DYLG | DRY | P INJ | 08-17-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.19 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.18 | | WKZONE | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-26-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.21 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.20 | | INCIDENT | NO | E | E | 2 | | | OTHER | NEG-DRVING | DYLG | DRY | PDO | 10-04-2008 |
| 55 | 70 | 69 | 10 | 10.20 | | | NO | W | W | 3 | 1 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 01-18-2010 |
| 55 | 70 | 69 | 10 | 10.20 | | | NO | E | - | 1 | 1 | | F-O | NEG-DRVING | DARK | DRY | I INJ | 03-06-2012 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.22 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.21 | | WKZONE | NO | E | E | 2 | | | OTHER | UNSAF-SPD | DYLG | DRY | PDO | 06-17-2011 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.32 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.31 | | | NO | W | W | 2 | | | REAR-END | IMP-TURN | DYLG | WET | PDO | 04-29-2009 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.33 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.32 | | | NO | W | W | 2 | | | REAR-END | UNSAF-SPD | DARK | DRY | PDO | 08-29-2008 |
| 55 | 70 | 69 | 10 | 10.32 | | | NO | W | W | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 06-11-2011 |
| 55 | 70 | 69 | 10 | 10.32 | | | NO | E | E | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 08-30-2011 |
| 55 | 70 | 69 | 10 | 10.32 | | WKZONE | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-25-2011 |
| 55 | 70 | 69 | 10 | 10.32 | | | NO | E | E | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 02-23-2012 |
| 55 | 70 | 69 | 10 | 10.32 | | | NO | W | W | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 05-24-2012 |
| 55 | 70 | 69 | 10 | 10.32 | | | NO | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-21-2012 |
| 55 | 70 | 69 | 10 | 10.32 | | | NO | W | W | 3 | 3 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 02-16-2013 |
| 55 | 70 | 69 | 10 | 10.32 | | | NO | W | W | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 09-30-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: AT: 151.43 | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 69 | 10 | 10.42 | | | NO | W | W | 3 | 1 | | OTHER | UNSAF-SPD | DYLG | DRY | P INJ | 10-28-2008 |
| 55 | 70 | 69 | 10 | 10.42 | | | NO | W | W | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-01-2008 |
| 55 | 70 | 69 | 10 | 10.42 | | | NO | E | E | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 06-10-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 125.77, 00.02 after 15 ST. S NB EXIT | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | | | OTHER | OTHER | DARK | DRY | PDO | 10-24-2008 |
| 55 | 70 | 15 | | 04.96 | | WKZONE | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 02-24-2009 |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | REAR-END | F-YIELD | DYLG | DRY | PDO | 11-18-2009 |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 11-25-2009 |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 01-21-2010 |
| 55 | 70 | 15 | | 04.96 | | | NO | N | - | 1 | 2 | | F-O | UNSAF-SPD | DARK | DRY | I INJ | 05-02-2010 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 125.77, 00.02 after 15 ST. S NB EXIT | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | - | 1 | 1 | | OTH-SINGLE-VEH | DEF-VEH | DYLG | DRY | N-I INJ | 06-08-2010 |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | WET | PDO | 07-06-2010 |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | 2 | | ANGLE-OTHER | D-W-I | DARK | DRY | P INJ | 09-18-2010 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|----------|----------|--------------|--------|--------|------------------------------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|--|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | | | ANGLE-OTHER | NEG-DRVING | DYLG | DRY | PDO | 10-29-2010 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-11-2011 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 06-21-2011 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 07-11-2011 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | S | 2 | 1 | | OTHER | DEF-VEH | DYLG | DRY | P INJ | 07-17-2011 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 08-19-2011 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 4 | 4 | | OTHER | INATT | DAWN | DRY | P INJ | 08-30-2011 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DARK | DRY | N-I INJ | 09-08-2011 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | - | 1 | 1 | | ROLLOVER | NO-IMP-ACT | DYLG | DRY | N-I INJ | 11-28-2011 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 02-29-2012 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 03-02-2012 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 05-18-2012 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-18-2012 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 07-20-2012 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 08-27-2012 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DARK | DRY | N-I INJ | 11-16-2012 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-06-2013 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | | | HEAD-ON | IMP-LN-CHG | DYLG | DRY | PDO | 03-15-2013 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | WET | P INJ | 05-15-2013 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-01-2013 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 08-25-2013 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-20-2013 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | DRY | PDO | 10-27-2013 | |
| 55 | 70 | 15 | | 04.96 | | | NO | S | S | 2 | | | REAR-END | NEG-DRVING | DYLG | DRY | PDO | 11-02-2013 | |
| 55 | 70 | 15 | | 04.96 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DARK | DRY | PDO | 12-12-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 125.78, | | | 00.03 after 15 ST. S NB EXIT | | | | | | | | | |
| 55 | 70 | 15 | | 04.97 | | | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | WET | PDO | 03-12-2009 | |
| 55 | 70 | 15 | | 04.97 | | | NO | N | - | 1 | 2 | | ROLLOVER | INATT | DYLG | DRY | I INJ | 07-01-2009 | |
| 55 | 70 | 15 | | 04.97 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 11-12-2009 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 125.80, | | | 00.05 after 15 ST. S NB EXIT | | | | | | | | | |
| 55 | 70 | 15 | | 04.99 | | | NO | E | - | 1 | 1 | | F-O BARR-CONCRETE | NO-IMP-ACT | DARK | DRY | I INJ | 04-01-2012 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 125.81, | | | 00.06 after 15 ST. S NB EXIT | | | | | | | | | |
| 55 | 70 | 15 | | 05.00 | | WKZONE | NO | N | N | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | DRY | PDO | 05-16-2011 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 125.82, | | | 00.05 before 15 ST. S UP | | | | | | | | | |
| 55 | 70 | 15 | | 05.01 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 03-28-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 125.83, | | | 00.04 before 15 ST. S UP | | | | | | | | | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|-------------|--------------|--------------|-------------|--------|--------|--------------------------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | | 05.02 | | | NO | N | N | 3 | | | OTHER | INATT | DYLG | DRY | PDO | 10-19-2008 |
| 55 | 70 | 15 | | 05.02 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 11-04-2011 |
| 55 | 70 | 15 | | 05.02 | | FR BTN RMP/L | NO | | - | 1 | | | F-O BARR-OTHER | D-W-I | DUSK | DRY | PDO | 02-11-2013 |
| 55 | 70 | 15 | | 05.02 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DUSK | DRY | PDO | 03-06-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | | AT: 125.85, | | | 00.02 before 15 ST. S UP | | | | | | | |
| 55 | 70 | 15 | | 05.04 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 05-29-2009 |
| 55 | 70 | 15 | | 05.04 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-27-2012 |
| 55 | 70 | 15 | | 05.04 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-11-2013 |
| 55 | 70 | 15 | | 05.04 | | | NO | E | E | 2 | | | ANGLE-TURNING | OTHER | DYLG | DRY | PDO | 07-06-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | | AT: 125.86, | | | 00.01 before 15 ST. S UP | | | | | | | |
| 55 | 70 | 15 | | 05.05 | | | NO | N | N | 4 | 1 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 08-13-2008 |
| 55 | 70 | 15 | | 05.05 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-12-2008 |
| 55 | 70 | 15 | | 05.05 | | INCIDENT | NO | S | - | 1 | 1 | | OTH-SINGLE-VEH | DEF-VEH | DARK | DRY | N-I INJ | 03-21-2009 |
| 55 | 70 | 15 | | 05.05 | | | NO | S | S | 2 | 4 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 04-10-2009 |
| 55 | 70 | 15 | | 05.05 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | WET | PDO | 04-17-2009 |
| 55 | 70 | 15 | | 05.05 | | | NO | N | N | 2 | | | REAR-END | OTHER | DYLG | DRY | PDO | 04-25-2009 |
| 55 | 70 | 15 | | 05.05 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | WET | P INJ | 10-06-2009 |
| 55 | 70 | 15 | | 05.05 | | | NO | S | S | 2 | | | ANGLE-OTHER | FOL-CLOSE | DYLG | DRY | PDO | 12-21-2009 |
| 55 | 70 | 15 | | 05.05 | | | NO | N | N | 4 | | | REAR-END | FOL-CLOSE | DYLG | WET | PDO | 06-03-2010 |
| 55 | 70 | 15 | | 05.05 | | INCIDENT | NO | S | S | 2 | | | OTHER | NEG-DRVING | DYLG | DRY | PDO | 06-25-2010 |
| 55 | 70 | 15 | | 05.05 | | | NO | S | S | 2 | 1 | | REAR-END | IMP-LN-CHG | DARK | DRY | N-I INJ | 10-17-2010 |
| 55 | 70 | 15 | | 05.05 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-23-2011 |
| 55 | 70 | 15 | | 05.05 | | | NO | S | S | 2 | | | REAR-END | IMP-BACK | DARK | DRY | PDO | 10-08-2012 |
| 55 | 70 | 15 | | 05.05 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-07-2012 |
| 55 | 70 | 15 | | 05.05 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | DRY | PDO | 06-23-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | | AT: 125.87, | | | 15 ST. S UP | | | | | | | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | W | S | 2 | 1 | | HEAD-ON | F-STOP | DYLG | DRY | I INJ | 08-14-2008 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | N | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 08-16-2008 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | S | W | 2 | | | RIGHT-ANGLE | F-STOP | DYLG | DRY | PDO | 08-25-2008 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | E | E | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DARK | DRY | PDO | 11-09-2008 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | S | E | 2 | 1 | | ANGLE-TURNING | D-W-I | DARK | DRY | P INJ | 01-15-2009 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | BRIDGE | NO | N | - | 1 | | | F-O BR-PIER | OTHER | DARK | DRY | PDO | 04-09-2009 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | W | W | 2 | 1 | | REAR-END | OTHER | DYLG | DRY | P INJ | 04-27-2009 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | W | W | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 05-27-2009 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | W | E | 2 | 1 | | ANGLE-TURNING | F-YIELD | DARK | WET | P INJ | 09-16-2009 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | W | N | 2 | | | ANGLE-TURNING | OTHER | DARK | DRY | PDO | 10-20-2009 |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | E | N | 2 | | | RIGHT-ANGLE | F-YIELD | DYLG | DRY | PDO | 11-06-2009 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

USE REPORT

| | | | | HIGHWAY COLLISIONS | | | | | | | | | | | | | | | |
|---------------|------|------|--------|--------------------|-------------|--------------|--------------|-----------|--------|--------|--------|-------------|-------------------|-----------------|----------------|-------------------------|----------|------------|--|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | E | 2 | | | ANGLE-TURNING | F-STOP | DYLGT | DRY | PDO | 11-12-2009 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | E | E | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 11-13-2009 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | W | E | 2 | 1 | | ANGLE-TURNING | NO-IMP-ACT | DARK | DRY | P INJ | 01-09-2010 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DYLGT | DRY | P INJ | 01-20-2010 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | N | 2 | | | ANGLE-TURNING | IMP-TURN | DYLGT | DRY | PDO | 02-10-2010 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | - | - | 2 | 2 | | ANGLE-TURNING | F-YIELD | DARK | DRY | N-I INJ | 04-04-2010 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 08-21-2010 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | S | E | 3 | 1 | | RIGHT-ANGLE | OTHER | DYLGT | DRY | N-I INJ | 10-09-2010 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | E | W | 2 | | | ANGLE-TURNING | F-YIELD | DARK | DRY | PDO | 12-09-2010 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | N | 2 | | | REAR-END | DEF-VEH | DYLGT | DRY | PDO | 10-12-2011 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | E | W | 2 | | | ANGLE-TURNING | F-YIELD | DYLGT | DRY | PDO | 10-20-2011 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | S | W | 2 | 2 | | RIGHT-ANGLE | F-STOP | DYLGT | DRY | N-I INJ | 12-09-2011 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | S | W | 2 | | | RIGHT-ANGLE | F-YIELD | DYLGT | DRY | PDO | 01-20-2012 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | E | - | 1 | 1 | | F-O POLE-OTHER | OTHER | DARK | WET | P INJ | 02-18-2012 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | - | 1 | | | ROLLOVER | NEG-DRIVING | DUSK | DRY | PDO | 04-03-2012 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | W | 2 | | | RIGHT-ANGLE | F-STOP | DYLGT | DRY | PDO | 04-04-2012 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | W | W | 2 | 2 | | OTHER | FOL-CLOSE | DYLGT | DRY | N-I INJ | 05-28-2012 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | W | S | 2 | | | HEAD-ON | F-STOP | DYLGT | DRY | PDO | 09-28-2012 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | - | 1 | | | F-O FENCE | F-STOP | DARK | DRY | PDO | 10-28-2012 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC LFT | YES | S | W | 2 | 1 | | RIGHT-ANGLE | F-YIELD | DYLGT | DRY | I INJ | 11-05-2012 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | E | 3 | 5 | | RIGHT-ANGLE | F-STOP | DYLGT | DRY | N-I INJ | 09-05-2013 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | W | E | 2 | | | ANGLE-TURNING | F-YIELD | DARK | DRY | PDO | 09-15-2013 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | W | W | 3 | 2 | | ANGLE-OTHER | INATT | DYLGT | DRY | P INJ | 10-18-2013 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | N | E | 2 | | | RIGHT-ANGLE | F-STOP | DYLGT | WET | PDO | 11-05-2013 | |
| 55 | 70 | 15 | | 05.06 | 15 ST. S UP | TERM LOC RIT | YES | W | E | 2 | | | ANGLE-TURNING | L-CENTER | DARK | WET | PDO | 12-20-2013 | |
| (55) OKLAHOMA | | | | (70) OKLA. CITY | | | | HWY: I-35 | | | | AT: 125.88, | | | | 00.01 after 15 ST. S UP | | | |
| 55 | 70 | 15 | | 05.07 | | | NO | S | S | 2 | | | REAR-END | NO-IMP-ACT | DYLGT | DRY | PDO | 09-12-2008 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 11-26-2008 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 3 | 3 | | OTHER | INATT | DYLGT | DRY | P INJ | 11-26-2008 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 01-06-2009 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DAWN | DRY | PDO | 01-22-2009 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 4 | 1 | | REAR-END | UNSAF-SPD | DYLGT | DRY | P INJ | 02-17-2009 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 5 | 1 | | OTHER | UNSAF-SPD | DAWN | DRY | P INJ | 02-19-2009 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | 2 | | REAR-END | IMP-LN-CHG | DYLGT | DRY | I INJ | 03-15-2009 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 03-15-2009 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 3 | | | ANGLE-OTHER | FOL-CLOSE | DYLGT | DRY | PDO | 03-19-2009 | |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 4 | | | OTHER | FOL-CLOSE | DYLGT | DRY | PDO | 03-25-2009 | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | OTHER | | DYLG | DRY | PDO | 05-31-2009 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | 1 | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | P INJ | 07-07-2009 |
| 55 | 70 | 15 | | 05.07 | | | NO | S | - | 1 | 3 | | ROLLOVER | NEG-DRVING | DYLG | DRY | I INJ | 08-28-2009 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-24-2009 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 11-25-2009 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 12-01-2009 |
| 55 | 70 | 15 | | 05.07 | | INCIDENT | NO | N | N | 2 | | | OTHER | NEG-DRVING | DYLG | DRY | PDO | 06-22-2010 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-01-2010 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 07-27-2010 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | 1 | | REAR-END | IMP-LN-CHG | DARK | DRY | N-I INJ | 12-06-2010 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 3 | 1 | | OTHER | FOL-CLOSE | DYLG | DRY | P INJ | 01-18-2011 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-06-2011 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 09-21-2011 |
| 55 | 70 | 15 | | 05.07 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-25-2012 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-25-2012 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-14-2013 |
| 55 | 70 | 15 | | 05.07 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-27-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 125.89, 00.02 after 15 ST. S UP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.08 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 12-10-2008 |
| 55 | 70 | 15 | | 05.08 | | | NO | N | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DARK | DRY | N-I INJ | 03-14-2009 |
| 55 | 70 | 15 | | 05.08 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-27-2010 |
| 55 | 70 | 15 | | 05.08 | | | NO | S | E | 2 | | | ANGLE-TURNING | IMP-TURN | DYLG | DRY | PDO | 10-19-2012 |
| 55 | 70 | 15 | | 05.08 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 03-27-2013 |
| 55 | 70 | 15 | | 05.08 | | | NO | N | N | 2 | | | OTHER | OTHER | DYLG | OTHER | PDO | 04-25-2013 |
| 55 | 70 | 15 | | 05.08 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-12-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 125.90, 00.03 after 15 ST. S UP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.09 | | | NO | N | N | 2 | 1 | | REAR-END | UNSAF-SPD | DARK | DRY | N-I INJ | 09-09-2008 |
| 55 | 70 | 15 | | 05.09 | | | NO | N | N | 4 | | | HEAD-ON | FOL-CLOSE | DARK | DRY | PDO | 01-21-2009 |
| 55 | 70 | 15 | | 05.09 | | | NO | S | S | 2 | 1 | | REAR-END | INATT | DARK | DRY | P INJ | 11-18-2010 |
| 55 | 70 | 15 | | 05.09 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 04-22-2011 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 125.91, 00.04 after 15 ST. S UP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.10 | | WKZONE | NO | N | N | 3 | 1 | | OTHER | FOL-CLOSE | DYLG | DRY | N-I INJ | 11-01-2011 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 125.92, 00.05 after 15 ST. S UP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.11 | | | NO | N | N | 5 | 1 | | REAR-END | UNSAF-SPD | DYLG | DRY | N-I INJ | 12-19-2008 |
| 55 | 70 | 15 | | 05.11 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-17-2012 |
| 55 | 70 | 15 | | 05.11 | | | NO | S | S | 3 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 02-08-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 125.93, 00.06 after 15 ST. S UP | | | | | | | | | | | | | | | | | | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|----------|----------|--------------|--------|--------|---------------------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|--|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date | |
| 55 | 70 | 15 | | 05.12 | | | NO | N | N | 3 | 1 | | REAR-END | UNSAF-SPD | DYLGT | WET | P INJ | 09-16-2009 | |
| 55 | 70 | 15 | | 05.12 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DARK | DRY | N-I INJ | 03-18-2012 | |
| 55 | 70 | 15 | | 05.12 | | | NO | N | N | 2 | 2 | | REAR-END | FOL-CLOSE | DYLGT | DRY | P INJ | 08-30-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 125.95, | | | 00.05 before MM 126 | | | | | | | | | |
| 55 | 70 | 15 | | 05.14 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 01-17-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 125.96, | | | 00.04 before MM 126 | | | | | | | | | |
| 55 | 70 | 15 | | 05.15 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 11-27-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 125.97, | | | 00.03 before MM 126 | | | | | | | | | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 08-12-2008 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | - | 1 | 1 | | F-O RET-WALL | NO-IMP-ACT | DARK | DRY | P INJ | 10-10-2008 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | HEAD-ON | INATT | DARK | DRY | P INJ | 10-10-2008 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DUSK | DRY | PDO | 11-14-2008 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DAWN | DRY | PDO | 11-20-2008 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 01-07-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | INATT | DYLGT | DRY | PDO | 01-21-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 01-22-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DARK | DRY | PDO | 01-23-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | HEAD-ON | IMP-LN-CHG | DARK | DRY | P INJ | 01-30-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 4 | | | OTHER | FOL-CLOSE | DYLGT | DRY | PDO | 04-03-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLGT | DRY | PDO | 05-12-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | N-I INJ | 06-29-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 07-22-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | ANGLE-OTHER | UNSAF-SPD | DYLGT | WET | PDO | 07-30-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | 1 | | ANGLE-OTHER | UNSAF-SPD | DYLGT | DRY | I INJ | 08-21-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DARK | DRY | PDO | 09-12-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 2 | | REAR-END | UNSAF-SPD | DYLGT | DRY | N-I INJ | 10-30-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLGT | DRY | PDO | 11-18-2009 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLGT | DRY | PDO | 01-07-2010 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | - | 1 | 1 | | ROLLOVER | SLEEPY | DARK | WET | I INJ | 01-17-2010 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLGT | DRY | PDO | 01-21-2010 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | 1 | | HEAD-ON | UNSAF-SPD | DARK | ICE | N-I INJ | 03-20-2010 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 06-02-2010 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | P INJ | 06-03-2010 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | 5 | | REAR-END | FOL-CLOSE | DYLGT | DRY | P INJ | 06-23-2010 | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | N-I INJ | 07-02-2010 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 07-21-2010 | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 09-16-2010 | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
|--------------------|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | 2 | | SIDESWIPE-SAME | IMP-LN-CHG | DYLGT | DRY | P INJ | 09-16-2010 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DUSK | DRY | PDO | 10-28-2010 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | N-I INJ | 11-24-2010 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 12-01-2010 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | P INJ | 12-02-2010 |
| 55 | 70 | 15 | | 05.16 | | X-MEDIAN | NO | S | S | 2 | 1 | | REAR-END | D-W-I | DARK | DRY | I INJ | 12-12-2010 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | ANGLE-OTHER | IMP-LN-CHG | DARK | DRY | I INJ | 12-24-2010 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | WET | PDO | 02-15-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | IMP-TURN | DYLGT | DRY | PDO | 03-16-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 03-18-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | N-I INJ | 04-06-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | 1 | | REAR-END | UNSAF-SPD | DYLGT | DRY | P INJ | 04-19-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | - | 1 | | | F-O TRAFF-SIGN | OTHER | DARK | DRY | PDO | 05-02-2011 |
| 55 | 70 | 15 | | 05.16 | | WKZONE | NO | N | N | 4 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 05-07-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 06-27-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 07-01-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | 1 | | OTHER | IMP-LN-CHG | DYLGT | DRY | I INJ | 07-10-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DAWN | DRY | PDO | 07-25-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 08-15-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | E | E | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLGT | DRY | PDO | 08-15-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | INATT | DYLGT | DRY | PDO | 08-22-2011 |
| 55 | 70 | 15 | | 05.16 | | WKZONE | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLGT | DRY | PDO | 09-06-2011 |
| 55 | 70 | 15 | | 05.16 | | WKZONE | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 09-19-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | N-I INJ | 09-27-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 10-04-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 10-14-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | - | 1 | 1 | | F-O GUARDRL-END | DEF-VEH | DARK | DRY | P INJ | 10-15-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 4 | 4 | | OTHER | FOL-CLOSE | DYLGT | DRY | I INJ | 10-21-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 11-04-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DUSK | DRY | PDO | 11-11-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | REAR-END | NO-IMP-ACT | DUSK | DRY | P INJ | 11-11-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 11-17-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 11-27-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DARK | DRY | PDO | 12-16-2011 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | HEAD-ON | IMP-LN-CHG | DYLGT | DRY | PDO | 01-11-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 01-26-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DAWN | DRY | PDO | 02-29-2012 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
|--------------------|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-06-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 04-17-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 4 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 06-01-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 3 | | | REAR-END | SLEEPY | DYLG | DRY | PDO | 06-04-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-04-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 06-14-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 06-27-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 3 | 1 | | OTHER | INATT | DYLG | DRY | P INJ | 06-28-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | OTHER | NEG-DRVING | DYLG | DRY | PDO | 07-17-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 07-24-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | - | 1 | 1 | | PEDESTRIAN | F-YIELD | DYLG | DRY | P INJ | 08-14-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 08-17-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-14-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 09-24-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-28-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 10-08-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DAWN | DRY | PDO | 10-11-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | SIDESWIPE-SAME | OTHER | DYLG | DRY | PDO | 10-17-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-18-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | - | 1 | | | F-O GUARDRL-FACE | D-W-I | DARK | DRY | PDO | 10-26-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-05-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 11-08-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | HEAD-ON | FOL-CLOSE | DYLG | DRY | PDO | 11-25-2012 |
| 55 | 70 | 15 | | 05.16 | | X-MEDIAN | NO | N | S | 2 | 2 | | HEAD-ON | IMP-START | DYLG | DRY | N-I INJ | 12-11-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-14-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 12-22-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | - | 1 | | | ROLLOVER | UNSAF-SPD | DARK | ICE | PDO | 12-25-2012 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-07-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | 2 | | REAR-END | FOL-CLOSE | DYLG | WET | P INJ | 01-10-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 01-16-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 4 | | | OTHER | FOL-CLOSE | DARK | DRY | PDO | 02-07-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 5 | | | HEAD-ON | FOL-CLOSE | DUSK | DRY | PDO | 02-08-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 02-19-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 03-05-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 4 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 03-13-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-05-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | - | 1 | | | ROLLOVER | NO-IMP-ACT | DYLG | DRY | PDO | 04-25-2013 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|----------|-----------------|--------------|--------|--------|-------------------------------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 04-26-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 1 | | REAR-END | UNSAF-SPD | DYLG | DRY | N-I INJ | 06-07-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | 1 | | OTHER | IMP-LN-CHG | DYLG | DRY | P INJ | 06-26-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | 2 | | OTHER | INATT | DYLG | DRY | P INJ | 06-28-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-11-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-27-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-30-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-18-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-18-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-24-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-24-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 10-01-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 10-04-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-07-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-23-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | - | 1 | 1 | | F-O DITCH | OTHER | DYLG | DRY | N-I INJ | 11-09-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | DRY | PDO | 11-14-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DARK | DRY | PDO | 11-20-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | 2 | | ANGLE-OTHER | UNSAF-SPD | DARK | ICE | N-I INJ | 11-25-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | - | 1 | | | F-O DROP-OFF | UNSAF-SPD | DARK | ICE | PDO | 11-25-2013 |
| 55 | 70 | 15 | | 05.16 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-05-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.00, | | | MM 126 | | | | | | | | |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | N | N | 3 | 2 | | ANGLE-OTHER | DEF-VEH | DYLG | DRY | P INJ | 10-08-2010 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | N | N | 2 | | | REAR-END | NEG-DRVING | DYLG | DRY | PDO | 08-05-2011 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | N | N | 4 | 1 | | OTHER | FOL-CLOSE | DYLG | WET | N-I INJ | 09-16-2011 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 10-11-2011 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | N | N | 3 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | I INJ | 10-28-2011 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-28-2011 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | N | N | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 11-07-2011 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 11-08-2011 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 11-18-2011 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-22-2011 |
| 55 | 70 | 15 | | 05.19 | | MM 126 | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-22-2011 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.03, | | | 15 ST. S NB ENT | | | | | | | | |
| 55 | 70 | 15 | | 05.22 | | 15 ST. S NB ENT | NO | N | - | 1 | | | ROLLOVER | NEG-DRVING | DARK | DRY | PDO | 12-17-2010 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.05, | | | 00.02 before 15 ST. S SB EXIT | | | | | | | | |
| 55 | 70 | 15 | | 05.24 | | | NO | S | - | 1 | | | F-O FENCE | UNSAF-SPD | DYLG | ICE | PDO | 01-26-2009 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
|--------------------|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 04-14-2009 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 2 | 1 | | REAR-END | IMP-LN-CHG | DYLG | DRY | N-I INJ | 04-22-2009 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 3 | 1 | | ANGLE-OTHER | UNSAF-SPD | DYLG | DRY | P INJ | 08-03-2009 |
| 55 | 70 | 15 | | 05.24 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | WET | PDO | 10-14-2009 |
| 55 | 70 | 15 | | 05.24 | | | NO | S | S | 3 | 2 | | OTHER | OTHER | DYLG | DRY | P INJ | 10-19-2009 |
| 55 | 70 | 15 | | 05.24 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-16-2010 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-04-2010 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | - | 1 | 2 | | F-O GUARDRL-FACE | UNSAF-SPD | DYLG | WET | P INJ | 07-06-2010 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DARK | DRY | PDO | 07-12-2010 |
| 55 | 70 | 15 | | 05.24 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 10-27-2010 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 11-23-2010 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DAWN | DRY | PDO | 12-02-2010 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | WET | PDO | 09-16-2011 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-04-2011 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-20-2012 |
| 55 | 70 | 15 | | 05.24 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 03-25-2012 |
| 55 | 70 | 15 | | 05.24 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-29-2012 |
| 55 | 70 | 15 | | 05.24 | | INCIDENT | NO | S | S | 3 | | | ANGLE-OTHER | DEF-VEH | DARK | DRY | PDO | 11-05-2012 |
| 55 | 70 | 15 | | 05.24 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-02-2013 |

(55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.06, 00.01 before 15 ST. S SB EXIT

| | | | | | | | | | | | | | | | | | | |
|----|----|----|--|-------|--|--|----|---|---|---|---|---|-------------------|------------|------|-----|---------|------------|
| 55 | 70 | 15 | | 05.25 | | | NO | N | - | 1 | | | F-O BARR-CONCRETE | NO-IMP-ACT | DARK | DRY | PDO | 11-14-2008 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-29-2008 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 01-21-2009 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 02-03-2009 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | - | 1 | 2 | 1 | ROLLOVER | NO-IMP-ACT | DYLG | WET | FAT | 05-10-2009 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 3 | 2 | | OTHER | FOL-CLOSE | DYLG | DRY | P INJ | 08-06-2010 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 09-20-2010 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 10-21-2010 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-27-2011 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 3 | 1 | | OTHER | FOL-CLOSE | DYLG | WET | P INJ | 02-15-2011 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | 3 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 03-01-2011 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 03-04-2011 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 03-09-2011 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 3 | | | OTHER | UNSAF-SPD | DYLG | DRY | PDO | 06-29-2011 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-19-2011 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | - | 1 | | 3 | F-O CULVERT | SLEEPY | DARK | DRY | FAT | 09-03-2011 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|------------------|--------------|--------------|-------------|--------|--------|------------------------------|--------|----------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DAWN | WET | PDO | 02-03-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-09-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-16-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 4 | 3 | | REAR-END | UNSAF-SPD | DYLG | DRY | N-I INJ | 03-26-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 04-04-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | - | 1 | 1 | | F-O GROUND | SLEEPY | DYLG | DRY | N-I INJ | 04-10-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 04-26-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | | | REAR-END | NEG-DRVING | DYLG | DRY | PDO | 07-26-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | 1 | | ANGLE-OTHER | UNSAF-SPD | DYLG | DRY | I INJ | 08-04-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 09-05-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-05-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | 1 | | REAR-END | IMP-LN-CHG | DYLG | DRY | P INJ | 10-25-2012 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-25-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 02-04-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | 1 | | HEAD-ON | UNSAF-SPD | DYLG | DRY | N-I INJ | 03-08-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | 1 | | OTHER | FOL-CLOSE | DAWN | WET | P INJ | 04-03-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 3 | 2 | | REAR-END | FOL-CLOSE | DYLG | WET | P INJ | 05-02-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-18-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 3 | | | REAR-END | IMP-LN-CHG | DARK | DRY | PDO | 11-12-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | - | 1 | 1 | | F-O BARR-CONCRETE | UNSAF-SPD | DARK | ICE | N-I INJ | 11-25-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 12-01-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 12-01-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 12-02-2013 |
| 55 | 70 | 15 | | 05.25 | | | NO | S | S | 2 | | | REAR-END | IMP-PARK | DARK | DRY | PDO | 12-21-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | | AT: 126.07, | | | 15 ST. S SB EXIT | | | | | | | |
| 55 | 70 | 15 | | 05.26 | 15 ST. S SB EXIT | M/L RAMP GOR | NO | W | S | 2 | 1 | | ANGLE-TURNING | OTHER | DYLG | DRY | P INJ | 04-10-2010 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | | AT: 126.08, | | | 00.01 after 15 ST. S SB EXIT | | | | | | | |
| 55 | 70 | 15 | | 05.27 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-29-2011 |
| 55 | 70 | 15 | | 05.27 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 07-01-2011 |
| 55 | 70 | 15 | | 05.27 | | | NO | N | N | 11 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 07-01-2011 |
| 55 | 70 | 15 | | 05.27 | | | NO | N | - | 1 | | | F-O BARR-CONCRETE | INATT | DYLG | DRY | PDO | 07-03-2011 |
| 55 | 70 | 15 | | 05.27 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-27-2011 |
| 55 | 70 | 15 | | 05.27 | | | NO | N | N | 2 | 5 | | REAR-END | IMP-LN-CHG | DYLG | DRY | P INJ | 02-10-2013 |
| 55 | 70 | 15 | | 05.27 | | | NO | N | - | 1 | | | F-O BARR-CONCRETE | FOL-CLOSE | DYLG | DRY | PDO | 03-18-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | | HWY: I-35 | | | AT: 126.09, | | | 00.02 after 15 ST. S SB EXIT | | | | | | | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--|------|------|--------|-----------|----------|---------------------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | | 05.28 | | | NO | N | N | 4 | | | OTHER | OTHER | DYLG | DRY | PDO | 10-30-2008 |
| 55 | 70 | 15 | | 05.28 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-31-2008 |
| 55 | 70 | 15 | | 05.28 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-24-2009 |
| 55 | 70 | 15 | | 05.28 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-01-2009 |
| 55 | 70 | 15 | | 05.28 | | | NO | S | S | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 06-10-2009 |
| 55 | 70 | 15 | | 05.28 | | | NO | S | S | 3 | 3 | | OTHER | FOL-CLOSE | DYLG | DRY | N-I INJ | 06-06-2011 |
| 55 | 70 | 15 | | 05.28 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-08-2011 |
| 55 | 70 | 15 | | 05.28 | | | NO | N | N | 3 | 1 | | OTHER | FOL-CLOSE | DYLG | DRY | P INJ | 08-22-2011 |
| 55 | 70 | 15 | | 05.28 | | WKZONE | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-21-2011 |
| 55 | 70 | 15 | | 05.28 | | | NO | N | N | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 11-03-2011 |
| 55 | 70 | 15 | | 05.28 | | | NO | S | - | 1 | | | F-O GUARDRL-END | UNSAF-SPD | DYLG | SNOW | PDO | 12-25-2012 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.10, 00.03 after 15 ST. S SB EXIT | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.29 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-13-2011 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.11, 00.04 after 15 ST. S SB EXIT | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.30 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-13-2011 |
| 55 | 70 | 15 | | 05.30 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 05-12-2011 |
| 55 | 70 | 15 | | 05.30 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 07-12-2011 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.12, 00.03 before BN RR OP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.31 | | X-RD/RMP GOR WKZONE | NO | W | - | 1 | | | F-O BARR-CONCRETE | UNSAF-SPD | DARK | DRY | PDO | 02-19-2012 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.13, 00.02 before BN RR OP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.32 | | | NO | N | N | 3 | | | OTHER | INATT | DYLG | WET | PDO | 11-05-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.14, 00.01 before BN RR OP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.33 | | | NO | S | S | 2 | 1 | 1 | ANGLE-OTHER | OTHER | DARK | DRY | FAT | 10-26-2008 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.17, 00.02 after BN RR OP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.36 | | | NO | N | - | 1 | | | F-O UTIL-POLE | NO-IMP-ACT | DYLG | DRY | PDO | 08-24-2008 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 3 | | | OTHER | UNSAF-SPD | DAWN | DRY | PDO | 09-16-2008 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-07-2008 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | - | 1 | 2 | | F-O BARR-CONCRETE | INATT | DARK | DRY | P INJ | 10-26-2008 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 3 | | | OTHER | UNSAF-SPD | DYLG | DRY | PDO | 11-26-2008 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 12-01-2008 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 01-29-2009 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | WET | PDO | 05-06-2009 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-17-2009 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | - | 1 | 1 | | F-O FENCE | OTHER | DYLG | DRY | P INJ | 10-19-2009 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 3 | 1 | | OTHER | UNSAF-SPD | DYLG | DRY | P INJ | 10-21-2009 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
|--------------------|----|----|--|-------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | OTHER | UNSAF-SPD | DYLG | DRY | PDO | 07-29-2010 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 07-29-2010 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | WET | PDO | 08-25-2010 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | 3 | | OTHER | FOL-CLOSE | DYLG | DRY | N-I INJ | 09-09-2010 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-28-2010 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-16-2010 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 11-18-2010 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | REAR-END | UNSAF-SPD | DARK | DRY | PDO | 12-16-2010 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 02-18-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | - | 1 | 1 | | F-O UTIL-POLE | INATT | DARK | DRY | N-I INJ | 02-19-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-03-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 05-12-2011 |
| 55 | 70 | 15 | | 05.36 | | WKZONE | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DARK | DRY | P INJ | 05-14-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 05-31-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | E | 2 | | | RIGHT-ANGLE | FOL-CLOSE | DYLG | DRY | PDO | 06-23-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | SIDESWIPE-SAME | UNSAF-SPD | DYLG | DRY | PDO | 07-08-2011 |
| 55 | 70 | 15 | | 05.36 | | WKZONE | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-12-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 3 | 1 | | OTHER | UNSAF-SPD | DYLG | DRY | P INJ | 07-28-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 08-05-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | 2 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 08-12-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 08-12-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | I INJ | 09-06-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DARK | DRY | PDO | 09-20-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 3 | 3 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 09-27-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 11-22-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DARK | WET | PDO | 12-12-2011 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 3 | 1 | | HEAD-ON | FOL-CLOSE | DYLG | DRY | P INJ | 02-21-2012 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-25-2012 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | HEAD-ON | IMP-LN-CHG | DYLG | DRY | PDO | 07-03-2012 |
| 55 | 70 | 15 | | 05.36 | | | NO | S | S | 2 | | | ANGLE-OTHER | OTHER | DYLG | DRY | PDO | 08-03-2012 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 09-14-2012 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | - | 4 | 1 | | REAR-END | INATT | DYLG | DRY | N-I INJ | 10-06-2012 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 11-07-2012 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | 2 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 12-18-2012 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-07-2013 |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 3 | | | OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 03-17-2013 |
| 55 | 70 | 15 | | 05.36 | | WKZONE | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 04-12-2013 |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|----------|----------|--------------|--------|--------|----------------------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|--|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date | |
| 55 | 70 | 15 | | 05.36 | | WKZONE | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-13-2013 | |
| 55 | 70 | 15 | | 05.36 | | WKZONE | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 04-13-2013 | |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-28-2013 | |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-10-2013 | |
| 55 | 70 | 15 | | 05.36 | | | NO | N | N | 2 | | | REAR-END | INATT | DARK | WET | PDO | 10-18-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.20, | | | 00.05 after BN RR OP | | | | | | | | | |
| 55 | 70 | 15 | | 05.39 | | | NO | W | W | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 12-23-2010 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.27, | | | 00.12 after BN RR OP | | | | | | | | | |
| 55 | 70 | 15 | | 05.46 | | | NO | S | S | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-19-2008 | |
| 55 | 70 | 15 | | 05.46 | | | NO | S | S | 3 | | | OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 02-18-2009 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-15-2009 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 12-10-2009 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 03-03-2010 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 05-08-2010 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 06-19-2010 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 10-04-2010 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | - | 1 | 1 | | ROLLOVER | NO-IMP-ACT | DARK | DRY | I INJ | 11-19-2010 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-02-2010 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-27-2011 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 05-02-2011 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-10-2011 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 06-10-2011 | |
| 55 | 70 | 15 | | 05.46 | | WKZONE | NO | N | N | 3 | 1 | | OTHER | UNSAF-SPD | DYLG | DRY | P INJ | 07-08-2011 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 08-08-2012 | |
| 55 | 70 | 15 | | 05.46 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-24-2012 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | WET | P INJ | 09-29-2012 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-13-2012 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-06-2012 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-14-2012 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | N | 6 | 2 | | ANGLE-OTHER | INATT | DYLG | DRY | P INJ | 02-01-2013 | |
| 55 | 70 | 15 | | 05.46 | | | NO | N | - | 1 | | | F-O CULVERT | IMP-LN-CHG | DYLG | DRY | PDO | 04-09-2013 | |
| 55 | 70 | 15 | | 05.46 | | WKZONE | NO | N | N | 3 | | | OTHER | UNSAF-SPD | DYLG | DRY | PDO | 04-13-2013 | |
| 55 | 70 | 15 | | 05.46 | | INCIDENT | NO | N | N | 2 | | | SIDESWIPE-OPP | DEF-VEH | DYLG | DRY | PDO | 10-06-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.28, | | | 00.13 after BN RR OP | | | | | | | | | |
| 55 | 70 | 15 | | 05.47 | | | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 03-16-2010 | |
| 55 | 70 | 15 | | 05.47 | | | NO | S | S | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-13-2010 | |
| 55 | 70 | 15 | | 05.47 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-30-2011 | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|---|------|------|--------|-----------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | | 05.47 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-22-2012 |
| 55 | 70 | 15 | | 05.47 | | | NO | S | S | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 05-16-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.30, 00.15 after BN RR OP | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.49 | | WKZONE | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DAWN | DRY | PDO | 09-14-2011 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.32, 00.15 before N. CANADIAN RIV BR | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.51 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-16-2008 |
| 55 | 70 | 15 | | 05.51 | | | NO | S | S | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 10-08-2010 |
| 55 | 70 | 15 | | 05.51 | | | NO | S | S | 2 | | | REAR-END | UNSAF-SPD | DYLG | WET | PDO | 04-03-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.33, 00.14 before N. CANADIAN RIV BR | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.52 | | | NO | N | N | 4 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 11-01-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.37, 00.10 before N. CANADIAN RIV BR | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-22-2008 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 2 | 1 | | REAR-END | UNSAF-SPD | DAWN | DRY | P INJ | 10-07-2008 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-30-2008 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 4 | | | OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 07-08-2009 |
| 55 | 70 | 15 | | 05.56 | | WKZONE | NO | N | N | 3 | 1 | | OTHER | UNSAF-SPD | DYLG | DRY | P INJ | 08-09-2009 |
| 55 | 70 | 15 | | 05.56 | | | NO | S | S | 2 | 1 | | REAR-END | IMP-LN-CHG | DYLG | DRY | P INJ | 08-18-2009 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 10-16-2009 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 12-01-2009 |
| 55 | 70 | 15 | | 05.56 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DUSK | DRY | P INJ | 12-16-2009 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 4 | 1 | | ANGLE-OTHER | UNSAF-SPD | DYLG | DRY | P INJ | 02-19-2010 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 2 | | | OTHER | UNSAF-SPD | DYLG | WET | PDO | 03-08-2010 |
| 55 | 70 | 15 | | 05.56 | | | NO | - | - | 2 | 1 | | REAR-END | OTHER | DYLG | DRY | P INJ | 03-25-2010 |
| 55 | 70 | 15 | | 05.56 | | | NO | S | - | 2 | | | ANGLE-OTHER | INATT | DYLG | WET | PDO | 11-15-2010 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 4 | | | OTHER | FOL-CLOSE | DARK | DRY | PDO | 09-16-2011 |
| 55 | 70 | 15 | | 05.56 | | WKZONE | NO | N | - | 1 | | | OTH-SINGLE-VEH | NO-IMP-ACT | DARK | DRY | PDO | 09-21-2011 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 3 | 2 | | OTHER | FOL-CLOSE | DYLG | DRY | P INJ | 11-04-2011 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 2 | 1 | | REAR-END | INATT | DYLG | DRY | P INJ | 08-27-2012 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | WET | PDO | 09-29-2012 |
| 55 | 70 | 15 | | 05.56 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-19-2012 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 3 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 11-15-2012 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 12-22-2012 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 07-23-2013 |
| 55 | 70 | 15 | | 05.56 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 09-30-2013 |
| 55 | 70 | 15 | | 05.56 | | | NO | N | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DARK | ICE | N-I INJ | 11-25-2013 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.38, 00.09 before N. CANADIAN RIV BR | | | | | | | | | | | | | | | | | | |
| 55 | 70 | 15 | | 05.57 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-09-2012 |
| (55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.42, 00.05 before N. CANADIAN RIV BR | | | | | | | | | | | | | | | | | | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|--------------------|----------|--------------|--------|--------|--------------------------------|--------|--------|----------------------|-----------------|----------------|---------------|----------|------------|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date |
| 55 | 70 | 15 | | 05.61 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-04-2013 |
| 55 | 70 | 15 | | 05.61 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 12-20-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.47, | | | N. CANADIAN RIV BR | | | | | | | | |
| 55 | 70 | 15 | | 05.66 | N. CANADIAN RIV BR | BRIDGE | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | WET | P INJ | 08-27-2009 |
| 55 | 70 | 15 | | 05.66 | N. CANADIAN RIV BR | BRIDGE | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-01-2011 |
| 55 | 70 | 15 | | 05.66 | N. CANADIAN RIV BR | BRIDGE | NO | S | S | 2 | | | SIDESWIPE-SAME | OTHER | DARK | DRY | PDO | 12-12-2011 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.53, | | | 00.01 after N. CANADIAN RIV BR | | | | | | | | |
| 55 | 70 | 15 | | 05.67 | | | NO | S | S | 2 | 3 | | REAR-END | UNSAF-SPD | DYLG | DRY | P INJ | 08-01-2008 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 08-22-2008 |
| 55 | 70 | 15 | | 05.67 | | WKZONE | NO | N | - | 1 | 1 | | ROLLOVER | UNSAF-SPD | DYLG | DRY | I INJ | 10-19-2008 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-24-2009 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 11-25-2009 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | - | 1 | 1 | | F-O BARR-CONCRETE | IMP-LN-CHG | DYLG | DRY | P INJ | 03-01-2011 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 05-31-2011 |
| 55 | 70 | 15 | | 05.67 | | WKZONE | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-29-2011 |
| 55 | 70 | 15 | | 05.67 | | WKZONE | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 07-06-2011 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 2 | 2 | | REAR-END | UNSAF-SPD | DYLG | DRY | N-I INJ | 07-11-2011 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 4 | 1 | | HEAD-ON | FOL-CLOSE | DYLG | DRY | N-I INJ | 08-05-2011 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 4 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-30-2011 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 01-10-2012 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | - | 1 | | | ROLLOVER | UNSAF-SPD | DYLG | DRY | PDO | 05-06-2012 |
| 55 | 70 | 15 | | 05.67 | | | NO | E | E | 2 | 1 | | REAR-END | IMP-LN-CHG | DYLG | DRY | P INJ | 08-22-2012 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DARK | DRY | I INJ | 11-07-2012 |
| 55 | 70 | 15 | | 05.67 | | | NO | N | N | 2 | | | HEAD-ON | IMP-LN-CHG | DYLG | DRY | PDO | 08-02-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.54, | | | 00.02 after N. CANADIAN RIV BR | | | | | | | | |
| 55 | 70 | 15 | | 05.68 | | | NO | N | N | 3 | 1 | | OTHER | UNSAF-SPD | DAWN | DRY | P INJ | 02-05-2009 |
| 55 | 70 | 15 | | 05.68 | | | NO | N | - | 1 | 1 | | ROLLOVER | NO-IMP-ACT | DAWN | DRY | N-I INJ | 04-02-2009 |
| 55 | 70 | 15 | | 05.68 | | | NO | S | S | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 10-02-2009 |
| 55 | 70 | 15 | | 05.68 | | | NO | N | N | 3 | | | OTHER | FOL-CLOSE | DARK | DRY | PDO | 11-24-2009 |
| 55 | 70 | 15 | | 05.68 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 02-05-2010 |
| 55 | 70 | 15 | | 05.68 | | | NO | N | N | 2 | 4 | | REAR-END | D-W-I | DARK | DRY | I INJ | 04-04-2010 |
| 55 | 70 | 15 | | 05.68 | | | NO | N | N | 5 | 1 | | OTHER | FOL-CLOSE | DARK | DRY | P INJ | 09-11-2010 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.56, | | | 00.04 after N. CANADIAN RIV BR | | | | | | | | |
| 55 | 70 | 15 | | 05.70 | | | NO | S | S | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 12-31-2011 |
| 55 | 70 | 15 | | 05.70 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | WET | PDO | 01-09-2013 |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.57, | | | 00.05 after N. CANADIAN RIV BR | | | | | | | | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| HIGHWAY COLLISIONS | | | | | | | | | | | | | | | | | | | |
|--------------------|------|-----------------|--------|-----------|----------|----------|--------------|--------|--------|--------------------------|--------|--------|----------------------|-----------------|----------------|---------------|----------|------------|--|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date | |
| 55 | 70 | 15 | | 05.71 | | | NO | S | S | 2 | | | REAR-END | IMP-LN-CHG | DYLG | WET | PDO | 04-29-2009 | |
| 55 | 70 | 15 | | 05.71 | | | NO | N | N | 3 | 3 | | OTHER | FOL-CLOSE | DYLG | DRY | N-I INJ | 05-25-2009 | |
| 55 | 70 | 15 | | 05.71 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | SNOW | PDO | 03-20-2010 | |
| 55 | 70 | 15 | | 05.71 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 06-18-2010 | |
| 55 | 70 | 15 | | 05.71 | | | NO | N | N | 4 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 07-03-2012 | |
| 55 | 70 | 15 | | 05.71 | | | NO | W | W | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-19-2012 | |
| 55 | 70 | 15 | | 05.71 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | WET | PDO | 12-13-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.60, | | | 00.06 before I-40 SB ENT | | | | | | | | | |
| 55 | 70 | 15 | | 05.74 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 07-23-2011 | |
| 55 | 70 | 15 | | 05.74 | | WKZONE | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 10-04-2011 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.62, | | | 00.04 before I-40 SB ENT | | | | | | | | | |
| 55 | 70 | 15 | | 05.76 | | | NO | S | S | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DARK | DRY | PDO | 04-14-2009 | |
| 55 | 70 | 15 | | 05.76 | | | NO | N | N | 3 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLG | DRY | PDO | 05-06-2011 | |
| 55 | 70 | 15 | | 05.76 | | WKZONE | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 09-29-2011 | |
| 55 | 70 | 15 | | 05.76 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 08-20-2013 | |
| (55) OKLAHOMA | | (70) OKLA. CITY | | HWY: I-35 | | | AT: 126.63, | | | 00.03 before I-40 SB ENT | | | | | | | | | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 08-05-2008 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 3 | | | OTHER | UNSAF-SPD | DYLG | DRY | PDO | 08-05-2008 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | WET | PDO | 11-10-2008 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DARK | DRY | PDO | 11-26-2008 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 11-26-2008 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DYLG | DRY | PDO | 03-30-2009 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 1 | | REAR-END | UNSAF-SPD | DAWN | DRY | P INJ | 03-30-2009 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 04-17-2009 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 05-24-2009 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | - | 3 | 1 | | OTHER | UNSAF-SPD | DYLG | DRY | N-I INJ | 08-10-2009 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 11-06-2009 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | S | 2 | | | REAR-END | INATT | DYLG | DRY | PDO | 02-24-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 3 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | N-I INJ | 03-03-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLG | DRY | PDO | 04-26-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLG | DRY | PDO | 06-11-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 5 | | | ANGLE-OTHER | IMP-LN-CHG | DYLG | DRY | PDO | 06-18-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | S | 3 | | | OTHER | FOL-CLOSE | DYLG | DRY | PDO | 06-25-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | S | 3 | 3 | | OTHER | FOL-CLOSE | DYLG | DRY | P INJ | 07-02-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | - | 1 | 1 | | F-O BARR-CONCRETE | NO-IMP-ACT | DYLG | DRY | P INJ | 08-08-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | S | 2 | 1 | | REAR-END | INATT | DYLG | DRY | P INJ | 08-23-2010 | |



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
Traffic Engineering Division
Collision Analysis and Safety Branch
(405) 522-0985
Created: 01/16/2014 by Tina Payton

| | | | | HIGHWAY COLLISIONS | | | | | | | | | | | | | | | |
|------|------|------|--------|--------------------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|--|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | IMP-LN-CHG | DYLGT | DRY | PDO | 10-05-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | - | 1 | 1 | | PEDESTRIAN | NO-IMP-ACT | DARK | DRY | I INJ | 10-25-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DAWN | DRY | PDO | 12-15-2010 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | OTHER | NEG-DRVING | DYLGT | DRY | PDO | 01-11-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 1 | | REAR-END | UNSAF-SPD | DYLGT | DRY | P INJ | 01-12-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | - | 1 | 1 | | ROLLOVER | D-W-I | DARK | DRY | N-I INJ | 03-17-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | N-I INJ | 05-24-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 3 | 1 | | OTHER | INATT | DYLGT | DRY | P INJ | 06-03-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 3 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | P INJ | 06-03-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 06-28-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 1 | | SIDESWIPE-SAME | IMP-LN-CHG | DYLGT | DRY | N-I INJ | 07-07-2011 | |
| 55 | 70 | 15 | | 05.77 | | WKZONE | NO | N | N | 5 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 07-11-2011 | |
| 55 | 70 | 15 | | 05.77 | | WKZONE | NO | N | N | 3 | | | OTHER | INATT | DYLGT | DRY | PDO | 07-13-2011 | |
| 55 | 70 | 15 | | 05.77 | | WKZONE | NO | N | N | 2 | | | SIDESWIPE-SAME | NEG-DRVING | DYLGT | DRY | PDO | 08-25-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | ANGLE-OTHER | FOL-CLOSE | DYLGT | DRY | PDO | 09-06-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | INATT | DYLGT | DRY | PDO | 09-09-2011 | |
| 55 | 70 | 15 | | 05.77 | | WKZONE | NO | N | - | 1 | 1 | | F-O RET-WALL | UNSAF-SPD | DYLGT | DRY | I INJ | 10-26-2011 | |
| 55 | 70 | 15 | | 05.77 | | WKZONE | NO | N | N | 5 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | N-I INJ | 11-11-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 11-27-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 3 | | | REAR-END | UNSAF-SPD | DYLGT | WET | PDO | 12-14-2011 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 3 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 01-07-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 2 | | OTHER | FOL-CLOSE | DYLGT | DRY | P INJ | 01-20-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | P INJ | 01-27-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | S | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 02-24-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 03-27-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | S | 2 | | | SIDESWIPE-SAME | IMP-LN-CHG | DYLGT | DRY | PDO | 04-30-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | S | 3 | | | HEAD-ON | FOL-CLOSE | DYLGT | DRY | PDO | 06-15-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 3 | | REAR-END | FOL-CLOSE | DYLGT | DRY | P INJ | 10-03-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 1 | | OTHER | NO-IMP-ACT | DYLGT | DRY | P INJ | 10-16-2012 | |
| 55 | 70 | 15 | | 05.77 | | WKZONE | NO | N | N | 2 | | | REAR-END | UNSAF-SPD | DARK | DRY | PDO | 10-26-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 3 | 1 | | REAR-END | UNSAF-SPD | DARK | DRY | P INJ | 11-05-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLGT | DRY | P INJ | 11-06-2012 | |
| 55 | 70 | 15 | | 05.77 | | WKZONE | NO | N | N | 3 | | | SIDESWIPE-OPP | IMP-LN-CHG | DYLGT | DRY | PDO | 11-21-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | ANGLE-OTHER | IMP-LN-CHG | DYLGT | DRY | PDO | 11-27-2012 | |
| 55 | 70 | 15 | | 05.77 | | | NO | S | - | 1 | | | F-O DITCH | UNSAF-SPD | DYLGT | DRY | PDO | 04-19-2013 | |
| 55 | 70 | 15 | | 05.77 | | | NO | N | N | 2 | | | REAR-END | FOL-CLOSE | DYLGT | DRY | PDO | 08-16-2013 | |

(55) OKLAHOMA (70) OKLA. CITY HWY: I-35 AT: 126.65, 00.01 before I-40 SB ENT



HIGHWAY SYSTEM COLLISION LISTING

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

| | | | | HIGHWAY COLLISIONS | | | | | | | | | | | | | | | |
|------|------|------|--------|--------------------|----------|----------|--------------|--------|--------|--------|--------|--------|-------------------|-----------------|----------------|---------------|----------|------------|--|
| Cnty | City | CS # | Int. # | Mile Post | Location | Features | Int. Related | Dir. 1 | Dir. 2 | # Veh. | # Inj. | # Fat. | Type of Collision | Unsafe Unlawful | Lighting Cond. | Roadway Cond. | Severity | Date | |
| 55 | 70 | 15 | | 05.79 | | | NO | N | N | 2 | 2 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 05-22-2009 | |
| 55 | 70 | 15 | | 05.79 | | | NO | N | N | 2 | 1 | | REAR-END | FOL-CLOSE | DYLG | DRY | P INJ | 09-28-2010 | |

* Location Near But Not At Mile Point

23 USC 409



STUDY CRITERIA

OKLAHOMA CO. I-35 NB AND SB AT THE I-40/I-35 JCT. - JP 30444(04) - 5 YEARS - 01-16-2014

Date Range: 08-01-2008 Thru 12-31-2013

Program Provided by:
 Traffic Engineering Division
 Collision Analysis and Safety Branch
 (405) 522-0985
 Created: 01/16/2014 by Tina Payton

ROADWAY / REGION

| QUERY OVER | SELECTIONS |
|-----------------|---|
| Control Section | County: 55, Control Section: 15, CS Query On: range, Mile Start: 04.94, Mile End: 06.12 |

DATE

| Date Range | 08-01-2008 to 12-31-2013 |
|------------|--------------------------|
|------------|--------------------------|

FILTER COLLISIONS

| Roadway Type | Hwy. Data Only |
|----------------------------|----------------|
| Incl. Hwy/Hwy Jct ID Colls | Checked |

REPORT SECTIONS

| | |
|---------------------------------|-----------------------------|
| Collision Map & Study Totals | (Included) |
| Collision Analysis Tables | (Included) |
| - Totals By City, Hwy Class | Checked |
| - Other Analysis Tables | Checked |
| Collision Listing | (Included) |
| - Highway Collision Listing | Checked, By Control Section |
| - City Street Collision Listing | Checked |
| - County Road Collision Listing | Checked |
| Query Criteria | (Included) |

REPORT FORMAT OPTIONS

| | |
|------------------------|-----------|
| Print Watermark | Checked |
| Print DPS Case Numbers | Unchecked |

23 USC 409

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
OKLAHOMA COUNTY
J/P 30444(04)

SECTION 06 – EXISTING BRIDGE CONDITION AND HYDROLOGICAL DATA

6.1 There are ten (10) bridges located within the Study Area. Structure Inventory and Appraisal (SI&A) sheets for these bridges were obtained from ODOT Bridge Division and are included here.

6.2 The drainage area associated with two of these bridges are as follows:

NBI 21356 & 21723

6.2.1 Total Area: 6,468,736 Acres (10,107.4 Square Miles)

6.2.2 NRCS Controlled Area: 8,576 Acres (13.4 Square Miles)

6.2.3 Effective Area: 6,640,160 Acres (10,094.0 Square Miles)

6.3 One (1) FEMA FIRMette for these bridges is included at the end of this section.

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **14239** Structure No.: **5515 0526 X** Local ID: **15526** Suff. Rating: **83.0** Health Index : **100.0**
 Not Deficient

| IDENTIFICATION | INSPECTION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|---|--|---|--|--|--|--|---|---|------------------------------|--|--------------------|--|---------------------------|--|------------------------|--|----------------------------------|--|---|--|---|--|---|---|---|---|--------------------------------------|--|--------------------------------------|--|--|---|--|---|----------------------------|---------------------------------------|-------------------------------------|----------------------------------|---|--------------------------------|--|-------------------------------|--------------------|---------------------------------|--|-------------|-----------------------------------|-------------------------------------|-----------------------|-------------------------|--------------------------|-------------------------|-------------------------------------|--|---|--|--------------------|------------------------|-------------------------------------|--------------------|--|---|--|-----------------------------------|---------------------------------------|
| Description: 2-10' X 6' X 250' R.C.BOX SK 60 DEG. 1. State: Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. CITY Admin. Area: Unknown 5. Inventory Route (Route On Structure): 1 - 1 - 1 - 00035 - 0 6. Feature Intersected: CREEK 7. Facility Carried: I-35 I-35 9. Location: 5.3 MI N CLEVELAND CO 11. Mile Post: 5.259 mi 13. LRS Inv. Route./ Subroute.: 5515HP0000 05 16. Latitude: 35 27 09.04 17. Longitude: 097 29 22.15 98. Border Br. Code: Not Applicab % Resp.: 0 99. Border Br. #: Unknown | <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>Type</th> <th>Insp Req.</th> <th>Insp Done</th> <th>Freq.</th> <th>Insp. Date:</th> <th>Next Insp.:</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>Y</td> <td>24</td> <td>7/16/2012</td> <td>7/16/2014</td> </tr> <tr> <td>FC Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> </tbody> </table> | Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | NBI: | | Y | 24 | 7/16/2012 | 7/16/2014 | FC Freq.: | N | N | NA | NA | NA | UW Freq.: | N | N | NA | NA | NA | OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NBI: | | Y | 24 | 7/16/2012 | 7/16/2014 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| FC Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| UW Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 43. Main Span Material and Design Type Concrete Culvert | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 44. Approach Span Material and Design Type Not Applicable (P) Not Applicable (P) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 45. No. of Spans Main Unit: 2 46. No. of Approach Spans: 0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 107. Deck Type: N N/A (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108A. Wearing Surface: N N/A (no deck (NBI)) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108B. Membrane: N N/A (no deck (NBI)) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108C. Deck Protection: N N/A (no deck (NBI)) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 12. Base Hwy Network : On Base Network | 20. Toll Facility: 3 On free road | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21. Custodian: 01 State Highway Agency | 22. Owner: 01 State Highway Agency | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26. Functional Class: 11 Urban Interstate | 37. Historical Sig.: 5 Not eligible for NRHP | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 100. Defense Highway: 1 On Interstate STRAF | 101. Parallel Structure: No bridge exists | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 102. Dir. of Traffic 2 2-way traffic | 103. Temp. Structure: Not Applicable (P) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 104. Highway System: 1 On the NHS | 105. Fed. Land Hwy 0 N/A (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 110. National Truck Network: 1 Part of natl ne | 112. NBIS Length: Long Enough | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 29. ADT: 136800 30. Year of ADT: 2010 109. Truck ADT %: 12 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 42A. Type of Service on: 1 Highway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 42B. Type of Service under: 5 Waterway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 51. Width Curb to Curb: 0.0 ft 52. Width Out to Out: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 54A/54B. Min. Vert. Underclearance : N Feature not hwy or RR 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <u>N/E</u> <u>S/W</u> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <u>Meas.</u> -1 -1 -1 -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| PROPOSED IMPROVEMENTS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 96. Total Cost: \$747,530 | 114. Future ADT: 218880 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 97. Year of Cost Est.: 2007 | 115. Year of Future ADT: 2030 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| APPRAISAL | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 36B. Transition: N N/A or not required | 36D. Approach Rail Ends: N N/A or not requ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 67. Str. Evaluation: 8 Equal Desirable Crit | 68. Deck Geometry: N Not applicable (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 69. Underclearance, Vertical and Horizontal: N Not applicable (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 71. Waterway Adequacy: 6 Equal Minimum | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 72. Approach Alignment: 8 Equal Desirable Crit | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 113. Scour Critical: 8 Stable Above Footing | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NAVIGATION DATA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 38. Navigation Control: Permit Not Required | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 39. Vertical Clearance: 0.0 ft | 40. Horizontal Clearance: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 111. Pier Protection: 1 Not Required | 116. Lift Bridge Vert. Clear.: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <table border="1" style="width: 100%; border-collapse: collapse;"> <tbody> <tr> <td>200c. Temperature: 96</td> <td>214a. Posted Weight Limit: NR</td> <td>243. Girder Spacing/Number: -1.0 / -1</td> </tr> <tr> <td>200d. Weather: CLEAR</td> <td>b. Posted Speed Limit: 60</td> <td>244. Span Lengths:</td> </tr> <tr> <td>201. Structural Steel ASTM Desig.: -1 -1</td> <td>c. Narrow/One Lane Bridge sign: N</td> <td>-1 -1 -1</td> </tr> <tr> <td>202. Waterproof Membrane :-1</td> <td>d. Vertical Clearance Sign: NO</td> <td>-1 -1 -1</td> </tr> <tr> <td>Date Installed: 1/1/1901</td> <td>Advanced Warning Sign: NO</td> <td>-1 -1 -1</td> </tr> <tr> <td>203. Type Exp. Dev.: _</td> <td>Existing/Recommended Posting: 9999 9999</td> <td>245. Girder Depth: -1.000</td> </tr> <tr> <td></td> <td>Min./ Max Vert. Clearance: 9999 9999</td> <td>246. Type of Overlay: _</td> </tr> <tr> <td>204. Type of Handrail: Sloped Faced Parapet</td> <td>e. Navigation Lights: NO</td> <td>246. Overlay Thickness: -1.0</td> </tr> <tr> <td>205. Material and Quantity: -1.0</td> <td>Working/Not Working: NO</td> <td>246. Overlay Date: 1/1/1901</td> </tr> <tr> <td>208. Type of Abutment: Other</td> <td>215. Overpass: A - Interstate</td> <td>246. Overlay Depth Changed > 1"? No</td> </tr> <tr> <td>Type of Foundation: Natural Foundation Matl.</td> <td>221. Substructure Cond. (U/W): -</td> <td>247. Protective Systems: 1: _</td> </tr> <tr> <td>209. Type of Pier / Found.: - -</td> <td>222. Fill over RCB: 36</td> <td>2: _ 3: _</td> </tr> <tr> <td></td> <td>223. Appr. Slab/Rdwy Cond.: Good</td> <td>4: _ 5: _</td> </tr> <tr> <td>210. Foundation Elev. -1.0 -3.0</td> <td>224. Critical Feature Type: -1</td> <td>248. No. of Field Splices w/ Corrosion: -1</td> </tr> <tr> <td> -1.0 -1.0 -1.0</td> <td>225. Paint Type: -</td> <td>249. Scour Crit. POA exists?: _</td> </tr> <tr> <td></td> <td>Overcoat: 0</td> <td>250. Culvert Headwall Dist: 250.0</td> </tr> <tr> <td>211. Wear. Surf. Prot. System: None</td> <td>226. Date Painted: -1</td> <td>254. Thru Truss Type: _</td> </tr> <tr> <td>Date Installed: 1/1/1901</td> <td>227. Paint Coloring: -1</td> <td>256. Chan. Profile Up/Down Stream?:</td> </tr> <tr> <td>213. Utilities Attached: Communication</td> <td>233. Deck Forming: Conventional Forming</td> <td>258. Plans w/ found. are in file at ODOT</td> </tr> <tr> <td>-1 -1 -1</td> <td>236. Deck Cleaning: -1</td> <td>259. Scour Eval. is in file at ODOT</td> </tr> <tr> <td>-1 -1 -1</td> <td>238. School Bus Rte: Current and Desired Route</td> <td>263. Interchange at Intersection N</td> </tr> <tr> <td></td> <td>240. Appr. Roadway Type: Concrete</td> <td>264. Interstate Milepoint 126.07</td> </tr> </tbody> </table> | 200c. Temperature: 96 | 214a. Posted Weight Limit: NR | 243. Girder Spacing/Number: -1.0 / -1 | 200d. Weather: CLEAR | b. Posted Speed Limit: 60 | 244. Span Lengths: | 201. Structural Steel ASTM Desig.: -1 -1 | c. Narrow/One Lane Bridge sign: N | -1 -1 -1 | 202. Waterproof Membrane :-1 | d. Vertical Clearance Sign: NO | -1 -1 -1 | Date Installed: 1/1/1901 | Advanced Warning Sign: NO | -1 -1 -1 | 203. Type Exp. Dev.: _ | Existing/Recommended Posting: 9999 9999 | 245. Girder Depth: -1.000 | | Min./ Max Vert. Clearance: 9999 9999 | 246. Type of Overlay: _ | 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights: NO | 246. Overlay Thickness: -1.0 | 205. Material and Quantity: -1.0 | Working/Not Working: NO | 246. Overlay Date: 1/1/1901 | 208. Type of Abutment: Other | 215. Overpass: A - Interstate | 246. Overlay Depth Changed > 1"? No | Type of Foundation: Natural Foundation Matl. | 221. Substructure Cond. (U/W): - | 247. Protective Systems: 1: _ | 209. Type of Pier / Found.: - - | 222. Fill over RCB: 36 | 2: _ 3: _ | | 223. Appr. Slab/Rdwy Cond.: Good | 4: _ 5: _ | 210. Foundation Elev. -1.0 -3.0 | 224. Critical Feature Type: -1 | 248. No. of Field Splices w/ Corrosion: -1 | -1.0 -1.0 -1.0 | 225. Paint Type: - | 249. Scour Crit. POA exists?: _ | | Overcoat: 0 | 250. Culvert Headwall Dist: 250.0 | 211. Wear. Surf. Prot. System: None | 226. Date Painted: -1 | 254. Thru Truss Type: _ | Date Installed: 1/1/1901 | 227. Paint Coloring: -1 | 256. Chan. Profile Up/Down Stream?: | 213. Utilities Attached: Communication | 233. Deck Forming: Conventional Forming | 258. Plans w/ found. are in file at ODOT | -1 -1 -1 | 236. Deck Cleaning: -1 | 259. Scour Eval. is in file at ODOT | -1 -1 -1 | 238. School Bus Rte: Current and Desired Route | 263. Interchange at Intersection N | | 240. Appr. Roadway Type: Concrete | 264. Interstate Milepoint 126.07 |
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| 202. Waterproof Membrane :-1 | d. Vertical Clearance Sign: NO | -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed: 1/1/1901 | Advanced Warning Sign: NO | -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 203. Type Exp. Dev.: _ | Existing/Recommended Posting: 9999 9999 | 245. Girder Depth: -1.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Min./ Max Vert. Clearance: 9999 9999 | 246. Type of Overlay: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights: NO | 246. Overlay Thickness: -1.0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 205. Material and Quantity: -1.0 | Working/Not Working: NO | 246. Overlay Date: 1/1/1901 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 208. Type of Abutment: Other | 215. Overpass: A - Interstate | 246. Overlay Depth Changed > 1"? No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type of Foundation: Natural Foundation Matl. | 221. Substructure Cond. (U/W): - | 247. Protective Systems: 1: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 209. Type of Pier / Found.: - - | 222. Fill over RCB: 36 | 2: _ 3: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 223. Appr. Slab/Rdwy Cond.: Good | 4: _ 5: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 210. Foundation Elev. -1.0 -3.0 | 224. Critical Feature Type: -1 | 248. No. of Field Splices w/ Corrosion: -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1.0 -1.0 -1.0 | 225. Paint Type: - | 249. Scour Crit. POA exists?: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Overcoat: 0 | 250. Culvert Headwall Dist: 250.0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 211. Wear. Surf. Prot. System: None | 226. Date Painted: -1 | 254. Thru Truss Type: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed: 1/1/1901 | 227. Paint Coloring: -1 | 256. Chan. Profile Up/Down Stream?: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| -1 -1 -1 | 236. Deck Cleaning: -1 | 259. Scour Eval. is in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 238. School Bus Rte: Current and Desired Route | 263. Interchange at Intersection N | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 240. Appr. Roadway Type: Concrete | 264. Interstate Milepoint 126.07 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 14239

Structure No.: 5515 0526 X

Local ID: 15526

Suff. Rating: 83.0
Not Deficient

Health Index :
100.0

Inspection Date: 7/16/2012

Reported By: GHINES

Invoice No.: -1

Inspected With: Gary Richardson

Agency :

Gary Hines

Digitally signed by Gary Hines
DN: cn=Gary Hines, o, ou=Gary Richardson,
email=ghines@odot.org, c=US
Date: 2012.08.22 08:01:14 -05'00'

Structure / Inspection Notes

BARREL LENGTH ALONG THE SKEW IS 438' (MEASURED WITH A LASER).

The center traffic rail is continuous from str #5515 0534X to str #5515 0506X with an open grassy median on the left of the South-bound lanes.

G Hines inspection comments - 7/16/2012

PX - The downstream channel needs cleaned & dredged * FX - Trees need to be removed along the headwalls * The roadways are 68' wide each direction * Satisfactory side drains *

| Elm. | Env. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|------|------|--------------------------------|------|------|-----------|-------|-----------|-----|-----------|-----|-----------|-----|-----------|-----|
| 241 | 4 | Reinforced Concrete Culvert | (LF) | 876 | 876 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 331 | 4 | Reinforced Conc Bridge Railing | (LF) | 26 | 26 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 334 | 4 | Metal Bridge Railing - Coated | (LF) | 26 | 26 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 365 | 4 | Debris Smart Flag | (EA) | 1 | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |

Additional Elements

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|--|
| 241 | No significant problems noted - can't see all areas closely due to silt & water depth. Some light vertical cracks with leaching noted in the outer walls . |
| 331 | The center traffic rail is continuous from 0534X to 0506X with an open grassy median on the left of the South-bound lanes. |
| 334 | Flex-beam rail is present on the RH side of the North-bound lanes only |
| 365 | Each barrel has 1' to 1.5' of silt build-up. No drift at this time. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 21335

Structure No.: 5515 0534EX

Local ID: 15534e

Suff. Rating: 86.3
Not Deficient

Health Index :
90.7

| IDENTIFICATION | INSPECTION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|---|-----------|---|-------------|--|-------------|--|------|--|---|----|-----------|-----------|-----------|---|---|----|----|----|-----------|---|---|----|----|----|-----------|---|---|----|----|----|
| <p>Description: 73'-98'-73' P.C. BM SPANS SK. 62 DEG.44' 56.13"</p> <p>1. State: Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. C11Y Admin. Area: Unknown</p> <p>5. Inventory Route (Route On Structure) : 1 - 1 - 1 - 00035 - 0 6. Feature Intersected: B.N. & S.F. R.R. UNDER</p> <p>7. Facility Carried: I-35 NB I-35 NORTH-BOUND 9. Location: 5.4 MI N CLEVELAND CO 11. Mile Post: 5.339 mi 13. LRS Inv. Route./Subroute.: 5515HP0000 05 16. Latitude: 35 27 14.78 17. Longitude: 097 29 20.99 98. Border Br. Code: Not Applicab % Resp.: 0 99. Border Br. #: Unknown</p> | <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;">Type</th> <th style="text-align: left;">Insp Req.</th> <th style="text-align: left;">Insp Done</th> <th style="text-align: left;">Freq.</th> <th style="text-align: left;">Insp. Date:</th> <th style="text-align: left;">Next Insp.:</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>Y</td> <td>24</td> <td>7/16/2012</td> <td>7/16/2014</td> </tr> <tr> <td>FC Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> </tbody> </table> | Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | NBI: | | Y | 24 | 7/16/2012 | 7/16/2014 | FC Freq.: | N | N | NA | NA | NA | UW Freq.: | N | N | NA | NA | NA | OS Freq.: | N | N | NA | NA | NA |
| Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NBI: | | Y | 24 | 7/16/2012 | 7/16/2014 | | | | | | | | | | | | | | | | | | | | | | | | | | |
| FC Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | |
| UW Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p style="text-align: center;"><u>STRUCTURE TYPE AND MATERIALS</u></p> <p>43. Main Span Material and Design Type Prestressed Concrete Stringer/Girder</p> <p>44. Approach Span Material and Design Type Not Applicable (P) Not Applicable (P)</p> <p>45. No. of Spans Main Unit: 3 46. No. of Approach Spans: 0</p> <p>107. Deck Type: 1 Concrete-Cast-in-Place 108A. Wearing Surface: 1 Monolithic Concrete 108B. Membrane: 0 None 108C. Deck Protection: 1 Epoxy Coated Reinforci</p> | <p style="text-align: center;"><u>CLASSIFICATION</u></p> <p>12. Base Hwy Network : On Base Network 20. Toll Facility: 3 On free road 21. Custodian: 01State Highway Agency 22. Owner: 01State Highway Agency 26. Functional Class: 11 Urban Interstate 37. Historical Sig.: 5 Not eligible for NRHP 100. Defense Highway: 1 On Interstate STRAF 101. Parallel Structure: Right of bridge 102. Dir. of Traffic 1 1-way traffic 103. Temp. Structure: Not Applicable (P) 104. Highway System: 1 On the NHS 105. Fed. Land Hwy 0 N/A (NBI) 110. National Truck Network: 1 Part of natl ne 112. NBIS Length: Long Enough</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p style="text-align: center;"><u>AGE AND SERVICE</u></p> <p>27. Year Built: 1986 106. Year Reconstructed: -4 28A. Lanes on: 4 28B. Lanes Under: 0 19. Detour Length: 0.1 mi 29. ADT: 68400 30. Year of ADT: 2010 109. Truck ADT %: 12 42A. Type of Service on: 1 Highway 42B. Type of Service under: 2 Railroad</p> | <p style="text-align: center;"><u>CONDITION</u></p> <p>58. Deck: 7 Good 59. Super.: 8 Very Good 60. Sub.: 5 Fair 62. Culvert: N N/A (NBI) 61. Channel/Channel Protection: N N/A (NBI) Flowline Notes:</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p style="text-align: center;"><u>GEOMETRIC DATA</u></p> <p>10. Inv. Rte. Min. Vert. Clr.: 328.1 ft 32. Approach Roadway Width (W/ Shoulders): 68.0 ft Deck Area: 17,502.1 sq. ft 33. Median: 0 No median 34. Skew: 28 35. Structure Flared: 0 No flare 47. Inv. Rte. Total Horiz. Clr.: 68.0 ft 48. Length Maximum Span: 93.0 ft 49. Structure Length: 250.0 ft 50A. Curb/Sdwk Wdth L: 0.0 ft 50B. Curb/Sidewalk Width R: 0.0 ft 51. Width Curb to Curb: 68.0 ft 52. Width Out to Out: 70.0 ft 53. Minimum Vertical Clearance Over Bridge: 328.1 ft 54A/54B. Min. Vert. Underclearance : R Railroad beneath struc 23.2 ft</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: center;">N/E</th> <th style="text-align: center;">S/W</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;">Meas. -1 -1 R2304 -1 -1 -1</td> <td></td> </tr> <tr> <td style="text-align: center;">Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U</td> <td></td> </tr> </tbody> </table> <p>55A/55B. Minimum Lateral Underclearance R: R Railroad beneath struc 22.5 ft 56. Minimum Lateral Underclearance L: 0.0 ft</p> | N/E | S/W | Meas. -1 -1 R2304 -1 -1 -1 | | Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U | | <p style="text-align: center;"><u>LOAD RATING AND POSTING</u></p> <p>31. Design Load: 5 MS 18 (HS 20) 41. Posting status: A Open, no restriction 63. Op. Rating Method: 1 LF Load Factor-Tor Alt. Op. Rating Meth.: 1 LF Load Factor-Tc 64. Operating Rating (H / HS / 3-3) : 45.7 78.4 93.6 66. Inventory Rating (H / HS / 3-3) : 31.5 46.1 58.7 65. Inv. Rating Method: 1 LF Load Factor-Tor Alt. Inv. Rating Meth.: 1 LF Load Factor-To 70. Posting: 5 At/Above Legal Loads Date Rated : 9/23/2010</p> | | | | | | | | | | | | | | | | | | | | | | | | |
| N/E | S/W | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Meas. -1 -1 R2304 -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p style="text-align: center;"><u>PROPOSED IMPROVEMENTS</u></p> <p>94. Bridge Cost: \$1,693,823 75. Type of Work: 31 Repl-Load Capacity 95. Roadway Cost: \$2,794,808 76. Lgth. of Improvement: 254.8 ft 96. Total Cost: \$4,742,704 114. Future ADT: 109440 97. Year of Cost Est.: 2007 115. Year of Future ADT: 2030</p> | <p style="text-align: center;"><u>NAVIGATION DATA</u></p> <p>38. Navigation Control: NA-no waterway 39. Vertical Clearance: 0.0 ft 40. Horizontal Clearance: 0.0 ft 111. Pier Protection: Not Applicable (P) 116. Lift Bridge Vert. Clear.: 0.0 ft</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p style="text-align: center;"><u>APPRAISAL</u></p> <p>36A. Bridge Rail: 1 Meets Standards 36C. Approach Rail: 1 Meets Standards 36B. Transition: 1 Meets Standards 36D. Approach Rail Ends: 1 Meets Standards 67. Str. Evaluation: 5 Above Min Tolerable 68. Deck Geometry: 6 Equal Min Criteria 69. Underclearance, Vertical and Horizontal: 9 Above Desirable 71. Waterway Adequacy: N Not applicable 72. Approach Alignment: 8 Equal Desirable Crit 113. Scour Critical: N Not Over Waterway</p> | <p>200c. Temperature: 91 200d. Weather: CLEAR 201. Structural Steel ASTM Desig.: A36 20 202. Waterproof Membrane :-1 Date Installed: 1/1/1901 203. Type Exp. Dev. : Elastomeric Sheet Type Strip Seal - 204. Type of Handrail: Sloped Faced Parapet 205. Material and Quantity : 44.0 208. Type of Abutment : Skeleton Type of Foundation : Steel Piling 209. Type of Pier / Found.: 4 No Drilled Shaft - No Footings 210. Foundation Elev. -1.0 -3.0 -3.0 -3.0 -1.0 211. Wear. Surf. Prot. System : Other Silanes Date Installed : 1/1/1901 213. Utilities Attached : Power -1 -1 -1 -1 -1 -1</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>214a. Posted Weight Limit: NR b. Posted Speed Limit : 60 c. Narrow/One Lane Bridge sign : N d. Vertical Clearance Sign: NO Advanced Warning Sign : NO Existing/Recommended Posting : 2301 2301 Min./ Max Vert. Clearance : 2306 2405 e. Navigation Lights : - Working/Not Working : - 215. Overpass : A - Interstate 221. Substructure Cond. (U/W) : - 222. Fill over RCB: -1 223. Appr. Slab/Rdwy Cond.: Good 224. Critical Feature Type: -1 225. Paint Type : Basic Lead Silico Overcoat : 0 226. Date Painted: -1 227. Paint Coloring: -1 233. Deck Forming: Conventional Forming 236. Deck Cleaning : -1 238. School Bus Rte: Current and Desired Route 240. Appr. Roadway Type: Concrete</p> | <p>243. Girder Spacing/Number : -1.0 / 9 244. Span Lengths : 73 -1 -1 98 -1 -1 73 -1 245. Girder Depth : -1.000 246. Type of Overlay : - 246. Overlay Thickness : 0 246. Overlay Date : 1/1/1901 246. Overlay Depth Changed > 1"? No 247. Protective Systems : 1: DPWR/Epoxy Coate 2: 3: - 4: 5: - 248. No. of Field Splices w/ Corrosion : -1 249. Scour Crit. POA exists?: - 250. Culvert Headwall Dist.: -1.0 254. Thru Truss Type : - 256. Chan. Profile Up/Down Stream?: 258. Plans w/ found. are in file at ODOT 259. Scour Eval. is in file at ODOT 263. Interchange at Intersection P 264. Interstate Milepoint 126.15</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 21335 Structure No.: 5515 0534EX Local ID: 15534e

Suff. Rating: 86.3 Health Index :
Not Deficient 90.7

Inspection Date: 7/16/2012 Reported By: GHINES
Invoice No.: -1 Inspected With: Gary Richardson
Agency :

Gary Hines

Digitally signed by Gary Hines
DN: cn=Gary Hines, o, ou=Gary Richardson,
email=ghines@odot.org, c=US
Date: 2012.08.22 08:02:56 -05'00'

Structure / Inspection Notes

THE 'EX' STRUCTURE (#21708) CONTROLS THE HEIGHT POSTING FOR THIS PAIR OF BRIDGES.

G Hines inspection comments - 7/16/2012

PX - The SE shoulder drain is completely plugged with a cavity developing along the wing & approach slab * There are three RR tracks below * The slopewalls are in good condition except for settlement at the top on the NE * The total horizontal clearance for the Railroad is 83' * The substructure was rated down due to the bearing anchors *

| Elm. | Env. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|------|------|--|------|--------|-----------|-------|-----------|-------|-----------|-------|-----------|------|-----------|-----|
| 26 | 4 | Concrete Deck - Protected w/ Coated Bars | (SF) | 17,000 | 0 | 0 % | 17,000 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 109 | 4 | P/S Conc Open Girder/Beam | (LF) | 1,980 | 1,980 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 119 | 4 | P/S Conc Open Girder/Beam Ends (5ft.) | (LF) | 270 | 270 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 201 | 4 | Unpainted Steel Column or Pile Extension | (EA) | 4 | 0 | 0 % | 0 | 0 % | 4 | 100 % | 0 | 0 % | 0 | 0 % |
| 205 | 4 | Reinforced Conc Column or Pile Extension | (EA) | 8 | 8 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 215 | 4 | Reinforced Conc Abutment | (LF) | 160 | 143 | 89 % | 11 | 7 % | 6 | 4 % | 0 | 0 % | 0 | 0 % |
| 234 | 4 | Reinforced Conc Cap | (LF) | 160 | 156 | 98 % | 4 | 3 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 300 | 4 | Strip Seal Expansion Joint | (LF) | 160 | 17 | 11 % | 119 | 74 % | 24 | 15 % | 0 | 0 % | 0 | 0 % |
| 309 | 4 | Pourable Joint Seal | (LF) | 160 | 70 | 44 % | 40 | 25 % | 50 | 31 % | 0 | 0 % | 0 | 0 % |
| 310 | 4 | Elastomeric Bearing | (EA) | 54 | 46 | 85 % | 8 | 15 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 316 | 4 | Steel Bearing Assembly | (EA) | 46 | 15 | 33 % | 15 | 33 % | 0 | 0 % | 16 | 35 % | 0 | 0 % |
| 321 | 4 | Reinforced Conc Approach Slab w/ or w/o AC O | (EA) | 2 | 0 | 0 % | 2 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 331 | 4 | Reinforced Conc Bridge Railing | (LF) | 499 | 417 | 84 % | 82 | 16 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 358 | 4 | Deck Cracking | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 359 | 4 | Soffit of Concrete Deck or Slab | (EA) | 1 | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 363 | 4 | Section Loss | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 366 | 4 | Exposed Abutment Piling | (EA) | 1 | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 368 | 4 | Erosion Smart Flag | (EA) | 1 | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |

Additional Elements

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|--|
| 26 | FX - Minor wear noted in the wheel lanes. Some minor to moderate spalling is present along the joints. The deck armor is damaged at each end with portions of the armor missing (22' N & 10' S). Also see SF #358. |
| 109 | < none > |
| 119 | < none > |
| 201 | PX - There are 4 piles exposed up to 6" at the NE area due to slippage of the slopewalls & erosion. All have light exfoliation present. |
| 205 | < none > |
| 215 | FX - Some light to moderate cracks & delaminations are developing at the corners of some pedestals on the South (# 3, 4, 5, & 6). Some cracks & a few spalls are present around the East anchor bolts (2010 photo; typical) on the North abutment at the 1st thru the 7th bearings (9 total). Some cracking also developing at the SE, NE, & NW areas below the seat. Minor water staining noted overall due to joint seepage. Still solid at this time. |
| 234 | Some very minor cracks & stains noted on the West end of the 1st cap & below the light pole pedestal on bent #2. Water staining is evident in most areas. |
| 300 | PX - There is 12 of the armor missing at each bent joint (2006 photo at bent #1) with seepage below. Lots of spalling along each joint as well (2006 photo at bent #1). |
| 309 | PX - The sealant over each abutment is damaged with seepage evident below. Some spalling along the North armor - the South armor is damaged for approximately 22. |
| 310 | The elastomeric pads are all in good condition. |
| 316 | PX - ALL OF THE ANCHOR BOLTS ON THE SOUTH ABUTMENT ARE BROKEN EXCEPT FOR BOTH ON #9 (2010 PHOTO). NEEDS REPAIR SOON. (16 = CS4, REST ARE 50/50 CS1 or 2. |
| 321 | Both slabs have cracks which have not been sealed. The south slab has also been mud-jacked in the past on the SE area. Still functioning as intended. |
| 331 | Some light vertical cracking is present in almost every 10' section on each side. Still solid at this time. |
| 358 | FX - Some light to moderate un-sealed cracks are present. |
| 359 | Some defects noted below the rails near the joints. Less than 2% of the total area is affected. |
| 363 | Loss to the exposed abutment piles is not serious. |
| 366 | PX - There are some piles exposed up to 4" at the NE due to slippage of the slopewalls & erosion. |
| 368 | PX - There are some piles exposed up to 4" at the NE due to slippage of the slopewalls & erosion. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **21356** Structure No.: **5515 0566EX** Local ID: **-1** Suff. Rating: **86.3** Health Index : **92.4**
 Not Deficient

| IDENTIFICATION | INSPECTION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|---|---|---|---|--|--|--|--|--|---|--|---|---|--|--|---|---|--|--|---|---|--|--|---------|--|----------------------------------|----------------------------|----------|-------------------------------|--------------------------|---|---------------------------|----------------------------|------------------------------------|--------------------------|------------------------------|----------------------------------|--------------------------------|-------------------------------------|-----------------------------------|-----------------------------------|---|--|------------------------|-----------|-----------------------------|--|-----------|---------------------------------|--------------------------------|---|----------------|---------------------|----------------------------------|--|--------------|-----------------------------------|--------------------------|-----------------------|--------------------------|---------------------------------|-------------------------|-------------------------------------|----------|---|--|----------|-------------------------|-------------------------------------|--|--|------------------------------------|--|-----------------------------------|----------------------------------|
| Description: 101',4(100'),2-100',101 PRESTRESSED CONC BM SPANS SK. 74 DEG. 1. State:Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. CITY Admin. Area: Unknown 5. Inventory Route (Route On Structure): 1 - 1 - 1 - 00035 - 0 6. Feature Intersected: OKLAHOMA RIVER 7. Facility Carried: I-35 I-35 9. Location: 5.6 MI N CLEVELAND CO 11. Mile Post: 5.659 mi 13. LRS Inv. Route./Subroute.: 5515HP0000 05 16. Latitude: 35 27 36.09 17. Longitude: 097 29 29.41 98. Border Br. Code: Jkknown (P) % Resp.: 0 99. Border Br. #: Unknown | <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;">Type</th> <th style="text-align: left;">Insp Req.</th> <th style="text-align: left;">Insp Done</th> <th style="text-align: left;">Freq.</th> <th style="text-align: left;">Insp. Date:</th> <th style="text-align: left;">Next Insp.:</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>Y</td> <td>24</td> <td>3/28/2012</td> <td>3/28/2014</td> </tr> <tr> <td>FC Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> </tbody> </table> | Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | NBI: | | Y | 24 | 3/28/2012 | 3/28/2014 | FC Freq.: | N | N | NA | NA | NA | UW Freq.: | N | N | NA | NA | NA | OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NBI: | | Y | 24 | 3/28/2012 | 3/28/2014 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| FC Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| UW Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 108B. Membrane: 8 Unknown | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108C. Deck Protection: 1 Epoxy Coated Reinforci | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 102. Dir. of Traffic 1 1-way traffic 103. Temp. Structure: Not Applicable (P) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 104. Highway System: 1 On the NHS 105. Fed. Land Hwy 0 N/A (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 110. National Truck Network: 1 Part of natl ne 112. NBIS Length: Long Enough | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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Underclearance, Vertical and Horizontal: N Not applicable (NBI) | 71. Waterway Adequacy: 8 Equal Desirable | 72. Approach Alignment: 8 Equal Desirable Crit | 113. Scour Critical: U Unknown Scour | <table border="1" style="width: 100%; border-collapse: collapse;"> <tbody> <tr> <td>200c. Temperature: 65</td> <td>214a. Posted Weight Limit: NR</td> <td>243. Girder Spacing/Number : -1.0 / 9</td> </tr> <tr> <td>200d. Weather: CLEAR</td> <td>b. Posted Speed Limit : 60</td> <td>244. Span Lengths :</td> </tr> <tr> <td>201. Structural Steel ASTM Desig.: A-36 20</td> <td>c. Narrow/One Lane Bridge sign : N</td> <td>101 100 100</td> </tr> <tr> <td>202. Waterproof Membrane :-1</td> <td>d. Vertical Clearance Sign: NO</td> <td>100 100 101</td> </tr> <tr> <td>Date Installed: 1/1/1901</td> <td>Advanced Warning Sign : NO</td> <td>100 100</td> </tr> <tr> <td>203. Type Exp. Dev. :Elastomeric Sheet Type Strip Seal</td> <td>Existing/Recommended Posting : N</td> <td>245. Girder Depth : -1.000</td> </tr> <tr> <td>Pourable</td> <td>Min./ Max Vert. Clearance : N</td> <td>246. Type of Overlay : _</td> </tr> <tr> <td>204. Type of Handrail: Sloped Faced Parapet</td> <td>e. Navigation Lights : NO</td> <td>246. Overlay Thickness : 0</td> </tr> <tr> <td>205. Material and Quantity : 136.0</td> <td>Working/Not Working : NO</td> <td>246. Overlay Date : 1/1/1901</td> </tr> <tr> <td>208. Type of Abutment : Skeleton</td> <td>215. Overpass : A - Interstate</td> <td>246. Overlay Depth Changed > 1"? No</td> </tr> <tr> <td>Type of Foundation : Steel Piling</td> <td>221. Substructure Cond. (U/W) : -</td> <td>247. Protective Systems : 1: DPWR/Epoxy Coate</td> </tr> <tr> <td>209. Type of Pier / Found.: 3 Piers No</td> <td>222. Fill over RCB: -1</td> <td>2: _ 3: _</td> </tr> <tr> <td>Drilled Shaft - No Footings</td> <td>223. Appr. Slab/Rdwy Cond.: Satisfactory</td> <td>4: _ 5: _</td> </tr> <tr> <td>210. Foundation Elev. -3.0 -3.0</td> <td>224. Critical Feature Type: -1</td> <td>248. No. of Field Splices w/ Corrosion : -1</td> </tr> <tr> <td>-3.0 -1.0 -1.0</td> <td>225. Paint Type : -</td> <td>249. Scour Crit. POA exists?: No</td> </tr> <tr> <td>211. Wear. Surf. Prot. System :Other Silanes</td> <td>Overcoat : 0</td> <td>250. Culvert Headwall Dist.: -1.0</td> </tr> <tr> <td>Date Installed: 1/1/1901</td> <td>226. Date Painted: -1</td> <td>254. Thru Truss Type : _</td> </tr> <tr> <td>213. Utilities Attached : Power</td> <td>227. Paint Coloring: -1</td> <td>256. Chan. Profile Up/Down Stream?:</td> </tr> <tr> <td>-1 -1 -1</td> <td>233. Deck Forming: Conventional Forming</td> <td>258. Plans w/ found. are in file at ODOT</td> </tr> <tr> <td>-1 -1 -1</td> <td>236. Deck Cleaning : -1</td> <td>259. Scour Eval. is in file at ODOT</td> </tr> <tr> <td></td> <td>238. School Bus Rte: Current and Desired Route</td> <td>263. Interchange at Intersection N</td> </tr> <tr> <td></td> <td>240. Appr. Roadway Type: Concrete</td> <td>264. Interstate Milepoint 126.47</td> </tr> </tbody> </table> | 200c. Temperature: 65 | 214a. Posted Weight Limit: NR | 243. Girder Spacing/Number : -1.0 / 9 | 200d. Weather: CLEAR | b. Posted Speed Limit : 60 | 244. Span Lengths : | 201. Structural Steel ASTM Desig.: A-36 20 | c. Narrow/One Lane Bridge sign : N | 101 100 100 | 202. Waterproof Membrane :-1 | d. Vertical Clearance Sign: NO | 100 100 101 | Date Installed: 1/1/1901 | Advanced Warning Sign : NO | 100 100 | 203. Type Exp. Dev. :Elastomeric Sheet Type Strip Seal | Existing/Recommended Posting : N | 245. Girder Depth : -1.000 | Pourable | Min./ Max Vert. Clearance : N | 246. Type of Overlay : _ | 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights : NO | 246. Overlay Thickness : 0 | 205. Material and Quantity : 136.0 | Working/Not Working : NO | 246. Overlay Date : 1/1/1901 | 208. Type of Abutment : Skeleton | 215. Overpass : A - Interstate | 246. Overlay Depth Changed > 1"? No | Type of Foundation : Steel Piling | 221. Substructure Cond. (U/W) : - | 247. Protective Systems : 1: DPWR/Epoxy Coate | 209. Type of Pier / Found.: 3 Piers No | 222. Fill over RCB: -1 | 2: _ 3: _ | Drilled Shaft - No Footings | 223. Appr. Slab/Rdwy Cond.: Satisfactory | 4: _ 5: _ | 210. Foundation Elev. -3.0 -3.0 | 224. Critical Feature Type: -1 | 248. No. of Field Splices w/ Corrosion : -1 | -3.0 -1.0 -1.0 | 225. Paint Type : - | 249. Scour Crit. POA exists?: No | 211. Wear. Surf. Prot. System :Other Silanes | Overcoat : 0 | 250. Culvert Headwall Dist.: -1.0 | Date Installed: 1/1/1901 | 226. Date Painted: -1 | 254. Thru Truss Type : _ | 213. Utilities Attached : Power | 227. Paint Coloring: -1 | 256. Chan. Profile Up/Down Stream?: | -1 -1 -1 | 233. Deck Forming: Conventional Forming | 258. Plans w/ found. are in file at ODOT | -1 -1 -1 | 236. Deck Cleaning : -1 | 259. Scour Eval. is in file at ODOT | | 238. School Bus Rte: Current and Desired Route | 263. Interchange at Intersection N | | 240. Appr. Roadway Type: Concrete | 264. Interstate Milepoint 126.47 |
| APPRAISAL | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 36A. Bridge Rail: 1 Meets Standards 36C. Approach Rail: 1 Meets Standards | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 36B. Transition: 1 Meets Standards 36D. Approach Rail Ends: 1 Meets Standards | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 67. Str. Evaluation: 5 Above Min Tolerable 68. Deck Geometry: 6 Equal Min Criteria | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 69. Underclearance, Vertical and Horizontal: N Not applicable (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 71. Waterway Adequacy: 8 Equal Desirable | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 72. Approach Alignment: 8 Equal Desirable Crit | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 113. Scour Critical: U Unknown Scour | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 200c. Temperature: 65 | 214a. Posted Weight Limit: NR | 243. Girder Spacing/Number : -1.0 / 9 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 200d. Weather: CLEAR | b. Posted Speed Limit : 60 | 244. Span Lengths : | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 201. Structural Steel ASTM Desig.: A-36 20 | c. Narrow/One Lane Bridge sign : N | 101 100 100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 202. Waterproof Membrane :-1 | d. Vertical Clearance Sign: NO | 100 100 101 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed: 1/1/1901 | Advanced Warning Sign : NO | 100 100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 203. Type Exp. Dev. :Elastomeric Sheet Type Strip Seal | Existing/Recommended Posting : N | 245. Girder Depth : -1.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Pourable | Min./ Max Vert. Clearance : N | 246. Type of Overlay : _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights : NO | 246. Overlay Thickness : 0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 205. Material and Quantity : 136.0 | Working/Not Working : NO | 246. Overlay Date : 1/1/1901 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 208. Type of Abutment : Skeleton | 215. Overpass : A - Interstate | 246. Overlay Depth Changed > 1"? No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type of Foundation : Steel Piling | 221. Substructure Cond. (U/W) : - | 247. Protective Systems : 1: DPWR/Epoxy Coate | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 209. Type of Pier / Found.: 3 Piers No | 222. Fill over RCB: -1 | 2: _ 3: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Drilled Shaft - No Footings | 223. Appr. Slab/Rdwy Cond.: Satisfactory | 4: _ 5: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 210. Foundation Elev. -3.0 -3.0 | 224. Critical Feature Type: -1 | 248. No. of Field Splices w/ Corrosion : -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -3.0 -1.0 -1.0 | 225. Paint Type : - | 249. Scour Crit. POA exists?: No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 211. Wear. Surf. Prot. System :Other Silanes | Overcoat : 0 | 250. Culvert Headwall Dist.: -1.0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed: 1/1/1901 | 226. Date Painted: -1 | 254. Thru Truss Type : _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 213. Utilities Attached : Power | 227. Paint Coloring: -1 | 256. Chan. Profile Up/Down Stream?: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 233. Deck Forming: Conventional Forming | 258. Plans w/ found. are in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 236. Deck Cleaning : -1 | 259. Scour Eval. is in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 238. School Bus Rte: Current and Desired Route | 263. Interchange at Intersection N | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 240. Appr. Roadway Type: Concrete | 264. Interstate Milepoint 126.47 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 21356

Structure No.: 5515 0566EX

Local ID:-1

Suff. Rating: 86.3
Not Deficient

Health Index :
92.4

Inspection Date: 3/28/2012

Reported By: JCARTER

Invoice No.: -1

Inspected With: -1

Agency :

Jason Carter

Digitally signed by Jason Carter
DN: cn=Jason Carter, c=US
Date: 2012.05.10 08:13:59 -0500'

Structure / Inspection Notes

NOTE:INSP.W/SNOOPER.G Hines inspection comments- 6/27/2005-

PX- The SE & the NE approach rail ends need to be updated to meet safety standards.

There are small reflectors mounted on the outer piers in the bents that are in the river - they could be larger in size * Overhead clearance to highway traffic sign = 18.6' * Good riprap on the river banks (some damage 150' west of the structure) * Light poles on the East side only * PX- needs slopewalls added on each abutment * Channel is clear & under spans #2 thru #7 * The girder end blocks at bents #2, 3, & 4 are cracking & spalling but they are not into the beams themselves YET * Structure needs joint repairs ASAP!

| Elem. | Env. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|-------|------|--|------|--------|-----------|------|-----------|------|-----------|-----|-----------|-----|-----------|-----|
| 26 | 4 | Concrete Deck - Protected w/ Coated Bars | (SF) | 54,536 | 0 | 0% | 54,536 | 100% | 0 | 0% | 0 | 0% | 0 | 0% |
| 109 | 4 | P/S Conc Open Girder/Beam | (LF) | 7,268 | 7,258 | 100% | 10 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| 119 | 4 | P/S Conc Open Girder/Beam Ends (5ft.) | (LF) | 450 | 400 | 89% | 45 | 10% | 5 | 1% | 0 | 0% | 0 | 0% |
| 201 | 4 | Unpainted Steel Column or Pile Extension | (EA) | 7 | 0 | 0% | 3 | 40% | 4 | 60% | 0 | 0% | 0 | 0% |
| 205 | 4 | Reinforced Conc Column or Pile Extension | (EA) | 22 | 18 | 82% | 3 | 14% | 1 | 5% | 0 | 0% | 0 | 0% |
| 215 | 4 | Reinforced Conc Abutment | (LF) | 188 | 170 | 90% | 18 | 10% | 0 | 0% | 0 | 0% | 0 | 0% |
| 234 | 4 | Reinforced Conc Cap | (LF) | 517 | 480 | 93% | 37 | 7% | 0 | 0% | 0 | 0% | 0 | 0% |
| 300 | 4 | Strip Seal Expansion Joint | (LF) | 280 | 79 | 28% | 187 | 67% | 14 | 5% | 0 | 0% | 0 | 0% |
| 309 | 4 | Pourable Joint Seal | (LF) | 408 | 160 | 39% | 248 | 61% | 0 | 0% | 0 | 0% | 0 | 0% |
| 310 | 4 | Elastomeric Bearing | (EA) | 154 | 154 | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| 316 | 4 | Steel Bearing Assembly | (EA) | 154 | 100 | 65% | 54 | 35% | 0 | 0% | 0 | 0% | 0 | 0% |
| 331 | 4 | Reinforced Conc Bridge Railing | (LF) | 1,604 | 1,393 | 87% | 211 | 13% | 0 | 0% | 0 | 0% | 0 | 0% |
| 358 | 4 | Deck Cracking | (EA) | 1 | 0 | 0% | 1 | 100% | 0 | 0% | 0 | 0% | 0 | 0% |
| 359 | 4 | Soffit of Concrete Deck or Slab | (EA) | 1 | 1 | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| 362 | 4 | Traffic Impact | (EA) | 1 | 0 | 0% | 1 | 100% | 0 | 0% | 0 | 0% | 0 | 0% |
| 363 | 4 | Section Loss | (EA) | 1 | 0 | 0% | 1 | 100% | 0 | 0% | 0 | 0% | 0 | 0% |
| 366 | 4 | Exposed Abutment Piling | (EA) | 1 | 0 | 0% | 1 | 100% | 0 | 0% | 0 | 0% | 0 | 0% |
| 368 | 4 | Erosion Smart Flag | (EA) | 1 | 0 | 0% | 1 | 100% | 0 | 0% | 0 | 0% | 0 | 0% |

Additional
Elements

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|--|
| 26 | FX - Initial wear in wheel lanes w/ exposed aggregate. Numerous small to moderate spalls along joint armor at piers # 1, 5, 6, & 7. |
| 109 | FX - Some minor to moderate damage on the outer beams in span # 3 from equipment used on the MAPS project. No exposed steel noted (photo).MINOR SPALLING & CRACKING @ LINKAGE BLOCKS. |
| 119 | FX:MINOR SPALLS @ P.5,BM.3,5 & 9. |
| 201 | PX:There are 3 exposed piles on the South abutment & 5 on the North abutment. There is some active exfoliation on the ones at the SE, NE, & near the center of the North abutment. Up to 40% loss estimated at the center one on the North abutment (photo). Heavy surface rust & scaling on the other two visible piles. Loss not critical YET. |
| 205 | FX:MINOR SPALL W/EXPOSED REBAR @ P.5,CENTER COL.Minor spall near the ground line on # 3 in bent # 2 (photo). Some minor cracking noted on # 2, 3, & 4 in bent # 7 where the form support holes were grouted in - not serious. |
| 215 | FX - Some minor cracking in scattered areas on each abutment mainly near the lower corners of some pedestals. Some cracks have minor staining & efflorescence. There are some light vertical cracks above the seat on the North abutment. The pedestals on the North have some minor separation from the backwall - not serious (photo). Solid overall |
| 234 | FX - Hairline vertical/horizontal cracks exist on each cap mainly near the upper edge of the seats at the corners of # 2, # 8, & the light pedestals due to cantilever effects (see close-up photo). Not serious at this time. |
| 300 | PX - Lots of soil & debris in joint - needs changed to a pourable type seal ASAP. |
| 309 | PX - The sealant is deteriorated & failing at each joint mainly in the wheel lanes. There's a total of 6 construction joints in spans # 2 thru # 5. The joints don't appear to be full-depth so there weren't any 'ends' counted for those areas. |
| 310 | FX - Some minor deformation noted on some pads at each abutment. |
| 316 | |
| 331 | FX - Light vertical cracking every 6' to 8' in rail on each side. Some minor traffic damage noted in a few areas. The water-proof coating is peeled & failing all areas. |
| 358 | FX - Some moderate transverse cracks noted in spans # 4 & # 5. Also some minor pattern cracking present in a few other areas |
| 359 | FX - Less than 2% of the total area is distressed - mainly below curbs near the expansion joints. No steel noted. |
| 362 | FX - Some minor to moderate damage on the outer beams in span # 3 from equipment used on the MAPS project. No exposed steel noted (photo). |
| 363 | PX:Loss to exposed piles not critical at this time - up to 40% estimated maximum |
| 366 | PX - Erosion is exposing the abutment piling at the corners of each abutment & near the center on the North. Up to 16 inches of the piles are visible. Both abutments need a slopewall added SOON. 3 @ S.ABUT.& 5 @ N.ABUT. |
| 368 | PX:Erosion at both abuts needs repaired. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **21708** Structure No.: **5515 0534WX** Local ID: **15534W** Suff. Rating: **86.3** Health Index : **91.0**
 Not Deficient

| <p>Description: <u>IDENTIFICATION</u></p> <p>75'-100'-75" P.C. BEAM SPANS SK. 60 DEG. 57'-11"</p> <p>1. State: Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. CITY Admin. Area: Unknown</p> <p>5. Inventory Route (Route On Structure): 1 - 1 - 1 - 00035 - 0 6. Feature Intersected: B.N. & S.F. R.R. UNDER 7. Facility Carried: I-35 SB I-35 SOUTH-BOUND 9. Location: 5.4 MI N CLEVELAND CO 11. Mile Post: 5.339 mi 13. LRS Inv. Route./ Subroute.: 5515HP0000 05 16. Latitude: 35 27 15.05 17. Longitude: 097 29 23.33 98. Border Br. Code: Not Applicab % Resp.: 0 99. Border Br. #: Unknown</p> <p style="text-align: center;"><u>STRUCTURE TYPE AND MATERIALS</u></p> <p>43. Main Span Material and Design Type Prestressed Concrete Stringer/Girder 44. Approach Span Material and Design Type Not Applicable (P) Not Applicable (P) 45. No. of Spans Main Unit: 3 46. No. of Approach Spans: 0 107. Deck Type: 1 Concrete-Cast-in-Place 108A. Wearing Surface: 1 Monolithic Concrete 108B. Membrane: 0 None 108C. Deck Protection: 1 Epoxy Coated Reinforci</p> <p style="text-align: center;"><u>AGE AND SERVICE</u></p> <p>27. Year Built: 1987 106. Year Reconstructed: -4 28A. Lanes on: 4 28B. Lanes Under: 0 19. Detour Length: 0.1 mi 29. ADT: 68400 30. Year of ADT: 2010 109. Truck ADT %: 12 42A. Type of Service on: 1 Highway 42B. Type of Service under: 2 Railroad</p> <p style="text-align: center;"><u>GEOMETRIC DATA</u></p> <p>10. Inv. Rte. Min. Vert. Clr.: 328.1 ft 32. Approach Roadway Width (W/ Shoulders): 68.0 ft Deck Area: 17,749.7 sq. ft 33. Median: 0 No median 34. Skew: 30 35. Structure Flared: 0 No flare 47. Inv. Rte. Total Horiz. Clr.: 68.0 ft 48. Length Maximum Span: 100.0 ft 49. Structure Length: 250.0 ft 50A. Curb/Sdwk Width L: 0.0 ft 50B. Curb/Sidewalk Width R: 0.0 ft 51. Width Curb to Curb: 68.0 ft 52. Width Out to Out: 71.0 ft 53. Minimum Vertical Clearance Over Bridge: 328.1 ft 54A/54B. Min. Vert. Underclearance : R Railroad beneath struc 23.2 ft</p> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 25%; text-align: center;"><u>N/E</u></td> <td style="width: 25%; text-align: center;"><u>S/W</u></td> <td style="width: 25%;"></td> <td style="width: 25%;"></td> </tr> <tr> <td>Meas. -1 -1 R2304 -1 -1 -1</td> <td></td> <td></td> <td></td> </tr> <tr> <td>Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U</td> <td></td> <td></td> <td></td> </tr> </table> <p>55A/55B. Minimum Lateral Underclearance R: R Railroad beneath struc 22.5 ft 56. Minimum Lateral Underclearance L: 0.0 ft</p> | <u>N/E</u> | <u>S/W</u> | | | Meas. -1 -1 R2304 -1 -1 -1 | | | | Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U | | | | <p style="text-align: center;"><u>INSPECTION</u></p> <table style="width: 100%; border-collapse: collapse;"> <tr> <th style="text-align: left;">Type</th> <th style="text-align: left;">Insp Req.</th> <th style="text-align: left;">Insp Done</th> <th style="text-align: left;">Freq:</th> <th style="text-align: left;">Insp. Date:</th> <th style="text-align: left;">Next Insp.:</th> </tr> <tr> <td>NBI:</td> <td></td> <td>Y</td> <td>24</td> <td>7/16/2012</td> <td>7/16/2014</td> </tr> <tr> <td>FC Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> </table> <p style="text-align: center;"><u>CLASSIFICATION</u></p> <p>12. Base Hwy Network : On Base Network 20. Toll Facility: 3 On free road 21. Custodian: 01 State Highway Agency 22. Owner: 01 State Highway Agency 26. Functional Class: 11 Urban Interstate 37. Historical Sig.: 5 Not eligible for NRHP 100. Defense Highway: 1 On Interstate STRAF 101. Parallel Structure: Left of bridge 102. Dir. of Traffic 1 1-way traffic 103. Temp. Structure: Not Applicable (P) 104. Highway System: 1 On the NHS 105. Fed. Land Hwy 0 N/A (NBI) 110. National Truck Network: 1 Part of natl ne 112. NBIS Length: Long Enough</p> <p style="text-align: center;"><u>CONDITION</u></p> <p>58. Deck: 6 Satisfactory 59. Super.: 8 Very Good 60. Sub.: 5 Fair 62. Culvert: N N/A (NBI) 61. Channel/Channel Protection: N N/A (NBI) Flowline Notes:</p> <p style="text-align: center;"><u>LOAD RATING AND POSTING</u></p> <p>31. Design Load: 5 MS 18 (HS 20) 41. Posting status: A Open, no restriction 63. Op. Rating Method: 1 LF Load Factor-Ton Alt. Op. Rating Meth.: 1 LF Load Factor-To 64. Operating Rating (H / HS / 3-3): 45.7 78.4 93.6 66. Inventory Rating (H / HS / 3-3) : 31.5 46.1 58.7 65. Inv. Rating Method: 1 LF Load Factor-Ton Alt. Inv. Rating Meth.: 1 LF Load Factor-To 70. Posting: 5 At/Above Legal Loads Date Rated : 9/23/2010</p> <p style="text-align: center;"><u>PROPOSED IMPROVEMENTS</u></p> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%;">94. Bridge Cost: \$1,726,920</td> <td style="width: 50%;">75. Type of Work: 31 Repl-Load Capacity</td> </tr> <tr> <td>95. Roadway Cost: \$2,849,418</td> <td>76. Lgth. of Improvement: 259.8 ft</td> </tr> <tr> <td>96. Total Cost: \$4,835,376</td> <td>114. Future ADT: 109440</td> </tr> <tr> <td>97. Year of Cost Est.: 2007</td> <td>115. Year of Future ADT: 2030</td> </tr> </table> <p style="text-align: center;"><u>NAVIGATION DATA</u></p> <p>38. Navigation Control: NA-no waterway 39. Vertical Clearance: 0.0 ft 40. Horizontal Clearance: 0.0 ft 111. Pier Protection: Not Applicable (P) 116. Lift Bridge Vert. Clear.: 0.0 ft</p> <p style="text-align: center;"><u>APPRAISAL</u></p> <p>36A. Bridge Rail: 1 Meets Standards 36C. Approach Rail: 1 Meets Standards 36B. Transition: 1 Meets Standards 36D. Approach Rail Ends: 1 Meets Standards 67. Str. Evaluation: 5 Above Min Tolerable 68. Deck Geometry: 6 Equal Min Criteria 69. Underclearance, Vertical and Horizontal: 9 Above Desirable 71. Waterway Adequacy: N Not applicable 72. Approach Alignment: 8 Equal Desirable Crit 113. Scour Critical: N Not Over Waterway</p> | Type | Insp Req. | Insp Done | Freq: | Insp. Date: | Next Insp.: | NBI: | | Y | 24 | 7/16/2012 | 7/16/2014 | FC Freq.: | N | N | NA | NA | NA | UW Freq.: | N | N | NA | NA | NA | OS Freq.: | N | N | NA | NA | NA | 94. Bridge Cost: \$1,726,920 | 75. Type of Work: 31 Repl-Load Capacity | 95. Roadway Cost: \$2,849,418 | 76. Lgth. of Improvement: 259.8 ft | 96. Total Cost: \$4,835,376 | 114. Future ADT: 109440 | 97. Year of Cost Est.: 2007 | 115. Year of Future ADT: 2030 |
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| <u>N/E</u> | <u>S/W</u> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Meas. -1 -1 R2304 -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type | Insp Req. | Insp Done | Freq: | Insp. Date: | Next Insp.: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NBI: | | Y | 24 | 7/16/2012 | 7/16/2014 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| FC Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| UW Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 94. Bridge Cost: \$1,726,920 | 75. Type of Work: 31 Repl-Load Capacity | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| <p>200c. Temperature: 91 200d. Weather: CLEAR 201. Structural Steel ASTM Desig.: A36 20 202. Waterproof Membrane :-1 Date Installed: 1/1/1901 203. Type Exp. Dev. :Elastomeric Sheet Type Strip Seal - 204. Type of Handrail: Sloped Faced Parapet 205. Material and Quantity : 44.0 208. Type of Abutment : Skeleton Type of Foundation : Steel Piling 209. Type of Pier / Found.: 4 No Drilled Shaft - No Footings 210. Foundation Elev. -1.0 -3.0 -3.0 -3.0 -1.0 211. Wear. Surf. Prot. System :Other Silanes Date Installed: 1/1/1901 213. Utilities Attached : Power -1 -1 -1 -1 -1 -1</p> | <p>214a. Posted Weight Limit: NR b. Posted Speed Limit : 60 c. Narrow/One Lane Bridge sign : N d. Vertical Clearance Sign: NO Advanced Warning Sign : NO Existing/Recommended Posting : 2301 2301 Min./ Max Vert. Clearance : 2304 2304 e. Navigation Lights : - Working/Not Working : - 215. Overpass : A - Interstate 221. Substructure Cond. (U/W) : - 222. Fill over RCB: -1 223. Appr. Slab/Rdwy Cond.: Good 224. Critical Feature Type: -1 225. Paint Type : - Overcoat : 0 226. Date Painted: -1 227. Paint Coloring: -1 233. Deck Forming: Conventional Forming 236. Deck Cleaning : -1 238. School Bus Rte: Current and Desired Route 240. Appr. Roadway Type: Concrete</p> | <p>243. Girder Spacing/Number : -1.0 / -1 244. Span Lengths : -1 -1 -1 -1 -1 -1 -1 -1 -1 245. Girder Depth : -1.000 246. Type of Overlay : - 246. Overlay Thickness : 0 246. Overlay Date : 1/1/1901 246. Overlay Depth Changed > 1"? No 247. Protective Systems : 1: DPWR/Epoxy Coate 2: 3: - 4: 5: - 248. No. of Field Splices w/ Corrosion : -1 249. Scour Crit. POA exists?: - 250. Culvert Headwall Dist.: -1.0 254. Thru Truss Type : - 256. Chan. Profile Up/Down Stream?: 258. Plans w/ found. are in file at ODOT 259. Scour Eval. is in file at ODOT 263. Interchange at Intersection P 264. Interstate Milepoint 126.15</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 21708 Structure No.: 5515 0534WX Local ID: 15534W

Suff. Rating: 86.3
Not Deficient

Health Index :
91.0

Inspection Date: 7/16/2012 Reported By: GHINES
Invoice No.: -1 Inspected With: Gary Richardson
Agency :

Gary Hines

Digitally signed by Gary Hines
DN: cn=Gary Hines, o=Gary Richardson,
email=ghines@dot.org, c=US
Date: 2012.08.22 08:04:29 -05'00'

Structure / Inspection Notes

THIS STRUCTURE CONTROLS THE HEIGHT POSTING FOR THIS PAIR OF BRIDGES.

G Hines inspection comments - 7/16/2012

Good side drains * Total horizontal clearance for the Railroad is 82.3' (4 tracks total) * Good slopewalls except for some settlement on the North near the top at 3rd & 4th pedestals *

| Elem. | Env. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|-------|------|--|------|--------|-----------|-------|-----------|-------|-----------|-------|-----------|------|-----------|-----|
| 26 | 4 | Concrete Deck - Protected w/ Coated Bars | (SF) | 17,000 | 0 | 0 % | 17,000 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 109 | 4 | P/S Conc Open Girder/Beam | (LF) | 1,980 | 1,980 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 119 | 4 | P/S Conc Open Girder/Beam Ends (5ft.) | (LF) | 270 | 268 | 99 % | 2 | 1 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 201 | 4 | Unpainted Steel Column or Pile Extension | (EA) | 2 | 0 | 0 % | 0 | 0 % | 2 | 100 % | 0 | 0 % | 0 | 0 % |
| 205 | 4 | Reinforced Conc Column or Pile Extension | (EA) | 8 | 6 | 75 % | 2 | 25 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 215 | 4 | Reinforced Conc Abutment | (LF) | 160 | 144 | 90 % | 13 | 8 % | 3 | 2 % | 0 | 0 % | 0 | 0 % |
| 234 | 4 | Reinforced Conc Cap | (LF) | 160 | 157 | 98 % | 2 | 1 % | 1 | 1 % | 0 | 0 % | 0 | 0 % |
| 300 | 4 | Strip Seal Expansion Joint | (LF) | 160 | 76 | 48 % | 60 | 38 % | 24 | 15 % | 0 | 0 % | 0 | 0 % |
| 309 | 4 | Pourable Joint Seal | (LF) | 160 | 76 | 48 % | 80 | 50 % | 4 | 3 % | 0 | 0 % | 0 | 0 % |
| 310 | 4 | Elastomeric Bearing | (EA) | 54 | 29 | 54 % | 25 | 46 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 316 | 4 | Steel Bearing Assenbly | (EA) | 54 | 14 | 26 % | 15 | 28 % | 0 | 0 % | 25 | 46 % | 0 | 0 % |
| 321 | 4 | Reinforced Conc Approach Slab w/ or w/o AC O | (EA) | 2 | 0 | 0 % | 1 | 50 % | 1 | 50 % | 0 | 0 % | 0 | 0 % |
| 331 | 4 | Reinforced Conc Bridge Railing | (LF) | 500 | 394 | 79 % | 106 | 21 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 358 | 4 | Deck Cracking | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 359 | 4 | Soffit of Concrete Deck or Slab | (EA) | 1 | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 363 | 4 | Section Loss | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 366 | 4 | Exposed Abutment Piling | (EA) | 1 | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 368 | 4 | Erosion Smart Flag | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |

Additional Elements _____

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|--|
| 26 | There are several patches on the west 1/3rd of span #2 with some scaling & spalls (2008 photo). Minor to moderate spalls noted along the joint armor - the armor is damaged at each end. |
| 109 | < none > |
| 119 | There are some very light diagonal cracks in the web of the 1st beam in span #1 at the South abutment. Not serious at this time. |
| 201 | PX - There is a 6" cavity below the 3rd & 4th beam on the North abutment with two piles visible. They have light exfoliation present. Need to be treated & covered soon. |
| 205 | Light pattern cracks noted at the bottom of #1 & 4 in bent #1. |
| 215 | Some moderate cracking noted on the 9th pedestal on the North abutment with a delamination present at the NW area. Minor cracking also at the SW area & below 3rd, 4th, & 5th pedestals on the South. Light water staining & cracks are present at the NW & NE corners. Still solid overall. |
| 234 | One light vertical crack present on the North face of bent #1 between the 4th & 5th pedestals Small spall on the West light pedestal. Lots of water stains noted. |
| 300 | PX - There is approx 24' of the joint armor missing with lots of spalling along each joint. |
| 309 | PX - Some spalls present along the armor at each end of the structure with seepage below. The North armor is missing 1' at the centerline & 10' on the East shoulder area. The South armor has damage to two areas with 4' total missing. Needs attention soon. |
| 310 | All pads are in good condition (see #316). |
| 316 | PX - There are 12 of 18 anchor bolts broken off on the South abutment (2006 photo; typical). There are 13 of 18 bolts broken on the North abutment as well. Some bolts on the bents appear stressed but a platform or snooper is needed to check closely. (25 = CS3, REST ARE 50/50 = CS1 & 2) |
| 321 | FX - Both approach slabs have un-sealed cracks; still functioning as intended. The North slab is broken at the West side drain - no movement noted. |
| 331 | Some light vertical cracks noted in each 10' section on each rail. Still solid overall. |
| 358 | FX - Light to moderate un-sealed cracks noted in a few areas. |
| 359 | Some spalls below the curbs near the joints. Less than 2% of the total area is affected. |
| 363 | Loss to the 2 exposed piles is not serious. |
| 366 | PX - There is a 6" cavity below the 3rd & 4th beam on the North abutment with two piles visible. They have light exfoliation present. Need to be treated & covered soon. |
| 368 | There is a 6" cavity below the 3rd & 4th beam on the North abutment with 2 piles visible. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **21722** Structure No.: **5569 1022 X** Local ID: **-1** Suff. Rating: **82.3** Health Index : **81.6**
 Not Deficient

| IDENTIFICATION | INSPECTION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|---|---|---|---|--|--|---|---|--|-------------------------------|---|-----------------|--|--|--|--------------------------------|---|--|--|--|--|---------------------------------------|------------------------------------|---|--|---|--|--------------------------------------|---|-----------------------------------|---|--|--|---|----------------------------|----------------------------------|--|---|---|---------------------------------------|----------------------------|---|--------------------------|--|---|-------------------------------------|-------------------------------------|-----------------------------------|-----------------------------------|---|--|------------------------|----------------|-----------------------------|----------------------------------|----------------|--------------------------------------|--------------------------------|---|--------------------------|---|---------------------------------|--|--------------|-----------------------------------|--|-------------------------|--------------------------|---------------------------|----------------------------|-------------------------------------|---------------------------------|---|--|--------------------|-------------------------|-------------------------------------|--------------------|--|---|--|-----------------------------------|--------------------------------------|
| Description: 160',2-205',142' CONT PLATE GIRDER SPANS EAST END SK.45 DEG. 1. State:Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. CITY Admin. Area: Unknown 5. Inventory Route (Route On Structure) : 1 - 1 - 1 - 00040 - 0 6. Feature Intersected: I-35 S-W, I-35 E-S UNDER 7. Facility Carried: I-40 EB I-40 E.B. 9. Location: 4.5 MI E OF MAY AVE. 11. Mile Post: 10.218 mi 13. LRS Inv. Route./ Subroute.: 5569HP0000 09 16. Latitude: 35 27 43.76 17. Longitude: 097 29 37.49 98. Border Br. Code: Not Applicab % Resp.: 0 99. Border Br. #: Unknown | <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>Type</th> <th>Insp Req.</th> <th>Insp Done</th> <th>Freq:</th> <th>Insp. Date:</th> <th>Next Insp.:</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>Y</td> <td>24</td> <td>1/5/2012</td> <td>1/5/2014</td> </tr> <tr> <td>FC Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> </tbody> </table> | Type | Insp Req. | Insp Done | Freq: | Insp. Date: | Next Insp.: | NBI: | | Y | 24 | 1/5/2012 | 1/5/2014 | FC Freq.: | N | N | NA | NA | NA | UW Freq.: | N | N | NA | NA | NA | OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| NBI: | | Y | 24 | 1/5/2012 | 1/5/2014 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| FC Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| UW Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 45. No. of Spans Main Unit: 4 46. No. of Approach Spans: 0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 107. Deck Type: 1 Concrete-Cast-in-Place | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108A. Wearing Surface: 1 Monolithic Concrete | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108B. Membrane: 0 None | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108C. Deck Protection: 1 Epoxy Coated Reinforci | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 12. Base Hwy Network : On Base Network | 20. Toll Facility: 3 On free road | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21. Custodian: 01State Highway Agency | 22. Owner: 01 State Highway Agency | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26. Functional Class: 11 Urban Interstate | 37. Historical Sig.: 5 Not eligible for NRHP | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 100. Defense Highway: 1 On Interstate STRAF | 101. Parallel Structure: No bridge exists | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 102. Dir. of Traffic 1 1-way traffic | 103. Temp. Structure: Not Applicable (P) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 104. Highway System: 1 On the NHS | 105. Fed. Land Hwy 0 N/A (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 110. National Truck Network: 1 Part of natl ne | 112. NBIS Length: Long Enough | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 42A. Type of Service on: 6 2d level interchg | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 42B. Type of Service under: 1 Highway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 10. Inv. Rte. Min. Vert. Clr.: 328.1 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 47. Inv. Rte. Total Horiz. Clr.: 92.2 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 48. Length Maximum Span: 205.0 ft 49. Structure Length: 712.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 50A. Curb/Sdwk Width L: 0.0 ft 50B. Curb/Sidewalk Width R: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 51. Width Curb to Curb: 92.2 ft 52. Width Out to Out: 95.2 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 53. Minimum Vertical Clearance Over Bridge: 328.1 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 54A/54B. Min. Vert. Underclearance : H Hwy beneath struct 17.3 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| N/E S/W | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Meas. - - NP1705 -1 -1 SP4507 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 55A/55B. Minimum Lateral Underclearance R: H Hwy beneath struct 5.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 56. Minimum Lateral Underclearance L: 10.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 63. Op. Rating Method: 1 LF Load Factor-Ton | Alt. Op. Rating Meth.: 1 LF Load Factor-To | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 64. Operating Rating (H / HS / 3-3) : | 31.3 56.3 76.8 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 65. Inv. Rating Method: 1 LF Load Factor-Ton | Alt. Inv. Rating Meth.: 1 LF Load Factor-To | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 70. Posting: 5 At/Above Legal Loads | Date Rated : 6/23/2011 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| NAVIGATION DATA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 38. Navigation Control: NA-no waterway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 111. Pier Protection: Not Applicable (P) | 116. Lift Bridge Vert. Clear.: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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Dev. :Elastomeric Sheet Type Strip Seal</td> <td>Existing/Recommended Posting : 1702 1605</td> <td>245. Girder Depth : 70.000</td> </tr> <tr> <td>-</td> <td>Min./ Max Vert. Clearance : 1705 2007</td> <td>246. Type of Overlay : _</td> </tr> <tr> <td>204. Type of Handrail: Sloped Faced Parapet</td> <td>e. Navigation Lights : -</td> <td>246. Overlay Thickness : 0</td> </tr> <tr> <td>205. Material and Quantity : -3.0</td> <td>Working/Not Working : -</td> <td>246. Overlay Date : 1/1/1901</td> </tr> <tr> <td>208. Type of Abutment : Skeleton</td> <td>215. Overpass : A - Interstate</td> <td>246. Overlay Depth Changed > 1"? No</td> </tr> <tr> <td>Type of Foundation : Steel Piling</td> <td>221. Substructure Cond. (U/W) : -</td> <td>247. Protective Systems : 1: DPWR/Epoxy Coate</td> </tr> <tr> <td>209. Type of Pier / Found.: 3 Piers No</td> <td>222. Fill over RCB: -1</td> <td>2: _ 3: _</td> </tr> <tr> <td>Drilled Shaft - No Footings</td> <td>223. Appr. Slab/Rdwy Cond.: Good</td> <td>4: _ 5: _</td> </tr> <tr> <td>210. Foundation Elev. -3.0 -3.0</td> <td>224. Critical Feature Type: -1</td> <td>248. No. of Field Splices w/ Corrosion : -1</td> </tr> <tr> <td>-3.0 -3.0 -1.0</td> <td>225. Paint Type : Inorganic Zinc 2 Coat</td> <td>249. Scour Crit. POA exists?: _</td> </tr> <tr> <td></td> <td>Overcoat : 0</td> <td>250. Culvert Headwall Dist.: -1.0</td> </tr> <tr> <td>211. Wear. Surf. Prot. System :Other Silanes</td> <td>226. Date Painted: 0687</td> <td>254. Thru Truss Type : _</td> </tr> <tr> <td>Date Installed : 1/1/1901</td> <td>227. Paint Coloring: Brown</td> <td>256. Chan. Profile Up/Down Stream?:</td> </tr> <tr> <td>213. Utilities Attached : Power</td> <td>233. Deck Forming: Conventional Forming</td> <td>258. Plans w/ found. are in file at ODOT</td> </tr> <tr> <td>-1 -1 -1</td> <td>236. Deck Cleaning : -1</td> <td>259. Scour Eval. is in file at ODOT</td> </tr> <tr> <td>-1 -1 -1</td> <td>238. 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| 36A. Bridge Rail: 1 Meets Standards | 36C. Approach Rail: 1 Meets Standards | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 36B. Transition: 1 Meets Standards | 36D. Approach Rail Ends: 1 Meets Standards | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 67. Str. Evaluation: 7 Above Min Criteria | 68. Deck Geometry: 9 Above Desirable Crit | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 69. Underclearance, Vertical and Horizontal: 4 Tolerable | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 71. Waterway Adequacy: N Not applicable | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 72. Approach Alignment: 8 Equal Desirable Crit | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 113. Scour Critical: N Not Over Waterway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 200c. Temperature: 48 | 214a. Posted Weight Limit: NR | 243. Girder Spacing/Number : 8,8 / -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 200d. Weather: CLEAR | b. Posted Speed Limit : 60 | 244. Span Lengths : | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 201. Structural Steel ASTM Desig.: A36 -1 | c. Narrow/One Lane Bridge sign : N | 160 142 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 202. Waterproof Membrane :-1 | d. Vertical Clearance Sign: YES | 202 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed : 1/1/1901 | Advanced Warning Sign : NO | 202 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 203. Type Exp. Dev. :Elastomeric Sheet Type Strip Seal | Existing/Recommended Posting : 1702 1605 | 245. Girder Depth : 70.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| - | Min./ Max Vert. Clearance : 1705 2007 | 246. Type of Overlay : _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights : - | 246. Overlay Thickness : 0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 205. Material and Quantity : -3.0 | Working/Not Working : - | 246. Overlay Date : 1/1/1901 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 208. Type of Abutment : Skeleton | 215. Overpass : A - Interstate | 246. Overlay Depth Changed > 1"? No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type of Foundation : Steel Piling | 221. Substructure Cond. (U/W) : - | 247. Protective Systems : 1: DPWR/Epoxy Coate | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 209. Type of Pier / Found.: 3 Piers No | 222. Fill over RCB: -1 | 2: _ 3: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Drilled Shaft - No Footings | 223. Appr. Slab/Rdwy Cond.: Good | 4: _ 5: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 210. Foundation Elev. -3.0 -3.0 | 224. Critical Feature Type: -1 | 248. No. of Field Splices w/ Corrosion : -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -3.0 -3.0 -1.0 | 225. Paint Type : Inorganic Zinc 2 Coat | 249. Scour Crit. POA exists?: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Overcoat : 0 | 250. Culvert Headwall Dist.: -1.0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 211. Wear. Surf. Prot. System :Other Silanes | 226. Date Painted: 0687 | 254. Thru Truss Type : _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed : 1/1/1901 | 227. Paint Coloring: Brown | 256. Chan. Profile Up/Down Stream?: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 213. Utilities Attached : Power | 233. Deck Forming: Conventional Forming | 258. Plans w/ found. are in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 236. Deck Cleaning : -1 | 259. Scour Eval. is in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 238. School Bus Rte: Current and Desired Route | 263. Interchange at Intersection 2 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 240. Appr. Roadway Type: Concrete | 264. Interstate Milepoint -1.00 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 21722 Structure No.: 5569 1022 X Local ID: -1

Suff. Rating: 82.3
Not Deficient

Health Index :
81.6

Inspection Date: 1/5/2012 Reported By: GHINES
Invoice No.: -1 Inspected With: Gary Richardson
Agency :

Gary Hines

Digitally signed by Gary Hines
DN: cn=Gary Hines, o=Gary Richardson,
email=ghines@dot.org, c=US
Date: 2012.02.01 10:11:56 -06'00'

Structure / Inspection Notes

I-35 N.B. TO I-40 W.B./ I-235 N.B. IS UNDER SPAN #2 (NBI #22096).
I-35 S.B./I-40 W.B. TO I-35 S.B. IS UNDER SPAN #3 (GROUND LEVEL).
STRUCTURE WAS WIDENED 14.5' TO THE SOUTH IN 2011.

G Hines inspection comments - 1/5/2012

FX - The pipes for the deck drains should be extended further below the bottom flanges to reduce water exposure * Good slopewalls * Good concrete roadway * PX - The approach railing needs updated end treatments & block-outs to meet current standards * FX - Posting for NBI 22096 needs to be moved to the South beam in Span #2 *

| Elm. | Qty. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|------|------|--|------|--------|-----------|-------|-----------|------|-----------|-------|-----------|-----|-----------|-----|
| 26 | 4 | Concrete Deck - Protected w/ Coated Bars | (SF) | 65,646 | 65,646 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 107 | 4 | Painted Steel Open Girder/Beam | (LF) | 8,280 | 1,260 | 15 % | 7,020 | 85 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 165 | 4 | Painted Steel Open Girder/Beam end | (LF) | 120 | 19 | 16 % | 96 | 80 % | 5 | 4 % | 0 | 0 % | 0 | 0 % |
| 205 | 4 | Reinforced Conc Column or Pile Extension | (EA) | 9 | 9 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 215 | 4 | Reinforced Conc Abutment | (LF) | 229 | 176 | 77 % | 53 | 23 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 234 | 4 | Reinforced Conc Cap | (LF) | 284 | 280 | 99 % | 4 | 1 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 300 | 4 | Strip Seal Expansion Joint | (LF) | 225 | 60 | 27 % | 50 | 22 % | 115 | 51 % | 0 | 0 % | 0 | 0 % |
| 310 | 4 | Elastomeric Bearing | (EA) | 60 | 60 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 316 | 4 | Steel Bearing Assembly | (EA) | 60 | 58 | 96 % | 2 | 4 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 321 | 4 | Reinforced Conc Approach Slab w/ or w/o AC O | (EA) | 2 | 2 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 331 | 4 | Reinforced Conc Bridge Railing | (LF) | 1,424 | 1,068 | 75 % | 356 | 25 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 358 | 4 | Deck Cracking | (EA) | 1 | 0 | 0 % | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % |
| 359 | 4 | Soffit of Concrete Deck or Slab | (EA) | 1 | 0 | 0 % | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % |

Additional
Elements

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|---|
| 26 | Superficial wear only (see #358). The deck was widened to the South 14.5' in 2011. |
| 107 | Paint system is chalky overall with some areas deterioration near the drains. No exposed steel at this time. The two added beams on the south have good paint. |
| 165 | The paint system is chalky overall with some areas deterioration near the drains. Some light surface rust is developing at the West abutment ends of #6 & 7. The two added beams on the south have good paint. |
| 205 | One pier added to each bent in 2011. |
| 215 | Lots of light pattern cracking on the East abutment with some cracks developing staining & efflorescence (2006 photo). There are 3 light vertical cracks present on the West abutment. Lots of water staining from joint seepage on East as well. Each abutment was widened to the South in 2011. |
| 234 | Very light cracks with staining noted at the lower area of the 1st & 2nd caps below the 3rd beam. Each cap was added to at the South in 2011. |
| 300 | The west joint has been redone. Some defects noted on the east. Some soil impaction noted. |
| 310 | < none > |
| 316 | The 8th & 9th bearing plates on the West have light surface rust. |
| 321 | The SE slab was repaired when the bridge was widened in 2011. |
| 331 | The water-proof surface treatment is peeling in some places & the majority of each concrete panel section has one or more light vertical cracks. Still solid overall. |
| 358 | PX - The deck has light to moderate transverse cracking of moderate to heavy density in each span. Needs sealed soon. |
| 359 | Lots of light cracks with efflorescence in each span (2006 photo). No spalls noted. |

| | | | |
|---|--|--|---|
| Roadway Name : I-35 RAMP S-W-N UNDER | | NBI Information Applicable To The Route Under The Structure | |
| 5. Inventory Route (Route Under Structure : A - I - 7 - 00035 - 0 | 28b. Lanes Und.: | 4 | 102. Traffic Dir.: |
| 10. Min. Vert. Clr.(ft.): 17.3 | 29. ADT : | 94100 | 104. Highway System : |
| 12. Base Hwy Network : On Base Network | 32. Appr. Roadway Width (ft.) : | 68.0 | 105. Fed Land Hwy : |
| 13. LRS Inv. Rt./ Subroute : 5515HP0000 / 05 | 47. Total Horiz. Clr.(ft.): | 68.0 | 109. Truck ADT% : |
| 19. Detour Len.(Mi.): 0.0 | 51. Roadway Width (ft.) : | 68.0 | 110. Natl. Truck Network : |
| 20. Toll Facility : 3 On free road | 100. Defense Highway : | 1 On Interstate STRAHNET | 114. Future ADT : |
| 26. Function Class.: 11 Urban Interstate | | | 150560 |
| Agency Field: 1.(Under Rte.): <input type="text" value="U"/> | 2.(Vert. X-Ref.): <input type="text" value="22096-1"/> | 3.(Compass Dir.): <input type="text" value="N"/> | 4.(Vert. Post. Inc.): <input type="text" value="1704"/> |
| | | | 5.(Vert. Post. Dec.): <input type="text" value="-1"/> |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 21723 Structure No.: 5515 0566WX Local ID:-1 Suff. Rating: 97.4 Health Index : 93.8
 Not Deficient

| IDENTIFICATION | INSPECTION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| Description: 101',4'-100',2'-100',101' PRESTRESSED CONC BM SPANS SK. 73 DEG. 1. State:Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. CITY Admin. Area: Unknown 5. Inventory Route (Route On Structure) : 1 - 1 - 1 - 00035 - 0 6. Feature Intersected: OKLAHOMA RIVER 7. Facility Carried: I-35 I-35 9. Location: 5.7 MI N CLEVELAND CO 11. Mile Post: 5.659 mi 13. LRS Inv. Route./Subroute.: 5515HP0000 05 16. Latitude: 35 27 27.89 17. Longitude: 097 29 30.55 98. Border Br. Code: Jnkwnn (P) % Resp.: 0 99. Border Br. #: Unknown | <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>Type</th> <th>Insp Req.</th> <th>Insp Done</th> <th>Freq.</th> <th>Insp. Date:</th> <th>Next Insp.:</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>Y</td> <td>24</td> <td>3/28/2012</td> <td>3/28/2014</td> </tr> <tr> <td>FC Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> </tbody> </table> | Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | NBI: | | Y | 24 | 3/28/2012 | 3/28/2014 | FC Freq.: | N | N | NA | NA | NA | UW Freq.: | N | N | NA | NA | NA | OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NBI: | | Y | 24 | 3/28/2012 | 3/28/2014 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| FC Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| UW Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 45. No. of Spans Main Unit: 8 46. No. of Approach Spans: 0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 108A. Wearing Surface: 1 Monolithic Concrete | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108B. Membrane: 8 Unknown | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108C. Deck Protection: 1 Epoxy Coated Reinforci | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 12. Base Hwy Network : On Base Network | 20. Toll Facility: 3 On free road | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 26. Functional Class: 11 Urban Interstate | 37. Historical Sig.: 5 Not eligible for NRHP | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 100. Defense Highway: 1 On Interstate STRAF | 101. Parallel Structure: Left of bridge | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 102. Dir. of Traffic 1 1-way traffic | 103. Temp. Structure: Not Applicable (P) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 104. Highway System: 1 On the NHS | 105. Fed. Land Hwy 0 N/A (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 110. National Truck Network: 1 Part of natl ne | 112. NBIS Length: Long Enough | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| AGE AND SERVICE | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 42A. Type of Service on: 1 Highway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 42B. Type of Service under: 5 Waterway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONDITION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 62. Culvert: N N/A (NBI) | 61. Channel/Channel Protection: 5 Bank Prot Eroded | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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Dev. :Elastomeric Sheet Type Strip Seal</td> <td>Existing/Recommended Posting : N</td> <td></td> </tr> <tr> <td>Pourable</td> <td>Min./ Max Vert. Clearance : N</td> <td>245. Girder Depth : -1.000</td> </tr> <tr> <td>204. Type of Handrail: Sloped Faced Parapet</td> <td>e. Navigation Lights : NO</td> <td>246. Type of Overlay : _</td> </tr> <tr> <td>205. Material and Quantity : 141.0</td> <td>Working/Not Working : NO</td> <td>246. Overlay Thickness : 0</td> </tr> <tr> <td>208. Type of Abutment : Skeleton</td> <td>215. Overpass : A - Interstate</td> <td>246. Overlay Date : 1/1/1901</td> </tr> <tr> <td>Type of Foundation : Steel Piling</td> <td>221. Substructure Cond. (U/W) : -</td> <td>246. Overlay Depth Changed > 1"? No</td> </tr> <tr> <td>209. Type of Pier / Found.: 3 Piers No</td> <td>222. Fill over RCB: -1</td> <td>247. Protective Systems : 1: DPWR/Epoxy Coate</td> </tr> <tr> <td>Drilled Shaft - No Footings</td> <td>223. Appr. Slab/Rdwy Cond.: Satisfactory</td> <td>2: _ 3: _</td> </tr> <tr> <td>210. Foundation Elev. -3.0 -3.0</td> <td>224. Critical Feature Type: -1</td> <td>4: _ 5: _</td> </tr> <tr> <td> -3.0 -1.0 -1.0</td> <td>225. Paint Type :</td> <td>248. No. of Field Splices w/ Corrosion : -1</td> </tr> <tr> <td>211. Wear. Surf. Prot. System :Other Silanes</td> <td>Overcoat : 0</td> <td>249. Scour Crit. POA exists?: No</td> </tr> <tr> <td>Date Installed : 1/1/1901</td> <td>226. Date Painted: -1</td> <td>250. Culvert Headwall Dist.: -1.0</td> </tr> <tr> <td>213. Utilities Attached : Power</td> <td>227. Paint Coloring: -1</td> <td>254. Thru Truss Type : _</td> </tr> <tr> <td>-1 -1 -1</td> <td>233. Deck Forming: Conventional Forming</td> <td>256. Chan. Profile Up/Down Stream?:</td> </tr> <tr> <td>-1 -1 -1</td> <td>236. Deck Cleaning : -1</td> <td>258. Plans w/ found. are in file at ODOT</td> </tr> <tr> <td></td> <td>238. School Bus Rte: Current and Desired Route</td> <td>259. 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| APPRAISAL | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 36A. Bridge Rail: 1 Meets Standards | 36C. Approach Rail: 1 Meets Standards | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 36B. Transition: 1 Meets Standards | 36D. Approach Rail Ends: 1 Meets Standards | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 67. Str. Evaluation: 6 Equal Min Criteria | 68. Deck Geometry: 6 Equal Min Criteria | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 69. Underclearance, Vertical and Horizontal: N Not applicable (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 71. Waterway Adequacy: 7 Above Minimum | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 72. Approach Alignment: 8 Equal Desirable Crit | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 113. Scour Critical: U Unknown Scour | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 200c. Temperature: 75 | 214a. Posted Weight Limit: NR | 243. Girder Spacing/Number : -1.0 / 9 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 200d. Weather: CLEAR | b. Posted Speed Limit : 60 | 244. Span Lengths : | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 201. Structural Steel ASTM Desig.: A36 20 | c. Narrow/One Lane Bridge sign : N | 101 100 100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 202. Waterproof Membrane :-1 | d. Vertical Clearance Sign: NO | 100 100 101 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed : 1/1/1901 | Advanced Warning Sign : NO | 100 100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 203. Type Exp. Dev. :Elastomeric Sheet Type Strip Seal | Existing/Recommended Posting : N | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Pourable | Min./ Max Vert. Clearance : N | 245. Girder Depth : -1.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights : NO | 246. Type of Overlay : _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 205. Material and Quantity : 141.0 | Working/Not Working : NO | 246. Overlay Thickness : 0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 208. Type of Abutment : Skeleton | 215. Overpass : A - Interstate | 246. Overlay Date : 1/1/1901 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type of Foundation : Steel Piling | 221. Substructure Cond. (U/W) : - | 246. Overlay Depth Changed > 1"? No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 209. Type of Pier / Found.: 3 Piers No | 222. Fill over RCB: -1 | 247. Protective Systems : 1: DPWR/Epoxy Coate | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Drilled Shaft - No Footings | 223. Appr. Slab/Rdwy Cond.: Satisfactory | 2: _ 3: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 210. Foundation Elev. -3.0 -3.0 | 224. Critical Feature Type: -1 | 4: _ 5: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -3.0 -1.0 -1.0 | 225. Paint Type : | 248. No. of Field Splices w/ Corrosion : -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 211. Wear. Surf. Prot. System :Other Silanes | Overcoat : 0 | 249. Scour Crit. POA exists?: No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed : 1/1/1901 | 226. Date Painted: -1 | 250. Culvert Headwall Dist.: -1.0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 213. Utilities Attached : Power | 227. Paint Coloring: -1 | 254. Thru Truss Type : _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 233. Deck Forming: Conventional Forming | 256. Chan. Profile Up/Down Stream?: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 236. Deck Cleaning : -1 | 258. Plans w/ found. are in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 238. School Bus Rte: Current and Desired Route | 259. Scour Eval. is in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 240. Appr. Roadway Type: Concrete | 263. Interchange at Intersection N | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | 264. Interstate Milepoint 126.47 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **21723** Structure No.: **5515 0566WX** Local ID: **-1**

Suff. Rating: **97.4**
Not Deficient

Health Index :
93.8

Inspection Date: **3/28/2012** Reported By: **JCARTER**
Invoice No.: **-1** Inspected With: **-1**
Agency :

Jason Carter
Digitally signed by Jason Carter
DN: cn=Jason Carter, c=US
Date: 2012.05.10 08:15:24 -05'00'

Structure / Inspection Notes

NOTE: INSP.W/SNOOPER.G Hines inspection comments - 6/227/2005 -

CX - There's heavy erosion along the SW wing from a failed curb drain & it's causing the footing for a light pole to be exposed up to 3.5' ! The pole could fall into traffic if not repaired ASAP!
Also the SW wing & shoulder are being undermined as well. (NOTE: I talked to Joseph Wellington about this problem & he was to look at it & inform the personnel responsible for repair)
PX - The bank protection on the South bank is badly damaged under the structure (photo) & in 2 other areas nearby PX - The NE approach rail end doesn't meet standards * There are light poles on the West only * Structures tapers down from North to South * The channel is clean & under spans # 3 thru 7 * The girder end blocks at bents #2, 3, & 4 are cracking & spalling - not into beams themselves at this time.

| Elem. | Env. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|-------|------|--|------|--------|-----------|-------|-----------|-------|-----------|-------|-----------|-----|-----------|-----|
| 26 | 4 | Concrete Deck - Protected w/ Coated Bars | (SF) | 54,536 | 0 | 0 % | 54,536 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 109 | 4 | P/S Conc Open Girder/Beam | (LF) | 7,150 | 7,134 | 100 % | 16 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 119 | 4 | P/S Conc Open Girder/Beam Ends (5ft.) | (LF) | 450 | 440 | 98 % | 5 | 1 % | 5 | 1 % | 0 | 0 % | 0 | 0 % |
| 201 | 4 | Unpainted Steel Column or Pile Extension | (EA) | 1 | 0 | 0 % | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % |
| 205 | 4 | Reinforced Conc Column or Pile Extension | (EA) | 22 | 21 | 95 % | 1 | 5 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 215 | 4 | Reinforced Conc Abutment | (LF) | 175 | 130 | 74 % | 38 | 22 % | 8 | 5 % | 0 | 0 % | 0 | 0 % |
| 234 | 4 | Reinforced Conc Cap | (LF) | 518 | 490 | 95 % | 26 | 5 % | 1 | 0 % | 0 | 0 % | 0 | 0 % |
| 300 | 4 | Strip Seal Expansion Joint | (LF) | 286 | 0 | 0 % | 286 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 309 | 4 | Pourable Joint Seal | (LF) | 417 | 226 | 54 % | 191 | 46 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 310 | 4 | Elastomeric Bearing | (EA) | 152 | 138 | 91 % | 14 | 9 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 316 | 4 | Steel Bearing Assembly | (EA) | 152 | 0 | 0 % | 152 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 331 | 4 | Reinforced Conc Bridge Railing | (LF) | 1,604 | 1,448 | 90 % | 156 | 10 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 358 | 4 | Deck Cracking | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 359 | 4 | Soffit of Concrete Deck or Slab | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 362 | 4 | Traffic Impact | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 363 | 4 | Section Loss | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 366 | 4 | Exposed Abutment Piling | (EA) | 1 | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 368 | 4 | Erosion Smart Flag | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |

Additional Elements _____

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|---|
| 26 | FX - Light wear in wheel lanes w/ exposed aggregate. |
| 109 | FX - Some light to moderate chipped areas on lower edges of a few girders in spans # 3 & 8 (photo). No steel visible (see element 362). MOD.CRACKING TO LINKAGE BLOCKS. |
| 119 | PX - Spalling on the North end of the West beam in span # 1 w/ rebar & strands visible (photo). Some minor cracking of end caps on each end of # 2 & 3 at bent # 1 also. MINOR SPALL @ P.5,BM.1 & P.1,BM.1. |
| 201 | PX: One pile is visible at the NE end of the abutment & it has minor exfoliation & loss (photo) Not serious at this time. |
| 205 | FX: MOD.CRACKING @ P.5,W.COL. |
| 215 | PX - Large spalls on # 2, 5, & 6 pedestals on the South abutment from bearing anchor movement (photo). Some small to moderate cracks noted on several other pedestals on the South abutment from the same cause. A few very small spalls noted on the lower edge of the North abutment as well. Some minor cracking exists on each abutment in scattered areas. Most North pedestals have minor separation at the backwall (not serious). |
| 234 | FX - Small spall on West end of bent # 1 cap w/ rust developing on EPOXY-COATED steel (photo). Each cap has very light vertical/diagonal cracks developing at the corners of the light pedestals & some girder pedestals due to the cantilever forces above the outer piers. (None serious at this time). |
| 300 | |
| 309 | PX - The sealant is deteriorated & failing in the wheel lanes in each joint (6 total). Some minor chipping noted in a few areas. JOINTS DON'T APPEAR TO BE FULL-DEPTH IN THE DECK. |
| 310 | PX - Almost every anchor on the South abutment is BROKEN due to movement (photo) - all need re-anchored to keep them on the pedestals! 5 are broken on the North abutment also. Some minor pad deformation noted. |
| 316 | |
| 331 | FX - Light vertical cracking every 5' to 6' on West rail w/ some light damage noted. The waterproof coating is peeling & failing in most areas. |
| 358 | FX - Light to moderate pattern cracking noted in almost all spans. Some moderate transverse cracks noted in a few areas. |
| 359 | FX - Minor stains, cracking, & delaminations noted below curbs at each joint. Less than 2% of total area affected. Some cracks w/ light efflorescence also noted on inner areas of spans # 4 & 6. |
| 362 | FX - Some damage to a few beams in spans # 3 & 8 along the lower edges. Caused by heavy equipment used for the Oklahoma River project (no road under structure). No steel exposed - none serious at this time. |
| 363 | FX - Loss to the exposed pile at the NE corner is not serious at this time. |
| 366 | PX: Heavy erosion from roadway run-off is exposing a pile on the NE abutment area up to 16" (photo). Needs fill soon - could use a sloped wall to prevent any future erosion. |
| 368 | FX - EROSION at light pole located at sw corner, needs repaired & ABUT.N.E.CORNER. |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **21723**

Structure No.: 5515 0566WX

Local ID:-1

Suff. Rating: 97.4
Not Deficient

Health Index :
93.8

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **22096** Structure No.: **5515 0585WXR** Local ID: **-1** Suff. Rating: **85.0** Health Index : **91.2**
 Not Deficient

| IDENTIFICATION | INSPECTION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|---|---|---|---------------------------------------|---|--|---|---|--|---|--|--------------------|---|----------------------------|--|---|--|--|---|---|--|--|---------------------------------------|---|---|---|---|--------------------------------------|--|---|---|--|---|---|------------------------|------------------------------|---|----------------------------------|--------------------------------------|---|-----------------------------|---|-------------------------------|---|---------------------------------|--|--------------|-----------------------------------|---|-------------------------|--------------------------|---------------------------|----------------------------|-------------------------------------|---------------------------------|---|--|--------------------|-------------------------|-------------------------------------|--------------------|--|---|--|-----------------------------------|---------------------------------------|
| Description: (147',2-129' CONT)(212,233' CONT)116' PLATE GIRDER SPANS SK. VARIES 1. State: Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. CITY Admin. Area: Unknown 5. Inventory Route (Route On Structure) : 1 - 1 - 7 - 00035 - 0 6. Feature Intersected: I-35 E-S, I-40 WB UNDER 7. Facility Carried: I-35 RAMP S-W-N I-35 RAMP S-W-N 9. Location: 6 MI N CLEVELAND CO 11. Mile Post: 5.849 mi 13. LRS Inv. Route./ Subroute.: -1 -1 16. Latitude: 35 27 41.86 17. Longitude: 097 29 32.52 98. Border Br. Code: Not Applicab % Resp.: 0 99. Border Br. #: Unknown | <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>Type</th> <th>Insp Req.</th> <th>Insp Done</th> <th>Freq:</th> <th>Insp. Date:</th> <th>Next Insp.:</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>Y</td> <td>24</td> <td>11/14/2012</td> <td>11/14/2014</td> </tr> <tr> <td>FC Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> </tbody> </table> | Type | Insp Req. | Insp Done | Freq: | Insp. Date: | Next Insp.: | NBI: | | Y | 24 | 11/14/2012 | 11/14/2014 | FC Freq.: | N | N | NA | NA | NA | UW Freq.: | N | N | NA | NA | NA | OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type | Insp Req. | Insp Done | Freq: | Insp. Date: | Next Insp.: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NBI: | | Y | 24 | 11/14/2012 | 11/14/2014 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| FC Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| UW Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 45. No. of Spans Main Unit: 5 46. No. of Approach Spans: 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 107. Deck Type: 1 Concrete-Cast-in-Place | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108A. Wearing Surface: 1 Monolithic Concrete | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108B. Membrane: 0 None | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108C. Deck Protection: 1 Epoxy Coated Reinforci | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 12. Base Hwy Network : Not on Base Network | 20. Toll Facility: 3 On free road | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21. Custodian: 01 State Highway Agency | 22. Owner: 01 State Highway Agency | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26. Functional Class: 11 Urban Interstate | 37. Historical Sig.: 5 Not eligible for NRHP | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 100. Defense Highway: 1 On Interstate STRAF | 101. Parallel Structure: No bridge exists | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 102. Dir. of Traffic 2 2-way traffic | 103. Temp. Structure: Not Applicable (P) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 104. Highway System: 1 On the NHS | 105. Fed. Land Hwy 0 N/A (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 110. National Truck Network: 1 Part of natl ne | 112. NBIS Length: Long Enough | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 42A. Type of Service on: 1 Highway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 42B. Type of Service under: 1 Highway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 47. Inv. Rte. Total Horiz. Clr.: 68.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 50A. Curb/Sdwk Width L: 0.0 ft 50B. Curb/Sidewalk Width R: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 53. Minimum Vertical Clearance Over Bridge: 17.3 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 54A/54B. Min. Vert. Underclearance : H Hwy beneath struct 19.8 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| N/E S/W | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 55A/55B. Minimum Lateral Underclearance R: H Hwy beneath struct 34.5 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 56. Minimum Lateral Underclearance L: 36.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| PROPOSED IMPROVEMENTS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 97. Year of Cost Est.: 2007 | 115. Year of Future ADT: 2030 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| APPRAISAL | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 36A. Bridge Rail: 1 Meets Standards | 36C. Approach Rail: 1 Meets Standards | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 36B. Transition: 1 Meets Standards | 36D. Approach Rail Ends: 1 Meets Standards | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 71. Waterway Adequacy: N Not applicable | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 72. Approach Alignment: 8 Equal Desirable Crit | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 113. Scour Critical: N Not Over Waterway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NAVIGATION DATA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 39. Vertical Clearance: 0.0 ft 40. Horizontal Clearance: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 111. Pier Protection: Not Applicable (P) | 116. Lift Bridge Vert. Clear.: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <table border="1" style="width: 100%; border-collapse: collapse;"> <tbody> <tr> <td>200c. Temperature: 55</td> <td>214a. Posted Weight Limit: NR</td> <td>243. Girder Spacing/Number : -1.0 / -1</td> </tr> <tr> <td>200d. Weather: CLEAR</td> <td>b. Posted Speed Limit : 60</td> <td>244. Span Lengths : -1 -1 -1</td> </tr> <tr> <td>201. Structural Steel ASTM Desig.: A588 27</td> <td>c. Narrow/One Lane Bridge sign : N</td> <td>-1 -1 -1</td> </tr> <tr> <td>202. Waterproof Membrane :-1 Date Installed : 1/1/1901</td> <td>d. Vertical Clearance Sign: YES</td> <td>-1 -1 -1</td> </tr> <tr> <td>203. Type Exp. Dev. : Elastomeric Sheet Type Strip Seal</td> <td>Advanced Warning Sign : NO</td> <td></td> </tr> <tr> <td></td> <td>Existing/Recommended Posting : N 1703</td> <td>245. Girder Depth : -1.000</td> </tr> <tr> <td></td> <td>Min./ Max Vert. Clearance : 1706 1800</td> <td>246. Type of Overlay : -</td> </tr> <tr> <td>204. Type of Handrail: Sloped Faced Parapet</td> <td>e. Navigation Lights : -</td> <td>246. Overlay Thickness : 0</td> </tr> <tr> <td>205. Material and Quantity : -3.0</td> <td>Working/Not Working : -</td> <td>246. Overlay Date : 1/1/1901</td> </tr> <tr> <td>208. Type of Abutment : Skeleton</td> <td>215. Overpass : A - Interstate</td> <td>246. Overlay Depth Changed > 1"? No</td> </tr> <tr> <td>Type of Foundation : Steel Piling</td> <td>221. Substructure Cond. (U/W) : -</td> <td>247. Protective Systems : 1: DPWR/Epoxy Coate</td> </tr> <tr> <td>209. Type of Pier / Found.: 2 Piers No</td> <td>222. Fill over RCB: -1</td> <td>2: - 3: -</td> </tr> <tr> <td>Steel Piling</td> <td>223. Appr. Slab/Rdwy Cond.: Good</td> <td>4: - 5: -</td> </tr> <tr> <td>210. Foundation Elev. -1.0 -1.0</td> <td>224. Critical Feature Type:</td> <td>248. No. of Field Splices w/ Corrosion : -1</td> </tr> <tr> <td> -3.0 -3.0 -1.0</td> <td>225. Paint Type : Inorganic Zinc 2 Coat</td> <td>249. Scour Crit. POA exists?: -</td> </tr> <tr> <td></td> <td>Overcoat : 0</td> <td>250. Culvert Headwall Dist.: -1.0</td> </tr> <tr> <td>211. Wear. Surf. Prot. System : Other Silanes</td> <td>226. Date Painted: 0688</td> <td>254. Thru Truss Type : -</td> </tr> <tr> <td>Date Installed : 1/1/1901</td> <td>227. Paint Coloring: Brown</td> <td>256. Chan. Profile Up/Down Stream?:</td> </tr> <tr> <td>213. Utilities Attached : Power</td> <td>233. Deck Forming: Perm. Metal Deck Forms (SIP)</td> <td>258. Plans w/ found. are in file at ODOT</td> </tr> <tr> <td>-1 -1 -1</td> <td>236. Deck Cleaning : -1</td> <td>259. Scour Eval. is in file at ODOT</td> </tr> <tr> <td>-1 -1 -1</td> <td>238. School Bus Rte: Current and Desired Route</td> <td>263. Interchange at Intersection Y</td> </tr> <tr> <td></td> <td>240. Appr. Roadway Type: Concrete</td> <td>264. Interstate Milepoint 126.66</td> </tr> </tbody> </table> | 200c. Temperature: 55 | 214a. Posted Weight Limit: NR | 243. Girder Spacing/Number : -1.0 / -1 | 200d. Weather: CLEAR | b. Posted Speed Limit : 60 | 244. Span Lengths : -1 -1 -1 | 201. Structural Steel ASTM Desig.: A588 27 | c. Narrow/One Lane Bridge sign : N | -1 -1 -1 | 202. Waterproof Membrane :-1 Date Installed : 1/1/1901 | d. Vertical Clearance Sign: YES | -1 -1 -1 | 203. Type Exp. Dev. : Elastomeric Sheet Type Strip Seal | Advanced Warning Sign : NO | | | Existing/Recommended Posting : N 1703 | 245. Girder Depth : -1.000 | | Min./ Max Vert. Clearance : 1706 1800 | 246. Type of Overlay : - | 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights : - | 246. Overlay Thickness : 0 | 205. Material and Quantity : -3.0 | Working/Not Working : - | 246. Overlay Date : 1/1/1901 | 208. Type of Abutment : Skeleton | 215. Overpass : A - Interstate | 246. Overlay Depth Changed > 1"? No | Type of Foundation : Steel Piling | 221. Substructure Cond. (U/W) : - | 247. Protective Systems : 1: DPWR/Epoxy Coate | 209. Type of Pier / Found.: 2 Piers No | 222. Fill over RCB: -1 | 2: - 3: - | Steel Piling | 223. Appr. Slab/Rdwy Cond.: Good | 4: - 5: - | 210. Foundation Elev. -1.0 -1.0 | 224. Critical Feature Type: | 248. No. of Field Splices w/ Corrosion : -1 | -3.0 -3.0 -1.0 | 225. Paint Type : Inorganic Zinc 2 Coat | 249. Scour Crit. POA exists?: - | | Overcoat : 0 | 250. Culvert Headwall Dist.: -1.0 | 211. Wear. Surf. Prot. System : Other Silanes | 226. Date Painted: 0688 | 254. Thru Truss Type : - | Date Installed : 1/1/1901 | 227. Paint Coloring: Brown | 256. Chan. Profile Up/Down Stream?: | 213. Utilities Attached : Power | 233. Deck Forming: Perm. Metal Deck Forms (SIP) | 258. Plans w/ found. are in file at ODOT | -1 -1 -1 | 236. Deck Cleaning : -1 | 259. Scour Eval. is in file at ODOT | -1 -1 -1 | 238. School Bus Rte: Current and Desired Route | 263. Interchange at Intersection Y | | 240. Appr. Roadway Type: Concrete | 264. Interstate Milepoint 126.66 |
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| 200d. Weather: CLEAR | b. Posted Speed Limit : 60 | 244. Span Lengths : -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 202. Waterproof Membrane :-1 Date Installed : 1/1/1901 | d. Vertical Clearance Sign: YES | -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 203. Type Exp. Dev. : Elastomeric Sheet Type Strip Seal | Advanced Warning Sign : NO | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Existing/Recommended Posting : N 1703 | 245. Girder Depth : -1.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Min./ Max Vert. Clearance : 1706 1800 | 246. Type of Overlay : - | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights : - | 246. Overlay Thickness : 0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 205. Material and Quantity : -3.0 | Working/Not Working : - | 246. Overlay Date : 1/1/1901 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 208. Type of Abutment : Skeleton | 215. Overpass : A - Interstate | 246. Overlay Depth Changed > 1"? No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type of Foundation : Steel Piling | 221. Substructure Cond. (U/W) : - | 247. Protective Systems : 1: DPWR/Epoxy Coate | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 209. Type of Pier / Found.: 2 Piers No | 222. Fill over RCB: -1 | 2: - 3: - | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Steel Piling | 223. Appr. Slab/Rdwy Cond.: Good | 4: - 5: - | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 210. Foundation Elev. -1.0 -1.0 | 224. Critical Feature Type: | 248. No. of Field Splices w/ Corrosion : -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -3.0 -3.0 -1.0 | 225. Paint Type : Inorganic Zinc 2 Coat | 249. Scour Crit. POA exists?: - | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Overcoat : 0 | 250. Culvert Headwall Dist.: -1.0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 211. Wear. Surf. Prot. System : Other Silanes | 226. Date Painted: 0688 | 254. Thru Truss Type : - | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed : 1/1/1901 | 227. Paint Coloring: Brown | 256. Chan. Profile Up/Down Stream?: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 213. Utilities Attached : Power | 233. Deck Forming: Perm. Metal Deck Forms (SIP) | 258. Plans w/ found. are in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 236. Deck Cleaning : -1 | 259. Scour Eval. is in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 238. School Bus Rte: Current and Desired Route | 263. Interchange at Intersection Y | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 240. Appr. Roadway Type: Concrete | 264. Interstate Milepoint 126.66 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 22096 Structure No.: 5515 0585WXR Local ID: -1

Suff. Rating: 85.0
Not Deficient

Health Index :
91.2

Inspection Date: 11/14/2012 Reported By: GHINES
Invoice No.: -1 Inspected With: Gary Richardson
Agency :

Gary Hines

Digitally signed by Gary Hines
DN: cn=Gary Hines, o, ou=Gary Richardson,
email=ghines@odot.org, c=US
Date: 2012.12.17 15:02:38 -0600

Structure / Inspection Notes

- * THIS STRUCTURE CONTROLS THE UNDER CLEARANCE FOR I-35 SB RAMP & I-40 WB.
- * THE ADT FOR THE FIRST ROUTE UNDER SEEMS EXCESSIVE (ONE-LANE I-35 RAMP) .

G Hines inspection comments - 11/14/2012

Satisfactory side drains * I-35 SB ramp is under span #1 * Good slopewalls * Good concrete approach roadway *

| Elm. | Qty. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|------|------|--|------|--------|-----------|------|-----------|------|-----------|-----|-----------|-----|-----------|-----|
| 26 | 4 | Concrete Deck - Protected w/ Coated Bars | (SF) | 65,757 | 0 | 0% | 65,757 | 100% | 0 | 0% | 0 | 0% | 0 | 0% |
| 107 | 4 | Painted Steel Open Girder/Beam | (LF) | 7,494 | 5,603 | 75% | 1,874 | 25% | 17 | 0% | 0 | 0% | 0 | 0% |
| 165 | 4 | Painted Steel Open Girder/Beam end | (LF) | 240 | 171 | 71% | 60 | 25% | 9 | 4% | 0 | 0% | 0 | 0% |
| 205 | 4 | Reinforced Conc Column or Pile Extension | (EA) | 10 | 10 | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| 215 | 4 | Reinforced Conc Abutment | (LF) | 182 | 173 | 95% | 9 | 5% | 0 | 0% | 0 | 0% | 0 | 0% |
| 234 | 4 | Reinforced Conc Cap | (LF) | 431 | 407 | 94% | 24 | 6% | 0 | 0% | 0 | 0% | 0 | 0% |
| 300 | 4 | Strip Seal Expansion Joint | (LF) | 243 | 224 | 92% | 10 | 4% | 9 | 4% | 0 | 0% | 0 | 0% |
| 310 | 4 | Elastomeric Bearing | (EA) | 72 | 72 | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| 316 | 4 | Steel Bearing Assenbly | (EA) | 72 | 63 | 88% | 9 | 13% | 0 | 0% | 0 | 0% | 0 | 0% |
| 321 | 4 | Reinforced Conc Approach Slab w/ or w/o AC O | (EA) | 2 | 0 | 0% | 2 | 100% | 0 | 0% | 0 | 0% | 0 | 0% |
| 331 | 4 | Reinforced Conc Bridge Railing | (LF) | 1,936 | 1,726 | 89% | 210 | 11% | 0 | 0% | 0 | 0% | 0 | 0% |
| 358 | 4 | Deck Cracking | (EA) | 1 | 0 | 0% | 1 | 100% | 0 | 0% | 0 | 0% | 0 | 0% |
| 359 | 4 | Soffit of Concrete Deck or Slab | (EA) | 1 | 1 | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |

Additional Elements _____

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|---|
| 26 | PX - Mainly superficial wear in the wheel lanes. There are 3 spalls (1', 1'X6', 2'X5') with exposed rebar at the NE area of span #6 (2008 photo) & minor spalls along the West abutment joint. There are also several areas where raised-pavement markers have been removed - these areas should be filled to prevent water absorption. Also see SF #358. |
| 107 | The outer areas in each span are chalky & faded. Small areas of peeling noted in spans #2, 3, & 4 on the #1 girder & in other spans in isolated areas. |
| 165 | The outer areas in each span are chalky & faded. Moderate surface rust on #2 & 4 at the South abutment & at the North ends of #1 & 2 in span #3 (2008 photo). |
| 205 | Superficial damage noted on the West column of bent #3. Minor chip present on the North column at bent #4. |
| 215 | Four light vertical cracks noted on the South & 5 present on the North. |
| 234 | Some hairline pattern cracking noted on each end of the 3rd cap & at the North end of the 5th cap. Minor defect (imbedded board) noted on SE area of the 1st cap. |
| 300 | PX - The armor has been removed from (2) 5' sections over the South abutment (2010 & 2006 photos). Lots of debris in the joints - needs cleaned out. |
| 310 | < none > |
| 316 | There are some bearings the have light surface rust developing; 3 at the South abutment, 4 at bent #3, & 2 at bent #1. |
| 321 | Each approach has some cracking & minor spalling (along the joint). |
| 331 | Light vertical cracks noted in several areas. Lots of minor traffic scrapes noted on RH rail. Still solid overall. |
| 358 | PX - Some light to moderate transverse cracks noted. |
| 359 | The soffit has galvanized S.I.P. forms. Some efflorescence is developing at a few seams. in span #3. |

| Roadway Name : I-35 RAMP E-S UNDER | | NBI Information Applicable To The Route Under The Structure | |
|---|-----------------------|---|--------------------------|
| 5. Inventory Route (Route Under Structure : | A - 1 - 7 - 00035 - 0 | 102. Traffic Dir.: | 1 1-way traffic |
| 10. Min. Vert. Clr.(ft.): | 16.9 | 104. Highway System : | 1 On the NHS |
| 12. Base Hwy Network : | Not on Base Network | 105. Fed Land Hwy : | 0 N/A (NBI) |
| 13. LRS Inv. Rt./ Subroute : | -1 / -1 | 109. Truck ADT% : | 12 |
| 19. Detour Len.(Mi.): | 0.0 | 110. Natl. Truck Network : | 1 Part of natl network |
| 20. Toll Facility : | 3 On free road | 114. Future ADT : | 95520 |
| 26. Function Class.: | 11 Urban Interstate | 100. Defense Highway : | 1 On Interstate STRAHNET |

Agency Field: 1.(Under Rte.): 2.(Vert. X-Ref.): 3.(Compass Dir.): 4.(Vert. Post. Inc.): 5.(Vert. Post. Dec.):

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **22096** Structure No.: **5515 0585WXR** Local ID: **-1** Suff. Rating: **85.0** Health Index : **91.2**
Not Deficient

| | | | | | | | | | |
|---|--------------------------------|---------------------------------|---|----------------------------|--------------------------------|-----------------------|-----------------------------------|-----------------------|---------------------------------|
| Roadway Name : I-40 WB UNDER | | | NBI Information Applicable To The Route Under The Structure | | | | | | |
| 5. Inventory Route (Route Under Structure : | B - 1 - 1 - 00040 - 0 | | 102. Traffic Dir.: | 1 | 1-way traffic | | | | |
| 10. Min. Vert. Clr.(ft.): | 20.0 | 28b. Lanes Und.: | 4 | 104. Highway System : | 1 On the NHS | | | | |
| 12. Base Hwy Network : | On Base Network | 29. ADT : | 51500 | 105. Fed Land Hwy : | 0 N/A (NBI) | | | | |
| 13. LRS Inv. Rt./ Subroute : | 5569HP0000 / 09 | 32. Appr. Roadway Width (ft.) : | 62.0 | 109. Truck ADT% : | 12 | | | | |
| 19. Detour Len.(Mi.): | 10.0 | 47. Total Horiz. Clr.(ft.): | 62.0 | 110. Natl. Truck Network : | 1 Part of natl network | | | | |
| 20. Toll Facility : | 3 On free road | 51. Roadway Width (ft.) : | 62.0 | 114. Future ADT : | 82400 | | | | |
| 26. Function Class.: | 11 Urban Interstate | 100. Defense Highway : | 1 | On Interstate STRAHNET | | | | | |
| Agency Field: 1.(Under Rte.): | <input type="text" value="U"/> | 2.(Vert. X-Ref.): | <input type="text" value="-1"/> | 3.(Compass Dir.): | <input type="text" value="E"/> | 4.(Vert. Post. Inc.): | <input type="text" value="1911"/> | 5.(Vert. Post. Dec.): | <input type="text" value="-1"/> |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **23635** Structure No.: **5515 0506 X** Local ID: **-1** Suff. Rating: **93.1** Health Index : **92.5**
 Not Deficient

| <p>DESCRIPTION: <u>IDENTIFICATION</u></p> <p>2-86' CONT PLATE GIRDER SPANS SK. 69 DEG. 69 00'59"</p> <p>1. State: Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. C11Y Admin. Area: Unknown</p> <p>5. Inventory Route (Route On Structure): 1 - 5 - 1 - 09440 - 0 6. Feature Intersected: I-35 UNDER 7. Facility Carried: FAU 9440 (SE 15) S.E 15TH ST 9. Location: 5.1 MIN CLEVELAND CO 11. Mile Post: 5.059 mi 13. LRS Inv. Route./ Subroute.: -1 -1 16. Latitude: 35 26 59.90 17. Longitude: 097 29 15.92 98. Border Br. Code: Not Applicab % Resp.: 0 99. Border Br. #: Unknown</p> <p style="text-align: center;"><u>STRUCTURE TYPE AND MATERIALS</u></p> <p>43. Main Span Material and Design Type Steel Continuous Stringer/Girder 44. Approach Span Material and Design Type Not Applicable (P) Not Applicable (P) 45. No. of Spans Main Unit: 2 46. No. of Approach Spans: 0 107. Deck Type: 1 Concrete-Cast-in-Place 108A. Wearing Surface: 1 Monolithic Concrete 108B. Membrane: 0 None 108C. Deck Protection: 1 Epoxy Coated Reinforci</p> <p style="text-align: center;"><u>AGE AND SERVICE</u></p> <p>27. Year Built: 1994 106. Year Reconstructed: -4 28A. Lanes on: 6 28B. Lanes Under: 6 19. Detour Length: 5.0 mi 29. ADT: 5000 30. Year of ADT: 2010 109. Truck ADT %: 5 42A. Type of Service on: 5 Highway-pedestrian 42B. Type of Service under: 1 Highway</p> <p style="text-align: center;"><u>GEOMETRIC DATA</u></p> <p>10. Inv. Rte. Min. Vert. Clr.: 328.1 ft 32. Approach Roadway Width (W/ Shoulders): 74.0 ft Deck Area: 16,511.8 sq. ft 33. Median: 0 No median 34. Skew: 20 35. Structure Flared: 0 No flare 47. Inv. Rte. Total Horiz. Clr.: 74.0 ft 48. Length Maximum Span: 86.0 ft 49. Structure Length: 172.0 ft 50A. Curb/Sdwk Width L: 10.0 ft 50B. Curb/Sidewalk Width R: 10.0 ft 51. Width Curb to Curb: 74.0 ft 52. Width Out to Out: 96.0 ft 53. Minimum Vertical Clearance Over Bridge: 328.1 ft 54A/54B. Min. Vert. Underclearance : H Hwy beneath struct 16.4 ft</p> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="text-align: center;"><u>N/E</u></td> <td style="text-align: center;"><u>S/W</u></td> </tr> <tr> <td>Meas. N1607 N1805 -1 S1904 S2100 -1</td> <td></td> </tr> <tr> <td>Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U</td> <td></td> </tr> </table> <p>55A/55B. Minimum Lateral Underclearance R: H Hwy beneath struct 12.0 ft 56. Minimum Lateral Underclearance L: 9.0 ft</p> | <u>N/E</u> | <u>S/W</u> | Meas. N1607 N1805 -1 S1904 S2100 -1 | | Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U | | <p style="text-align: center;"><u>INSPECTION</u></p> <table style="width: 100%; border-collapse: collapse;"> <tr> <th style="text-align: left;">Type</th> <th style="text-align: left;">Insp Req.</th> <th style="text-align: left;">Insp Done</th> <th style="text-align: left;">Freq:</th> <th style="text-align: left;">Insp. Date:</th> <th style="text-align: left;">Next Insp.:</th> </tr> <tr> <td>NBI:</td> <td></td> <td>Y</td> <td>24</td> <td>7/16/2012</td> <td>7/16/2014</td> </tr> <tr> <td>FC Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> </table> <p style="text-align: center;"><u>CLASSIFICATION</u></p> <p>12. Base Hwy Network : Not on Base Network 20. Toll Facility: 3 On free road 21. Custodian: 01 State Highway Agency 22. Owner: 01 State Highway Agency 26. Functional Class: 16 Urban Minor Arter 37. Historical Sig.: 5 Not eligible for NRHP 100. Defense Highway: 0 Not a STRAHNET h 101. Parallel Structure: No bridge exists 102. Dir. of Traffic: 2 2-way traffic 103. Temp. Structure: Not Applicable (P) 104. Highway System: 0 Not on NHS 105. Fed. Land Hwy 0 N/A (NBI) 110. National Truck Network: 0 Not part of nat 112. NBIS Length: Long Enough</p> <p style="text-align: center;"><u>CONDITION</u></p> <p>58. Deck: 7 Good 59. Super.: 7 Good 60. Sub.: 8 Very Good 62. Culvert: N N/A (NBI) 61. Channel/Channel Protection: N N/A (NBI) Flowline Notes:</p> <p style="text-align: center;"><u>LOAD RATING AND POSTING</u></p> <p>31. Design Load: 5 MS 18 (HS 20) 41. Posting status: A Open, no restriction 63. Op. Rating Method: 1 LF Load Factor-Ton Alt. Op. Rating Meth.: 1 LF Load Factor-Ton 64. Operating Rating (H / HS / 3-3): 48.6 49.0 -1.1 66. Inventory Rating (H / HS / 3-3) : 35.7 36.0 -1.1 65. Inv. Rating Method: 1 LF Load Factor-Ton Alt. Inv. Rating Meth.: 1 LF Load Factor-Ton 70. Posting: 5 At/Above Legal Loads Date Rated : 1/1/1901</p> <p style="text-align: center;"><u>PROPOSED IMPROVEMENTS</u></p> <p>94. Bridge Cost: \$2,184,191 75. Type of Work: 31 Repl-Load Capacity 95. Roadway Cost: \$3,603,915 76. Lgth. of Improvement: 279.3 ft 96. Total Cost: \$6,115,735 114. Future ADT: 8000 97. Year of Cost Est.: 2007 115. Year of Future ADT: 2030</p> <p style="text-align: center;"><u>NAVIGATION DATA</u></p> <p>38. Navigation Control: NA-no waterway 39. Vertical Clearance: 0.0 ft 40. Horizontal Clearance: 0.0 ft 111. Pier Protection: Not Applicable (P) 116. Lift Bridge Vert. Clear.: 0.0 ft</p> <p style="text-align: center;"><u>APPRAISAL</u></p> <p>36A. Bridge Rail: 1 Meets Standards 36C. Approach Rail: 0 Substandard 36B. Transition: 0 Substandard 36D. Approach Rail Ends: 0 Substandard 67. Str. Evaluation: 7 Above Min Criteria 68. Deck Geometry: 4 Tolerable 69. Underclearance, Vertical and Horizontal: 5 Above Tolerable 71. Waterway Adequacy: N Not applicable 72. Approach Alignment: 8 Equal Desirable Crit 113. Scour Critical: N Not Over Waterway</p> | Type | Insp Req. | Insp Done | Freq: | Insp. Date: | Next Insp.: | NBI: | | Y | 24 | 7/16/2012 | 7/16/2014 | FC Freq.: | N | N | NA | NA | NA | UW Freq.: | N | N | NA | NA | NA | OS Freq.: | N | N | NA | NA | NA |
|--|---|---|--|-------------|--|--|---|------|-----------|-----------|-------|-------------|-------------|------|--|---|----|-----------|-----------|-----------|---|---|----|----|----|-----------|---|---|----|----|----|-----------|---|---|----|----|----|
| <u>N/E</u> | <u>S/W</u> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Meas. N1607 N1805 -1 S1904 S2100 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type | Insp Req. | Insp Done | Freq: | Insp. Date: | Next Insp.: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NBI: | | Y | 24 | 7/16/2012 | 7/16/2014 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| FC Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| UW Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>200c. Temperature: 99 200d. Weather: CLEAR 201. Structural Steel ASTM Desig.: -1 -1 202. Waterproof Membrane :-1 Date Installed: 1/1/1901 203. Type Exp. Dev. : Elastomeric Sheet Type Strip Seal - 204. Type of Handrail: Concrete Parapet - Steel Rail 205. Material and Quantity : 709.0 208. Type of Abutment : Skeleton Type of Foundation : Natural Foundation Matl. 209. Type of Pier / Found.: 3 Piers No Drilled Shaft - No Footings 210. Foundation Elev. -3.0 -3.0 -1.0 -3.0 -1.0 211. Wear. Surf. Prot. System : None Date Installed: 1/1/1901 213. Utilities Attached : Power -1 -1 -1 -1 -1 -1</p> | <p>214a. Posted Weight Limit: NR b. Posted Speed Limit : 35 c. Narrow/One Lane Bridge sign : N d. Vertical Clearance Sign: YES Advanced Warning Sign : NO Existing/Recommended Posting : 1604 1901 Min./ Max Vert. Clearance : 1607 2100 e. Navigation Lights : Working/Not Working : 215. Overpass : A - Interstate 221. Substructure Cond. (U/W) : - 222. Fill over RCB: -1 223. Appr. Slab/Rdwy Cond.: Good 224. Critical Feature Type: -1 225. Paint Type : Inorganic Zinc 2 Coat Overcoat : 0 226. Date Painted: 9206 227. Paint Coloring: Brown 233. Deck Forming: Conventional Forming 236. Deck Cleaning : -1 238. School Bus Rte: Current and Desired Route 240. Appr. Roadway Type: Concrete</p> | <p>243. Girder Spacing/Number : -1.0 / -1 244. Span Lengths : -1 -1 -1 -1 -1 -1 -1 -1 245. Girder Depth : -1.000 246. Type of Overlay : 246. Overlay Thickness : 0 246. Overlay Date : 1/1/1901 246. Overlay Depth Changed > 1"? No 247. Protective Systems : 1: Epoxy Coated Bars 2: 3: 4: 5: 248. No. of Field Splices w/ Corrosion : -1 249. Scour Crit. POA exists?: 250. Culvert Headwall Dist.: -1.0 254. Thru Truss Type : 256. Chan. Profile Up/Down Stream?: 258. Plans w/ found. are in file at ODOT 259. Scour Eval. is in file at ODOT 263. Interchange at Intersection 2 264. Interstate Milepoint 125.87</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 23635 Structure No.: 5515 0506 X Local ID: -1

Suff. Rating: 93.1
Not Deficient

Health Index :
92.5

Inspection Date: 7/16/2012 Reported By: GHINES
Invoice No.: -1 Inspected With: Gary Richardson
Agency :

Gary Hines

Digitally signed by Gary Hines
DN: cn=Gary Hines, o, ou=Gary Richardson,
email=ghines@odot.org, c=US
Date: 2012.08.22 08:03:44 -05'00'

Structure / Inspection Notes

* MAXIMUM HORIZONTAL UNDER CLEARANCE: SB = 59', NB = 59'.

G Hines inspection comments - 7/16/2012

Satisfactory side drains * There is a power line for the street lights under the deck between the 1st & 2nd beams * Safety below = 1111 * The clearance for the North-bound traffic is posted at 16'-5" on the utility bridge (0505X)

| Elem. | Qty. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|-------|------|--|------|--------|-----------|-------|-----------|-------|-----------|-------|-----------|-----|-----------|-----|
| 26 | 4 | Concrete Deck - Protected w/ Coated Bars | (SF) | 12,728 | 12,728 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 107 | 4 | Painted Steel Open Girder/Beam | (LF) | 1,782 | 1,070 | 60 % | 712 | 40 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 165 | 4 | Painted Steel Open Girder/Beam end | (LF) | 110 | 77 | 70 % | 33 | 30 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 205 | 4 | Reinforced Conc Column or Pile Extension | (EA) | 3 | 1 | 33 % | 2 | 67 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 215 | 4 | Reinforced Conc Abutment | (LF) | 213 | 206 | 97 % | 7 | 3 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 234 | 4 | Reinforced Conc Cap | (LF) | 107 | 107 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 300 | 4 | Strip Seal Expansion Joint | (LF) | 208 | 201 | 97 % | 7 | 3 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 310 | 4 | Elastomeric Bearing | (EA) | 33 | 33 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 316 | 4 | Steel Bearing Assenbly | (EA) | 33 | 33 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 321 | 4 | Reinforced Conc Approach Slab w/ or w/o AC O | (EA) | 2 | 1 | 50 % | 1 | 50 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 331 | 4 | Reinforced Conc Bridge Railing | (LF) | 344 | 318 | 92 % | 26 | 8 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 334 | 4 | Metal Bridge Railing - Coated | (LF) | 344 | 0 | 0 % | 344 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 358 | 4 | Deck Cracking | (EA) | 1 | 0 | 0 % | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % |
| 359 | 4 | Soffit of Concrete Deck or Slab | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |

Additional Elements

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|---|
| 26 | Superficial wear only (see SF # 358). |
| 107 | The paint is chalky & faded on many areas with very light freckle rust developing - mainly the outer beams. |
| 165 | The paint is chalky & faded on many areas with very light freckle rust developing - mainly the outer beams. |
| 205 | Light vertical cracking noted on the North face of #3 & the South face of #1. |
| 215 | There are 5 light vertical cracks on the East abutment. Light diagonal cracking noted near the top at the SE. One crack noted on the West abutment at the 8th pedestal. Not serious at this time. |
| 234 | < none > |
| 300 | FX - Joint #2 is leaking in a few small areas. |
| 310 | < none > |
| 316 | < none > |
| 321 | Moderate cracking noted in the West slab. |
| 331 | Some light cracks noted in a few areas. |
| 334 | The paint is faded & chalky overall. |
| 358 | PX - LOTS of light to moderate transverse cracks every 2' to 3' in a 50' section. Similar cracks exist every 2' to 7' overall. |
| 359 | There are some transverse cracks with efflorescence on the soffit mainly in the outer panels (photo). |

| Roadway Name : I-35 | | NBI Information Applicable To The Route Under The Structure | |
|---|--------------------------------|---|-----------------------------------|
| 5. Inventory Route (Route Under Structure : | 2 - 1 - 1 - 00035 - 0 | 102. Traffic Dir.: | 2 2-way traffic |
| 10. Min. Vert. Clr.(ft.): | 16.4 | 104. Highway System : | 1 On the NHS |
| 12. Base Hwy Network : | On Base Network | 105. Fed Land Hwy : | 0 N/A (NBI) |
| 13. LRS Inv. Rt./ Subroute : | 5515HP0000 / 05 | 109. Truck ADT% : | 12 |
| 19. Detour Len.(Mi.): | 0.0 | 110. Natl. Truck Network : | 1 Part of natl network |
| 20. Toll Facility : | 3 On free road | 114. Future ADT : | 183680 |
| 26. Function Class.: | 11 Urban Interstate | 100. Defense Highway : | 1 On Interstate STRAHNET |
| Agency Field: 1.(Under Rte.): | <input type="text" value="U"/> | 2.(Vert. X-Ref.): | <input type="text" value="-1"/> |
| | | 3.(Compass Dir.): | <input type="text" value="N"/> |
| | | 4.(Vert. Post. Inc.): | <input type="text" value="1605"/> |
| | | 5.(Vert. Post. Dec.): | <input type="text" value="1902"/> |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **26296** Structure No.: **5515 0671SX** Local ID: **-1** Suff. Rating: **96.4** Health Index : **98.3**
 Not Deficient

| IDENTIFICATION | INSPECTION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|---|--|---|---------------------------------------|--|--|---|---|--|------------------------------|--|--------------------|---|----------------------------|--|---|---|--|--|---|--|---|--------------------------------------|---|---|---|---|--------------------------------------|---|---|--|--|---|----------------------------------|------------------------|------------------------------|---|----------------------------------|-----------------------------------|---|--------------------------------|---|-------------------------------|---------------------|---------------------------------|-----------------------------------|--------------|-----------------------------------|---------------------------|-----------------------|--------------------------|---------------------------------|-------------------------|-------------------------------------|--------------------|---|--|--------------------|-------------------------|-------------------------------------|--|--|---|--|-----------------------------------|---------------------------------------|
| Description: 46.25'-98'-46.25' PC BEAM SPANS 1. State: Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. CITY Admin. Area: Unknown 5. Inventory Route (Route On Structure): 1 - 1 - 1 - 00035 - 0 6. Feature Intersected: FAU 9421 (EASTERN) UNDER 7. Facility Carried: I-35 NB, I-40 EB I-35 NB, I-40 EB 9. Location: EASTERN AVENUE 11. Mile Post: 6.708 mi 13. LRS Inv. Route./Subroute.: 5515HP0000 07 16. Latitude: 35 27 46.00 17. Longitude: 097 28 37.51 98. Border Br. Code: Not Applicab % Resp.: 0 99. Border Br. #: Unknown | <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>Type</th> <th>Insp Req.</th> <th>Insp Done</th> <th>Freq.</th> <th>Insp. Date:</th> <th>Next Insp.:</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>Y</td> <td>24</td> <td>11/14/2012</td> <td>11/14/2014</td> </tr> <tr> <td>FC Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS Freq.:</td> <td>N</td> <td>N</td> <td>NA</td> <td>NA</td> <td>NA</td> </tr> </tbody> </table> | Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | NBI: | | Y | 24 | 11/14/2012 | 11/14/2014 | FC Freq.: | N | N | NA | NA | NA | UW Freq.: | N | N | NA | NA | NA | OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type | Insp Req. | Insp Done | Freq. | Insp. Date: | Next Insp.: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NBI: | | Y | 24 | 11/14/2012 | 11/14/2014 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| FC Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| UW Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OS Freq.: | N | N | NA | NA | NA | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| STRUCTURE TYPE AND MATERIALS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 43. Main Span Material and Design Type Prestressed Concrete Stringer/Girder | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 44. Approach Span Material and Design Type Not Applicable (P) Not Applicable (P) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 45. No. of Spans Main Unit: 3 46. No. of Approach Spans: 0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 107. Deck Type: 1 Concrete-Cast-in-Place | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108A. Wearing Surface: 1 Monolithic Concrete | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108B. Membrane: 0 None | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 108C. Deck Protection: 1 Epoxy Coated Reinforci | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CLASSIFICATION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 12. Base Hwy Network : On Base Network | 20. Toll Facility: 3 On free road | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21. Custodian: 01 State Highway Agency | 22. Owner: 01 State Highway Agency | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26. Functional Class: 11 Urban Interstate | 37. Historical Sig.: 5 Not eligible for NRHP | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 100. Defense Highway: 1 On Interstate STRAF | 101. Parallel Structure: Right of bridge | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 102. Dir. of Traffic 1 1-way traffic | 103. Temp. Structure: Not Applicable (P) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 104. Highway System: 1 On the NHS | 105. Fed. Land Hwy 0 N/A (NBI) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 110. National Truck Network: 1 Part of natl ne | 112. NBIS Length: Long Enough | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 42A. Type of Service on: 1 Highway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 42B. Type of Service under: 1 Highway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 53. Minimum Vertical Clearance Over Bridge: 328.1 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 54A/54B. Min. Vert. Underclearance : H Hwy beneath struct 17.1 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 56. Minimum Lateral Underclearance L: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 71. Waterway Adequacy: N Not applicable | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 72. Approach Alignment: 8 Equal Desirable Crit | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 113. Scour Critical: N Not Over Waterway | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 39. Vertical Clearance: 0.0 ft | 40. Horizontal Clearance: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 111. Pier Protection: Not Applicable (P) | 116. Lift Bridge Vert. Clear.: 0.0 ft | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <table border="1" style="width: 100%; border-collapse: collapse;"> <tbody> <tr> <td>200c. Temperature: 51</td> <td>214a. Posted Weight Limit: NR</td> <td>243. Girder Spacing/Number : -1.0 / -1</td> </tr> <tr> <td>200d. Weather: CLEAR</td> <td>b. Posted Speed Limit : 60</td> <td>244. Span Lengths :</td> </tr> <tr> <td>201. Structural Steel ASTM Desig.: -1 -1</td> <td>c. Narrow/One Lane Bridge sign : N</td> <td>-1 -1 -1</td> </tr> <tr> <td>202. Waterproof Membrane :-1</td> <td>d. Vertical Clearance Sign: YES</td> <td>-1 -1 -1</td> </tr> <tr> <td>Date Installed: 1/1/1901</td> <td>Advanced Warning Sign : NO</td> <td>-1 -1 -1</td> </tr> <tr> <td>203. Type Exp. Dev. : Elastomeric Sheet Type Strip Seal</td> <td>Existing/Recommended Posting : 1611 1611</td> <td>245. Girder Depth : -1.000</td> </tr> <tr> <td></td> <td>Min./ Max Vert. Clearance : 1702 1706</td> <td>246. Type of Overlay : _</td> </tr> <tr> <td>204. Type of Handrail: Sloped Faced Parapet</td> <td>e. Navigation Lights : -</td> <td>246. Overlay Thickness : 0</td> </tr> <tr> <td>205. Material and Quantity : -1.0</td> <td>Working/Not Working : -</td> <td>246. Overlay Date : 1/1/1901</td> </tr> <tr> <td>208. Type of Abutment : Skeleton</td> <td>215. Overpass : A - Interstate</td> <td>246. Overlay Depth Changed > 1"? No</td> </tr> <tr> <td>Type of Foundation : Steel Piling</td> <td>221. Substructure Cond. (U/W) : -</td> <td>247. Protective Systems : 1: Epoxy Coated Bars</td> </tr> <tr> <td>209. Type of Pier / Found.: 4 No</td> <td>222. Fill over RCB: -1</td> <td>2: _ 3: _</td> </tr> <tr> <td>Steel Piling</td> <td>223. Appr. Slab/Rdwy Cond.: Good</td> <td>4: _ 5: _</td> </tr> <tr> <td>210. Foundation Elev. -1.0 -1.0</td> <td>224. Critical Feature Type: -1</td> <td>248. No. of Field Splices w/ Corrosion : -1</td> </tr> <tr> <td> -1.0 -1.0 -1.0</td> <td>225. Paint Type : -</td> <td>249. Scour Crit. POA exists?: -</td> </tr> <tr> <td>211. Wear. Surf. Prot. System : _</td> <td>Overcoat : 0</td> <td>250. Culvert Headwall Dist.: -1.0</td> </tr> <tr> <td>Date Installed : 1/1/1901</td> <td>226. Date Painted: -1</td> <td>254. Thru Truss Type : -</td> </tr> <tr> <td>213. Utilities Attached : Power</td> <td>227. Paint Coloring: -1</td> <td>256. Chan. Profile Up/Down Stream?:</td> </tr> <tr> <td>-1 -1 -1</td> <td>233. Deck Forming: Conventional Forming</td> <td>258. Plans w/ found. are in file at ODOT</td> </tr> <tr> <td>-1 -1 -1</td> <td>236. Deck Cleaning : -1</td> <td>259. Scour Eval. is in file at ODOT</td> </tr> <tr> <td></td> <td>238. School Bus Rte: Current and Desired Route</td> <td>263. Interchange at Intersection Y</td> </tr> <tr> <td></td> <td>240. Appr. Roadway Type: Concrete</td> <td>264. Interstate Milepoint 127.52</td> </tr> </tbody> </table> | 200c. Temperature: 51 | 214a. Posted Weight Limit: NR | 243. Girder Spacing/Number : -1.0 / -1 | 200d. Weather: CLEAR | b. Posted Speed Limit : 60 | 244. Span Lengths : | 201. Structural Steel ASTM Desig.: -1 -1 | c. Narrow/One Lane Bridge sign : N | -1 -1 -1 | 202. Waterproof Membrane :-1 | d. Vertical Clearance Sign: YES | -1 -1 -1 | Date Installed: 1/1/1901 | Advanced Warning Sign : NO | -1 -1 -1 | 203. Type Exp. Dev. : Elastomeric Sheet Type Strip Seal | Existing/Recommended Posting : 1611 1611 | 245. Girder Depth : -1.000 | | Min./ Max Vert. Clearance : 1702 1706 | 246. Type of Overlay : _ | 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights : - | 246. Overlay Thickness : 0 | 205. Material and Quantity : -1.0 | Working/Not Working : - | 246. Overlay Date : 1/1/1901 | 208. Type of Abutment : Skeleton | 215. Overpass : A - Interstate | 246. Overlay Depth Changed > 1"? No | Type of Foundation : Steel Piling | 221. Substructure Cond. (U/W) : - | 247. Protective Systems : 1: Epoxy Coated Bars | 209. Type of Pier / Found.: 4 No | 222. Fill over RCB: -1 | 2: _ 3: _ | Steel Piling | 223. Appr. Slab/Rdwy Cond.: Good | 4: _ 5: _ | 210. Foundation Elev. -1.0 -1.0 | 224. Critical Feature Type: -1 | 248. No. of Field Splices w/ Corrosion : -1 | -1.0 -1.0 -1.0 | 225. Paint Type : - | 249. Scour Crit. POA exists?: - | 211. Wear. Surf. Prot. System : _ | Overcoat : 0 | 250. Culvert Headwall Dist.: -1.0 | Date Installed : 1/1/1901 | 226. Date Painted: -1 | 254. Thru Truss Type : - | 213. Utilities Attached : Power | 227. Paint Coloring: -1 | 256. Chan. Profile Up/Down Stream?: | -1 -1 -1 | 233. Deck Forming: Conventional Forming | 258. Plans w/ found. are in file at ODOT | -1 -1 -1 | 236. Deck Cleaning : -1 | 259. Scour Eval. is in file at ODOT | | 238. School Bus Rte: Current and Desired Route | 263. Interchange at Intersection Y | | 240. Appr. Roadway Type: Concrete | 264. Interstate Milepoint 127.52 |
| 200c. Temperature: 51 | 214a. Posted Weight Limit: NR | 243. Girder Spacing/Number : -1.0 / -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 200d. Weather: CLEAR | b. Posted Speed Limit : 60 | 244. Span Lengths : | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 201. Structural Steel ASTM Desig.: -1 -1 | c. Narrow/One Lane Bridge sign : N | -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 202. Waterproof Membrane :-1 | d. Vertical Clearance Sign: YES | -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed: 1/1/1901 | Advanced Warning Sign : NO | -1 -1 -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| | Min./ Max Vert. Clearance : 1702 1706 | 246. Type of Overlay : _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 204. Type of Handrail: Sloped Faced Parapet | e. Navigation Lights : - | 246. Overlay Thickness : 0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 205. Material and Quantity : -1.0 | Working/Not Working : - | 246. Overlay Date : 1/1/1901 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 208. Type of Abutment : Skeleton | 215. Overpass : A - Interstate | 246. Overlay Depth Changed > 1"? No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Type of Foundation : Steel Piling | 221. Substructure Cond. (U/W) : - | 247. Protective Systems : 1: Epoxy Coated Bars | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 209. Type of Pier / Found.: 4 No | 222. Fill over RCB: -1 | 2: _ 3: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Steel Piling | 223. Appr. Slab/Rdwy Cond.: Good | 4: _ 5: _ | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 210. Foundation Elev. -1.0 -1.0 | 224. Critical Feature Type: -1 | 248. No. of Field Splices w/ Corrosion : -1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1.0 -1.0 -1.0 | 225. Paint Type : - | 249. Scour Crit. POA exists?: - | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 211. Wear. Surf. Prot. System : _ | Overcoat : 0 | 250. Culvert Headwall Dist.: -1.0 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Date Installed : 1/1/1901 | 226. Date Painted: -1 | 254. Thru Truss Type : - | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 213. Utilities Attached : Power | 227. Paint Coloring: -1 | 256. Chan. Profile Up/Down Stream?: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 233. Deck Forming: Conventional Forming | 258. Plans w/ found. are in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1 -1 -1 | 236. Deck Cleaning : -1 | 259. Scour Eval. is in file at ODOT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 238. School Bus Rte: Current and Desired Route | 263. Interchange at Intersection Y | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 240. Appr. Roadway Type: Concrete | 264. Interstate Milepoint 127.52 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: **26296** Structure No.: **5515 0671SX** Local ID: **-1** Suff. Rating: **96.4** Health Index : **98.3**
 Not Deficient

Inspection Date: 11/14/2012 Reported By: GHINES
 Invoice No.: -1 Inspected With: Gary Richardson
 Agency :

Gary Hines

Digitally signed by Gary Hines
 DN: cn=Gary Hines, o, ou=Gary Richardson,
 email=ghines@odot.org, c=US
 Date: 2012.12.17 15:07:46 -06'00'

Structure / Inspection Notes

* THIS BRIDGE CONTROLS THE UNDER CLEARANCE FOR THIS PAIR OF BRIDGES AT 17'-2" - MEASURED AT THE CENTERLINE ON THE SOUTH BEAM.
 ** THE ADT BELOW IS MUCH MORE THAN 700 - PROBABLY > 5000 **

G Hines inspection comments - 11/14/2012
 Good slopewalls * Good side drains * Good concrete approach roadway * Safety below = 1111

| Elm. | Env. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|------|------|--|------|--------|-----------|-------|-----------|-------|-----------|-----|-----------|-----|-----------|-----|
| 26 | 4 | Concrete Deck - Protected w/ Coated Bars | (SF) | 15,978 | 15,978 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 109 | 4 | P/S Conc Open Girder/Beam | (LF) | 1,589 | 1,589 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 119 | 4 | P/S Conc Open Girder/Beam Ends (5ft.) | (LF) | 300 | 300 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 205 | 4 | Reinforced Conc Column or Pile Extension | (EA) | 8 | 4 | 50 % | 4 | 50 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 215 | 4 | Reinforced Conc Abutment | (LF) | 171 | 165 | 96 % | 5 | 3 % | 1 | 1 % | 0 | 0 % | 0 | 0 % |
| 234 | 4 | Reinforced Conc Cap | (LF) | 168 | 168 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 300 | 4 | Strip Seal Expansion Joint | (LF) | 171 | 164 | 96 % | 7 | 4 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 301 | 4 | Pourable Joint Seal | (LF) | 171 | 97 | 57 % | 74 | 43 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 310 | 4 | Elastomeric Bearing | (EA) | 60 | 60 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 316 | 4 | Steel Bearing Assenbly | (EA) | 60 | 60 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 321 | 4 | Reinforced Conc Approach Slab w/ or w/o AC O | (EA) | 2 | 0 | 0 % | 2 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 331 | 4 | Reinforced Conc Bridge Railing | (LF) | 385 | 381 | 99 % | 4 | 1 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 358 | 4 | Deck Cracking | (EA) | 1 | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 359 | 4 | Soffit of Concrete Deck or Slab | (EA) | 1 | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |

Additional Elements

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|--|
| 26 | Superficial wear overall - one cavity needs filled where a RPM is missing. Also see SF #358. |
| 109 | < none > |
| 119 | < none > |
| 205 | Light vertical cracking noted on #1 & #4 in each bent and #1 & #3 in bent #2. Also, #1 in bent #1 has 3 light horizontal cracks. |
| 215 | One small spall is present near the SW corner. Minor vertical cracking present on each side with some water stains. |
| 234 | < none > |
| 300 | FX - Debris needs cleaned out. Some initial exfoliation is starting to develop at the South shoulder. |
| 301 | Some deterioration noted at each joint. |
| 310 | < none > |
| 316 | < none > |
| 321 | Each approach has minor cracking which has been sealed. No movement noted. |
| 331 | Three or four light vertical cracks noted. |
| 358 | The deck has some minor cracking most of which have been sealed. |
| 359 | < none > |

| Roadway Name : FAU 9421 (EASTERN AVE.) | | NBI Information Applicable To The Route Under The Structure | |
|---|-----------------------|---|--------------------------|
| 5. Inventory Route (Route Under Structure : | 2 - 5 - 1 - 09421 - 0 | 102. Traffic Dir.: | 2 2-way traffic |
| 10. Min. Vert. Clr.(ft.): | 17.1 | 104. Highway System : | 0 Not on NHS |
| 12. Base Hwy Network : | Not on Base Network | 105. Fed Land Hwy : | 0 N/A (NBI) |
| 13. LRS Inv. Rt./ Subroute : | -1 / -1 | 109. Truck ADT% : | 5 |
| 19. Detour Len.(Mi.): | 0.0 | 110. Natl. Truck Network : | 0 Not part of natl netwo |
| 20. Toll Facility : | 3 On free road | 114. Future ADT : | 1120 |
| 26. Function Class.: | 17 Urban Collector | 28b. Lanes Und.: | 6 |
| | | 29. ADT : | 700 |
| | | 32. Appr. Roadway Width (ft.): | 76.3 |
| | | 47. Total Horiz. Clr.(ft.): | 92.1 |
| | | 51. Roadway Width (ft.): | 76.3 |
| | | 100. Defense Highway : | 0 Not a STRAHNET hwy |

Agency Field: 1.(Under Rte.): 2.(Vert. X-Ref.): 3.(Compass Dir.): 4.(Vert. Post. Inc.): 5.(Vert. Post. Dec.):

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 27869

Structure No.: 5515 0671NX

Local ID: -1

Suff. Rating: 96.4
Not Deficient

Health Index :
98.2

| Description: | INSPECTION | |
|---|---|---|
| IDENTIFICATION | | |
| 46.25'-98'-46.25' PC BEAM SPANS 1. State: Oklahoma 2. SHD District: Division 4 3. County Code: OKLAHOMA 4. Place Code: OKLA. CITY Admin. Area: Unknown 5. Inventory Route (Route On Structure) : 1 - 1 - 1 - 00035 - 0 6. Feature Intersected: FAU 9421 (EASTERN) UNDER 7. Facility Carried: I-35 SB & I-40 WB I-35 SB & I-40 WB 9. Location: EASTERN AVENUE 11. Mile Post: 6.708 mi 13. LRS Inv. Route./ Subroute.: 5515HP0000 07 16. Latitude: 35 27 47.69 17. Longitude: 097 28 37.49 98. Border Br. Code: Not Applicab % Resp. : 0 99. Border Br. #: Unknown | | |
| STRUCTURE TYPE AND MATERIALS | | |
| 43. Main Span Material and Design Type Prestressed Concrete Stringer/Girder 44. Approach Span Material and Design Type Not Applicable (P) Not Applicable (P) 45. No. of Spans Main Unit: 3 46. No. of Approach Spans: 0 107. Deck Type: 1 Concrete-Cast-in-Place 108A. Wearing Surface: 1 Monolithic Concrete 108B. Membrane: 0 None 108C. Deck Protection: 1 Epoxy Coated Reinforce | | |
| AGE AND SERVICE | | |
| 27. Year Built: 2000 106. Year Reconstructed: -4 28A. Lanes on: 4 28B. Lanes Under: 6 19. Detour Length: 0.1 mi 29. ADT: 60350 30. Year of ADT: 2010 109. Truck ADT %: 12 42A. Type of Service on: 1 Highway 42B. Type of Service under: 1 Highway | | |
| GEOMETRIC DATA | | |
| 10. Inv. Rte. Min. Vert. Clr.: 328.1 ft 32. Approach Roadway Width (W/ Shoulders): 83.0 ft Deck Area: 16,555.1 sq. ft 33. Median: 0 No median 34. Skew: 7 35. Structure Flared: 0 No flare 47. Inv. Rte. Total Horiz. Clr.: 83.0 ft 48. Length Maximum Span: 98.0 ft 49. Structure Length: 192.5 ft 50A. Curb/Sdwk Wdth L: 0.0 ft 50B. Curb/Sidewalk Width R: 0.0 ft 51. Width Curb to Curb: 83.0 ft 52. Width Out to Out: 86.0 ft 53. Minimum Vertical Clearance Over Bridge: 328.1 ft 54A/54B. Min. Vert. Underclearance : H Hwy beneath struct 17.7 ft N/E S/W Meas. N1702 N1709 -1 S1702 S1704 -1 Post. DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U DO NOT U 55A/55B. Minimum Lateral Underclearance R: H Hwy beneath struct 8.0 ft 56. Minimum Lateral Underclearance L: 0.0 ft | | |
| LOAD RATING AND POSTING | | |
| 31. Design Load: 5 MS 18 (HS 20) 41. Posting status: A Open, no restriction 63. Op. Rating Method: 1 LF Load Factor-Ton Alt. Op. Rating Meth.: 1 LF Load Factor-Ton 64. Operating Rating (H / HS / 3-3) : 48.6 65.5 -1.1 66. Inventory Rating (H / HS / 3-3) : 35.7 39.3 -1.1 65. Inv. Rating Method: 1 LF Load Factor-Ton Alt. Inv. Rating Meth.: 1 LF Load Factor-Ton 70. Posting: 5 At/Above Legal Loads Date Rated : 1/1/1901 | | |
| CONDITION | | |
| 58. Deck: 7 Good 59. Super.: 8 Very Good 60. Sub.: 8 Very Good 62. Culvert: N N/A (NBI) 61. Channel/Channel Protection: N N/A (NBI) Flowline Notes: | | |
| PROPOSED IMPROVEMENTS | | |
| 94. Bridge Cost: \$1,362,217 75. Type of Work: 31 Repl-Load Capacity 95. Roadway Cost: \$2,247,658 76. Lgth. of Improvement: 204.9 ft 96. Total Cost: \$3,814,208 114. Future ADT: 96560 97. Year of Cost Est.: 2007 115. Year of Future ADT: 2030 | | |
| NAVIGATION DATA | | |
| 38. Navigation Control: NA-no waterway 39. Vertical Clearance: 0.0 ft 40. Horizontal Clearance: 0.0 ft 111. Pier Protection: Not Applicable (P) 116. Lift Bridge Vert. Clear.: 0.0 ft | | |
| APPRAISAL | | |
| 36A. Bridge Rail: 1 Meets Standards 36C. Approach Rail: 1 Meets Standards 36B. Transition: 1 Meets Standards 36D. Approach Rail Ends: 1 Meets Standards 67. Str. Evaluation: 8 Equal Desirable Crit 68. Deck Geometry: 9 Above Desirable Crit 69. Underclearance, Vertical and Horizontal: 5 Above Tolerable 71. Waterway Adequacy: N Not applicable 72. Approach Alignment: 8 Equal Desirable Crit 113. Scour Critical: N Not Over Waterway | | |
| 200c. Temperature: 53 200d. Weather: CLEAR 201. Structural Steel ASTM Desig.: -1 -1 202. Waterproof Membrane :-1 Date Installed: 1/1/1901 203. Type Exp. Dev. :Elastomeric Sheet Type Strip Seal - 204. Type of Handrail: Sloped Faced Parapet 205. Material and Quantity : -1.0 208. Type of Abutment : Skeleton Type of Foundation : Steel Piling 209. Type of Pier / Found.: 4 No Steel Piling 210. Foundation Elev. -1.0 -1.0 -1.0 -1.0 -1.0 211. Wear. Surf. Prot. System : _ Date Installed: 1/1/1901 213. Utilities Attached : Power -1 -1 -1 -1 -1 -1 | 214a. Posted Weight Limit: NR b. Posted Speed Limit : 60 c. Narrow/One Lane Bridge sign : N d. Vertical Clearance Sign: YES Advanced Warning Sign : NO Existing/Recommended Posting : 1611 1611 Min./ Max Vert. Clearance : 1801 1807 e. Navigation Lights : - Working/Not Working : - 215. Overpass : A - Interstate 221. Substructure Cond. (U/W) : - 222. Fill over RCB: -1 223. Appr. Slab/Rdwy Cond.: Good 224. Critical Feature Type: -1 225. Paint Type : - Overcoat : 0 226. Date Painted: -1 227. Paint Coloring: -1 233. Deck Forming: Conventional Forming 236. Deck Cleaning : -1 238. School Bus Rte: Current and Desired Route 240. Appr. Roadway Type: Concrete | 243. Girder Spacing/Number : -1.0 / -1 244. Span Lengths : -1 -1 -1 -1 -1 -1 -1 -1 245. Girder Depth : -1.000 246. Type of Overlay : _ 246. Overlay Thickness : 0 246. Overlay Date : 1/1/1901 246. Overlay Depth Changed > 1"? No 247. Protective Systems : 1: Epoxy Coated Bars 2: _ 3: _ 4: _ 5: _ 248. No. of Field Splices w/ Corrosion : -1 249. Scour Crit. POA exists?: _ 250. Culvert Headwall Dist.: -1.0 254. Thru Truss Type : _ 256. Chan. Profile Up/Down Stream?: 258. Plans w/ found. are in file at ODOT 259. Scour Eval. is in file at ODOT 263. Interchange at Intersection Y 264. Interstate Milepoint 127.52 |

OKLAHOMA DEPARTMENT OF TRANSPORTATION - Bridge Inspection Report

NBI No.: 27869 Structure No.: 5515 0671NX Local ID: -1

Suff. Rating: 96.4
Not Deficient

Health Index :
98.2

Inspection Date: 11/14/2012 Reported By: GHINES
Invoice No.: -1 Inspected With: Gary Richardson
Agency :

Gary Hines

Digitally signed by Gary Hines
DN: cn=Gary Hines, o, ou=Gary Richardson,
email=ghines@odot.org, c=US
Date: 2012.12.17 15:06:50 -06'00'

Structure / Inspection Notes

* THE 'SX' BRIDGE CONTROLS THE UNDER CLEARANCE FOR THIS PAIR OF BRIDGES AT 17'-2" on C/L at THE SOUTH EDGE.
** THE ADT BELOW IS MUCH MORE THAN 700 - PROBABLY > 5000 **

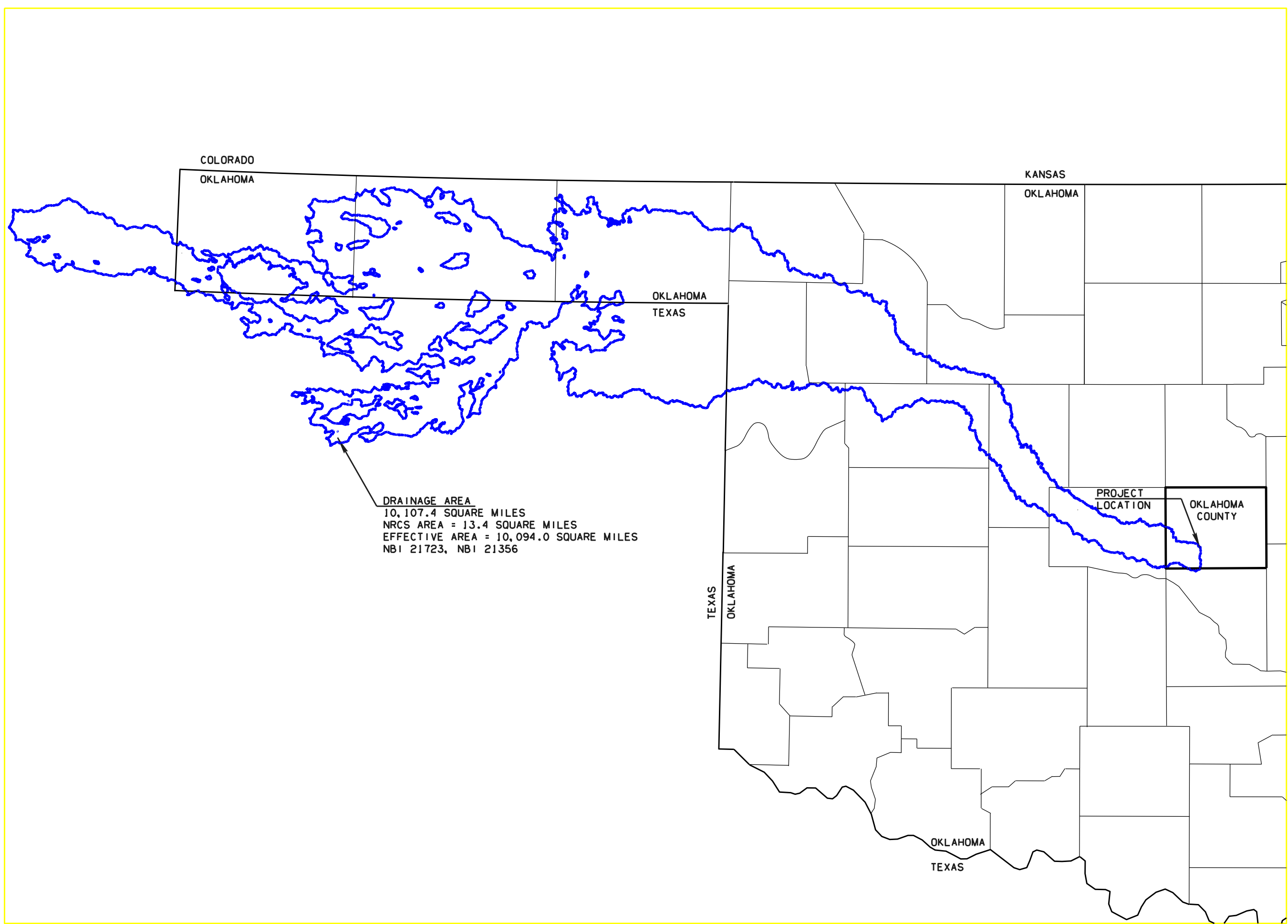
G Hines inspection comments - 11/14/2012
Good Slopewalls * Good side drains * Good concrete roadway * Safety below = 1111

| Elm. Env. | Description | Un. | Qty. | Qty.St. 1 | % 1 | Qty.St. 2 | % 2 | Qty.St. 3 | % 3 | Qty.St. 4 | % 4 | Qty.St. 5 | % 5 |
|-----------|--|------|--------|-----------|-------|-----------|------|-----------|-------|-----------|-----|-----------|-----|
| 26 | 4 Concrete Deck - Protected w/ Coated Bars | (SF) | 15,978 | 15,978 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 109 | 4 P/S Conc Open Girder/Beam | (LF) | 1,589 | 1,589 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 119 | 4 P/S Conc Open Girder/Beam Ends (5ft.) | (LF) | 300 | 300 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 205 | 4 Reinforced Conc Column or Pile Extension | (EA) | 8 | 5 | 63 % | 3 | 38 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 215 | 4 Reinforced Conc Abutment | (LF) | 171 | 163 | 95 % | 8 | 5 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 234 | 4 Reinforced Conc Cap | (LF) | 342 | 342 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 300 | 4 Strip Seal Expansion Joint | (LF) | 171 | 161 | 94 % | 10 | 6 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 301 | 4 Pourable Joint Seal | (LF) | 171 | 74 | 43 % | 97 | 57 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 310 | 4 Elastomeric Bearing | (EA) | 60 | 60 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 316 | 4 Steel Bearing Assenbly | (EA) | 60 | 60 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 321 | 4 Reinforced Conc Approach Slab w/ or w/o AC O | (EA) | 2 | 2 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 331 | 4 Reinforced Conc Bridge Railing | (LF) | 385 | 373 | 97 % | 12 | 3 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |
| 358 | 4 Deck Cracking | (EA) | 1 | 0 | 0 % | 0 | 0 % | 1 | 100 % | 0 | 0 % | 0 | 0 % |
| 359 | 4 Soffit of Concrete Deck or Slab | (EA) | 1 | 1 | 100 % | 0 | 0 % | 0 | 0 % | 0 | 0 % | 0 | 0 % |

Additional Elements _____

| Elem. | Element Notes (Include Size and Location of Deterioration) |
|-------|---|
| 26 | Superficial wear only (also see SF #358). |
| 109 | < none > |
| 119 | < none > |
| 205 | Hairline to light sized vertical cracks noted in the 3rd & 4th columns in bent #1 and on #4 in bent #2. |
| 215 | Light vertical cracks noted; 4 on each abutment with some water staining. |
| 234 | < none > |
| 300 | Soil & debris needs cleaned out. Minor exfoliation started near the North shoulder. |
| 301 | PX - Approximately half of the joint over the West abutment has deterioration. Small areas of deterioration noted over the East abutment. Some seepage evident below. |
| 310 | < none > |
| 316 | FX - One nut gone on the East abutment. |
| 321 | Light cracking noted - most have been sealed in the past. |
| 331 | Some light vertical cracks noted. |
| 358 | FX - Some minor to moderate cracks are present most of which have been sealed but need treated again. Lots of light pattern cracks in the North lane over bent #1. |
| 359 | Some light cracks with efflorescence in span #2 (2008 photo). Less than 2% of the total area is affected. |

| Roadway Name : FAU 9421 (EASTERN AVE.) | NBI Information Applicable To The Route Under The Structure | | | |
|---|---|--|---|---|
| 5. Inventory Route (Route Under Structure : 2 - 5 - 1 - 09421 - 0 | 28b. Lanes Und.: | 6 | 102. Traffic Dir.: | 2 2-way traffic |
| 10. Min. Vert. Clr.(ft.): 17.7 | 29. ADT : | 700 | 104. Highway System : | 0 Not on NHS |
| 12. Base Hwy Network : Not on Base Network | 32. Appr. Roadway Width (ft.) : | 76.3 | 105. Fed Land Hwy : | 0 N/A (NBI) |
| 13. LRS Inv. Rt./ Subroute : -1 / -1 | 47. Total Horiz. Clr.(ft.): | 92.1 | 109. Truck ADT% : | 5 |
| 19. Detour Len.(Mi.): 0.0 | 51. Roadway Width (ft.) : | 76.3 | 110. Natl. Truck Network : | 0 Not part of natl netwo |
| 20. Toll Facility : 3 On free road | 100. Defense Highway : | 0 Not a STRAHNET hwy | 114. Future ADT : | 1120 |
| 26. Function Class.: 17 Urban Collector | | | | |
| Agency Field: 1.(Under Rte.): <input type="text" value="-1"/> | 2.(Vert. X-Ref.): <input type="text" value="-1"/> | 3.(Compass Dir.): <input type="text" value="N"/> | 4.(Vert. Post. Inc.): <input type="text" value="1700"/> | 5.(Vert. Post. Dec.): <input type="text" value="1700"/> |



DRAINAGE AREA
10,107.4 SQUARE MILES
NRCS AREA = 13.4 SQUARE MILES
EFFECTIVE AREA = 10,094.0 SQUARE MILES
NBI 21723, NBI 21356

LEGEND

 DRAINAGE AREA



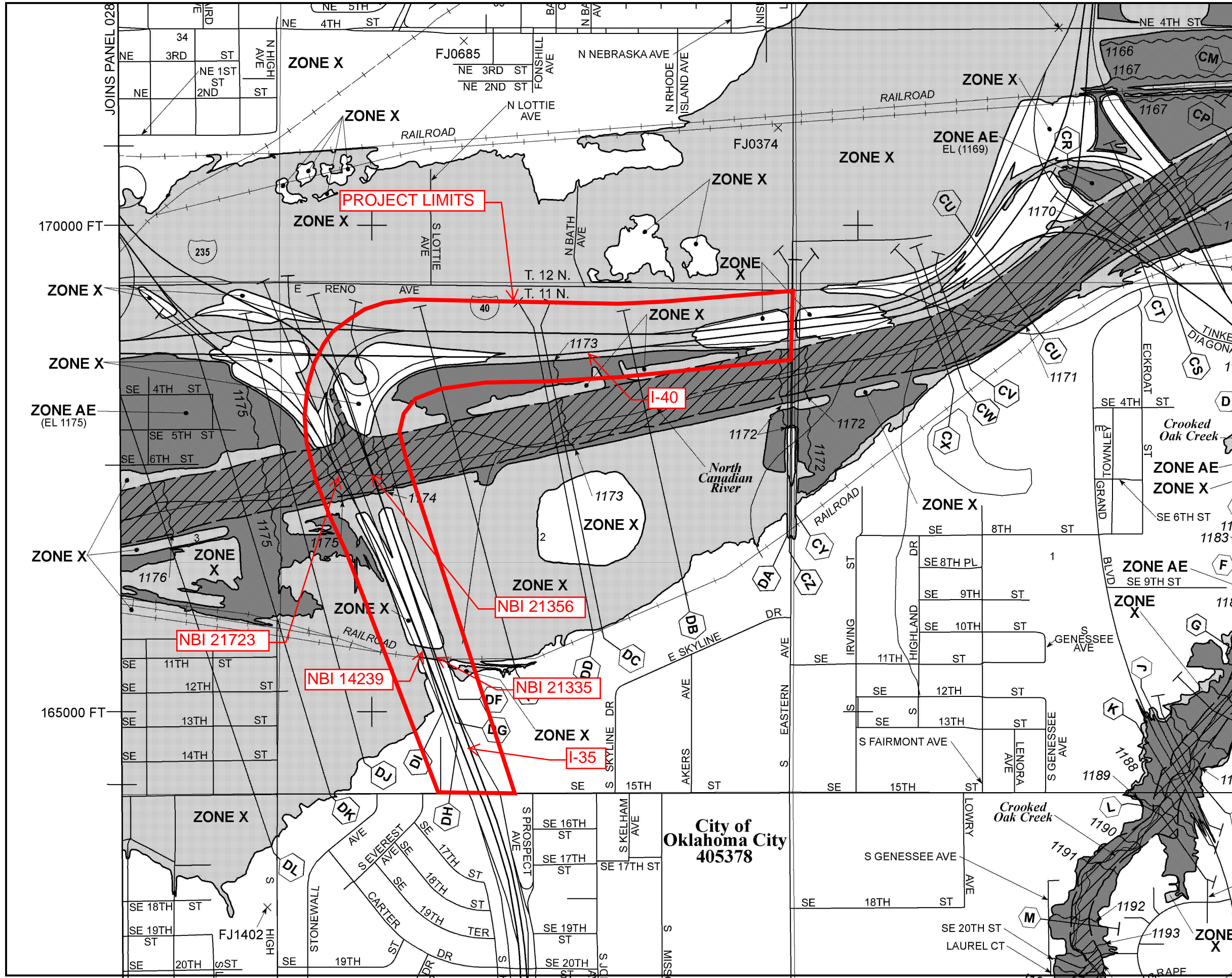
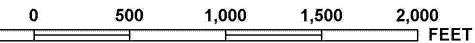
0 72,000 144,000
SCALE IN FEET

SCHEMMER
ARCHITECTS | ENGINEERS | PLANNERS
DRAINAGE AREA MAP

DIVISION 4, OKLAHOMA COUNTY
NBI 21723, NBI 21356
J/P 30444(04)



MAP SCALE 1" = 1000'



PANEL 0305H

FIRM
FLOOD INSURANCE RATE MAP
OKLAHOMA COUNTY
OKLAHOMA
AND INCORPORATED AREAS

PANEL 305 OF 370
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS:

| COMMUNITY | NUMBER | PANEL | SUFFIX |
|------------------------|--------|-------|--------|
| DEL CITY, CITY OF | 400233 | 0305 | H |
| FOREST PARK, TOWN OF | 400379 | 0305 | H |
| OKLAHOMA CITY, CITY OF | 405378 | 0305 | H |
| SMITH VILLAGE, TOWN OF | 400549 | 0305 | H |

Notice to User: The Map Number shown below should be used when placing map orders; the Community Number shown above should be used on insurance applications for the subject community.

MAP NUMBER
40109C0305H



REVISED DATE
DECEMBER 18, 2009
Federal Emergency Management Agency

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at www.msc.fema.gov

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
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OKLAHOMA COUNTY
J/P 30444(04)

SECTION 07 – CULTURAL RESOURCES

A cultural resource reconnaissance review was conducted by Oklahoma Department of Transportation Cultural Resources Program. The report is included herein.

**ODOT CULTURAL RESOURCES PROGRAM
PROJECT RECONNAISSANCE REVIEW**

| | | | |
|--------------------|------------|--------------------------------------|------------------|
| County: | Oklahoma | JP No: | 30444(04) |
| Project No: | Unassigned | Date of Field Reconnaissance: | 25 February 2014 |
| Preparer: | M. McKay | Report Date: | 26 February 2014 |

Project Description: Modifications to bridges and alignment along the I-40 / I-35 interchange (the Dallas Junction) in OKC, OK., extending NE to SW from the MLK Blvd. intersection to the SE 15th St. intersection.

Definition of Project Area: Approximately 1.5 miles long extending 200 feet from the existing centerlines of each roadway, whether north-, south-, east-, or westbound.

1 File Review - the preparer completed a desktop review of the following files:

- State Archaeological Site Files at Oklahoma Archeological Survey (OAS)
USGS Quadrangle(s): Midwest City (1956; photorev. 1969 & 1975)
- SHPO's NRHP list and Determination of Eligibility (DOE) list
- Thematic or other inventory-based Oklahoma Historic Resource Reviews (listed below):
 - 1) Oklahoma Historic Bridge Survey: Spans of Time (June, 1993) --
<http://www.okladot.state.ok.us/hqdiv/p-r-div/spansoftime/index.htm>
 - 2) Oklahoma Historic Bridge Survey: Reevaluation of Spans of Time (May, 2007) --
<http://www.okladot.state.ok.us/hqdiv/p-r-div/spansoftime/pdfs/survey-phase1.pdf>
 - 3) Oklahoma Landmarks Inventory (OLI) -- <http://www.ocgi.okstate.edu/shpo/>
 - 4) ODOT Tribal Liaison's Database Concerning Potential Culturally Sensitive Areas
- Photographic and Mapped Resources (listed below):
 - 1) General Land Office (GLO) survey map (1873) -- <http://www.glorerecords.blm.gov/>
 - 2) Moore 15 minute topographic map (1892)
 - 3) Purcell 30 minute topographic map (1898)
 - 4) USDA Soil Conservation service (SCS) Soils Map (1906)
 - 5) Crutch Creek 7.5 minute topographic map (1934)
 - 6) General Highway Map (GHM) of Oklahoma County (1936) --
http://figshare.com/articles/1936_1940_General_Highway_Maps_for_Counties_in_Oklahoma/812606
 - 7) USDA SCS Aerial Imagery (1940; 1951)
 - 8) Google Earth Timeline (1995 - 2012)

A. Results of File Review - based on the file review, the preparer identified the following 45-year-old or older properties or conditions documented as existing within the reconnaissance area:

Bridge(s) in Reconnaissance Area:

X There are bridges identified within reconnaissance area

| <u>ODOT No.</u> | <u>NBI No.</u> | <u>Const. Date</u> | <u>Bridge Type</u> | <u>NRHP Status</u> |
|-----------------|----------------|--------------------|----------------------------|--------------------|
| 55N3100E1070001 | 26128 | 1996 | Pre-stressed Concrete Beam | Program Comment |
| 5515 0671SX | 26296 | 2000 | Pre-stressed Concrete Beam | Program Comment |
| 5515 0671NX | 27869 | 2000 | Pre-stressed Concrete Beam | Program Comment |
| 5515 0585WRX | 22096 | 1988 | Steel Stringer | Program Comment |
| 5569 1022X | 21722 | 1987 | Steel Stringer | Program Comment |
| 5515 0566EX | 21356 | 1986 | Pre-stressed Concrete Beam | Program Comment |
| 5515 0566WX | 21723 | 1987 | Pre-stressed Concrete Beam | Program Comment |
| 5515 0534EX | 21335 | 1986 | Pre-stressed Concrete Beam | Program Comment |
| 5515 0534WX | 21708 | 1987 | Pre-stressed Concrete Beam | Program Comment |
| 5515 0526X | 14239 | 1959 | Concrete Culvert | Program Comment |
| 5515 0506X | 23635 | 1994 | Steel Stringer | Program Comment |

Comments: In accordance with the February 2005 ACHP Section 106 Exemption regarding effects to the Interstate Highway System, these bridges do not require documenting and review.

SHPO NRHP/DOE listed Properties in Reconnaissance Area:

X There are no previously listed NRHP or DOE properties located within the reconnaissance area

Comments: There are no documented historic properties located within the study area based upon review of NRHP and NRHP-eligible properties or thematic surveys collected within SHPO databases.

Archaeological Sites in Reconnaissance Area as mapped at the OAS:

There are no sites mapped within the reconnaissance area

Comments: There are no mapped archaeological sites located within the study area based upon review of the Oklahoma Archaeological Survey's (OAS) site files.

B. Based on the file review, the preparer identified the potential for the following 45-year-old or older properties or conditions to exist within the reconnaissance area:

X The preparer identified the potential presence of the following 45 year old or older properties

in the reconnaissance area

Source: GLO map OAS map Aerial photograph Other

Property Type: Prehistoric site 19th Century site 20th century site Other

Comments: Based upon comparisons with georeferenced maps and aerial imagery (see above), there may be two 45-year-old or older buildings located within the NW portion of the recon corridor north of existing I-40 centerline between I-40 and Reno Avenue.

C. Based on the file review, the preparer identified the following cultural resources surveys or studies previously conducted within the reconnaissance area:

Comments: Although this portion of the Dallas Junction was likely to have been surveyed as part of USACE permitting for Oklahoma River construction and for tie-in of the recent Crosstown Expressway project, no mapped cultural resources are identified within the recon corridor as determined through comparisons with reference databases (see above).

2 Reconnaissance or windshield survey

X The preparer completed a windshield review of the reconnaissance area and has made the following observations

Bridge(s) in Reconnaissance Area:

X The preparer did not observe additional bridges in the reconnaissance area

Comments: The existing bridges are exempt from documenting according to the February 2005 ACHP Section 106 Exemption regarding effects to the Interstate Highway System.

The study area also includes a large, two-box concrete culvert and a railway culvert comprised of (4) large corrugated metal drainpipes (CGMP). These drainage structures have no associated NBI data since they are less than 20 feet in length and will not need to be documented.

Properties in the Reconnaissance Area that appear to be 45 Years Old or Older:

X The preparer observed the following 45-year-old or older properties in the reconnaissance area (see mapped locations below)

| # | <u>Property Type</u> | <u>Description</u> | <u>Location within reconnaissance area</u> |
|----------|-----------------------------|--|---|
| 1 | Building | Large metal-clad commercial building noted on georeferenced maps; low NRHP potential. | Central portion of the recon corridor, north of existing I-40 centerline. |

Comments: Although this building is considered to have low potential for being determined eligible for inclusion in the NRHP, it will need to be documented

then reviewed by SHPO to assess its NRHP eligibility, however; this will not hinder or delay project development or delivery.

Possible Archaeological Sites Observed:

X The preparer did not observe potential archaeological sites in the reconnaissance area

Comments: No possible archaeological sites remnant to the previous presence of properties 45-years-old or older were noted within the reconnaissance area during windshield scoping.

Areas Potentially Supporting Prehistoric Archaeological Sites:

Comments: Based on the landforms present within the reconnaissance corridor, the archaeological sensitivity of the area, the level of landscape disturbance, and the types of sites mapped on the USGS quadrangle at the OAS, there is very limited likelihood of prehistoric archaeological sites existing within the recon corridor.

Additional Comments: The recon corridor has been extensively disturbed by past highway and city street improvements and by recent commercial development so the likelihood of intact cultural deposits existing within the recon corridor is extremely limited.

Additionally, since the bridges and culverts included within this proposed project area are exempted from documenting and review under the February 2005 Program Comment, it's unlikely that this project would be subjected to a full cultural resources survey and would likely be relegated to screened review alone, further expediting project delivery.

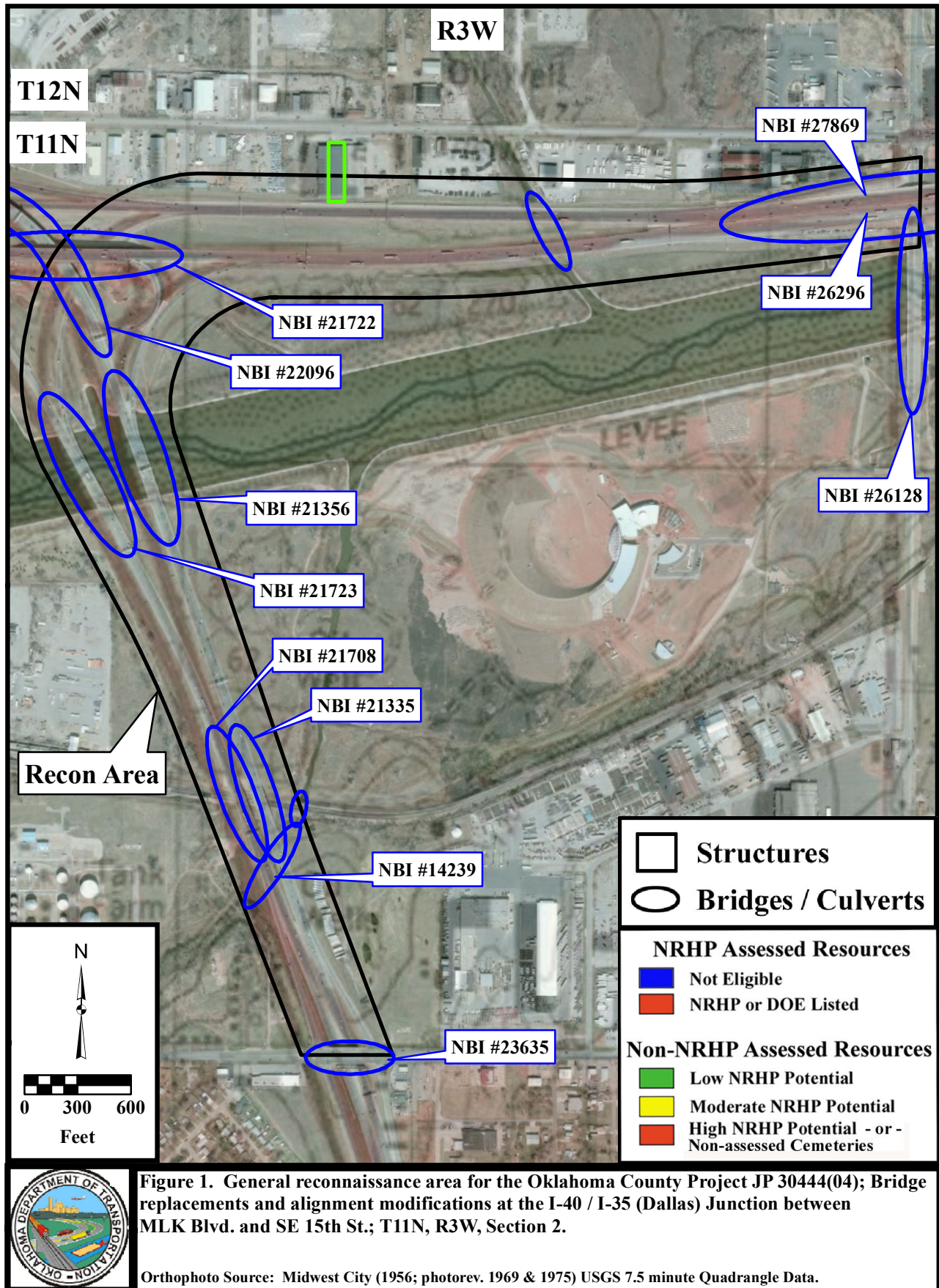


Figure 1. General reconnaissance area for the Oklahoma County Project JP 30444(04); Bridge replacements and alignment modifications at the I-40 / I-35 (Dallas) Junction between MLK Blvd. and SE 15th St.; T11N, R3W, Section 2.

Orthophoto Source: Midwest City (1956; photorev. 1969 & 1975) USGS 7.5 minute Quadrangle Data.

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 OKLAHOMA COUNTY
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SECTION 08 – HAZARDOUS WASTE/LUST SITES

Enercon Services, Inc. performed a hazardous waste/LUST site assessment. Their report is included herein.

8.1 Hazardous Waste Sites

Review of a radius map report prepared by Environmental Data Resources (EDR) identified the following state and/or federal regulatory database listings indicating hazardous waste sites within the proximity of the study area using ASTM E1527-00 radius guidelines. Sites located outside the study area that may warrant additional consideration are also included:

| Regulatory Database(s) | Facility Name | Address | Approximate Distance from Study Area (Feet) |
|---|----------------------------------|---------------------------|--|
| Delisted NPL, CERCLIS, US ENG CONTROLS, US INST CONTROL, ROD, FINDS | Fourth Street Abandoned Refinery | 2200 Block, NE 4th Street | 2,100 |
| US Brownfields, FINDS | 1001 E. Reno Property | 1001 E. Reno | 950 |
| RCRCA-CESQG | Interstate Metals Corporation | 1101 E. Reno | 0 |
| RCRA-SQG | The Waggoners Trucking | 1501 SE 15th | 250 |

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8.2 Aboveground Storage Tanks (AST), Underground Storage Tanks (UST), Leaking Underground Storage Tank (LUST) Sites, and Oil and Gas Production Sites

Review of a radius map report prepared by Environmental Data Resources (EDR) identified the following state and/or federal regulatory database listings within the proximity of the study area using ASTM E1527-00 radius guidelines:

| Regulatory Database(s) | Facility Name | Address | Approximate Distance from Study Area (Feet) |
|-------------------------------|---------------------------------------|--------------------------|--|
| LUST, UST, HIST UST | Gary Dales 66, | 1948 E. Reno | 0 |
| Regulatory Database(s) | Facility Name | Address | Approximate Distance from Study Area (Feet) |
| UST, LUST, HIST UST | Oklahoma Department of Transportation | 1100 E. Reno | 0 |
| UST, LUST, HIST UST | Interstate Metals Corporation | 1101 E. Reno | 0 |
| UST, LUST, HIST UST | Yellow Freight System | 1600 E. Reno | 0 |
| LUST, HIST UST, UST | The Waggoners Trucking | 1501 SE 15 th | 250 |
| LUST, UST, HIST UST | Metro Mart | 1320 SE 15 th | 0 |
| LUST, UST, HIST UST | Ker McGee #121-6584 | 1301 SE 15 th | 0 |

No AST database listings were identified within the study area by the EDR radius map report. Visual observation identified six ASTs associated with oil and gas production in the SE/4 of the SW/4 of Section 2, Township 11 North, Range 3 West, and two ASTs in

OKLAHOMA DEPARTMENT OF TRANSPORTATION

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OKLAHOMA COUNTY

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the NE/4 of the SW/4 of Section 2, Township 11 North, Range 3 West. Two ASTs associated with oil and gas production were observed in the NW/4 of the NW/4 of Section 1, Township 11 North, Range 3 West.


















According to the Oklahoma Corporation Commission, 32 well sites are located within 1/8th of a mile of the study area. OCC records and field reconnaissance indicate that three of these well sites are currently active. Plugging records are on file for 27 of the remaining wells. The current disposition of two wells which do not have plugging records but which were not identified through field reconnaissance could not be determined. See attached well records for further information.

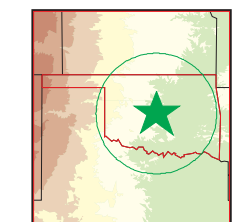
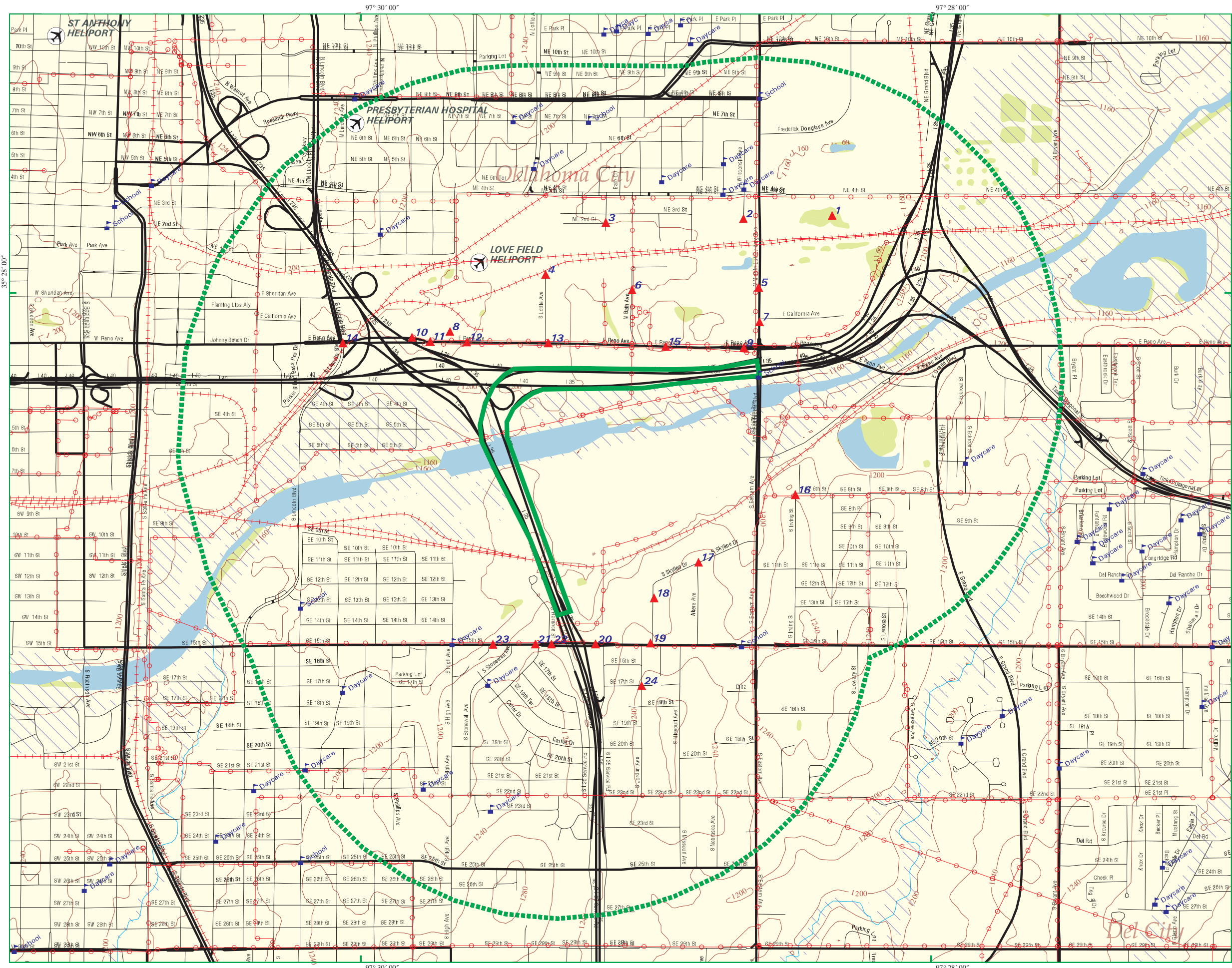
8.3 Coal Mining Operations

No coal mining operations were indicated by a review of the USGS topographic map.

EDR DataMap® Area Study

I-35/I-40

-  Listed Sites
-  Earthquake Epicenters (Richter 5 or greater)
-  Search Boundary
-  Roads
-  Major Roads
-  Waterways
-  Railroads
-  Contour Lines
-  Pipelines
-  Powerlines
-  Fault Lines
-  Water
-  Superfund Sites
-  Federal DOD Sites
-  Indian Reservations BIA
-  100-Yr Flood Zones
-  National Wetland Inventory



Oklahoma City, OK

0 1/4 1/2



Scale in Miles



I-35/I-40

Oklahoma City, OK 73117

Inquiry Number: 3824799.20s

January 08, 2014

EDR DataMap™ Area Study

Thank you for your business.
Please contact EDR at 1-800-352-0050
with any questions or comments.

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EXECUTIVE SUMMARY

TARGET PROPERTY INFORMATION

ADDRESS

OKLAHOMA CITY, OK 73117
OKLAHOMA CITY, OK 73117

DATABASES WITH NO MAPPED SITES

No mapped sites were found in EDR's search of available ("reasonably ascertainable ") government records within the requested search area for the following databases:

FEDERAL RECORDS

| | |
|-----------------|---|
| NPL | National Priority List |
| Proposed NPL | Proposed National Priority List Sites |
| NPL LIENS | Federal Superfund Liens |
| LIENS 2 | CERCLA Lien Information |
| CORRACTS | Corrective Action Report |
| RCRA-TSDF | RCRA - Treatment, Storage and Disposal |
| RCRA-LQG | RCRA - Large Quantity Generators |
| ERNS | Emergency Response Notification System |
| HMIRS | Hazardous Materials Information Reporting System |
| DOT OPS | Incident and Accident Data |
| US CDL | Clandestine Drug Labs |
| DOD | Department of Defense Sites |
| FUDS | Formerly Used Defense Sites |
| LUCIS | Land Use Control Information System |
| CONSENT | Superfund (CERCLA) Consent Decrees |
| UMTRA | Uranium Mill Tailings Sites |
| ODI | Open Dump Inventory |
| DEBRIS REGION 9 | Torres Martinez Reservation Illegal Dump Site Locations |
| US MINES | Mines Master Index File |
| TRIS | Toxic Chemical Release Inventory System |
| TSCA | Toxic Substances Control Act |
| FTTS | FIFRA/ TSCA Tracking System - FIFRA (Federal Insecticide, Fungicide, & Rodenticide Act)/TSCA (Toxic Substances Control Act) |
| HIST FTTS | FIFRA/TSCA Tracking System Administrative Case Listing |
| SSTS | Section 7 Tracking Systems |
| ICIS | Integrated Compliance Information System |
| PADS | PCB Activity Database System |
| MLTS | Material Licensing Tracking System |
| RADINFO | Radiation Information Database |
| RAATS | RCRA Administrative Action Tracking System |
| RMP | Risk Management Plans |
| LEAD SMELTERS | Lead Smelter Sites |
| 2020 COR ACTION | 2020 Corrective Action Program List |
| PCB TRANSFORMER | PCB Transformer Registration Database |
| US HIST CDL | National Clandestine Laboratory Register |

EXECUTIVE SUMMARY

| | |
|-----------------------|--|
| FEDERAL FACILITY..... | Federal Facility Site Information listing |
| COAL ASH EPA..... | Coal Combustion Residues Surface Impoundments List |
| FEMA UST..... | Underground Storage Tank Listing |
| US FIN ASSUR..... | Financial Assurance Information |
| COAL ASH DOE..... | Steam-Electric Plant Operation Data |
| EPA WATCH LIST..... | EPA WATCH LIST |
| SCRD DRYCLEANERS..... | State Coalition for Remediation of Drycleaners Listing |
| PRP..... | Potentially Responsible Parties |

STATE AND LOCAL RECORDS

| | |
|-------------------|--|
| SHWS..... | The Land Report |
| SWF/LF..... | Permitted Solid Waste Disposal & Processing Facilities |
| UIC..... | Underground Injection Wells Database Listing |
| INST CONTROL..... | Institutional Control Sites |
| DRYCLEANERS..... | Drycleaner Facility Listing |
| RGA HWS..... | Recovered Government Archive State Hazardous Waste Facilities List |

TRIBAL RECORDS

| | |
|--------------------|--|
| INDIAN RESERV..... | Indian Reservations |
| INDIAN ODI..... | Report on the Status of Open Dumps on Indian Lands |
| INDIAN LUST..... | Leaking Underground Storage Tanks on Indian Land |
| INDIAN UST..... | Underground Storage Tanks on Indian Land |
| INDIAN VCP..... | Voluntary Cleanup Priority Listing |

EDR PROPRIETARY RECORDS

| | |
|--------------|---|
| EDR MGP..... | EDR Proprietary Manufactured Gas Plants |
|--------------|---|

SURROUNDING SITES: SEARCH RESULTS

Surrounding sites were identified.

Page numbers and map identification numbers refer to the EDR Radius Map report where detailed data on individual sites can be reviewed.

Sites listed in ***bold italics*** are in multiple databases.

Unmappable (orphan) sites are not considered in the foregoing analysis.

FEDERAL RECORDS

Delisted NPL: The National Oil and Hazardous Substances Pollution Contingency Plan (NCP) establishes the criteria that the EPA uses to delete sites from the NPL. In accordance with 40 CFR 300.425.(e), sites may be deleted from the NPL where no further response is appropriate.

A review of the Delisted NPL list, as provided by EDR, and dated 04/26/2013 has revealed that there is 1 Delisted NPL site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--|---------------------------------|-----------------|-----------------|
| <i>FOURTH STREET ABANDONED REFINERY</i> | <i>2200 BLOCK NE 4TH</i> | <i>1</i> | <i>3</i> |

EXECUTIVE SUMMARY

CERCLIS: The Comprehensive Environmental Response, Compensation and Liability Information System contains data on potentially hazardous waste sites that have been reported to the USEPA by states, municipalities, private companies and private persons, pursuant to Section 103 of the Comprehensive Environmental Response, Compensation and Liability Act (CERCLA). CERCLIS contains sites which are either proposed to or on the National Priorities List (NPL) and sites which are in the screening and assessment phase for possible inclusion on the NPL.

A review of the CERCLIS list, as provided by EDR, and dated 04/26/2013 has revealed that there are 2 CERCLIS sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--|--------------------------|---------------|-------------|
| FOURTH STREET ABANDONED REFINERIE | 2200 BLOCK NE 4TH | 1 | 3 |
| HENLEY'S SEALANT/ZONOLITE WR G | 200 WISCONSIN | 2 | 21 |

CERC-NFRAP: Archived sites are sites that have been removed and archived from the inventory of CERCLIS sites. Archived status indicates that, to the best of EPA's knowledge, assessment at a site has been completed and that EPA has determined no further steps will be taken to list this site on the National Priorities List (NPL), unless information indicates this decision was not appropriate or other considerations require a recommendation for listing at a later time. This decision does not necessarily mean that there is no hazard associated with a given site; it only means that, based upon available information, the location is not judged to be a potential NPL site.

A review of the CERC-NFRAP list, as provided by EDR, and dated 04/26/2013 has revealed that there are 4 CERC-NFRAP sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--------------------------|-------------------------|---------------|-------------|
| POND (E RENO AVE SITE) | 1001 E RENO AVE | 8 | 34 |
| EASTERN AVENUE SITE | 1600' N OF RENO AVE & E | 9 | 38 |
| PARAWAS REFINERY | 801 SOUTH IRVING STREET | 16 | 64 |
| TIME-DC TRUCKING COMPANY | 1400 SE SKYLINE | 18 | 72 |

RCRA-SQG: RCRAInfo is EPA's comprehensive information system, providing access to data supporting the Resource Conservation and Recovery Act (RCRA) of 1976 and the Hazardous and Solid Waste Amendments (HSWA) of 1984. The database includes selective information on sites which generate, transport, store, treat and/or dispose of hazardous waste as defined by the Resource Conservation and Recovery Act (RCRA). Small quantity generators (SQGs) generate between 100 kg and 1,000 kg of hazardous waste per month.

A review of the RCRA-SQG list, as provided by EDR, and dated 09/10/2013 has revealed that there is 1 RCRA-SQG site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|-------------------------------|-------------------------------|---------------|-------------|
| WAGGONERS TRUCKING THE | 1501 SE 15TH - SUITE A | 19 | 77 |

EXECUTIVE SUMMARY

RCRA-CESQG: RCRAInfo is EPA's comprehensive information system, providing access to data supporting the Resource Conservation and Recovery Act (RCRA) of 1976 and the Hazardous and Solid Waste Amendments (HSWA) of 1984. The database includes selective information on sites which generate, transport, store, treat and/or dispose of hazardous waste as defined by the Resource Conservation and Recovery Act (RCRA). Conditionally exempt small quantity generators (CESQGs) generate less than 100 kg of hazardous waste, or less than 1 kg of acutely hazardous waste per month.

A review of the RCRA-CESQG list, as provided by EDR, and dated 09/10/2013 has revealed that there is 1 RCRA-CESQG site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|---------------------------------------|--------------------|---------------|-------------|
| <i>INTERSTATE METALS C/1101 E REN</i> | <i>1101 E RENO</i> | <i>12</i> | <i>55</i> |

RCRA NonGen / NLR: RCRAInfo is EPA's comprehensive information system, providing access to data supporting the Resource Conservation and Recovery Act (RCRA) of 1976 and the Hazardous and Solid Waste Amendments (HSWA) of 1984. The database includes selective information on sites which generate, transport, store, treat and/or dispose of hazardous waste as defined by the Resource Conservation and Recovery Act (RCRA). Non-Generators do not presently generate hazardous waste.

A review of the RCRA NonGen / NLR list, as provided by EDR, and dated 09/10/2013 has revealed that there are 2 RCRA NonGen / NLR sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|---------------------------------|---------------------------|---------------|-------------|
| <i>BEACON WELL SERVICE INC</i> | <i>1001 E RENO</i> | <i>8</i> | <i>35</i> |
| <i>CONSOLIDATED FREIGHTWAYS</i> | <i>1400 SE SKYLINE DR</i> | <i>19</i> | <i>83</i> |

US ENG CONTROLS: A listing of sites with engineering controls in place.

A review of the US ENG CONTROLS list, as provided by EDR, and dated 06/17/2013 has revealed that there is 1 US ENG CONTROLS site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--------------------------------------|--------------------------|---------------|-------------|
| <i>FOURTH STREET ABANDONED REFIN</i> | <i>2200 BLOCK NE 4TH</i> | <i>1</i> | <i>3</i> |

US INST CONTROL: A listing of sites with institutional controls in place. Institutional controls include administrative measures, such as groundwater use restrictions, construction restrictions, property use restrictions, and post remediation care requirements intended to prevent exposure to contaminants remaining on site. Deed restrictions are generally required as part of the institutional controls.

A review of the US INST CONTROL list, as provided by EDR, and dated 06/17/2013 has revealed that there is 1 US INST CONTROL site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--------------------------------------|--------------------------|---------------|-------------|
| <i>FOURTH STREET ABANDONED REFIN</i> | <i>2200 BLOCK NE 4TH</i> | <i>1</i> | <i>3</i> |

EXECUTIVE SUMMARY

US BROWNFIELDS: The EPA's listing of Brownfields properties from the Cleanups in My Community program, which provides information on Brownfields properties for which information is reported back to EPA, as well as areas served by Brownfields grant programs.

A review of the US BROWNFIELDS list, as provided by EDR, and dated 09/24/2013 has revealed that there is 1 US BROWNFIELDS site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|------------------------------|---------------------|---------------|-------------|
| 1001 E. RENO PROPERTY | 1001 E. RENO | 11 | 48 |

ROD: Record of Decision. ROD documents mandate a permanent remedy at an NPL (Superfund) site containing technical and health information to aid the cleanup.

A review of the ROD list, as provided by EDR, and dated 04/26/2013 has revealed that there is 1 ROD site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|---------------------------------------|--------------------------|---------------|-------------|
| FOURTH STREET ABANDONED REFINE | 2200 BLOCK NE 4TH | 1 | 3 |

FINDS: The Facility Index System contains both facility information and "pointers" to other sources of information that contain more detail. These include: RCRIS; Permit Compliance System (PCS); Aerometric Information Retrieval System (AIRS); FATES (FIFRA [Federal Insecticide Fungicide Rodenticide Act] and TSCA Enforcement System, FTTS [FIFRA/TSCA Tracking System]; CERCLIS; DOCKET (Enforcement Docket used to manage and track information on civil judicial enforcement cases for all environmental statutes); Federal Underground Injection Control (FURS); Federal Reporting Data System (FRDS); Surface Impoundments (SIA); TSCA Chemicals in Commerce Information System (CICS); PADS; RCRA-J (medical waste transporters/disposers); TRIS; and TSCA. The source of this database is the U.S. EPA/NTIS.

A review of the FINDS list, as provided by EDR, and dated 03/08/2013 has revealed that there are 6 FINDS sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|---------------------------------------|-------------------------------|---------------|-------------|
| FOURTH STREET ABANDONED REFINE | 2200 BLOCK NE 4TH | 1 | 3 |
| BEACON WELL SERVICE INC | 1001 E RENO | 8 | 35 |
| 1001 E. RENO PROPERTY | 1001 E. RENO | 11 | 48 |
| INTERSTATE METALS C/1101 E REN | 1101 E RENO | 12 | 55 |
| WAGGONERS TRUCKING THE | 1501 SE 15TH - SUITE A | 19 | 77 |
| CONSOLIDATED FREIGHTWAYS | 1400 SE SKYLINE DR | 19 | 83 |

US AIRS: The database is a sub-system of Aerometric Information Retrieval System (AIRS). AFS contains compliance data on air pollution point sources regulated by the U.S. EPA and/or state and local air regulatory agencies. This information comes from source reports by various stationary sources of air pollution, such as electric power plants, steel mills, factories, and universities, and provides information about the air pollutants they produce. Action, air program, air program pollutant, and general level plant data. It is used to track emissions and compliance data from industrial plants.

A review of the US AIRS list, as provided by EDR, and dated 10/23/2013 has revealed that there is 1

EXECUTIVE SUMMARY

US AIRS site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|---------------------------------------|--------------------|---------------|-------------|
| INTERSTATE METALS C/1101 E REN | 1101 E RENO | 12 | 55 |

STATE AND LOCAL RECORDS

SWRCY: A listing of recycling facility locations.

A review of the SWRCY list, as provided by EDR, and dated 10/21/2013 has revealed that there are 2 SWRCY sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|-----------------------|-----------------|---------------|-------------|
| OILS RECOVERY | 308 N. FONSHILL | 3 | 23 |
| DERICHEBOURG RECYCLIN | 100 N BATH AVE | 6 | 25 |

LUST: The Leaking Underground Storage Tank Incident Reports contain an inventory of reported leaking underground storage tank incidents. The data come from the Oklahoma Corporation Commission's Leaking UST list.

A review of the LUST list, as provided by EDR, and dated 11/01/2013 has revealed that there are 18 LUST sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--|--------------------------------|---------------|-------------|
| BRUCE RYAN NOMINEE STATUS: Closed | 100 SOUTH LOTTIE | 4 | 24 |
| PETRO PSC PROPERTIES L.P. STATUS: Closed STATUS: Open | 20 S MARTIN LUTHER KING | 7 | 26 |
| GARY DALES 66 STATUS: Closed | 1948 E RENO | 9 | 36 |
| JJ'S #5 STATUS: Closed | 1935 E RENO | 9 | 39 |
| CHECKERS TRUCK STOP INC STATUS: Closed | 1901 E RENO | 9 | 41 |
| Not reported STATUS: Closed | 927 E RENO | 10 | 47 |
| Not reported STATUS: Closed | 1100 E RENO | 12 | 52 |
| INTERSTATE METALS CORPORATION STATUS: Closed | 1101 E RENO | 12 | 54 |
| Not reported STATUS: Closed | 1600 E RENO | 15 | 63 |
| MID-AMERICA CHEMICAL, INC. STATUS: Closed | 1801 SKYLINE DRIVE | 17 | 65 |
| BEAVER EXPRESS LLC STATUS: Closed | 1515 SE 15TH | 19 | 72 |

EXECUTIVE SUMMARY

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--|-------------------------------|---------------|-------------|
| WAGGONERS TRUCKING THE STATUS: Closed | 1501 SE 15TH - SUITE A | 19 | 77 |
| CONSOLIDATED FREIGHTWAYS STATUS: Closed | 1400 SE SKYLINE DR | 19 | 83 |
| SAIA MOTOR FREIGHT LINES STATUS: Closed | 1715 S SKYLINE DRIVE | 19 | 88 |
| METRO MART STATUS: Closed | 1320 SE 15TH | 20 | 92 |
| SUNOCO #23 STATUS: Closed | 1400 SE 15TH | 20 | 96 |
| KERR MCGEE #121-6584 STATUS: Closed | 1301 SE 15TH | 20 | 98 |
| CENTRAL EXPLORATION CO., INC. STATUS: Closed | 1800 S JORDAN | 24 | 102 |

UST: The Underground Storage Tank database contains registered USTs. USTs are regulated under Subtitle I of the Resource Conservation and Recovery Act (RCRA). The data come from the Oklahoma Corporation Commission's State UST List, List II Version.

A review of the UST list, as provided by EDR, and dated 11/05/2013 has revealed that there are 19 UST sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|---------------------------------------|--------------------------------|---------------|-------------|
| W.R. GRACE & CO. | 201 N EASTERN | 5 | 24 |
| PETRO PSC PROPERTIES L.P. | 20 S MARTIN LUTHER KING | 7 | 26 |
| ELLSWORTH MTR FREIGHT LINES IN | 1001 E RENO | 8 | 34 |
| GARY DALES 66 | 1948 E RENO | 9 | 36 |
| JJ'S #5 | 1935 E RENO | 9 | 39 |
| CHECKERS TRUCK STOP INC | 1901 E RENO | 9 | 41 |
| OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO | 12 | 53 |
| INTERSTATE METALS CORPORATION | 1101 E RENO | 12 | 54 |
| FORMER ICX | 1315 E RENO AVENUE | 13 | 60 |
| YELLOW FREIGHT SYSTEM, INC. | 1600 E RENO | 15 | 62 |
| BEAVER EXPRESS LLC | 1515 SE 15TH | 19 | 72 |
| WAGGONER'S TRUCKING | 1501 SE 15 STR | 19 | 74 |
| CONSOLIDATED FREIGHTWAYS | 1400 SE SKYLINE DR | 19 | 83 |
| SAIA MOTOR FREIGHT LINES | 1715 S SKYLINE DRIVE | 19 | 88 |
| METRO MART | 1320 SE 15TH | 20 | 92 |
| SUNOCO #23 | 1400 SE 15TH | 20 | 96 |
| KERR MCGEE #121-6584 | 1301 SE 15TH | 20 | 98 |
| 7 ELEVEN #79 | 1000 SE 15TH ST | 23 | 101 |
| CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN | 24 | 102 |

EXECUTIVE SUMMARY

HIST UST: This underground storage tank listing includes tank information through March 2003. This listing is no longer updated by the Oklahoma Corporation Commission.

A review of the HIST UST list, as provided by EDR, and dated 03/21/2003 has revealed that there are 21 HIST UST sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|---------------------------------------|--------------------------------|---------------|-------------|
| BRUCE RYAN NOMINEE | 100 SOUTH LOTTIE | 4 | 24 |
| W.R. GRACE & CO. | 201 N EASTERN | 5 | 24 |
| PETRO PSC PROPERTIES L.P. | 20 S MARTIN LUTHER KING | 7 | 26 |
| ELLSWORTH MTR FREIGHT LINES IN | 1001 E RENO | 8 | 34 |
| GARY DALES 66 | 1948 E RENO | 9 | 36 |
| JJ'S #5 | 1935 E RENO | 9 | 39 |
| CHECKERS TRUCK STOP INC | 1901 E RENO | 9 | 41 |
| Not reported | 927 E RENO | 10 | 47 |
| Not reported | 1100 E RENO | 12 | 52 |
| INTERSTATE METALS CORPORATION | 1101 E RENO | 12 | 54 |
| FORMER ICX | 1315 E RENO AVENUE | 13 | 61 |
| Not reported | 1600 E RENO | 15 | 63 |
| BEAVER EXPRESS LLC | 1515 SE 15TH | 19 | 72 |
| WAGGONERS TRUCKING THE | 1501 SE 15TH - SUITE A | 19 | 77 |
| CONSOLIDATED FREIGHTWAYS | 1400 SE SKYLINE DR | 19 | 83 |
| SAIA MOTOR FREIGHT LINES | 1715 S SKYLINE DRIVE | 19 | 88 |
| METRO MART | 1320 SE 15TH | 20 | 92 |
| SUNOCO #23 | 1400 SE 15TH | 20 | 96 |
| KERR MCGEE #121-6584 | 1301 SE 15TH | 20 | 98 |
| 7 ELEVEN #79 | 1000 SE 15TH ST | 23 | 101 |
| CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN | 24 | 102 |

LAST: The Leaking Aboveground Storage Tank database.

A review of the LAST list, as provided by EDR, and dated 11/01/2013 has revealed that there are 2 LAST sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|---|--------------------------------|---------------|-------------|
| PETRO PSC PROPERTIES L.P. STATUS: Closed | 20 S MARTIN LUTHER KING | 7 | 26 |
| MID-AMERICA CHEMICAL, INC. STATUS: Closed | 1801 SKYLINE DRIVE | 17 | 65 |

AST: The Aboveground Storage Tank database contains registered ASTs. The data come from the Oklahoma Corporation Commission's State AST List, List II Version.

A review of the AST list, as provided by EDR, and dated 11/05/2013 has revealed that there are 3 AST sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|-----------------------------------|--------------------------------|---------------|-------------|
| PETRO PSC PROPERTIES L.P. | 20 S MARTIN LUTHER KING | 7 | 26 |
| MID-AMERICA CHEMICAL, INC. | 1801 SKYLINE DRIVE | 17 | 65 |
| WAGGONER'S TRUCKING | 1501 SE 15 STR | 19 | 74 |

EXECUTIVE SUMMARY

VCP: Voluntary Cleanup Site Inventory.

A review of the VCP list, as provided by EDR, and dated 11/07/2013 has revealed that there is 1 VCP site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--------------|----------------|---------------|-------------|
| METRO TOWING | 1001 EAST RENO | 8 | 33 |

BROWNFIELDS:

A review of the BROWNFIELDS list, as provided by EDR, and dated 09/07/2012 has revealed that there is 1 BROWNFIELDS site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--------------------------------|-------------------------|---------------|-------------|
| OKLAHOMA CITY URBAN RENEWAL AU | SOUTH RENO AVE. & BYERS | 14 | 61 |

AIRS: A listing of permitted AIRS facility locations.

A review of the AIRS list, as provided by EDR, and dated 10/09/2013 has revealed that there is 1 AIRS site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--------------------------------------|--------------------|---------------|-------------|
| <i>INTERSTATE METALS CORPORATION</i> | <i>1101 E RENO</i> | <i>12</i> | <i>54</i> |

TIER 2: A listing of facilities which store or manufacture hazardous materials and submit a chemical inventory report.

A review of the TIER 2 list, as provided by EDR, and dated 12/31/2011 has revealed that there are 3 TIER 2 sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|-----------------------------------|--------------------------------|---------------|-------------|
| <i>PETRO PSC PROPERTIES L.P.</i> | <i>20 S MARTIN LUTHER KING</i> | <i>7</i> | <i>26</i> |
| <i>MID-AMERICA CHEMICAL, INC.</i> | <i>1801 SKYLINE DRIVE</i> | <i>17</i> | <i>65</i> |
| <i>SAIA MOTOR FREIGHT LINES</i> | <i>1715 S SKYLINE DRIVE</i> | <i>19</i> | <i>88</i> |

OK COMPLAINT: Environmental complaints report to the Oklahoma corporation commission.

A review of the OK COMPLAINT list, as provided by EDR, and dated 09/30/2012 has revealed that there is 1 OK COMPLAINT site within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|-----------------------------------|---------------------------|---------------|-------------|
| <i>MID-AMERICA CHEMICAL, INC.</i> | <i>1801 SKYLINE DRIVE</i> | <i>17</i> | <i>65</i> |

EXECUTIVE SUMMARY

RGA LUST: The EDR Recovered Government Archive Leaking Underground Storage Tank database provides a list of LUST incidents derived from historical databases and includes many records that no longer appear in current government lists.

A review of the RGA LUST list, as provided by EDR, has revealed that there are 20 RGA LUST sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--------------------------------------|--------------------------------|---------------|-------------|
| BRUCE RYAN NOMINEE | 100 SOUTH LOTTIE | 4 | 24 |
| PETRO PSC PROPERTIES L.P. | 20 S MARTIN LUTHER KING | 7 | 26 |
| GARY DALES 66 | 1948 E RENO | 9 | 36 |
| JJ'S #5 | 1935 E RENO | 9 | 39 |
| Not reported | 1901 E RENO AVE | 9 | 41 |
| CHECKERS TRUCK STOP INC | 1901 E RENO | 9 | 41 |
| Not reported | 927 E RENO | 10 | 47 |
| Not reported | 1100 E RENO | 12 | 52 |
| INTERSTATE METALS CORPORATION | 1101 E RENO | 12 | 54 |
| Not reported | 1600 E RENO | 15 | 63 |
| MID-AMERICA CHEMICAL, INC. | 1801 SKYLINE DRIVE | 17 | 65 |
| BEAVER EXPRESS LLC | 1515 SE 15TH | 19 | 72 |
| Not reported | 1515 SE 15TH STR | 19 | 74 |
| WAGGONERS TRUCKING THE | 1501 SE 15TH - SUITE A | 19 | 77 |
| CONSOLIDATED FREIGHTWAYS | 1400 SE SKYLINE DR | 19 | 83 |
| SAIA MOTOR FREIGHT LINES | 1715 S SKYLINE DRIVE | 19 | 88 |
| METRO MART | 1320 SE 15TH | 20 | 92 |
| SUNOCO #23 | 1400 SE 15TH | 20 | 96 |
| KERR MCGEE #121-6584 | 1301 SE 15TH | 20 | 98 |
| CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN | 24 | 102 |

EDR PROPRIETARY RECORDS

EDR US Hist Auto Stat: EDR has searched selected national collections of business directories and has collected listings of potential gas station/filling station/service station sites that were available to EDR researchers. EDR's review was limited to those categories of sources that might, in EDR's opinion, include gas station/filling station/service station establishments. The categories reviewed included, but were not limited to gas, gas station, gasoline station, filling station, auto, automobile repair, auto service station, service station, etc. This database falls within a category of information EDR classifies as "High Risk Historical Records", or HRHR. EDR's HRHR effort presents unique and sometimes proprietary data about past sites and operations that typically create environmental concerns, but may not show up in current government records searches.

A review of the EDR US Hist Auto Stat list, as provided by EDR, has revealed that there are 19 EDR US Hist Auto Stat sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--------------------------------|-------------------------|---------------|-------------|
| Not reported | 20 S MARTIN LUTHER KING | 7 | 26 |
| PETRO TRUCK FUEL STOP DIESEL S | 20 MARTIN LUTHER KING | 7 | 26 |
| WOOLF PEARL R FILL STA | 1900 RENO AVE E | 9 | 38 |
| SUNSHINE TRUCK STOP | 1903 RENO AVE E | 9 | 38 |
| KINGS SKELY SERVICE | 1935 RENO AVE E | 9 | 39 |
| TEXACO SERVICE STATION | 1935 RENO AVE E | 9 | 39 |
| BUTTONS TEXACO | 1901 RENO AVE E | 9 | 41 |
| PILOT TRAVEL CENTER NO GAS STA | 1901 E RENO AVE | 9 | 46 |
| SELF AUTO REPAIR | 1014 RENO AVE E | 11 | 47 |
| RENO GAS HOUSE FILL STA | 1308 RENO AVE E | 13 | 60 |

EXECUTIVE SUMMARY

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|--------------------------------|-----------------|---------------|-------------|
| SAXTON SERVICE STATION GAS STA | 1306 RENO AVE E | 13 | 60 |
| Not reported | 1301 E RENO AVE | 13 | 61 |
| ARMSTRONG TEXACO SERVICE | 1320 15TH ST SE | 20 | 91 |
| SUNOCO GAS STATIONS | 1400 SE 15TH ST | 20 | 94 |
| D X SERVICE STATION | 1400 15TH ST SE | 20 | 94 |
| EMBREE D X SERVICE | 1300 15TH ST SE | 20 | 95 |
| FUEL AT THE FLAG NO 50 CONOCO | 1300 15TH ST SE | 20 | 95 |
| CASEYS KERR MC GEE SERV STA | 1301 15TH ST SE | 20 | 95 |
| SUNOCO GAS ST | 1400 15TH ST SE | 20 | 98 |

EDR US Hist Cleaners: EDR has searched selected national collections of business directories and has collected listings of potential dry cleaner sites that were available to EDR researchers. EDR's review was limited to those categories of sources that might, in EDR's opinion, include dry cleaning establishments. The categories reviewed included, but were not limited to dry cleaners, cleaners, laundry, laundromat, cleaning/laundry, wash & dry etc. This database falls within a category of information EDR classifies as "High Risk Historical Records", or HRHR. EDR's HRHR effort presents unique and sometimes proprietary data about past sites and operations that typically create environmental concerns, but may not show up in current government records searches.

A review of the EDR US Hist Cleaners list, as provided by EDR, has revealed that there are 3 EDR US Hist Cleaners sites within the searched area.

| <u>Site</u> | <u>Address</u> | <u>Map ID</u> | <u>Page</u> |
|---------------------------|-----------------|---------------|-------------|
| ROSEBUD COLN | 1141 15TH SE | 21 | 100 |
| TOP 0 THE TOWN INDRY A OR | 1139 15TH SE | 21 | 101 |
| HASTY LAUNDRY COIN | 1212 15TH ST SE | 22 | 101 |

EXECUTIVE SUMMARY

Please refer to the end of the findings report for unmapped orphan sites due to poor or inadequate address information.

MAP FINDINGS SUMMARY

| <u>Database</u> | <u>Total Plotted</u> |
|-------------------------------|--------------------------|
| <u>FEDERAL RECORDS</u> | |
| NPL | 0 |
| Proposed NPL | 0 |
| Delisted NPL | 1 |
| NPL LIENS | 0 |
| CERCLIS | 2 |
| CERC-NFRAP | 4 |
| LIENS 2 | 0 |
| CORRACTS | 0 |
| RCRA-TSDF | 0 |
| RCRA-LQG | 0 |
| RCRA-SQG | 1 |
| RCRA-CESQG | 1 |
| RCRA NonGen / NLR | 2 |
| US ENG CONTROLS | 1 |
| US INST CONTROL | 1 |
| ERNS | 0 |
| HMIRS | 0 |
| DOT OPS | 0 |
| US CDL | 0 |
| US BROWNFIELDS | 1 |
| DOD | 0 |
| FUDS | 0 |
| LUCIS | 0 |
| CONSENT | 0 |
| ROD | 1 |
| UMTRA | 0 |
| ODI | 0 |
| DEBRIS REGION 9 | 0 |
| US MINES | 0 |
| TRIS | 0 |
| TSCA | 0 |
| FTTS | 0 |
| HIST FTTS | 0 |
| SSTS | 0 |
| ICIS | 0 |
| PADS | 0 |
| MLTS | 0 |
| RADINFO | 0 |
| FINDS | 6 |
| RAATS | 0 |
| RMP | 0 |
| LEAD SMELTERS | 0 |
| 2020 COR ACTION | 0 |
| PCB TRANSFORMER | 0 |
| US HIST CDL | 0 |
| FEDERAL FACILITY | 0 |
| COAL ASH EPA | 0 |
| FEMA UST | 0 |

MAP FINDINGS SUMMARY

| <u>Database</u> | <u>Total Plotted</u> |
|---|--------------------------|
| US FIN ASSUR | 0 |
| COAL ASH DOE | 0 |
| EPA WATCH LIST | 0 |
| SCRD DRYCLEANERS | 0 |
| PRP | 0 |
| US AIRS | 1 |
| <u>STATE AND LOCAL RECORDS</u> | |
| SHWS | 0 |
| SWF/LF | 0 |
| UIC | 0 |
| SWRCY | 2 |
| LUST | 18 |
| UST | 19 |
| HIST UST | 21 |
| LAST | 2 |
| AST | 3 |
| INST CONTROL | 0 |
| VCP | 1 |
| DRYCLEANERS | 0 |
| BROWNFIELDS | 1 |
| AIRS | 1 |
| TIER 2 | 3 |
| OK COMPLAINT | 1 |
| RGA LUST | 20 |
| RGA HWS | 0 |
| <u>TRIBAL RECORDS</u> | |
| INDIAN RESERV | 0 |
| INDIAN ODI | 0 |
| INDIAN LUST | 0 |
| INDIAN UST | 0 |
| INDIAN VCP | 0 |
| <u>EDR PROPRIETARY RECORDS</u> | |
| EDR MGP | 0 |
| EDR US Hist Auto Stat | 19 |
| EDR US Hist Cleaners | 3 |

NOTES:

Sites may be listed in more than one database

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

1 **FOURTH STREET ABANDONED REFINERY**
2200 BLOCK NE 4TH
OKLAHOMA CITY, OK

Delisted NPL **1000370122**
CERCLIS **OKD980696470**
US ENG CONTROLS
US INST CONTROL
ROD
FINDS

Delisted NPL:
 EPA ID: OKD980696470
 Site ID: 0601297
 EPA Region: 06
 Federal: No
 Deleted Date: 2008-08-21 00:00:00

Category Details:
 NPL Status: Currently on the Final NPL
 Category Description: Depth To Aquifer-> 10 And <= 25 Feet
 Category Value: 13

NPL Status: Currently on the Final NPL
 Category Description: Distance To Nearest Population-> 0 And <= 1/4 Mile
 Category Value: 600

Site Details:
 Site Name: FOURTH STREET ABANDONED REFINERY
 Site Status: Final
 Site Zip: 73117
 Site City: OKLAHOMA CITY
 Site State: OK
 Federal Site: No
 Site County: OKLAHOMA
 EPA Region: 06
 Date Proposed: 06/24/88
 Date Deleted: Not reported
 Date Finalized: 03/31/89

Substance Details:
 NPL Status: Currently on the Final NPL
 Substance ID: Not reported
 Substance: Not reported
 CAS #: Not reported
 Pathway: Not reported
 Scoring: Not reported

NPL Status: Currently on the Final NPL
 Substance ID: D005
 Substance: BARIUM
 CAS #: 7440-39-3
 Pathway: DIRECT CONTACT PATHWAY
 Scoring: 3

NPL Status: Currently on the Final NPL
 Substance ID: D005
 Substance: BARIUM
 CAS #: 7440-39-3
 Pathway: GROUND WATER PATHWAY
 Scoring: 3

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

NPL Status: Currently on the Final NPL
 Substance ID: D008
 Substance: LEAD (PB)
 CAS #: 7439-92-1
 Pathway: DIRECT CONTACT PATHWAY
 Scoring: 3

NPL Status: Currently on the Final NPL
 Substance ID: D008
 Substance: LEAD (PB)
 CAS #: 7439-92-1
 Pathway: GROUND WATER PATHWAY
 Scoring: 3

NPL Status: Currently on the Final NPL
 Substance ID: U036
 Substance: CHLORDANE
 CAS #: 57-74-9
 Pathway: NO PATHWAY INDICATED
 Scoring: 1

Summary Details:

Conditions at proposal June 24, 1988): The Fourth Street Abandoned Refinery Site occupies approximately 42 acres at 2200 4th Street in an industrial area in Oklahoma City, Oklahoma County, Oklahoma. The refinery operated from about 1940 to 1968. Tax records list Slayer Oil as a former owner, although several other businesses have occupied the site and may have contributed to the contamination. Numerous oil and sludge pits were in use while the refinery was in operation. There is recent evidence of dumping, including old concrete/building materials and what appears to be Government surplus supplies. An inactive gas/oil well is also on site. Soil samples collected by EPA in June 1985 and April 1986 contain barium and lead substantially above background levels. Numerous constituents of crude oil and chlordane were detected; several other pesticides may also be present. Two interconnected aquifers are present beneath the site. The upper one is associated with alluvial deposits of the North Canadian River and the lower one is associated with the Garber- Wellington Formation. The combined aquifers range from about 300 to 900 feet thick. Soil permeability is moderate to low. Ground water is shallow 15 feet in some cases), which facilitates movement of contaminants into ground water. About 32,500 people obtain drinking water from wells within 3 miles of the site, including municipal wells serving Del City and Spencer. The nearest surface water is the North Canadian River, 2,600 feet south of the site. Drainage at the site is to the south and the east along a ditch until run-off collects along the eastern perimeter road. The site is not completely fenced, making it possible for people and animals to come into direct contact with hazardous substances. Status March 31, 1989): EPA has evaluated the site and determined that emergency action is not warranted at this time.

Site Status Details:

NPL Status: Final
 Proposed Date: 06/24/1988
 Final Date: 03/31/1989
 Deleted Date: Not reported

Narratives Details:

NPL Name: FOURTH STREET ABANDONED REFINERY

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

City: OKLAHOMA CITY
 State: OK

CERCLIS:

Site ID: 0601297
 EPA ID: OKD980696470
 Facility County: OKLAHOMA
 Short Name: FOURTH STREET ABANDONED R
 Congressional District: 06
 IFMS ID: 06E5
 SMSA Number: 5880
 USGC Hydro Unit: 11100302
 Federal Facility: Not a Federal Facility
 DMNSN Number: 28.00000
 Site Orphan Flag: N
 RCRA ID: Not reported
 USGS Quadrangle: Not reported
 Site Init By Prog: Not reported
 NFRAP Flag: Not reported
 Parent ID: Not reported
 RST Code: Not reported
 EPA Region: 06
 Classification: Other
 Site Settings Code: SU
 NPL Status: Deleted from the Final NPL
 DMNSN Unit Code: ACRE
 RBRAC Code: Not reported
 RResp Fed Agency Code: Not reported
 Non NPL Status: Not reported
 Non NPL Status Date: / /
 Site Fips Code: 40109
 CC Concurrence Date: 09/27/96
 CC Concurrence FY: 1996
 Alias EPA ID: Not reported
 Site FUDS Flag: Not reported

CERCLIS Site Contact Name(s):

Contact ID: 6270181.00000
 Contact Name: Not reported
 Contact Tel: Not reported
 Contact Title: Remedial Project Manager (RPM)
 Contact Email: Not reported

CERCLIS Site Alias Name(s):

Alias ID: 101
 Alias Name: FOURTH STREET ABANDONED REFINERY
 Alias Address: Not reported
 OKLAHOMA, OK
 Alias ID: 102
 Alias Name: FOURTH STREET ABANDONED REFINERY
 Alias Address: 2200 BLOCK NE 4TH
 OKLAHOMA CITY, OK 73117
 Alias Comments: Not reported
 Site Description: The Fourth Street Refinery (FSR) is located in Northeast Oklahoma City, Oklahoma. The site collected, stored, and re-refined used oils and distributed

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

the recycled product. The refinery was active in the early 1940's and was noted on historical aerial photographs as early as 1941. Refining operations were conducted on land owned by the Chicago, Rock Island and Pacific Railway Company. Planet Oil and Refining Company participated in the waste oil reclamation business during the early part of the 1940's through the early 1960's. Elliot Refining Company conducted waste oil reclamation activities during the late 1940's through the 1960's. Salyer Refining Company performed waste oil reclamation operations from the late 1940's through the 1960's. These three companies conducted waste oil reclamation activities on-site. Operations ceased in the late 1960's or early 1970's. An adjacent site, Double Eagle Refinery (DER), shares similar past operations, contaminants and a common groundwater operable unit. A Record of Decision (ROD) was signed on April 19, 1994 for the DER Groundwater Operable Unit, and a similar ROD was signed on September 30, 1993 for the FSR Groundwater Operable Unit. Sulfates in the waste suggest the use of sulfuric acid in clarification of the used oils. This indicates that refinery operations at the FSR site apparently recycled used oils in a similar manner to that process employed at the DER site. Sludges generated by the reclamation process were disposed of in on-site impoundments. The DER and FSR sites were added to the National Priorities List (NPL) in March 1989. The contaminants found at these sites were primarily organic chemicals and heavy metals related to the refinery processes. The most commonly found pollutants were chlorinated hydrocarbons, and benzene compounds such as: xylene, ethylbenzene, trichloroethane, chrysene, phenanthrene, and naphthalene. Lead was the primary metal contaminant found in groundwater samples. Some or all of the contaminants identified are "hazardous substances" as defined in Section 104(14) of CERCLA, 42, U.S.C. Section 9601(14), and 40 C.F.R. Section 302.4. Approximately 43,000 cubic yards of contaminated soils and sediments were identified in DER and 47,000 cubic yards in FSR. An Explanation of Significant Differences (ESD) addressing the Fourth Street Refinery site was completed in January 2006.

CERCLIS Assessment History:

Action Code: 001
 Action: DISCOVERY
 Date Started: / /
 Date Completed: 07/01/80
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: SITE INSPECTION
 Date Started: 05/01/85
 Date Completed: 05/01/85
 Priority Level: Higher priority for further assessment
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: PRELIMINARY ASSESSMENT
 Date Started: 05/01/85
 Date Completed: 05/01/85
 Priority Level: Higher priority for further assessment
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: NATIONAL PRIORITIES LIST RESPONSIBLE PARTY SEARCH
 Date Started: 10/01/87
 Date Completed: 04/01/88
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: PROPOSAL TO NATIONAL PRIORITIES LIST
 Date Started: / /
 Date Completed: 06/24/88
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: HAZARD RANKING SYSTEM PACKAGE
 Date Started: / /
 Date Completed: 06/24/88
 Priority Level: Being considered for proposal to the NPL
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Action Code: 002
 Action: SITE INSPECTION
 Date Started: 11/01/86
 Date Completed: 09/01/88
 Priority Level: Higher priority for further assessment
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: FINAL LISTING ON NATIONAL PRIORITIES LIST
 Date Started: / /
 Date Completed: 03/31/89
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: REMOVAL ASSESSMENT
 Date Started: 09/05/89
 Date Completed: 09/05/89
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: REMOVAL
 Date Started: 09/05/89
 Date Completed: 09/27/89
 Priority Level: Stabilized
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Time Critical
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: REMEDIAL INVESTIGATION/FEASIBILITY STUDY NEGOTIATIONS
 Date Started: 10/06/89
 Date Completed: 10/06/89

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Primary
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: REMOVAL ASSESSMENT
 Date Started: 12/13/89
 Date Completed: 06/28/90
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: NATIONAL PRIORITIES LIST RESPONSIBLE PARTY SEARCH
 Date Started: 05/23/90
 Date Completed: 10/01/90
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 003
 Action: REMOVAL ASSESSMENT
 Date Started: 02/15/91
 Date Completed: 02/15/91
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: Public Notice Published
 Date Started: / /
 Date Completed: 09/03/91
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: RISK/HEALTH ASSESSMENT
 Date Started: / /
 Date Completed: 05/02/92
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: ECOLOGICAL RISK ASSESSMENT
 Date Started: / /
 Date Completed: 05/02/92
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: COMBINED REMEDIAL INVESTIGATION/FEASIBILITY STUDY
 Date Started: 12/29/89
 Date Completed: 09/28/92
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Primary
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: ADMINISTRATIVE RECORDS
 Date Started: 01/10/91
 Date Completed: 09/28/92
 Priority Level: Admin Record Compiled for a Removal Event
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: TREATABILITY STUDY
 Date Started: 09/25/91
 Date Completed: 09/28/92
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Primary
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: RECORD OF DECISION
 Date Started: / /
 Date Completed: 09/28/92
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 003
 Action: NATIONAL PRIORITIES LIST RESPONSIBLE PARTY SEARCH
 Date Started: 03/25/92
 Date Completed: 10/13/92
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: ISSUE REQUEST LETTERS (104E)
 Date Started: / /
 Date Completed: 12/22/92
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Action: REMEDIAL DESIGN/REMEDIAL ACTION NEGOTIATIONS
 Date Started: 03/12/93
 Date Completed: 06/10/93
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Primary
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: RISK/HEALTH ASSESSMENT
 Date Started: / /
 Date Completed: 07/15/93
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: ECOLOGICAL RISK ASSESSMENT
 Date Started: / /
 Date Completed: 07/15/93
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: COMBINED REMEDIAL INVESTIGATION/FEASIBILITY STUDY
 Date Started: 06/29/92
 Date Completed: 09/30/93
 Priority Level: Not reported
 Operable Unit: GROUNDWATER
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 004
 Action: REMOVAL ASSESSMENT
 Date Started: 09/30/93
 Date Completed: 09/30/93
 Priority Level: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: RECORD OF DECISION
 Date Started: / /
 Date Completed: 09/30/93
 Priority Level: Final Remedy Selected at Site
 Operable Unit: GROUNDWATER
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: REMEDIAL DESIGN/REMEDIAL ACTION NEGOTIATIONS
 Date Started: 01/20/94
 Date Completed: 03/28/94
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Primary
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: REMEDIAL DESIGN
 Date Started: 06/21/93
 Date Completed: 08/10/94
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Primary
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: REMEDIAL DESIGN
 Date Started: 03/28/94
 Date Completed: 03/17/95
 Priority Level: Not reported
 Operable Unit: GROUNDWATER
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Primary
 Urgency Indicator: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: REMEDIAL ACTION
 Date Started: 09/20/94
 Date Completed: 03/21/96
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Primary
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: PRELIMINARY CLOSE-OUT REPORT PREPARED
 Date Started: / /
 Date Completed: 09/27/96
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: REMEDIAL ACTION
 Date Started: 07/17/95
 Date Completed: 02/20/97
 Priority Level: Not reported
 Operable Unit: GROUNDWATER
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: TECHNICAL ASSISTANCE GRANT
 Date Started: 06/18/94
 Date Completed: 05/31/97
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: COMMUNITY INVOLVEMENT
 Date Started: 09/29/89

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 Database(s) EPA ID Number

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Date Completed: 12/01/99
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Primary
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: TECHNICAL ASSISTANCE GRANT
 Date Started: 06/11/91
 Date Completed: 02/29/00
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: FIVE-YEAR REVIEW
 Date Started: / /
 Date Completed: 10/18/00
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 003
 Action: TECHNICAL ASSISTANCE GRANT
 Date Started: 05/08/98
 Date Completed: 05/31/01
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: FIVE-YEAR REVIEW
 Date Started: 06/28/01
 Date Completed: 07/29/02
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: Explanation Of Significant Differences
 Date Started: / /
 Date Completed: 01/19/06
 Priority Level: Not reported
 Operable Unit: GROUNDWATER
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: CLOSE OUT REPORT
 Date Started: / /
 Date Completed: 03/07/06
 Priority Level: Not reported
 Operable Unit: GROUNDWATER
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: LONG TERM RESPONSE ACTION
 Date Started: 02/01/97
 Date Completed: 03/30/06
 Priority Level: Not reported
 Operable Unit: GROUNDWATER
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 003
 Action: FIVE-YEAR REVIEW
 Date Started: 10/17/06
 Date Completed: 05/15/07
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Action Code: 002
 Action: Explanation Of Significant Differences
 Date Started: / /
 Date Completed: 05/14/08
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: DELETION FROM NATIONAL PRIORITIES LIST
 Date Started: 10/02/07
 Date Completed: 08/21/08
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: STATE SUPPORT AGENCY COOPERATIVE AGREEMENT
 Date Started: 03/31/94
 Date Completed: 09/30/09
 Priority Level: Not reported
 Operable Unit: SOURCE CONTROL
 Primary Responsibility: State, Fund Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 004
 Action: FIVE-YEAR REVIEW
 Date Started: 09/20/11
 Date Completed: 05/15/12
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Federal Register Details:
 Fed Register Date: 08/21/08
 Fed Register Volume: 73
 Page Number: 49354

 Fed Register Date: 03/31/89

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Fed Register Volume: 54
 Page Number: 13296

Fed Register Date: 06/24/88
 Fed Register Volume: 53
 Page Number: 23988

[Click this hyperlink](#) while viewing on your computer to access
 460 additional US CERCLIS Financial: record(s) in the EDR Site Report.

US ENG CONTROLS:

EPA ID: OKD980696470
 Site ID: 0601297
 Name: FOURTH STREET ABANDONED REFINERY
 Address: 2200 BLOCK NE 4TH
 OKLAHOMA CITY, OK 73117
 EPA Region: 06
 County: OKLAHOMA
 Event Code: Not reported
 Actual Date: 11/9/2006

Action ID: 001
 Action Name: Explanation Of Significant Differences
 Action Completion date: 1/19/2006
 Operable Unit: 02
 Contaminated Media : Groundwater
 Engineering Control: No Further Action

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Sludge
 Engineering Control: Consolidate

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Sludge
 Engineering Control: Disposal

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Sludge
 Engineering Control: Dredging, Excavation, or Removal

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Sludge
 Engineering Control: Solidification/Stabilization (Ex-Situ)

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Soil
 Engineering Control: Consolidate

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Soil
 Engineering Control: Disposal

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Soil
 Engineering Control: Excavation

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Soil
 Engineering Control: Neutralization

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Soil
 Engineering Control: Solidification/Stabilization (Ex-Situ)

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Solid Waste
 Engineering Control: Consolidate

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Solid Waste
 Engineering Control: Disposal

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Solid Waste
 Engineering Control: Excavation

Action ID: 001

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Solid Waste
 Engineering Control: Neutralization

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Solid Waste
 Engineering Control: Solidification/Stabilization (Ex-Situ)

Action ID: 001
 Action Name: RECORD OF DECISION
 Action Completion date: 9/28/1992
 Operable Unit: 01
 Contaminated Media : Solid Waste
 Engineering Control: Solidification/Stabilization (In-Situ)

Action ID: 002
 Action Name: RECORD OF DECISION
 Action Completion date: 9/30/1993
 Operable Unit: 02
 Contaminated Media : Groundwater
 Engineering Control: Monitoring

Action ID: 002
 Action Name: RECORD OF DECISION
 Action Completion date: 9/30/1993
 Operable Unit: 02
 Contaminated Media : Groundwater
 Engineering Control: Natural Attenuation

US INST CONTROL:

EPA ID: OKD980696470
 Site ID: 0601297
 Name: FOURTH STREET ABANDONED REFINERY
 Action Name: Explanation Of Significant Differences
 Address: 2200 BLOCK NE 4TH
 OKLAHOMA CITY, OK 73117
 EPA Region: 06
 County: OKLAHOMA
 Event Code: Not reported
 Inst. Control: Deed Notices
 Actual Date: Not reported
 Complet. Date: 5/14/2008
 Operable Unit: 01
 Contaminated Media : Soil

ROD:

Full-text of USEPA Record of Decision(s) is available from EDR.

FINDS:

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

FOURTH STREET ABANDONED REFINERY (Continued)

1000370122

Registry ID: 110009343629

Environmental Interest/Information System

CERCLIS (Comprehensive Environmental Response, Compensation, and Liability Information System) is the Superfund database that is used to support management in all phases of the Superfund program. The system contains information on all aspects of hazardous waste sites, including an inventory of sites, planned and actual site activities, and financial information.

**2 HENLEY'S SEALANT/ZONOLITE WR GRACE
 200 WISCONSIN
 OKLAHOMA CITY, OK 73117**

**CERCLIS 1012209984
 OKN000607040**

CERCLIS:

Site ID: 0607040
 EPA ID: OKN000607040
 Facility County: OKLAHOMA
 Short Name: HENLEY'S SEALANT/ZONOLITE
 Congressional District: 05
 IFMS ID: A6R4
 SMSA Number: Not reported
 USGC Hydro Unit: Not reported
 Federal Facility: Not a Federal Facility
 DMNSN Number: 0.00000
 Site Orphan Flag: Not reported
 RCRA ID: Not reported
 USGS Quadrangle: Not reported
 Site Init By Prog: S
 NFRAP Flag: Not reported
 Parent ID: Not reported
 RST Code: Not reported
 EPA Region: 06
 Classification: Not reported
 Site Settings Code: Not reported
 NPL Status: Not on the NPL
 DMNSN Unit Code: Not reported
 RBRAC Code: Not reported
 RResp Fed Agency Code: Not reported
 Non NPL Status: Removal Only Site (No Site Assessment Work Needed)
 Non NPL Status Date: 10/04/11
 Site Fips Code: 40109
 CC Concurrence Date: / /
 CC Concurrence FY: Not reported
 Alias EPA ID: Not reported
 Site FUDS Flag: Not reported

CERCLIS Site Contact Name(s):

Contact ID: 13000872.00000
 Contact Name: Not reported
 Contact Tel: Not reported
 Contact Title: On-Scene Coordinator (OSC)
 Contact Email: Not reported

Contact ID: 13004317.00000
 Contact Name: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 Database(s) EPA ID Number

HENLEY'S SEALANT/ZONOLITE WR GRACE (Continued)

1012209984

Contact Tel: Not reported
 Contact Title: Community Involvement Coordinator
 Contact Email: Not reported

CERCLIS Site Alias Name(s):

Alias ID: 101
 Alias Name: HENLEY'S SEALANT
 Alias Address: 200 WISCONSIN
 OKLAHOMA CITY, OK 73117

Alias Comments: Not reported
 Site Description: This site was a vermiculite exfoliation plant that was operated by W. R. Grace.
 Libby vermiculite with amphibole asbestos.

CERCLIS Assessment History:

Action Code: 001
 Action: NON-NATIONAL PRIORITIES LIST POTENTIALLY RESPONSIBLE PARTY SEARCH
 Date Started: / /
 Date Completed: 01/20/10
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: SECTION 104(E) REF LITIGATION
 Date Started: / /
 Date Completed: 06/28/10
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: ISSUE REQUEST LETTERS (104E)
 Date Started: / /
 Date Completed: 06/28/10
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: Notice Letters Issued

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

HENLEY'S SEALANT/ZONOLITE WR GRACE (Continued)

1012209984

Date Started: / /
 Date Completed: 06/28/10
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 002
 Action: ISSUE REQUEST LETTERS (104E)
 Date Started: / /
 Date Completed: 05/05/11
 Priority Level: Not reported
 Operable Unit: SITEWIDE
 Primary Responsibility: Federal Enforcement
 Planning Status: Not reported
 Urgency Indicator: Not reported
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

Action Code: 001
 Action: REMOVAL
 Date Started: 03/26/10
 Date Completed: 07/29/11
 Priority Level: Cleaned up
 Operable Unit: SITEWIDE
 Primary Responsibility: EPA Fund-Financed
 Planning Status: Delayed
 Urgency Indicator: Time Critical
 Action Anomaly: Not reported

For detailed financial records, contact EDR for a Site Report.:

[Click this hyperlink](#) while viewing on your computer to access
 200 additional US CERCLIS Financial: record(s) in the EDR Site Report.

3

**OILS RECOVERY
 308 N. FONSHILL
 OKC, OK 73117**

**SWRCY S109997110
 N/A**

SWRCY:
 Telephone #: 405-943-8696
 Business Type: Not reported
 Materials Accepted: used oil, filters, antifreeze and waste water

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

**4 BRUCE RYAN NOMINEE
 100 SOUTH LOTTIE
 OKLAHOMA CITY, OK 73117**

**LUST U001884423
 HIST UST N/A
 RGA LUST**

LUST:
 Facility ID: 5503040
 Case Number: 064-2094
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 06/19/1998
Close Date: 08/18/1999
 Lat/Long: 35.465600000 / -97.489000000
Status: Closed

HIST UST:
 Facility ID: 5503040
 Owner Name: Bruce Ryan
 Owner Address: 100 S. Lottie
 Owner City,St,Zip: Oklahoma City, OK 73117
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 4/11/1971 0:00:00
 Tank Capacity: 6000
 Product: Gasoline

Facility ID: 5503040
 Owner Name: Bruce Ryan
 Owner Address: 100 S. Lottie
 Owner City,St,Zip: Oklahoma City, OK 73117
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: 4/11/1971 0:00:00
 Tank Capacity: 10000
 Product: Diesel

RGA LUST:
 2012 BRUCE RYAN NOMINEE 100 SOUTH LOTTIE
 2011 BRUCE RYAN NOMINEE 100 SOUTH LOTTIE
 2010 BRUCE RYAN NOMINEE 100 SOUTH LOTTIE
 2009 BRUCE RYAN NOMINEE 100 SOUTH LOTTIE

**5 W.R. GRACE & CO.
 201 N EASTERN
 OKLAHOMA CITY, OK 73111**

**UST U001884610
 HIST UST N/A**

UST:
 Facility ID: 5505436
 Contact Name: W R Grace & Co
 Contact Address: PO BOX 35608
 Contact City,St,Zip: Tulsa, OK 74153
 Lat/Long: 35.1364999 / -97.4773
 Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 6000
 Substance: Diesel

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

W.R. GRACE & CO. (Continued)

U001884610

Date Installed: 04/22/1974
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 1000
 Substance: Gasoline
 Date Installed: Not reported
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

HIST UST:

Facility ID: 5505436
 Owner Name: W R GRACE & CO
 Owner Address: 201 N EASTERN
 Owner City,St,Zip: Oklahoma City, OK 73111
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 4/22/1974 0:00:00
 Tank Capacity: 6000
 Product: Diesel

Facility ID: 5505436
 Owner Name: W R GRACE & CO
 Owner Address: 201 N EASTERN
 Owner City,St,Zip: Oklahoma City, OK 73111
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: Not reported
 Tank Capacity: 1000
 Product: Gasoline

6

**DERICHEBOURG RECYCLIN
 100 N BATH AVE
 OKLAHOMA CITY, OK**

**SWRCY S109997308
 N/A**

SWRCY:

Telephone #: (405) 239-7144
 Business Type: Hour 8 to 5 Mon. to Fri. Call for Saturday
 Materials Accepted: Auto body, ABC, AL, BR, CP, CS, MT

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site
 Database(s)
 EPA ID Number
 EDR ID Number

7 **20 S MARTIN LUTHER KING AVE** **EDR US Hist Auto Stat** **1015300752**
OKLAHOMA CITY, OK 73117 **N/A**

EDR Historical Auto Stations:
 Name: PETRO LUBE
 Year: 2008
 Address: 20 S MARTIN LUTHER KING AVE

 Name: LUBE BAY
 Year: 2010
 Address: 20 S MARTIN LUTHER KING AVE

7 **PETRO TRUCK FUEL STOP DIESEL STA** **EDR US Hist Auto Stat** **1014182212**
20 MARTIN LUTHER KING BLVD **N/A**
OKLAHOMA CITY S, OK 73117

EDR Historical Auto Stations:
 Name: PETRO TRUCK FUEL STOP DIESEL STA
 Year: 1993
 Type: GASOLINE STATIONS

7 **PETRO PSC PROPERTIES L.P.** **LUST** **U001884994**
20 S MARTIN LUTHER KING BLVD **UST** **N/A**
OKLAHOMA CITY, OK 73117 **HIST UST**
LAST
AST
TIER 2
RGA LUST

LUST:
 Facility ID: 5511197
 Case Number: SOR-3550
 Case Type: Suspicion of Release
Tank Type: UST
Release Date: 10/20/2009
Close Date: 12/08/2009
 Lat/Long: 35.466799999 / -97.47610000
Status: Closed

 Facility ID: 5511197
 Case Number: 064-3286
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 09/06/2007
Close Date: 02/29/2008
 Lat/Long: 35.466799999 / -97.47610000
Status: Closed

 Facility ID: 5511197
 Case Number: 064-3550
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 11/24/2009
Close Date: Not reported
 Lat/Long: 35.466799999 / -97.47610000
Status: Open

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

PETRO PSC PROPERTIES L.P. (Continued)

U001884994

UST:

Facility ID: 5511197
 Contact Name: HPT PSC Properties Trust
 Contact Address: 24601 Center Ridge Rd
 Contact City,St,Zip: Westlake, OH 44145
 Lat/Long: 35.4667999 / -97.476100

Tank ID: 1
 Tank Status: Currently In Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 03/30/1988
 Tank Type: UST
 Tank Material: Steel With Fiberglass
 Tank Construction: Secondarily Contained / Jacketed
 Pipe Material: Fiberglass

Tank ID: 2
 Tank Status: Currently In Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 03/30/1988
 Tank Type: UST
 Tank Material: Steel With Fiberglass
 Tank Construction: Secondarily Contained / Jacketed
 Pipe Material: Fiberglass

Tank ID: 3
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 03/30/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Secondarily Contained / Jacketed
 Pipe Material: Fiberglass

Tank ID: 4
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 03/30/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Secondarily Contained / Jacketed
 Pipe Material: Fiberglass

Tank ID: 5
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 03/30/1988
 Tank Type: UST

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

PETRO PSC PROPERTIES L.P. (Continued)

U001884994

Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Secondarily Contained / Jacketed
 Pipe Material: Fiberglass

Tank ID: 6
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Diesel
 Date Installed: 03/30/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Secondarily Contained / Jacketed
 Pipe Material: Fiberglass

Tank ID: 7
 Tank Status: Currently In Use
 Total Capacity: 8000
 Substance: Motor Oil
 Date Installed: 03/30/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Secondarily Contained / Jacketed
 Pipe Material: Fiberglass

Tank ID: 8
 Tank Status: Currently In Use
 Total Capacity: 8000
 Substance: Motor Oil
 Date Installed: 03/30/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Secondarily Contained / Jacketed
 Pipe Material: Fiberglass

Tank ID: 9
 Tank Status: Currently In Use
 Total Capacity: 8000
 Substance: Used Oil
 Date Installed: 03/30/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Secondarily Contained / Jacketed
 Pipe Material: Fiberglass

HIST UST:

Facility ID: 5511197
 Owner Name: PETRO STOPPING CENTERS LP
 Owner Address: 6080 SURETY DRIVE
 Owner City,St,Zip: El Paso, TX 79905
 Tank ID: 1
 Tank Status: Currently in Use
 Installed Date: 3/30/1988 0:00:00
 Tank Capacity: 20000

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

PETRO PSC PROPERTIES L.P. (Continued)

U001884994

Product: Diesel

Facility ID: 5511197
 Owner Name: PETRO STOPPING CENTERS LP
 Owner Address: 6080 SURETY DRIVE
 Owner City,St,Zip: El Paso, TX 79905
 Tank ID: 2
 Tank Status: Currently in Use
 Installed Date: 3/30/1988 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5511197
 Owner Name: PETRO STOPPING CENTERS LP
 Owner Address: 6080 SURETY DRIVE
 Owner City,St,Zip: El Paso, TX 79905
 Tank ID: 3
 Tank Status: Currently in Use
 Installed Date: 3/30/1988 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5511197
 Owner Name: PETRO STOPPING CENTERS LP
 Owner Address: 6080 SURETY DRIVE
 Owner City,St,Zip: El Paso, TX 79905
 Tank ID: 4
 Tank Status: Currently in Use
 Installed Date: 3/30/1988 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5511197
 Owner Name: PETRO STOPPING CENTERS LP
 Owner Address: 6080 SURETY DRIVE
 Owner City,St,Zip: El Paso, TX 79905
 Tank ID: 5
 Tank Status: Currently in Use
 Installed Date: 3/30/1988 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5511197
 Owner Name: PETRO STOPPING CENTERS LP
 Owner Address: 6080 SURETY DRIVE
 Owner City,St,Zip: El Paso, TX 79905
 Tank ID: 6
 Tank Status: Currently in Use
 Installed Date: 3/30/1988 0:00:00
 Tank Capacity: 10000
 Product: Diesel

Facility ID: 5511197
 Owner Name: PETRO STOPPING CENTERS LP
 Owner Address: 6080 SURETY DRIVE
 Owner City,St,Zip: El Paso, TX 79905
 Tank ID: 7

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

PETRO PSC PROPERTIES L.P. (Continued)

U001884994

Tank Status: Currently in Use
 Installed Date: 3/30/1988 0:00:00
 Tank Capacity: 8000
 Product: Other

Facility ID: 5511197
 Owner Name: PETRO STOPPING CENTERS LP
 Owner Address: 6080 SURETY DRIVE
 Owner City,St,Zip: El Paso, TX 79905
 Tank ID: 8
 Tank Status: Currently in Use
 Installed Date: 3/30/1988 0:00:00
 Tank Capacity: 8000
 Product: Other

Facility ID: 5511197
 Owner Name: PETRO STOPPING CENTERS LP
 Owner Address: 6080 SURETY DRIVE
 Owner City,St,Zip: El Paso, TX 79905
 Tank ID: 9
 Tank Status: Currently in Use
 Installed Date: 3/30/1988 0:00:00
 Tank Capacity: 8000
 Product: Used Oil

LAST:

Case Number: 064-3424
 Case Type: Confirmed Release
 Facility ID: 5511197
Status: Closed
 Lat/Long: 35.466799999 / -97.47610000
Tank Type: AST
Release Date: 09/28/2008
Close Date: 07/16/2009

AST:

Facility ID: 5511197
 Contact Name: HPT PSC Properties Trust
 Contact Address: 24601 Center Ridge Rd
 Contact City,St,Zip: Westlake, OH 44145
 Lat/Long: 35.4667999 / -97.476100

Tank ID: 11
 Tank Status: Currently In Use
 Total Capacity: 1000
 Substance: Motor Oil
 Install Date: 06/01/1996
 Tank Type: AST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Copper

Tank ID: 10
 Tank Status: Currently In Use
 Total Capacity: 350000

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

PETRO PSC PROPERTIES L.P. (Continued)

U001884994

Substance: Diesel
 Install Date: 05/01/1988
 Tank Type: AST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

OK TIER 2:

Facility ID: FATR20111FYFBW021KTZ
 Facility Country: USA
 All Chems. Same as Last Year: Not reported
 Date Tier 2 Signed: 1/31/2012
 Dike/Other Safeguards Employed: Not reported
 Facility Department: Not reported
 Facility Date Modified: 6/20/2012
 State Fees Total: 30
 Facility Fire District: Not reported
 Mailing Address: PO BOX 856
 Mailing City,St,Zip: SALINA, KS 67402
 Mailing Country: USA
 Latitude: 35.467719
 Longitude: -97.472293
 Lat/Long Location Description: Not reported
 Lat/Long Method: A1 - Address Matching (House Number)
 Number of Employees on Site: 37
 Notes: Not reported
 Validation Report: Not reported
 Reporting Year: 2011
 Site Coordinate Abbrvtions Submitted: Not reported
 State 1Require Contact: Not reported
 ID: 7542
 Facility Type: SIC
 Facility Description: Not reported
 Facility Last Modified: 2/11/2002
 ID: 070900105
 Facility Type: Dun & Bradstreet
 Facility Description: Not reported
 Facility Last Modified: 2/11/2002
 ID: 45
 Facility Type: Community Right to Know #
 Facility Description: Not reported
 Facility Last Modified: 2/11/2002
 ID: 811192
 Facility Type: NAICS
 Facility Description: Not reported
 Facility Last Modified: 1/16/2009
 Contact Record ID: CTRR20111FRHC00371XE
 Contact Name: Gen Mgr John Butler
 Contact Email: Not reported
 Contact Mail Address: 20 S. Martin Luther King
 Contact Mail City,St,Zip: Oklahoma City, OK 73117
 Contact Mail Country: USA
 Contact Type: Owner / Operator
 Contact Type: Emergency Contact
 Contact Modified Date: 3/20/2012
 Contact Record ID: CTRR20112TDVQV0016YA
 Contact Name: Environmental Compliance Mgr. Tina Dorf

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

PETRO PSC PROPERTIES L.P. (Continued)

U001884994

Contact Email: tinad@bluebeacon.com
 Contact Mail Address: PO Box 856
 Contact Mail City,St,Zip: Salina, KS 67402
 Contact Mail Country: USA
 Contact Type: Regulatory Point of Contact
 Contact Type: Parent Company
 Contact Modified Date: 2/8/2010
 Acute Health Risks: Not reported
 Average Daily Amount: Not reported
 Average Daily Amount Code: Not reported
 Chemical Inventory Record ID: Not reported
 Chemical Same As Last Year: Not reported
 Chronic Health Risks: Not reported
 CAS Number: Not reported
 EHS Substance: Not reported
 Last Modified: Not reported
 State Max Daily Amt Required: Not reported
 State Unit Required: Not reported
 Days on Site: Not reported
 Chemical Name: Not reported
 Fire Hazard: Not reported
 Gas: Not reported
 Liquid: Not reported
 Max Daily Amount: Not reported
 Max Daily Amount Code: Not reported
 Max Amount in Largest Container: Not reported
 Mixture Form: Not reported
 "Sudden Release of Pressure" Hazard: Not reported
 Pure Form: Not reported
 Reactive Hazard: Not reported
 Solid: Not reported
 State Contact Field: Not reported
 State Contact Comment: Not reported
 State EHS Comment: Not reported
 State Label Code: Not reported
 Max Daily Amount Required: Not reported
 State Max Per Container Required: Not reported
 State Req Heading: Not reported
 Trade Secret: Not reported
 Mixture Chemical: Not reported
 Mixture Percentage: Not reported
 Mixture CAS: Not reported
 Mixture EHS: Not reported
 Mixture Last Modified: Not reported
 Amount of Substance: Not reported
 Amount Units: Not reported
 Type of Storage: Not reported
 Number Code for Storage Pressure: Not reported
 Number Code for Storage Temperature: Not reported
 Last Modified: Not reported
 Location: Not reported

RGA LUST:

2012 PETRO PSC PROPERTIES L.P. 20 S MARTIN LUTHER KING BLVD
 2011 PETRO PSC PROPERTIES L.P. 20 S MARTIN LUTHER KING BLVD

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

PETRO PSC PROPERTIES L.P. (Continued)

U001884994

2010 PETRO PSC PROPERTIES L.P. 20 S MARTIN LUTHER KING BLVD
 2009 PETRO PSC PROPERTIES L.P. 20 S MARTIN LUTHER KING BLVD

8

**METRO TOWING
 1001 EAST RENO
 OKLAHOMA CITY, OK**

**VCP S109376759
 N/A**

VCP:

Status: Consultant: Search, Inc. | Phase II review
 Start Date: 09/13/1996
 Complete Date: Not reported
 Inactive Date: 10/01/1996
 Maco Date: Not reported
 Consent Order Date: Not reported
 Case Number: Not reported
 Issue Description: Media: soil | Hc
 Brownfields: Not reported
 Institutional Controls: Not reported
 Project Manager: George Thomas
 Latitude: 35.464433
 Longitude: -97.49427

Contact:

Sitename: METRO TOWING
 Location: 1001 East Reno
 Organization Contact Name: Not reported
 Organization Contact Email: Not reported
 Organization Contact Company: Not reported
 Organization Contact Phone: Not reported
 Organization Contact Ext: Not reported
 Organization Contact Street: Not reported
 Organization Contact City: Not reported
 Organization Contact State: Not reported
 Organization Contact Zip: Not reported
 Site Contact First Name: Not reported
 Site Contact Last Name: Not reported
 Site Contact Name: Not reported
 Site Contact Email: Not reported
 Site Contact Company: Not reported
 Site Contact Phone: Not reported
 Site Contact Ext: Not reported
 Site Contact Street: Not reported
 Site Contact City: Not reported
 Site Contact State: Not reported
 Site Contact Zip: Not reported
 Other Contact First Name: Not reported
 Other Contact Last Name: Not reported
 Other Contact Name: Not reported
 Other Contact Email: Not reported
 Other Contact Company: Not reported
 Other Contact Phone: Not reported
 Other Contact Ext: Not reported
 Other Contact Street: Not reported
 Other Contact City: Not reported
 Other Contact State: Not reported
 Other Contact Zip: Not reported

MAP FINDINGS

| | | | |
|----------------|------|-------------|---------------|
| Map ID | | | EDR ID Number |
| Direction | | | |
| Distance | | | |
| Distance (ft.) | Site | Database(s) | EPA ID Number |

| | | | |
|----------|---|-------------------|--|
| 8 | POND (E RENO AVE SITE) 1001 E RENO AVE OKLAHOMA CITY, OK 73117 | CERC-NFRAP | 1003873785 OKD980511208 |
|----------|---|-------------------|--|

CERC-NFRAP:

| | |
|-------------------|---|
| Site ID: | 0601228 |
| Federal Facility: | Not a Federal Facility |
| NPL Status: | Not on the NPL |
| Non NPL Status: | NFRAP-Site does not qualify for the NPL based on existing information |

CERCLIS-NFRAP Site Contact Details:

| | |
|----------------------|---------------|
| Contact Sequence ID: | 6424621.00000 |
| Person ID: | 6270175.00000 |

CERCLIS-NFRAP Assessment History:

| | |
|-----------------|---|
| Action: | PRELIMINARY ASSESSMENT |
| Date Started: | 06/01/80 |
| Date Completed: | 06/01/80 |
| Priority Level: | NFRAP-Site does not qualify for the NPL based on existing information |

| | |
|-----------------|--------------|
| Action: | ARCHIVE SITE |
| Date Started: | / / |
| Date Completed: | 06/01/80 |
| Priority Level: | Not reported |

| | |
|-----------------|--------------|
| Action: | DISCOVERY |
| Date Started: | / / |
| Date Completed: | 10/01/80 |
| Priority Level: | Not reported |

| | | | |
|----------|--|-------------------------------|---------------------------------|
| 8 | ELLSWORTH MTR FREIGHT LINES INC 1001 E RENO OKLAHOMA CITY, OK 73116 | UST HIST UST | U001884511 N/A |
|----------|--|-------------------------------|---------------------------------|

UST:

| | |
|----------------------|-------------------------------|
| Facility ID: | 5504091 |
| Contact Name: | Ellsworth Motor Freight Lines |
| Contact Address: | 2120 N 161 E. AVENUE |
| Contact City,St,Zip: | Tulsa, OK 74116 |
| Lat/Long: | 35.4662000 / -97.492500 |

| | |
|--------------------|------------------------|
| Tank ID: | 1 |
| Tank Status: | Permanently Out Of Use |
| Total Capacity: | 14000 |
| Substance: | Diesel |
| Date Installed: | 01/01/1982 |
| Tank Type: | UST |
| Tank Material: | Steel |
| Tank Construction: | Single Walled |
| Pipe Material: | Steel |

HIST UST:

| | |
|--------------------|-------------------------------|
| Facility ID: | 5504091 |
| Owner Name: | ELLSWORTH MOTOR FREIGHT LINES |
| Owner Address: | 2120 N 161 E. AVENUE |
| Owner City,St,Zip: | Tulsa, OK 74116 |
| Tank ID: | 1 |
| Tank Status: | Permanently Out of Use |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

ELLSWORTH MTR FREIGHT LINES INC (Continued)

U001884511

Installed Date: 1/1/1982 0:00:00
 Tank Capacity: 14000
 Product: Diesel

8

**BEACON WELL SERVICE INC
 1001 E RENO
 OKLAHOMA CITY, OK**

**RCRA NonGen / NLR 1000179770
 FINDS OKD052876299**

RCRA NonGen / NLR:

Date form received by agency: 06/30/1980
 Facility name: BEACON WELL SERVICE INC
 Facility address: 1001 E RENO
 OKLAHOMA CITY, OK 73117
 EPA ID: OKD052876299
 Mailing address: E RENO
 OKLAHOMA CITY, OK 73117
 Contact: GARY PURYEAR
 Contact address: 1001 E RENO
 OKLAHOMA CITY, OK 73117
 Contact country: US
 Contact telephone: (405) 324-8852
 Contact email: Not reported
 EPA Region: 06
 Classification: Non-Generator
 Description: Handler: Non-Generators do not presently generate hazardous waste

Owner/Operator Summary:

Owner/operator name: UNKNOWN
 Owner/operator address: UNKNOWN
 UNKNOWN, OK 00000
 Owner/operator country: Not reported
 Owner/operator telephone: (000) 000-0000
 Legal status: Private
 Owner/Operator Type: Owner
 Owner/Op start date: Not reported
 Owner/Op end date: Not reported

Handler Activities Summary:

U.S. importer of hazardous waste: No
 Mixed waste (haz. and radioactive): No
 Recycler of hazardous waste: No
 Transporter of hazardous waste: No
 Treater, storer or disposer of HW: No
 Underground injection activity: No
 On-site burner exemption: No
 Furnace exemption: No
 Used oil fuel burner: No
 Used oil processor: No
 User oil refiner: No
 Used oil fuel marketer to burner: No
 Used oil Specification marketer: No
 Used oil transfer facility: No
 Used oil transporter: No

Violation Status: No violations found

FINDS:

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

BEACON WELL SERVICE INC (Continued)

1000179770

Registry ID: 110004748934

Environmental Interest/Information System

RCRAInfo is a national information system that supports the Resource Conservation and Recovery Act (RCRA) program through the tracking of events and activities related to facilities that generate, transport, and treat, store, or dispose of hazardous waste. RCRAInfo allows RCRA program staff to track the notification, permit, compliance, and corrective action activities required under RCRA.

9

**GARY DALES 66
 1948 E RENO
 OKLAHOMA CITY, OK 73117**

**LUST U001884561
 UST N/A
 HIST UST
 RGA LUST**

LUST:

Facility ID: 5505096
 Case Number: 064-1334
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 02/09/1995
Close Date: 01/26/2000
 Lat/Long: 35.463799999 / -97.49429999
Status: Closed

UST:

Facility ID: 5505096
 Contact Name: Gary Dale Underwood
 Contact Address: 1948 E. Reno Avenue
 Contact City,St,Zip: Oklahoma City, OK 73117
 Lat/Long: 35.4637999 / -97.4942999

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 4000
 Substance: Gasoline
 Date Installed: 04/19/1971
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 04/19/1971
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 3
 Tank Status: Permanently Out Of Use

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

GARY DALES 66 (Continued)

U001884561

Total Capacity: 8000
 Substance: Gasoline
 Date Installed: 04/19/1971
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

HIST UST:

Facility ID: 5505096
 Owner Name: Gary Dale Underwood
 Owner Address: 1948 E. Reno Avenue
 Owner City,St,Zip: Oklahoma City, OK 73117
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 4/19/1971 0:00:00
 Tank Capacity: 4000
 Product: Gasoline

Facility ID: 5505096
 Owner Name: Gary Dale Underwood
 Owner Address: 1948 E. Reno Avenue
 Owner City,St,Zip: Oklahoma City, OK 73117
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: 4/19/1971 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5505096
 Owner Name: Gary Dale Underwood
 Owner Address: 1948 E. Reno Avenue
 Owner City,St,Zip: Oklahoma City, OK 73117
 Tank ID: 3
 Tank Status: Permanently Out of Use
 Installed Date: 4/19/1971 0:00:00
 Tank Capacity: 8000
 Product: Gasoline

RGA LUST:

| | | |
|------|---------------|-------------|
| 2012 | GARY DALES 66 | 1948 E RENO |
| 2011 | GARY DALES 66 | 1948 E RENO |
| 2010 | GARY DALES 66 | 1948 E RENO |
| 2009 | GARY DALES 66 | 1948 E RENO |

MAP FINDINGS

| | | | |
|----------------|------|-------------|---------------|
| Map ID | | | EDR ID Number |
| Direction | | | |
| Distance | | | |
| Distance (ft.) | Site | Database(s) | EPA ID Number |

| | | | |
|----------|---|------------------------------|---------------------------------|
| 9 | WOOLF PEARL R FILL STA 1900 RENO AVE E OKLAHOMA CITY, OK | EDR US Hist Auto Stat | 1014197376 N/A |
|----------|---|------------------------------|---------------------------------|

EDR Historical Auto Stations:

| | |
|-------|-----------------------------------|
| Name: | WOOLF PEARL R FILL STA |
| Year: | 1940 |
| Type: | GASOLINE AND OIL SERVICE STATIONS |

| | | | |
|----------|--|-------------------|--|
| 9 | EASTERN AVENUE SITE 1600' N OF RENO AVE & EASTERN AVE. INT. OKLAHOMA CITY, OK 73101 | CERC-NFRAP | 1003875425 OKD982311433 |
|----------|--|-------------------|--|

CERC-NFRAP:

| | |
|-------------------|---|
| Site ID: | 0603658 |
| Federal Facility: | Not a Federal Facility |
| NPL Status: | Not on the NPL |
| Non NPL Status: | NFRAP-Site does not qualify for the NPL based on existing information |

CERCLIS-NFRAP Site Contact Details:

| | |
|----------------------|---------------|
| Contact Sequence ID: | 6424111.00000 |
| Person ID: | 6270175.00000 |

CERCLIS-NFRAP Site Alias Name(s):

| | |
|----------------|--------------------------------|
| Alias Name: | UNIDENTIFIED EASTERN AVE. SITE |
| Alias Address: | Not reported |
| | OK |

CERCLIS-NFRAP Assessment History:

| | |
|-----------------|---|
| Action: | PRELIMINARY ASSESSMENT |
| Date Started: | 08/01/88 |
| Date Completed: | 08/01/88 |
| Priority Level: | NFRAP-Site does not qualify for the NPL based on existing information |

| | |
|-----------------|--------------|
| Action: | ARCHIVE SITE |
| Date Started: | / / |
| Date Completed: | 08/01/88 |
| Priority Level: | Not reported |

| | |
|-----------------|--------------|
| Action: | DISCOVERY |
| Date Started: | / / |
| Date Completed: | 11/01/87 |
| Priority Level: | Not reported |

| | | | |
|----------|--|------------------------------|---------------------------------|
| 9 | SUNSHINE TRUCK STOP 1903 RENO AVE E OKLAHOMA CITY S, OK 73104 | EDR US Hist Auto Stat | 1014196911 N/A |
|----------|--|------------------------------|---------------------------------|

EDR Historical Auto Stations:

| | |
|-------|---------------------|
| Name: | SUNSHINE TRUCK STOP |
| Year: | 1993 |
| Type: | GASOLINE STATIONS |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

JJ'S #5 (Continued)

U001230055

Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 3
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 02/22/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 4
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 02/22/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

HIST UST:

Facility ID: 5508258
 Owner Name: JACK MASTERS INC
 Owner Address: 2132 A WEST MAIN
 Owner City,St,Zip: Norman, OK 73070
 Tank ID: 1
 Tank Status: Currently in Use
 Installed Date: 2/22/1988 0:00:00
 Tank Capacity: 12000
 Product: Gasoline

Facility ID: 5508258
 Owner Name: JACK MASTERS INC
 Owner Address: 2132 A WEST MAIN
 Owner City,St,Zip: Norman, OK 73070
 Tank ID: 2
 Tank Status: Currently in Use
 Installed Date: 2/22/1988 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5508258
 Owner Name: JACK MASTERS INC
 Owner Address: 2132 A WEST MAIN
 Owner City,St,Zip: Norman, OK 73070
 Tank ID: 3
 Tank Status: Currently in Use
 Installed Date: 2/22/1988 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5508258

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

JJ'S #5 (Continued)

U001230055

Owner Name: JACK MASTERS INC
 Owner Address: 2132 A WEST MAIN
 Owner City,St,Zip: Norman, OK 73070
 Tank ID: 4
 Tank Status: Currently in Use
 Installed Date: 2/22/1988 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

RGA LUST:

| | | |
|------|---------|-------------|
| 2012 | JJ'S #5 | 1935 E RENO |
| 2011 | JJ'S #5 | 1935 E RENO |
| 2010 | JJ'S #5 | 1935 E RENO |
| 2009 | JJ'S #5 | 1935 E RENO |

9

**1901 E RENO AVE
 OKLAHOMA CITY, OK**

**RGA LUST S114359335
 N/A**

RGA LUST:

| | | |
|------|------------------------------|-----------------|
| 2008 | PILOT TRAVEL CENTER LLC #325 | 1901 E RENO AVE |
| 2007 | PILOT TRAVEL CENTER LLC #325 | 1901 E RENO AVE |
| 2006 | PILOT TRAVEL CENTER LLC #325 | 1901 E RENO AVE |
| 2005 | PILOT TRAVEL CENTER LLC #325 | 1901 E RENO AVE |

9

**BUTTONS TEXACO
 1901 RENO AVE E
 OKLAHOMA CITY, OK 73104**

**EDR US Hist Auto Stat 1014165552
 N/A**

EDR Historical Auto Stations:

| | |
|-------|---------------------------|
| Name: | BUTTONS TEXACO |
| Year: | 1975 |
| Type: | GASOLINE STATIONS |
| Name: | SUNSHINE FUEL STOP GAS ST |
| Year: | 1986 |
| Type: | GASOLINE STATIONS |

9

**CHECKERS TRUCK STOP INC
 1901 E RENO
 OKLAHOMA CITY, OK 73117**

**LUST U003181990
 UST N/A
 HIST UST
 RGA LUST**

LUST:

| | |
|-------------------|-------------------|
| Facility ID: | 5510187 |
| Case Number: | 064-0166 |
| Case Type: | Confirmed Release |
| Tank Type: | UST |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CHECKERS TRUCK STOP INC (Continued)

U003181990

Release Date: 04/12/1991
Close Date: 12/27/1999
 Lat/Long: 35.464300000 / -97.47880000
Status: Closed

Facility ID: 5510187
 Case Number: 064-1663
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 12/04/1990
Close Date: 12/27/1999
 Lat/Long: 35.464300000 / -97.47880000
Status: Closed

Facility ID: 5510187
 Case Number: 064-2823
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 03/29/2004
Close Date: 03/27/2006
 Lat/Long: 35.464300000 / -97.47880000
Status: Closed

Facility ID: 5510187
 Case Number: 064-E4
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 10/24/1988
Close Date: 03/01/1989
 Lat/Long: 35.464300000 / -97.47880000
Status: Closed

Facility ID: 5510187
 Case Number: 064-X3
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 08/08/1989
Close Date: 04/25/1991
 Lat/Long: 35.464300000 / -97.47880000
Status: Closed

Facility ID: 5510187
 Case Number: 6C-531
 Case Type: Suspicion of Release
Tank Type: UST
Release Date: 06/22/1993
Close Date: 10/06/1993
 Lat/Long: 35.464300000 / -97.47880000
Status: Closed

Facility ID: 5510187
 Case Number: SOR-2571
 Case Type: Suspicion of Release
Tank Type: UST
Release Date: 11/06/2001
Close Date: 06/03/2002
 Lat/Long: 35.464300000 / -97.47880000

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CHECKERS TRUCK STOP INC (Continued)

U003181990

Status: Closed

UST:

Facility ID: 5510187
 Contact Name: Checkers Truck Stop Inc
 Contact Address: 1901 E Reno Ave
 Contact City,St,Zip: Oklahoma City, OK 73117
 Lat/Long: 35.4643000 / -97.478800

Tank ID: 1
 Tank Status: Currently In Use
 Total Capacity: 12000
 Substance: Gasoline
 Date Installed: 05/06/1981
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 2
 Tank Status: Currently In Use
 Total Capacity: 12000
 Substance: Gasoline
 Date Installed: 05/06/1981
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 3
 Tank Status: Currently In Use
 Total Capacity: 12000
 Substance: Gasoline
 Date Installed: 05/06/1981
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 4
 Tank Status: Currently In Use
 Total Capacity: 12000
 Substance: Diesel
 Date Installed: 05/06/1981
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 5
 Tank Status: Currently In Use
 Total Capacity: 20000
 Substance: Diesel

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CHECKERS TRUCK STOP INC (Continued)

U003181990

Date Installed: 05/06/1981
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 6
 Tank Status: Currently In Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 05/06/1981
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 7
 Tank Status: Currently In Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 05/06/1981
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 8
 Tank Status: Currently In Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 05/06/1981
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 9
 Tank Status: Currently In Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 05/06/1981
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

HIST UST:

Facility ID: 5510187
 Owner Name: PILOT TRAVEL CENTERS LLC
 Owner Address: P O BOX 10146
 Owner City,St,Zip: Knoxville, TN 37939
 Tank ID: 1
 Tank Status: Currently in Use

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CHECKERS TRUCK STOP INC (Continued)

U003181990

Installed Date: 5/6/1981 0:00:00
 Tank Capacity: 12000
 Product: Gasoline

Facility ID: 5510187
 Owner Name: PILOT TRAVEL CENTERS LLC
 Owner Address: P O BOX 10146
 Owner City,St,Zip: Knoxville, TN 37939
 Tank ID: 2
 Tank Status: Currently in Use
 Installed Date: 5/6/1981 0:00:00
 Tank Capacity: 12000
 Product: Gasoline

Facility ID: 5510187
 Owner Name: PILOT TRAVEL CENTERS LLC
 Owner Address: P O BOX 10146
 Owner City,St,Zip: Knoxville, TN 37939
 Tank ID: 3
 Tank Status: Currently in Use
 Installed Date: 5/6/1981 0:00:00
 Tank Capacity: 12000
 Product: Gasoline

Facility ID: 5510187
 Owner Name: PILOT TRAVEL CENTERS LLC
 Owner Address: P O BOX 10146
 Owner City,St,Zip: Knoxville, TN 37939
 Tank ID: 4
 Tank Status: Currently in Use
 Installed Date: 5/6/1981 0:00:00
 Tank Capacity: 12000
 Product: Diesel

Facility ID: 5510187
 Owner Name: PILOT TRAVEL CENTERS LLC
 Owner Address: P O BOX 10146
 Owner City,St,Zip: Knoxville, TN 37939
 Tank ID: 5
 Tank Status: Currently in Use
 Installed Date: 5/6/1981 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5510187
 Owner Name: PILOT TRAVEL CENTERS LLC
 Owner Address: P O BOX 10146
 Owner City,St,Zip: Knoxville, TN 37939
 Tank ID: 6
 Tank Status: Currently in Use
 Installed Date: 5/6/1981 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5510187
 Owner Name: PILOT TRAVEL CENTERS LLC
 Owner Address: P O BOX 10146

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CHECKERS TRUCK STOP INC (Continued)

U003181990

Owner City,St,Zip: Knoxville, TN 37939
 Tank ID: 7
 Tank Status: Currently in Use
 Installed Date: 5/6/1981 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5510187
 Owner Name: PILOT TRAVEL CENTERS LLC
 Owner Address: P O BOX 10146
 Owner City,St,Zip: Knoxville, TN 37939
 Tank ID: 8
 Tank Status: Currently in Use
 Installed Date: 5/6/1981 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5510187
 Owner Name: PILOT TRAVEL CENTERS LLC
 Owner Address: P O BOX 10146
 Owner City,St,Zip: Knoxville, TN 37939
 Tank ID: 9
 Tank Status: Currently in Use
 Installed Date: 5/6/1981 0:00:00
 Tank Capacity: 20000
 Product: Diesel

RGALUST:

| | | |
|------|-----------------------|-------------|
| 2012 | JRS TRAVEL CENTER LLC | 1901 E RENO |
| 2011 | JRS TRAVEL CENTER LLC | 1901 E RENO |
| 2010 | JRS TRAVEL CENTER LLC | 1901 E RENO |
| 2009 | JRS TRAVEL CENTER LLC | 1901 E RENO |
| 1991 | CARDON CORP | 1901 E RENO |

9

**PILOT TRAVEL CENTER NO GAS STATIONS
 1901 E RENO AVE
 OKLAHOMA CITY, OK 73117**

**EDR US Hist Auto Stat 1014168084
 N/A**

EDR Historical Auto Stations:

Name: PILOT TRAVEL CENTER NO GAS STATIONS
 Year: 2000
 Type: GASOLINE STATIONS

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

10

**927 E RENO
 OKLAHOMA CITY, OK**

**LUST U001884248
 HIST UST N/A
 RGA LUST**

LUST:

Facility ID: 5500497
 Case Number: 064-UZ
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 11/19/1990
Close Date: 04/30/1992
 Lat/Long: 35.465299999 / -97.496399999
Status: Closed

HIST UST:

Facility ID: 5500497
 Owner Name: JONES TRUCK LINES INC
 Owner Address: 610 E EMMA AVE
 Owner City,St,Zip: Springdale, AR 72764
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 1/17/1967 0:00:00
 Tank Capacity: 2000
 Product: Diesel

Facility ID: 5500497
 Owner Name: JONES TRUCK LINES INC
 Owner Address: 610 E EMMA AVE
 Owner City,St,Zip: Springdale, AR 72764
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: 1/17/1967 0:00:00
 Tank Capacity: 4000
 Product: Diesel

RGA LUST:

| | | |
|------|-----------------------|------------|
| 2012 | JONES TRUCK LINES INC | 927 E RENO |
| 2011 | JONES TRUCK LINES INC | 927 E RENO |
| 2010 | JONES TRUCK LINES INC | 927 E RENO |
| 2009 | JONES TRUCK LINES INC | 927 E RENO |

11

**SELF AUTO REPAIR
 1014 RENO AVE E
 OKLAHOMA CITY, OK 73104**

**EDR US Hist Auto Stat 1014192108
 N/A**

EDR Historical Auto Stations:

Name: SELF AUTO REPAIR
 Year: 1975
 Type: AUTOMOBILE REPAIRING

MAP FINDINGS

| | | | |
|----------------|------|-------------|---------------|
| Map ID | | | EDR ID Number |
| Direction | | | |
| Distance | | | |
| Distance (ft.) | Site | Database(s) | EPA ID Number |

| | | | |
|----|---|---------------------------------------|---------------------------------|
| 11 | 1001 E. RENO PROPERTY 1001 E. RENO OKLAHOMA CITY, OK | US BROWNFIELDS FINDS | 1014905288 N/A |
|----|---|---------------------------------------|---------------------------------|

US BROWNFIELDS:

| | |
|-----------------------------|--|
| Recipient name: | Oklahoma City, City of |
| Grant type: | Assessment |
| Property name: | 1001 E. RENO PROPERTY |
| Property #: | Not reported |
| Parcel size: | 1.4 |
| Property Description: | <p>The Property is currently improved with one 5,680 square foot, single-story shop building constructed in 1956. The Property building is used to dismantle automobiles for parts. The remainder of the Property consists primarily of gravel parking areas used to store automobiles. The Property was developed with residences from at least 1936 until sometime between 1941 and 1946, when the Property was redeveloped for use as a natural gas pressure station. From at least 1966 until the mid-1970s, the Property was used for auto salvage. From approximately 1976 until 1981, the Property was listed as a well service company. From 1986 until the present, the Property has been occupied by Metro Towing, which is a wrecking company that performs auto salvage and repairs at the Property. The use of the Property as a salvage yard for the last 40 years was considered a Recognized Environmental Condition. Phase I Property observations considered RECs included stained soil/stressed vegetation throughout the Property, most notably the extreme northern corner approximately ten square feet and on the east side of the building. Other RECs included a septic system that was last pumped in 1990 and building interior spills associated with leaking drums. Review of Federal and State environmental databases revealed that the Property is listed on the Voluntary Cleanup Program VCP database due to a Phase II ESA performed in 1996. The database listing is considered a Historical REC. Review of historical sources Sanborn Maps and City Directories and Federal and State environmental databases indicated that the Property is located in a light industrial area. The Property is adjoined to the north by the Union Pacific Railroad line with a Dolese Batch Plant beyond, to the east by Interstate Metals, Inc., to the south by East Reno Avenue, and to the west by unimproved</p> |
| Latitude: | 35.464828 |
| Longitude: | -97.494096 |
| HCM label: | Address Matching-House Number |
| Map scale: | Not reported |
| Point of reference: | Entrance Point of a Facility or Station |
| Datum: | North American Datum of 1983 |
| ACRES property ID: | 134224 |
| Start date: | Not reported |
| Completed date: | Not reported |
| Acres cleaned up: | Not reported |
| Cleanup funding: | Not reported |
| Cleanup funding source: | Not reported |
| Assessment funding: | 15657 |
| Assessment funding source: | US EPA - Brownfields Assessment Cooperative Agreement |
| Redevelopment funding: | Not reported |
| Redev. funding source: | Not reported |
| Redev. funding entity name: | Not reported |
| Redevelopment start date: | Not reported |
| Assessment funding entity: | EPA |
| Cleanup funding entity: | Not reported |
| Grant type: | P |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

1001 E. RENO PROPERTY (Continued)

1014905288

Accomplishment type: Phase II Environmental Assessment
 Accomplishment count: 0
 Cooperative agreement #: 96636801
 Ownership entity: Not reported
 Current owner: Not reported
 Did owner change: Not reported
 Cleanup required: Unknown
 Video available: Not reported
 Photo available: Yes
 Institutional controls required: U
 IC Category proprietary controls: Not reported
 IC cat. info. devices: Not reported
 IC cat. gov. controls: Not reported
 IC cat. enforcement permit tools: Not reported
 IC in place date: Not reported
 IC in place: Not reported
 State/tribal program date: Not reported
 State/tribal program ID: Not reported
 State/tribal NFA date: Not reported
 Air contaminated: Not reported
 Air cleaned: Not reported
 Asbestos found: Y
 Asbestos cleaned: Not reported
 Controlled substance found: Not reported
 Controlled substance cleaned: Not reported
 Drinking water affected: Y
 Drinking water cleaned: Not reported
 Groundwater affected: Y
 Groundwater cleaned: Not reported
 Lead contaminant found: Y
 Lead cleaned up: Not reported
 No media affected: Not reported
 Unknown media affected: Not reported
 Other cleaned up: Not reported
 Other metals found: Y
 Other metals cleaned: Not reported
 Other contaminants found: Not reported
 Other contams found description: Not reported
 PAHs found: Y
 PAHs cleaned up: Not reported
 PCBs found: Y
 PCBs cleaned up: Not reported
 Petro products found: Y
 Petro products cleaned: Not reported
 Sediments found: Not reported
 Sediments cleaned: Not reported
 Soil affected: Y
 Soil cleaned up: Not reported
 Surface water cleaned: Not reported
 Unknown found: Not reported
 VOCs found: Y
 VOCs cleaned: Not reported
 Cleanup other description: Not reported
 Num. of cleanup and re-dev. jobs: Not reported
 Past use greenspace acreage: Not reported
 Past use residential acreage: Not reported
 Past use commercial acreage: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

1001 E. RENO PROPERTY (Continued)

1014905288

Past use industrial acreage: Not reported
 Future use greenspace acreage: Not reported
 Future use residential acreage: Not reported
 Future use commercial acreage: Not reported
 Future use industrial acreage: Not reported
 Greenspace acreage and type: Not reported
 Superfund Fed. landowner flag: Not reported

Recipient name: Oklahoma City, City of
 Grant type: Assessment
 Property name: 1001 E. RENO PROPERTY
 Property #: Not reported
 Parcel size: 1.4

Property Description: The Property is currently improved with one 5,680 square foot, single-story shop building constructed in 1956. The Property building is used to dismantle automobiles for parts. The remainder of the Property consists primarily of gravel parking areas used to store automobiles. The Property was developed with residences from at least 1936 until sometime between 1941 and 1946, when the Property was redeveloped for use as a natural gas pressure station. From at least 1966 until the mid-1970s, the Property was used for auto salvage. From approximately 1976 until 1981, the Property was listed as a well service company. From 1986 until the present, the Property has been occupied by Metro Towing, which is a wrecking company that performs auto salvage and repairs at the Property. The use of the Property as a salvage yard for the last 40 years was considered a Recognized Environmental Condition. Phase I Property observations considered RECs included stained soil/stressed vegetation throughout the Property, most notably the extreme northern corner approximately ten square feet and on the east side of the building. Other RECs included a septic system that was last pumped in 1990 and building interior spills associated with leaking drums. Review of Federal and State environmental databases revealed that the Property is listed on the Voluntary Cleanup Program VCP database due to a Phase II ESA performed in 1996. The database listing is considered a Historical REC. Review of historical sources Sanborn Maps and City Directories and Federal and State environmental databases indicated that the Property is located in a light industrial area. The Property is adjoined to the north by the Union Pacific Railroad line with a Dolese Batch Plant beyond, to the east by Interstate Metals, Inc., to the south by East Reno Avenue, and to the west by unimproved

Latitude: 35.464828
 Longitude: -97.494096
 HCM label: Address Matching-House Number
 Map scale: Not reported
 Point of reference: Entrance Point of a Facility or Station
 Datum: North American Datum of 1983
 ACRES property ID: 134224
 Start date: Not reported
 Completed date: Not reported
 Acres cleaned up: Not reported
 Cleanup funding: Not reported
 Cleanup funding source: Not reported
 Assessment funding: 22776
 Assessment funding source: US EPA - Brownfields Assessment Cooperative Agreement
 Redevelopment funding: Not reported
 Redev. funding source: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

1001 E. RENO PROPERTY (Continued)

1014905288

Redev. funding entity name: Not reported
 Redevelopment start date: Not reported
 Assessment funding entity: EPA
 Cleanup funding entity: Not reported
 Grant type: P
 Accomplishment type: Phase II Environmental Assessment
 Accomplishment count: 1
 Cooperative agreement #: 96695701
 Ownership entity: Not reported
 Current owner: Not reported
 Did owner change: Not reported
 Cleanup required: Unknown
 Video available: Not reported
 Photo available: Yes
 Institutional controls required: U
 IC Category proprietary controls: Not reported
 IC cat. info. devices: Not reported
 IC cat. gov. controls: Not reported
 IC cat. enforcement permit tools: Not reported
 IC in place date: Not reported
 IC in place: Not reported
 State/tribal program date: Not reported
 State/tribal program ID: Not reported
 State/tribal NFA date: Not reported
 Air contaminated: Not reported
 Air cleaned: Not reported
 Asbestos found: Y
 Asbestos cleaned: Not reported
 Controled substance found: Not reported
 Controled substance cleaned: Not reported
 Drinking water affected: Y
 Drinking water cleaned: Not reported
 Groundwater affected: Y
 Groundwater cleaned: Not reported
 Lead contaminant found: Y
 Lead cleaned up: Not reported
 No media affected: Not reported
 Unknown media affected: Not reported
 Other cleaned up: Not reported
 Other metals found: Y
 Other metals cleaned: Not reported
 Other contaminants found: Not reported
 Other contams found description: Not reported
 PAHs found: Y
 PAHs cleaned up: Not reported
 PCBs found: Y
 PCBs cleaned up: Not reported
 Petro products found: Y
 Petro products cleaned: Not reported
 Sediments found: Not reported
 Sediments cleaned: Not reported
 Soil affected: Y
 Soil cleaned up: Not reported
 Surface water cleaned: Not reported
 Unknown found: Not reported
 VOCs found: Y
 VOCs cleaned: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

1001 E. RENO PROPERTY (Continued)

1014905288

Cleanup other description: Not reported
 Num. of cleanup and re-dev. jobs: Not reported
 Past use greenspace acreage: Not reported
 Past use residential acreage: Not reported
 Past use commercial acreage: Not reported
 Past use industrial acreage: Not reported
 Future use greenspace acreage: Not reported
 Future use residential acreage: Not reported
 Future use commercial acreage: Not reported
 Future use industrial acreage: Not reported
 Greenspace acreage and type: Not reported
 Superfund Fed. landowner flag: Not reported

FINDS:

Registry ID: 110044215676

Environmental Interest/Information System
 US EPA Assessment, Cleanup and Redevelopment Exchange System (ACRES)
 is an federal online database for Brownfields Grantees to
 electronically submit data directly to EPA.

12

**1100 E RENO
 OKLAHOMA CITY, OK**

**LUST U001884327
 HIST UST N/A
 RGA LUST**

LUST:

Facility ID: 5501515
 Case Number: 064-1347
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 02/16/1995
Close Date: 01/23/1996
 Lat/Long: 35.464799999 / -97.4602
Status: Closed

HIST UST:

Facility ID: 5501515
 Owner Name: OKLA DEPT OF TRANSPORTATION
 Owner Address: PO BOX 471
 Owner City,St,Zip: Perry, OK 73077
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 4/2/1982 0:00:00
 Tank Capacity: 1000
 Product: Diesel

Facility ID: 5501515
 Owner Name: OKLA DEPT OF TRANSPORTATION
 Owner Address: PO BOX 471
 Owner City,St,Zip: Perry, OK 73077
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: 4/2/1982 0:00:00
 Tank Capacity: 500
 Product: Gasoline

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

(Continued)

U001884327

RGA LUST:

| | | |
|------|---------------------------------|-------------|
| 2012 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2011 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2010 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2009 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2008 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2007 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2006 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2005 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2003 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2002 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2001 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 2000 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 1997 | OKLAHOMA DEPARTMENT OF TRANSPOR | 1100 E RENO |
| 1996 | OKLA DEPT OF TRANSPORTATION | 1100 E RENO |
| 1995 | OKLA DEPT OF TRANSPORTATION | 1100 E RENO |

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OKLAHOMA DEPARTMENT OF TRANSPOR
1100 E RENO
OKLAHOMA CITY, OK 73117

UST U004133129
N/A

UST:

Facility ID: 5501515
 Contact Name: Okla Dept Of Transportation
 Contact Address: DIVISION 4PO BOX 471
 Contact City,St,Zip: Perry, OK 73077
 Lat/Long: 35.4647999 / -97.4602

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 1000
 Substance: Diesel
 Date Installed: 04/02/1982
 Tank Type: UST

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 500
 Substance: Gasoline
 Date Installed: 04/02/1982
 Tank Type: UST

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

12 INTERSTATE METALS CORPORATION
 1101 E RENO
 OKLAHOMA CITY, OK 73117

LUST U001884535
 UST N/A
 HIST UST
 AIRS
 RGA LUST

LUST:

Facility ID: 5504605
 Case Number: 064-2008
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 01/06/1998
Close Date: 10/10/2000
 Lat/Long: 35.464599999 / -97.49250000
Status: Closed

UST:

Facility ID: 5504605
 Contact Name: Interstate Metals Corporation
 Contact Address: 1101 E. Reno
 Contact City,St,Zip: Oklahoma City, OK 73124
 Lat/Long: 35.4645999 / -97.492500

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 1000
 Substance: Diesel
 Date Installed: 04/16/1966
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 1000
 Substance: Gasoline
 Date Installed: 04/16/1966
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

HIST UST:

Facility ID: 5504605
 Owner Name: Interstate Metals Corporation
 Owner Address: 1101 E. Reno
 Owner City,St,Zip: Oklahoma City, OK 73124
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 4/16/1966 0:00:00
 Tank Capacity: 1000
 Product: Diesel

Facility ID: 5504605
 Owner Name: Interstate Metals Corporation
 Owner Address: 1101 E. Reno

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

INTERSTATE METALS CORPORATION (Continued)

U001884535

Owner City,St,Zip: Oklahoma City, OK 73124
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: 4/16/1966 0:00:00
 Tank Capacity: 1000
 Product: Gasoline

AIRS:

AIRS ID: 109-00905
 Company: INTERSTATE METALS CORP
 Latitude: 35.46450
 Longitude: -97.49755
 Operating Status: Operating
 NAICS: 423930
 SIC Code: 5093
 Permit Number: 95-510-O
 Issue Date: 02/08/1996
 Contact First Name: MICHAEL
 Contact Last Name: GALOOB
 Contact Phone: (405) 235-2424

RGA LUST:

| | | |
|------|-------------------------------|-------------|
| 2012 | INTERSTATE METALS CORPORATION | 1101 E RENO |
| 2011 | INTERSTATE METALS CORPORATION | 1101 E RENO |
| 2010 | INTERSTATE METALS CORPORATION | 1101 E RENO |
| 2009 | INTERSTATE METALS CORPORATION | 1101 E RENO |

12

**INTERSTATE METALS C/1101 E RENO ALUMINUM
 1101 E RENO
 OKLAHOMA CITY, OK**

**RCRA-CESQG 1000834461
 FINDS OKD033030545
 US AIRS**

RCRA-CESQG:

Date form received by agency: 08/26/2010
 Facility name: INTEGRITY METALS
 Facility address: 1101 E RENO
 OKLAHOMA CITY, OK 73124
 EPA ID: OKD033030545
 Mailing address: PO BOX 24063
 OKLAHOMA CITY, OK 731240063
 Contact: MICHAEL GALOOB
 Contact address: PO BOX 24063
 OKLAHOMA CITY, OK 731240063
 Contact country: US
 Contact telephone: (405) 235-2424
 Contact email: Not reported
 EPA Region: 06
 Land type: Private
 Classification: Conditionally Exempt Small Quantity Generator
 Description: Handler: generates 100 kg or less of hazardous waste per calendar month, and accumulates 1000 kg or less of hazardous waste at any time; or generates 1 kg or less of acutely hazardous waste per calendar month, and accumulates at any time: 1 kg or less of acutely hazardous waste; or 100 kg or less of any residue or contaminated soil, waste or other debris resulting from the cleanup of a spill, into or on any

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

INTERSTATE METALS C/1101 E RENO ALUMINUM (Continued)

1000834461

land or water, of acutely hazardous waste; or generates 100 kg or less of any residue or contaminated soil, waste or other debris resulting from the cleanup of a spill, into or on any land or water, of acutely hazardous waste during any calendar month, and accumulates at any time: 1 kg or less of acutely hazardous waste; or 100 kg or less of any residue or contaminated soil, waste or other debris resulting from the cleanup of a spill, into or on any land or water, of acutely hazardous waste

Owner/Operator Summary:

Owner/operator name: INTERSTATE METALS INC
 Owner/operator address: PO BOX 24063
 OKLAHOMA CITY, OK 73124
 Owner/operator country: Not reported
 Owner/operator telephone: (405) 235-2424
 Legal status: Private
 Owner/Operator Type: Owner
 Owner/Op start date: 01/01/0001
 Owner/Op end date: Not reported

Handler Activities Summary:

U.S. importer of hazardous waste: No
 Mixed waste (haz. and radioactive): No
 Recycler of hazardous waste: No
 Transporter of hazardous waste: No
 Treater, storer or disposer of HW: No
 Underground injection activity: No
 On-site burner exemption: No
 Furnace exemption: No
 Used oil fuel burner: No
 Used oil processor: No
 User oil refiner: No
 Used oil fuel marketer to burner: No
 Used oil Specification marketer: No
 Used oil transfer facility: No
 Used oil transporter: No

Historical Generators:

Date form received by agency: 04/08/1996
 Facility name: INTEGRITY METALS
 Site name: INTERSTATE METALS CORP
 Classification: Conditionally Exempt Small Quantity Generator

Hazardous Waste Summary:

Waste code: D008
 Waste name: LEAD

 Waste code: D008
 Waste name: LEAD

Facility Has Received Notices of Violations:

Regulation violated: Not reported
 Area of violation: Generators - General
 Date violation determined: 10/02/1992
 Date achieved compliance: 03/29/1998

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

INTERSTATE METALS C/1101 E RENO ALUMINUM (Continued)

1000834461

Violation lead agency: State
 Enforcement action: FINAL CIVIL JUDICIAL ACTION FOR COMPLIANCE AND/OR MONETARY PENALTY
 Enforcement action date: 03/29/1996
 Enf. disposition status: Not reported
 Enf. disp. status date: Not reported
 Enforcement lead agency: State
 Proposed penalty amount: Not reported
 Final penalty amount: 8000
 Paid penalty amount: Not reported

Regulation violated: Not reported
 Area of violation: Generators - General
 Date violation determined: 10/02/1992
 Date achieved compliance: 03/29/1998
 Violation lead agency: State
 Enforcement action: Not reported
 Enforcement action date: 01/25/1993
 Enf. disposition status: Not reported
 Enf. disp. status date: Not reported
 Enforcement lead agency: State
 Proposed penalty amount: 68000
 Final penalty amount: Not reported
 Paid penalty amount: Not reported

Regulation violated: Not reported
 Area of violation: Generators - General
 Date violation determined: 10/02/1992
 Date achieved compliance: 03/29/1998
 Violation lead agency: State
 Enforcement action: INITIAL 3008(A) COMPLIANCE
 Enforcement action date: 12/07/1992
 Enf. disposition status: Not reported
 Enf. disp. status date: Not reported
 Enforcement lead agency: State
 Proposed penalty amount: 68000
 Final penalty amount: Not reported
 Paid penalty amount: Not reported

Evaluation Action Summary:
 Evaluation date: 03/06/2013
 Evaluation: FOCUSED COMPLIANCE INSPECTION
 Area of violation: Not reported
 Date achieved compliance: Not reported
 Evaluation lead agency: State

Evaluation date: 03/29/1998
 Evaluation: NOT A SIGNIFICANT NON-COMPLIER
 Area of violation: Not reported
 Date achieved compliance: Not reported
 Evaluation lead agency: State

Evaluation date: 10/02/1992
 Evaluation: COMPLIANCE EVALUATION INSPECTION ON-SITE
 Area of violation: Generators - General
 Date achieved compliance: 03/29/1998
 Evaluation lead agency: State

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

INTERSTATE METALS C/1101 E RENO ALUMINUM (Continued)

1000834461

Evaluation date: 10/02/1992
 Evaluation: SIGNIFICANT NON-COMPLIER
 Area of violation: Not reported
 Date achieved compliance: Not reported
 Evaluation lead agency: State

FINDS:

Registry ID: 110004747409

Environmental Interest/Information System

AFS (Aerometric Information Retrieval System (AIRS) Facility Subsystem) replaces the former Compliance Data System (CDS), the National Emission Data System (NEDS), and the Storage and Retrieval of Aerometric Data (SAROAD). AIRS is the national repository for information concerning airborne pollution in the United States. AFS is used to track emissions and compliance data from industrial plants. AFS data are utilized by states to prepare State Implementation Plans to comply with regulatory programs and by EPA as an input for the estimation of total national emissions. AFS is undergoing a major redesign to support facility operating permits required under Title V of the Clean Air Act.

RCRAInfo is a national information system that supports the Resource Conservation and Recovery Act (RCRA) program through the tracking of events and activities related to facilities that generate, transport, and treat, store, or dispose of hazardous waste. RCRAInfo allows RCRA program staff to track the notification, permit, compliance, and corrective action activities required under RCRA.

AIRS (AFS):

Airs Minor Details:

EPA plant ID: 110004747409
 Plant name: INTERSTATE METALS C/1101 E RENO ALUMINUM
 Plant address: 1101 E RENO
 OKLAHOMA CITY, OK 73117
 County: OKLAHOMA
 Region code: 06
 Dunn & Bradst #: 033030545
 Air quality cntrl region: 184
 Sic code: 5093
 Sic code desc: SCRAP AND WASTE MATERIALS
 North Am. industrial classf: 423930
 NAIC code description: Recyclable Material Merchant Wholesalers
 Default compliance status: IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS
 Default classification: POTENTIAL UNCONTROLLED EMISSIONS < 100 TONS/YEAR
 Govt facility: ALL OTHER FACILITIES NOT OWNED OR OPERATED BY A FEDERAL, STATE, OR LOCAL GOVERNMENT
 Current HPV: Not reported

Historical Compliance Minor Sources:

State compliance status: IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS
 Hist compliance date: 1004

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

INTERSTATE METALS C/1101 E RENO ALUMINUM (Continued)

1000834461

| | |
|--------------------------|--|
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1101 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1102 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1103 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1104 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1201 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1202 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1203 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1204 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1301 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1302 |
| Air prog code hist file: | SIP SOURCE |
| State compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Hist compliance date: | 1303 |
| Air prog code hist file: | SIP SOURCE |

Compliance & Violation Data by Minor Sources:

| | |
|-----------------------------------|--|
| Air program code: | SIP SOURCE |
| Plant air program pollutant: | TOTAL PARTICULATE MATTER |
| Default pollutant classification: | POTENTIAL UNCONTROLLED EMISSIONS < 100 TONS/YEAR |
| Def. poll. compliance status: | IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS |
| Def. attainment/non atnmnt: | ATTAINMENT AREA FOR GIVEN POLLUTANT |
| Repeat violator date: | Not reported |
| Turnover compliance: | Not reported |
| Air program code: | SIP SOURCE |
| Plant air program pollutant: | Not reported |

MAP FINDINGS

| | | | |
|--------------------|--|-------------|---------------|
| Map ID | | | EDR ID Number |
| Direction | | | |
| Distance | | | |
| Distance (ft.)Site | | Database(s) | EPA ID Number |

INTERSTATE METALS C/1101 E RENO ALUMINUM (Continued)

1000834461

Default pollutant classification: POTENTIAL UNCONTROLLED EMISSIONS < 100 TONS/YEAR
 Def. poll. compliance status: IN COMPLIANCE WITH PROCEDURAL REQUIREMENTS
 Def. attainment/non atnmnt: ATTAINMENT AREA FOR GIVEN POLLUTANT
 Repeat violator date: Not reported
 Turnover compliance: Not reported

| | | | |
|----|--|-----------------------|-------------------|
| 13 | RENO GAS HOUSE FILL STA 1308 RENO AVE E OKLAHOMA CITY, OK | EDR US Hist Auto Stat | 1014191725 N/A |
|----|--|-----------------------|-------------------|

EDR Historical Auto Stations:

Name: RENO GAS HOUSE FILL STA
 Year: 1940
 Type: GASOLINE AND OIL SERVICE STATIONS

| | | | |
|----|---|-----------------------|-------------------|
| 13 | SAXTON SERVICE STATION GAS STA 1306 RENO AVE E OKLAHOMA CITY, OK | EDR US Hist Auto Stat | 1014191930 N/A |
|----|---|-----------------------|-------------------|

EDR Historical Auto Stations:

Name: SAXTON SERVICE STATION GAS STA
 Year: 1965
 Type: GASOLINE STATIONS

| | | | |
|----|--|-----|-------------------|
| 13 | FORMER ICX 1315 E RENO AVENUE OKLAHOMA CITY, OK 73117 | UST | U004132696 N/A |
|----|--|-----|-------------------|

UST:

Facility ID: 5520961
 Contact Name: Standard Iron & Metal Company Inc.
 Contact Address: PO Box 302
 Contact City,St,Zip: Oklahoma City, OK 73101
 Lat/Long: 35.4643999 / -97.488100

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 0
 Substance: Diesel
 Date Installed: Not reported
 Tank Type: UST

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 0
 Substance: Diesel
 Date Installed: Not reported
 Tank Type: UST

Tank ID: 3
 Tank Status: Permanently Out Of Use
 Total Capacity: 0
 Substance: Gasoline
 Date Installed: Not reported
 Tank Type: UST

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 Database(s)
 EPA ID Number

13 **FORMER ICX** **HIST UST** **U003859604**
1315 E RENO AVENUE **N/A**
OKLAHOMA CITY, OK 73117

HIST UST:

Facility ID: 9919638
 Owner Name: Standard Iron & Metal company Inc.
 Owner Address: P.O .Box 302
 Owner City,St,Zip: Oklahoma City, OK 73101
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: Not reported
 Tank Capacity: Not reported
 Product: Diesel

Facility ID: 9919638
 Owner Name: Standard Iron & Metal company Inc.
 Owner Address: P.O .Box 302
 Owner City,St,Zip: Oklahoma City, OK 73101
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: Not reported
 Tank Capacity: Not reported
 Product: Diesel

Facility ID: 9919638
 Owner Name: Standard Iron & Metal company Inc.
 Owner Address: P.O .Box 302
 Owner City,St,Zip: Oklahoma City, OK 73101
 Tank ID: 3
 Tank Status: Permanently Out of Use
 Installed Date: Not reported
 Tank Capacity: Not reported
 Product: Gasoline

13 **1301 E RENO AVE** **EDR US Hist Auto Stat** **1015202040**
OKLAHOMA CITY, OK 73117 **N/A**

EDR Historical Auto Stations:

Name: AUTO ENGINE INSTALLATION
 Year: 2010
 Address: 1301 E RENO AVE

Name: AUTO ENGINE EXCHANGE INC
 Year: 2011
 Address: 1301 E RENO AVE

14 **OKLAHOMA CITY URBAN RENEWAL AUTHORITY PHASE IA** **BROWNFIELDS** **S107998256**
SOUTH RENO AVE. & BYERS AVE. **N/A**
OKLAHOMA CITY, OK 73102

BROWNFIELDS:

Region: 1
 Property Id: 7128
 Overall Bfld Status: Active
 Current Activity: AssessInProg
 Site Name Alias: OCURA Phase IA; E. Reno Phillips 66; Red Rock Petr

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

OKLAHOMA CITY URBAN RENEWAL AUTHORITY PHASE IA (Continued)

S107998256

Zoning: Not reported
 Assessment Type: Not reported
 Assessment Contaminant: Not reported
 Brownfield Area Designation Date: Not reported
 Brownfield Site Designation Date: Not reported
 Remediation(CleanUp) Activity Status: Not reported
 Remediation(CleanUp) Activity Type: Not reported
 Type of Assistance Req or Req: Not reported
 Redevelopment Status: Not reported
 Leak Stakeholder: Owner
 Acres Total: 0
 Site Use Of Env Interest: Not reported
 Sitecond Storage Tanks: Not reported
 Cleanup Start Date: Not reported
 Program Comment: Not reported
 For Sale Or Lease: Not reported
 Ownership Type: Municipally
 Ownership/lease Status: Not reported
 Location Description: Not reported
 General Narrative: Not reported
 Assessment Status: In Progress
 Lat/Long: Not reported

15

**YELLOW FREIGHT SYSTEM, INC.
 1600 E RENO
 OKLAHOMA CITY, OK 73117**

**UST U004133019
 N/A**

UST:

Facility ID: 5507078
 Contact Name: Yellow Transportation Inc
 Contact Address: ATTN ENVIRONMENTAL SERVICES 10990 ROE AVE MS A605
 Contact City,St,Zip: Overland Park, KS 66211
 Lat/Long: 35.4636 / -97.481800

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Diesel
 Date Installed: Not reported
 Tank Type: UST

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Diesel
 Date Installed: Not reported
 Tank Type: UST

Tank ID: 3
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Diesel
 Date Installed: Not reported
 Tank Type: UST

Tank ID: 4
 Tank Status: Permanently Out Of Use
 Total Capacity: 20000

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

YELLOW FREIGHT SYSTEM, INC. (Continued)

U004133019

Substance: Diesel
 Date Installed: Not reported
 Tank Type: UST

 Tank ID: 5
 Tank Status: Permanently Out Of Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: Not reported
 Tank Type: UST

15

**1600 E RENO
 OKLAHOMA CITY, OK**

**LUST U001229809
 HIST UST N/A
 RGA LUST**

LUST:

Facility ID: 5507078
 Case Number: 064-AJ
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 10/06/1989
Close Date: 10/24/1994
 Lat/Long: 35.4636 / -97.48180000
Status: Closed

HIST UST:

Facility ID: 5507078
 Owner Name: YELLOW FREIGHT SYSTEM INC
 Owner Address: ATTN ENVIRONMENTAL SERVICES 10990 ROE AVE
 Owner City,St,Zip: Shawnee Mission, KS 66207
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: Not reported
 Tank Capacity: 10000
 Product: Diesel

Facility ID: 5507078
 Owner Name: YELLOW FREIGHT SYSTEM INC
 Owner Address: ATTN ENVIRONMENTAL SERVICES 10990 ROE AVE
 Owner City,St,Zip: Shawnee Mission, KS 66207
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: Not reported
 Tank Capacity: 10000
 Product: Diesel

Facility ID: 5507078
 Owner Name: YELLOW FREIGHT SYSTEM INC
 Owner Address: ATTN ENVIRONMENTAL SERVICES 10990 ROE AVE
 Owner City,St,Zip: Shawnee Mission, KS 66207
 Tank ID: 3
 Tank Status: Permanently Out of Use
 Installed Date: Not reported
 Tank Capacity: 10000
 Product: Diesel

Facility ID: 5507078
 Owner Name: YELLOW FREIGHT SYSTEM INC

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

(Continued)

U001229809

Owner Address: ATTN ENVIRONMENTAL SERVICES 10990 ROE AVE
 Owner City,St,Zip: Shawnee Mission, KS 66207
 Tank ID: 4
 Tank Status: Permanently Out of Use
 Installed Date: Not reported
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5507078
 Owner Name: YELLOW FREIGHT SYSTEM INC
 Owner Address: ATTN ENVIRONMENTAL SERVICES 10990 ROE AVE
 Owner City,St,Zip: Shawnee Mission, KS 66207
 Tank ID: 5
 Tank Status: Permanently Out of Use
 Installed Date: Not reported
 Tank Capacity: 20000
 Product: Diesel

RGA LUST:

| | | |
|------|-----------------------------|-------------|
| 2012 | YELLOW FREIGHT SYSTEM, INC. | 1600 E RENO |
| 2011 | YELLOW FREIGHT SYSTEM, INC. | 1600 E RENO |
| 2010 | YELLOW FREIGHT SYSTEM, INC. | 1600 E RENO |
| 2009 | YELLOW FREIGHT SYSTEM, INC. | 1600 E RENO |

16

PARAWAS REFINERY
801 SOUTH IRVING STREET
OKLAHOMA CITY, OK 73129

CERC-NFRAP 1003073120
OK0000605326

CERC-NFRAP:
 Site ID: 0605326
 Federal Facility: Not a Federal Facility
 NPL Status: Not on the NPL
 Non NPL Status: Addressed as Part of Another non-NPL Site

CERCLIS-NFRAP Site Contact Details:

Contact Sequence ID: 6424581.00000
 Person ID: 6270175.00000

CERCLIS-NFRAP Assessment History:

Action: DISCOVERY
 Date Started: / /
 Date Completed: 03/15/00
 Priority Level: Not reported

Action: ARCHIVE SITE
 Date Started: / /
 Date Completed: 10/31/07
 Priority Level: Not reported

MAP FINDINGS

| | | | |
|--------------------|--|-------------|---------------|
| Map ID | | | EDR ID Number |
| Direction | | | |
| Distance | | | |
| Distance (ft.)Site | | Database(s) | EPA ID Number |

17 MID-AMERICA CHEMICAL, INC.
1801 SKYLINE DRIVE
OKC, OK 73129

LUST S109417837
LAST N/A
AST
TIER 2
OK COMPLAINT
RGA LUST

LUST:
Facility ID: 5514905
Case Number: SOR-1335
Case Type: Suspicion of Release
Tank Type: **Not reported**
Release Date: **11/12/1999**
Close Date: **03/09/2000**
Lat/Long: 35.453000000 / -97.48019999
Status: **Closed**

LAST:
Case Number: 064-2379
Case Type: Confirmed Release
Facility ID: 5514905
Status: **Closed**
Lat/Long: 35.453000000 / -97.48019999
Tank Type: **AST**
Release Date: **12/13/1999**
Close Date: **05/20/2011**

AST:
Facility ID: 5514905
Contact Name: Red Rock Distributing
Contact Address: PO BOX 18755
Contact City,St,Zip: Oklahoma City, OK 73154
Lat/Long: 35.4530000 / -97.480199

Tank ID: 24
Tank Status: Permanently Out of Use
Total Capacity: 6100
Substance: Not Listed
Install Date: 01/01/1980
Tank Type: AST

Tank ID: 2
Tank Status: Permanently Out of Use
Total Capacity: 1800
Substance: Gasoline
Install Date: 01/01/1999
Tank Type: AST

Tank ID: 16
Tank Status: Permanently Out of Use
Total Capacity: 10000
Substance: Not Listed
Install Date: 01/01/1980
Tank Type: AST

Tank ID: 6
Tank Status: Permanently Out of Use
Total Capacity: 15000

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

MID-AMERICA CHEMICAL, INC. (Continued)

S109417837

| | |
|-----------------|------------------------|
| Substance: | Gasoline |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 5 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 7800 |
| Substance: | Gasoline |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 1 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 2000 |
| Substance: | Gasoline |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 4 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 2000 |
| Substance: | Gasoline |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 9 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 12000 |
| Substance: | Kerosene |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 21 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 10000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 20 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 10000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 15 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 10000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 11 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 10000 |
| Substance: | Not Listed |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

MID-AMERICA CHEMICAL, INC. (Continued)

S109417837

| | |
|-----------------|------------------------|
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 26 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 8000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 10 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 5700 |
| Substance: | Diesel |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 14 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 5700 |
| Substance: | Diesel |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 18 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 10000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 7 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 15000 |
| Substance: | Gasoline |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 19 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 10000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| | |
| Tank ID: | 29 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 350 |
| Substance: | Used Oil |
| Install Date: | 01/01/1990 |
| Tank Type: | AST |
| | |
| Tank ID: | 25 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 6100 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

MID-AMERICA CHEMICAL, INC. (Continued)

S109417837

| | |
|-----------------|------------------------|
| Tank Type: | AST |
| Tank ID: | 23 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 5000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| Tank ID: | 3 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 6000 |
| Substance: | Gasoline |
| Install Date: | 01/01/1989 |
| Tank Type: | AST |
| Tank ID: | 8 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 12000 |
| Substance: | Diesel |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| Tank ID: | 30 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 350 |
| Substance: | Used Oil |
| Install Date: | 01/01/1990 |
| Tank Type: | AST |
| Tank ID: | 22 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 5000 |
| Substance: | Not Listed |
| Install Date: | 09/28/1999 |
| Tank Type: | AST |
| Tank ID: | 17 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 10000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| Tank ID: | 12 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 10000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |
| Tank ID: | 13 |
| Tank Status: | Permanently Out of Use |
| Total Capacity: | 10000 |
| Substance: | Not Listed |
| Install Date: | 01/01/1980 |
| Tank Type: | AST |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

MID-AMERICA CHEMICAL, INC. (Continued)

S109417837

Tank ID: 27
 Tank Status: Permanently Out of Use
 Total Capacity: 8000
 Substance: Not Listed
 Install Date: 01/01/1980
 Tank Type: AST

Tank ID: 28
 Tank Status: Permanently Out of Use
 Total Capacity: 8000
 Substance: Not Listed
 Install Date: 01/01/1980
 Tank Type: AST

OK TIER 2:

Facility ID: FATR20113J2XFR002VNH
 Facility Country: USA
 All Chems. Same as Last Year: Not reported
 Date Tier 2 Signed: 2/27/2012
 Dike/Other Safeguards Employed: Not reported
 Facility Department: OKC
 Facility Date Modified: 6/20/2012
 State Fees Total: Not reported
 Facility Fire District: Not reported
 Mailing Address: Not reported
 Mailing City,St,Zip: Not reported
 Mailing Country: Not reported
 Latitude: 35.453992
 Longitude: -97.480035
 Lat/Long Location Description: Not reported
 Lat/Long Method: Not reported
 Number of Employees on Site: Not reported
 Notes: Not reported
 Validation Report: Not reported
 Reporting Year: 2011
 Site Coordinate Abbrvtions Submitted: Not reported
 State 1Require Contact: Not reported
 ID: 2899
 Facility Type: SIC
 Facility Description: CHEMICALS & CHEM PREP, NEC
 Facility Last Modified: 1/31/2005
 ID: Not reported
 Facility Type: Dun & Bradstreet
 Facility Description: Not reported
 Facility Last Modified: 1/31/2005
 ID: 325199
 Facility Type: NAICS
 Facility Description: All Other Basic Organic Chemical Manufacturing
 Facility Last Modified: 2/10/2010
 Contact Record ID: CTTR20113J2Z0Q005PKQ
 Contact Name: OWNER ROBERT WARD
 Contact Email: robertward5452@att.net
 Contact Mail Address: P.O. BOX 2365
 Contact Mail City,St,Zip: OKC, OK 73101
 Contact Mail Country: USA
 Contact Type: Owner / Operator
 Contact Modified Date: 3/20/2012
 Contact Record ID: CTTR20113J323F00M9G6

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

MID-AMERICA CHEMICAL, INC. (Continued)

S109417837

Contact Name: PRESIDENT ROBERT WARD
 Contact Email: Not reported
 Contact Mail Address: Not reported
 Contact Mail City,St,Zip: Not reported
 Contact Mail Country: USA
 Contact Type: Emergency Contact
 Contact Modified Date: 3/20/2012
 Contact Record ID: CTTR20113J324M00S9RM
 Contact Name: OPS MANAGER ROBERT BARNES
 Contact Email: Not reported
 Contact Mail Address: Not reported
 Contact Mail City,St,Zip: Not reported
 Contact Mail Country: USA
 Contact Type: Emergency Contact
 Contact Modified Date: 3/20/2012
 Acute Health Risks: Not reported
 Average Daily Amount: Not reported
 Average Daily Amount Code: Not reported
 Chemical Inventory Record ID: Not reported
 Chemical Same As Last Year: Not reported
 Chronic Heath Risks: Not reported
 CAS Number: Not reported
 EHS Substance: Not reported
 Last Modified: Not reported
 State Max Daily Amt Required: Not reported
 State Unit Required: Not reported
 Days on Site: Not reported
 Chemical Name: Not reported
 Fire Hazard: Not reported
 Gas: Not reported
 Liquid: Not reported
 Max Daily Amount: Not reported
 Max Daily Amount Code: Not reported
 Max Amount in Largest Container: Not reported
 Mixture Form: Not reported
 "Sudden Release of Preasue" Hazard: Not reported
 Pure Form: Not reported
 Reactive Hazard: Not reported
 Solid: Not reported
 State Contact Field: Not reported
 State Contact Comment: Not reported
 State EHS Comment: Not reported
 State Label Code: Not reported
 Max Daily Amount Required: Not reported
 State Mac Per Container Required: Not reported
 State Req Heading: Not reported
 Trade Secret: Not reported
 Mixture Chemical: Not reported
 Mixture Percentage: Not reported
 Mixture CAS: Not reported
 Mixture EHS: Not reported
 Mixture Last Modified: Not reported
 Amount of Substnce: Not reported
 Amount Units: Not reported
 Type of Storage: Not reported
 Number Code for Storage Pressure: Not reported
 Number Code for Storage Temperature: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

MID-AMERICA CHEMICAL, INC. (Continued)

S109417837

Last Modified: Not reported
 Location: Not reported

OK COMPLAINT:

Agency Receiving Complaint: 040
 Agency with Jurisdiction: 040
 Complaint Number: PMRPA0110
 Complaint Number: 173
 Complaint Number 2: PMR-10-00173
 Complaint Date Closed: 3/12/2010
 Agency Person Contacted: Not reported
 Date Referred to Agency: Not reported
 Date Agency Received: Not reported
 Anonymous Complaint: No
 Confirmation Status: No
 Complainant Name: ODAFF
 Complainant Address: 1801 SKYLINE DR
 Complainant Work Phone: Not reported
 Complainant Home Phone: Not reported
 Complainant City,St,Zip: OKLAHOMA CITY, OK
 Date Complaint was Received: 1/27/201
 Time Complaint was Received: Not reported
 Source and Type of Complaint: NOT DETERMINED AT THIS TIME
 Name of Affected Waterbody: Not reported
 Waterbody was affected: No
 Fish or Wildlife Kill Occured: No
 Legal Subdivision of Complaint Site: Not reported
 Section, Township, Range: Not reported
 Township: Not reported
 Range: Not reported
 Lat/Long (dms): Not reported
 Latitude Decimal: Not reported
 Longitude Decimal: Not reported
 Date Agency Responded: 3/12/2010
 First Response Time: Not reported
 Referred To: Not reported
 Date Referred: Not reported
 Pollution: Not reported
 Locate Meridian: Not reported
 Date Investigation: Not reported
 Officer Name: Not reported
 Investigator Initials: Not reported
 Responsible Party Telephone: Not reported
 Responsible Party Telephone2: Not reported
 Leased Well Name: Not reported
 Facility Contact: Mike Vandeventer
 Date Under Investigation: Not reported
 Date Under Litigation: Not reported
 Date Under Remediation: Not reported
 Date Under Mediation: Not reported
 Date Resolved: Not reported
 Confirmation Status: Unconfirmed
 County Number: 55
 General Location: Not reported
 Locate QT1: Not reported
 Locate QT2: Not reported
 Locate QT3: Not reported

MAP FINDINGS

| | | |
|----------------|------|---------------------------|
| Map ID | | EDR ID Number |
| Direction | | |
| Distance | | |
| Distance (ft.) | Site | Database(s) EPA ID Number |

MID-AMERICA CHEMICAL, INC. (Continued)

S109417837

| | |
|--------------|--------------|
| Locate QT4: | Not reported |
| Fiscal Year: | 2010 |
| Allegation: | Not reported |

RGA LUST:

| | | |
|------|-----------------------|--------------------|
| 2012 | RED ROCK DISTRIBUTING | 1801 SE SKYLINE DR |
| 2011 | RED ROCK DISTRIBUTING | 1801 SE SKYLINE DR |
| 2010 | RED ROCK DISTRIBUTING | 1801 SE SKYLINE DR |

**18 TIME-DC TRUCKING COMPANY
1400 SE SKYLINE
OKLAHOMA CITY, OK 73129**

**CERC-NFRAP 1003873851
OKD980696660**

CERC-NFRAP:

| | |
|-------------------|---|
| Site ID: | 0601305 |
| Federal Facility: | Not a Federal Facility |
| NPL Status: | Not on the NPL |
| Non NPL Status: | NFRAP-Site does not qualify for the NPL based on existing information |

CERCLIS-NFRAP Site Contact Details:

| | |
|----------------------|---------------|
| Contact Sequence ID: | 6424854.00000 |
| Person ID: | 6270175.00000 |

CERCLIS-NFRAP Assessment History:

| | |
|-----------------|--------------|
| Action: | DISCOVERY |
| Date Started: | / / |
| Date Completed: | 06/01/80 |
| Priority Level: | Not reported |

| | |
|-----------------|--------------|
| Action: | ARCHIVE SITE |
| Date Started: | / / |
| Date Completed: | 05/01/80 |
| Priority Level: | Not reported |

| | |
|-----------------|---|
| Action: | PRELIMINARY ASSESSMENT |
| Date Started: | 05/01/80 |
| Date Completed: | 05/01/80 |
| Priority Level: | NFRAP-Site does not qualify for the NPL based on existing information |

**19 BEAVER EXPRESS LLC
1515 SE 15TH
OKLAHOMA CITY, OK 73126**

**LUST U001885127
UST N/A
HIST UST
RGA LUST**

LUST:

| | |
|----------------------|-------------------------|
| Facility ID: | 5513212 |
| Case Number: | SOR-3782 |
| Case Type: | Suspicion of Release |
| Tank Type: | UST |
| Release Date: | 10/26/2012 |
| Close Date: | 04/16/2013 |
| Lat/Long: | 35.451099999 / -97.4816 |
| Status: | Closed |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

BEAVER EXPRESS LLC (Continued)

U001885127

Facility ID: 5513212
 Case Number: 064-0404
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 01/07/1992
Close Date: 11/02/1992
 Lat/Long: 35.451099999 / -97.4816
Status: Closed

Facility ID: 5513212
 Case Number: 064-13
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 01/11/1989
Close Date: 08/31/1989
 Lat/Long: 35.451099999 / -97.4816
Status: Closed

UST:

Facility ID: 5513212
 Contact Name: Beaver Express Service Inc.
 Contact Address: DRAWER 1147
 Contact City,St,Zip: Woodward, OK 73802
 Lat/Long: 35.4510999 / -97.4816

Tank ID: 1
 Tank Status: Currently In Use
 Total Capacity: 12000
 Substance: Diesel
 Date Installed: 12/01/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

HIST UST:

Facility ID: 5513212
 Owner Name: BEAVER EXPRESS SERVICE INC.
 Owner Address: DRAWER 1147
 Owner City,St,Zip: Woodward, OK 73802
 Tank ID: 1
 Tank Status: Currently in Use
 Installed Date: 12/1/1988 0:00:00
 Tank Capacity: 12000
 Product: Diesel

RGA LUST:

| | | |
|------|--------------------|--------------|
| 2012 | BEAVER EXPRESS LLC | 1515 SE 15TH |
| 2011 | BEAVER EXPRESS LLC | 1515 SE 15TH |
| 2010 | BEAVER EXPRESS LLC | 1515 SE 15TH |
| 2009 | BEAVER EXPRESS LLC | 1515 SE 15TH |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

19

**1515 SE 15TH STR
 OKLAHOMA CITY, OK**

**RGA LUST S114359175
 N/A**

RGALUST:

| | | |
|------|------------------------|------------------|
| 2008 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2007 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2006 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2006 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2005 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2005 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2003 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2003 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2002 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2002 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2001 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2001 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2000 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 2000 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 1997 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 1997 | BEAVER EXPRESS | 1515 SE 15TH STR |
| 1996 | CHEROKEE TERMINALS CO. | 1515 SE 15TH STR |
| 1996 | CHEROKEE TERMINALS CO. | 1515 SE 15TH STR |
| 1995 | CHEROKEE TERMINALS CO. | 1515 SE 15TH STR |
| 1995 | CHEROKEE TERMINALS CO. | 1515 SE 15TH STR |
| 1994 | CHEROKEE TERMINALS CO. | 1515 SE 15TH STR |
| 1994 | CHEROKEE TERMINALS CO. | 1515 SE 15TH STR |

19

**WAGGONER'S TRUCKING
 1501 SE 15 STR
 OKLAHOMA CITY, OK 73129**

**UST U004133102
 AST N/A**

UST:

Facility ID: 5503703
 Contact Name: The Waggoners Trucking
 Contact Address: 5200 W Airport Freeway
 Contact City,St,Zip: Irving, TX 75062
 Lat/Long: 35.4498000 / -97.483800

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

WAGGONER'S TRUCKING (Continued)

U004133102

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Not Listed
 Date Installed: 02/13/1961
 Tank Type: UST

Tank ID: 10
 Tank Status: Permanently Out Of Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 02/14/1971
 Tank Type: UST

Tank ID: 11
 Tank Status: Permanently Out Of Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 02/14/1964
 Tank Type: UST

Tank ID: 12
 Tank Status: Permanently Out Of Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 02/14/1964
 Tank Type: UST

Tank ID: 13
 Tank Status: Permanently Out Of Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 02/14/1964
 Tank Type: UST

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Not Listed
 Date Installed: 02/13/1961
 Tank Type: UST

Tank ID: 3
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 02/13/1961
 Tank Type: UST

Tank ID: 4
 Tank Status: Permanently Out Of Use
 Total Capacity: 1500
 Substance: Used Oil
 Date Installed: 02/14/1964
 Tank Type: UST

Tank ID: 5

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

WAGGONER'S TRUCKING (Continued)

U004133102

Tank Status: Permanently Out Of Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 02/14/1971
 Tank Type: UST

Tank ID: 6
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Not Listed
 Date Installed: 02/13/1969
 Tank Type: UST

Tank ID: 7
 Tank Status: Permanently Out Of Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 02/14/1971
 Tank Type: UST

Tank ID: 8
 Tank Status: Permanently Out Of Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 02/14/1971
 Tank Type: UST

Tank ID: 9
 Tank Status: Permanently Out Of Use
 Total Capacity: 20000
 Substance: Diesel
 Date Installed: 02/14/1964
 Tank Type: UST

AST:
 Facility ID: 5503703
 Contact Name: The Waggoners Trucking
 Contact Address: 5200 W Airport Freeway
 Contact City,St,Zip: Irving, TX 75062
 Lat/Long: 35.4498000 / -97.483800

Tank ID: 14
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Diesel
 Install Date: 09/01/1999
 Tank Type: AST
 Tank Material: Steel
 Tank Construction: Double Walled
 Pipe Material: Steel

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

WAGGONERS TRUCKING THE (Continued)

1000319955

Historical Generators:

Date form received by agency: 04/11/1999
 Facility name: WAGGONERS TRUCKING THE
 Classification: Small Quantity Generator

Hazardous Waste Summary:

Waste code: D000
 Waste name: Not Defined

Waste code: D001
 Waste name: IGNITABLE HAZARDOUS WASTES ARE THOSE WASTES WHICH HAVE A FLASHPOINT OF LESS THAN 140 DEGREES FAHRENHEIT AS DETERMINED BY A PENSKEY-MARTENS CLOSED CUP FLASH POINT TESTER. ANOTHER METHOD OF DETERMINING THE FLASH POINT OF A WASTE IS TO REVIEW THE MATERIAL SAFETY DATA SHEET, WHICH CAN BE OBTAINED FROM THE MANUFACTURER OR DISTRIBUTOR OF THE MATERIAL. LACQUER THINNER IS AN EXAMPLE OF A COMMONLY USED SOLVENT WHICH WOULD BE CONSIDERED AS IGNITABLE HAZARDOUS WASTE.

Waste code: D002
 Waste name: A WASTE WHICH HAS A PH OF LESS THAN 2 OR GREATER THAN 12.5 IS CONSIDERED TO BE A CORROSIVE HAZARDOUS WASTE. SODIUM HYDROXIDE, A CAUSTIC SOLUTION WITH A HIGH PH, IS OFTEN USED BY INDUSTRIES TO CLEAN OR DEGREASE PARTS. HYDROCHLORIC ACID, A SOLUTION WITH A LOW PH, IS USED BY MANY INDUSTRIES TO CLEAN METAL PARTS PRIOR TO PAINTING. WHEN THESE CAUSTIC OR ACID SOLUTIONS BECOME CONTAMINATED AND MUST BE DISPOSED, THE WASTE WOULD BE A CORROSIVE HAZARDOUS WASTE.

Waste code: F002
 Waste name: THE FOLLOWING SPENT HALOGENATED SOLVENTS: TETRACHLOROETHYLENE, METHYLENE CHLORIDE, TRICHLOROETHYLENE, 1,1,1-TRICHLOROETHANE, CHLOROBENZENE, 1,1,2-TRICHLORO-1,2,2-TRIFLUOROETHANE, ORTHO-DICHLOROBENZENE, TRICHLOROFLUOROMETHANE, AND 1,1,2-TRICHLOROETHANE; ALL SPENT SOLVENT MIXTURES/BLENDS CONTAINING, BEFORE USE, A TOTAL OF TEN PERCENT OR MORE (BY VOLUME) OF ONE OR MORE OF THE ABOVE HALOGENATED SOLVENTS OR THOSE LISTED IN F001, F004, OR F005, AND STILL BOTTOMS FROM THE RECOVERY OF THESE SPENT SOLVENTS AND SPENT SOLVENT MIXTURES.

Waste code: F004
 Waste name: THE FOLLOWING SPENT NON-HALOGENATED SOLVENTS: CRESOLS AND CRESYLIC ACID, AND NITROBENZENE; ALL SPENT SOLVENT MIXTURES/BLENDS CONTAINING, BEFORE USE, A TOTAL OF TEN PERCENT OR MORE (BY VOLUME) OF ONE OR MORE OF THE ABOVE NON-HALOGENATED SOLVENTS OR THOSE SOLVENTS LISTED IN F001, F002, AND F005; AND STILL BOTTOMS FROM THE RECOVERY OF THESE SPENT SOLVENTS AND SPENT SOLVENT MIXTURES.

Waste code: D000
 Waste name: Not Defined

Waste code: D001
 Waste name: IGNITABLE HAZARDOUS WASTES ARE THOSE WASTES WHICH HAVE A FLASHPOINT OF LESS THAN 140 DEGREES FAHRENHEIT AS DETERMINED BY A PENSKEY-MARTENS CLOSED CUP FLASH POINT TESTER. ANOTHER METHOD OF DETERMINING THE FLASH POINT OF A WASTE IS TO REVIEW THE MATERIAL SAFETY DATA SHEET, WHICH CAN BE OBTAINED FROM THE MANUFACTURER OR DISTRIBUTOR OF THE MATERIAL. LACQUER THINNER IS AN EXAMPLE OF A COMMONLY USED SOLVENT

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

WAGGONERS TRUCKING THE (Continued)

1000319955

WHICH WOULD BE CONSIDERED AS IGNITABLE HAZARDOUS WASTE.

Waste code: D002
 Waste name: A WASTE WHICH HAS A PH OF LESS THAN 2 OR GREATER THAN 12.5 IS CONSIDERED TO BE A CORROSIVE HAZARDOUS WASTE. SODIUM HYDROXIDE, A CAUSTIC SOLUTION WITH A HIGH PH, IS OFTEN USED BY INDUSTRIES TO CLEAN OR DEGREASE PARTS. HYDROCHLORIC ACID, A SOLUTION WITH A LOW PH, IS USED BY MANY INDUSTRIES TO CLEAN METAL PARTS PRIOR TO PAINTING. WHEN THESE CAUSTIC OR ACID SOLUTIONS BECOME CONTAMINATED AND MUST BE DISPOSED, THE WASTE WOULD BE A CORROSIVE HAZARDOUS WASTE.

Waste code: F002
 Waste name: THE FOLLOWING SPENT HALOGENATED SOLVENTS: TETRACHLOROETHYLENE, METHYLENE CHLORIDE, TRICHLOROETHYLENE, 1,1,1-TRICHLOROETHANE, CHLOROBENZENE, 1,1,2-TRICHLORO-1,2,2-TRIFLUOROETHANE, ORTHO-DICHLOROBENZENE, TRICHLOROFLUOROMETHANE, AND 1,1,2-TRICHLOROETHANE; ALL SPENT SOLVENT MIXTURES/BLENDS CONTAINING, BEFORE USE, A TOTAL OF TEN PERCENT OR MORE (BY VOLUME) OF ONE OR MORE OF THE ABOVE HALOGENATED SOLVENTS OR THOSE LISTED IN F001, F004, OR F005, AND STILL BOTTOMS FROM THE RECOVERY OF THESE SPENT SOLVENTS AND SPENT SOLVENT MIXTURES.

Waste code: F004
 Waste name: THE FOLLOWING SPENT NON-HALOGENATED SOLVENTS: CRESOLS AND CRESYLIC ACID, AND NITROBENZENE; ALL SPENT SOLVENT MIXTURES/BLENDS CONTAINING, BEFORE USE, A TOTAL OF TEN PERCENT OR MORE (BY VOLUME) OF ONE OR MORE OF THE ABOVE NON-HALOGENATED SOLVENTS OR THOSE SOLVENTS LISTED IN F001, F002, AND F005; AND STILL BOTTOMS FROM THE RECOVERY OF THESE SPENT SOLVENTS AND SPENT SOLVENT MIXTURES.

Facility Has Received Notices of Violations:

Regulation violated: Not reported
 Area of violation: Generators - General
 Date violation determined: 09/20/2007
 Date achieved compliance: 10/22/2007
 Violation lead agency: State
 Enforcement action: Not reported
 Enforcement action date: 10/08/2007
 Enf. disposition status: Not reported
 Enf. disp. status date: Not reported
 Enforcement lead agency: State
 Proposed penalty amount: Not reported
 Final penalty amount: Not reported
 Paid penalty amount: Not reported

Regulation violated: Not reported
 Area of violation: Listing - General
 Date violation determined: 09/20/2007
 Date achieved compliance: 11/19/2007
 Violation lead agency: State
 Enforcement action: Not reported
 Enforcement action date: 10/08/2007
 Enf. disposition status: Not reported
 Enf. disp. status date: Not reported
 Enforcement lead agency: State
 Proposed penalty amount: Not reported
 Final penalty amount: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

WAGGONERS TRUCKING THE (Continued)

1000319955

Paid penalty amount: Not reported

Regulation violated: Not reported
 Area of violation: Universal Waste - Small Quantity Handlers
 Date violation determined: 09/20/2007
 Date achieved compliance: 10/22/2007
 Violation lead agency: State
 Enforcement action: Not reported
 Enforcement action date: 10/08/2007
 Enf. disposition status: Not reported
 Enf. disp. status date: Not reported
 Enforcement lead agency: State
 Proposed penalty amount: Not reported
 Final penalty amount: Not reported
 Paid penalty amount: Not reported

Evaluation Action Summary:

Evaluation date: 09/20/2007
 Evaluation: COMPLIANCE EVALUATION INSPECTION ON-SITE
 Area of violation: Listing - General
 Date achieved compliance: 11/19/2007
 Evaluation lead agency: State

Evaluation date: 09/20/2007
 Evaluation: COMPLIANCE EVALUATION INSPECTION ON-SITE
 Area of violation: Generators - General
 Date achieved compliance: 10/22/2007
 Evaluation lead agency: State

Evaluation date: 09/20/2007
 Evaluation: COMPLIANCE EVALUATION INSPECTION ON-SITE
 Area of violation: Universal Waste - Small Quantity Handlers
 Date achieved compliance: 10/22/2007
 Evaluation lead agency: State

FINDS:

Registry ID: 110004745330

Environmental Interest/Information System

RCRAInfo is a national information system that supports the Resource Conservation and Recovery Act (RCRA) program through the tracking of events and activities related to facilities that generate, transport, and treat, store, or dispose of hazardous waste. RCRAInfo allows RCRA program staff to track the notification, permit, compliance, and corrective action activities required under RCRA.

LUST:

Facility ID: 5503703
 Case Number: 064-2143
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 08/20/1998
Close Date: 05/30/2000
 Lat/Long: 35.449800000 / -97.483800000
Status: Closed

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

WAGGONERS TRUCKING THE (Continued)

1000319955

HIST UST:

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 2/13/1961 0:00:00
 Tank Capacity: 10000
 Product: Other

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: 2/13/1961 0:00:00
 Tank Capacity: 10000
 Product: Other

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 3
 Tank Status: Permanently Out of Use
 Installed Date: 2/13/1961 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 4
 Tank Status: Permanently Out of Use
 Installed Date: 2/14/1964 0:00:00
 Tank Capacity: 1500
 Product: Used Oil

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 5
 Tank Status: Permanently Out of Use
 Installed Date: 2/14/1971 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 6
 Tank Status: Permanently Out of Use

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

WAGGONERS TRUCKING THE (Continued)

1000319955

Installed Date: 2/13/1969 0:00:00
 Tank Capacity: 10000
 Product: Other

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 7
 Tank Status: Permanently Out of Use
 Installed Date: 2/14/1971 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 8
 Tank Status: Permanently Out of Use
 Installed Date: 2/14/1971 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 9
 Tank Status: Permanently Out of Use
 Installed Date: 2/14/1964 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 10
 Tank Status: Permanently Out of Use
 Installed Date: 2/14/1971 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 11
 Tank Status: Permanently Out of Use
 Installed Date: 2/14/1964 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

WAGGONERS TRUCKING THE (Continued)

1000319955

Owner City,St,Zip: Billings, MT 59107
 Tank ID: 12
 Tank Status: Permanently Out of Use
 Installed Date: 2/14/1964 0:00:00
 Tank Capacity: 20000
 Product: Diesel

Facility ID: 5503703
 Owner Name: WAGGONERS FAMILY TRUCKING CO
 Owner Address: P.O. BOX 1516
 Owner City,St,Zip: Billings, MT 59107
 Tank ID: 13
 Tank Status: Permanently Out of Use
 Installed Date: 2/14/1964 0:00:00
 Tank Capacity: 20000
 Product: Diesel

RGA LUST:

| | | |
|------|---------------------|----------------|
| 2012 | WAGGONER'S TRUCKING | 1501 SE 15 STR |
| 2011 | WAGGONER'S TRUCKING | 1501 SE 15 STR |
| 2010 | WAGGONER'S TRUCKING | 1501 SE 15 STR |
| 2009 | WAGGONER'S TRUCKING | 1501 SE 15 STR |

19

**CONSOLIDATED FREIGHTWAYS
 1400 SE SKYLINE DR
 OKLAHOMA CITY, OK 73129**

**RCRA NonGen / NLR 1000641243
 FINDS OKD987082690
 LUST
 UST
 HIST UST
 RGA LUST**

RCRA NonGen / NLR:

Date form received by agency: 03/03/2003
 Facility name: CONSOLIDATED FREIGHTWAYS
 Facility address: 1400 SE SKYLINE DR
 OKLAHOMA CITY, OK 73129
 EPA ID: OKD987082690
 Mailing address: PO BOX 24121
 OKLAHOMA CITY, OK 73124
 Contact: JIM ELERICK
 Contact address: PO BOX 24121
 OKLAHOMA CITY, OK 73124
 Contact country: US
 Contact telephone: (405) 672-6065
 Contact email: Not reported
 EPA Region: 06
 Classification: Non-Generator
 Description: Handler: Non-Generators do not presently generate hazardous waste

Owner/Operator Summary:

Owner/operator name: BURR & TEMKIN
 Owner/operator address: BANK OF CA CENTER
 SEATTLE, WA 98164
 Owner/operator country: US
 Owner/operator telephone: (206) 624-6500

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

CONSOLIDATED FREIGHTWAYS (Continued)

1000641243

| | |
|---------------------------|---|
| Legal status: | Private |
| Owner/Operator Type: | Operator |
| Owner/Op start date: | 03/03/2003 |
| Owner/Op end date: | Not reported |
| Owner/operator name: | BURR & TEMKIN |
| Owner/operator address: | BANK OF CA CENTER SEATTLE, WA 98164 |
| Owner/operator country: | US |
| Owner/operator telephone: | (206) 624-6500 |
| Legal status: | Private |
| Owner/Operator Type: | Owner |
| Owner/Op start date: | 03/03/2003 |
| Owner/Op end date: | Not reported |
| Owner/operator name: | BURR & TEMKIN |
| Owner/operator address: | 3030 BANK OF CA CENTER SEATTLE, WA 98164 |
| Owner/operator country: | Not reported |
| Owner/operator telephone: | (206) 624-6500 |
| Legal status: | Private |
| Owner/Operator Type: | Owner |
| Owner/Op start date: | 01/01/0001 |
| Owner/Op end date: | Not reported |

Handler Activities Summary:

| | |
|-------------------------------------|----|
| U.S. importer of hazardous waste: | No |
| Mixed waste (haz. and radioactive): | No |
| Recycler of hazardous waste: | No |
| Transporter of hazardous waste: | No |
| Treater, storer or disposer of HW: | No |
| Underground injection activity: | No |
| On-site burner exemption: | No |
| Furnace exemption: | No |
| Used oil fuel burner: | No |
| Used oil processor: | No |
| User oil refiner: | No |
| Used oil fuel marketer to burner: | No |
| Used oil Specification marketer: | No |
| Used oil transfer facility: | No |
| Used oil transporter: | No |

Historical Generators:

| | |
|-------------------------------|--------------------------|
| Date form received by agency: | 02/13/1991 |
| Facility name: | CONSOLIDATED FREIGHTWAYS |
| Classification: | Small Quantity Generator |

| | |
|-------------------|---------------------|
| Violation Status: | No violations found |
|-------------------|---------------------|

FINDS:

| | |
|--------------|--------------|
| Registry ID: | 110004762864 |
|--------------|--------------|

Environmental Interest/Information System

RCRAInfo is a national information system that supports the Resource Conservation and Recovery Act (RCRA) program through the tracking of events and activities related to facilities that generate, transport,

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

CONSOLIDATED FREIGHTWAYS (Continued)

1000641243

and treat, store, or dispose of hazardous waste. RCRAInfo allows RCRA program staff to track the notification, permit, compliance, and corrective action activities required under RCRA.

LUST:

Facility ID: 5510474
 Case Number: 064-QC
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 08/23/1990
Close Date: 04/25/2000
 Lat/Long: 35.45060000 / -97.48229999
Status: Closed

UST:

Facility ID: 5510474
 Contact Name: Old Dominion Freight Line Inc
 Contact Address: 500 OLD DOMINION WAY
 Contact City,St,Zip: Thomasville, NC 27360
 Lat/Long: 35.4506000 / -97.482299

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 12000
 Substance: Diesel
 Date Installed: 02/05/1965
 Tank Type: UST

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 12000
 Substance: Diesel
 Date Installed: 02/05/1965
 Tank Type: UST

Tank ID: 3
 Tank Status: Permanently Out Of Use
 Total Capacity: 12000
 Substance: Diesel
 Date Installed: 02/05/1975
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Double Walled
 Pipe Material: Fiberglass

Tank ID: 4
 Tank Status: Permanently Out Of Use
 Total Capacity: 12000
 Substance: Diesel
 Date Installed: 02/05/1975
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Double Walled
 Pipe Material: Fiberglass

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CONSOLIDATED FREIGHTWAYS (Continued)

1000641243

Tank ID: 5
 Tank Status: Permanently Out Of Use
 Total Capacity: 12000
 Substance: Diesel
 Date Installed: 02/05/1965
 Tank Type: UST

Tank ID: 6
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Not Listed
 Date Installed: 02/05/1965
 Tank Type: UST

Tank ID: 7
 Tank Status: Permanently Out Of Use
 Total Capacity: 800
 Substance: Used Oil
 Date Installed: 02/05/1965
 Tank Type: UST

Tank ID: 8
 Tank Status: Currently In Use
 Total Capacity: 12000
 Substance: Diesel
 Date Installed: 11/01/1998
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Double Walled
 Pipe Material: Fiberglass

Tank ID: 9
 Tank Status: Currently In Use
 Total Capacity: 12000
 Substance: Diesel
 Date Installed: 11/01/1998
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Double Walled
 Pipe Material: Fiberglass

HIST UST:

Facility ID: 5510474
 Owner Name: Consolidated Freightways
 Owner Address: 16400 SE CF Way
 Owner City,St,Zip: Vancouver, WA 98683
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 2/5/1965 0:00:00
 Tank Capacity: 12000
 Product: Diesel

Facility ID: 5510474
 Owner Name: Consolidated Freightways
 Owner Address: 16400 SE CF Way
 Owner City,St,Zip: Vancouver, WA 98683

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CONSOLIDATED FREIGHTWAYS (Continued)

1000641243

Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: 2/5/1965 0:00:00
 Tank Capacity: 12000
 Product: Diesel

Facility ID: 5510474
 Owner Name: Consolidated Freightways
 Owner Address: 16400 SE CF Way
 Owner City,St,Zip: Vancouver, WA 98683
 Tank ID: 3
 Tank Status: Permanently Out of Use
 Installed Date: 2/5/1975 0:00:00
 Tank Capacity: 12000
 Product: Diesel

Facility ID: 5510474
 Owner Name: Consolidated Freightways
 Owner Address: 16400 SE CF Way
 Owner City,St,Zip: Vancouver, WA 98683
 Tank ID: 4
 Tank Status: Permanently Out of Use
 Installed Date: 2/5/1975 0:00:00
 Tank Capacity: 12000
 Product: Diesel

Facility ID: 5510474
 Owner Name: Consolidated Freightways
 Owner Address: 16400 SE CF Way
 Owner City,St,Zip: Vancouver, WA 98683
 Tank ID: 5
 Tank Status: Permanently Out of Use
 Installed Date: 2/5/1965 0:00:00
 Tank Capacity: 12000
 Product: Diesel

Facility ID: 5510474
 Owner Name: Consolidated Freightways
 Owner Address: 16400 SE CF Way
 Owner City,St,Zip: Vancouver, WA 98683
 Tank ID: 6
 Tank Status: Permanently Out of Use
 Installed Date: 2/5/1965 0:00:00
 Tank Capacity: 10000
 Product: Not Listed

Facility ID: 5510474
 Owner Name: Consolidated Freightways
 Owner Address: 16400 SE CF Way
 Owner City,St,Zip: Vancouver, WA 98683
 Tank ID: 7
 Tank Status: Permanently Out of Use
 Installed Date: 2/5/1965 0:00:00
 Tank Capacity: 800
 Product: Used Oil

Facility ID: 5510474

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

CONSOLIDATED FREIGHTWAYS (Continued)

1000641243

Owner Name: Consolidated Freightways
 Owner Address: 16400 SE CF Way
 Owner City,St,Zip: Vancouver, WA 98683
 Tank ID: 8
 Tank Status: Currently In Use
 Installed Date: 11/1/1998 0:00:00
 Tank Capacity: 12000
 Product: Diesel

Facility ID: 5510474
 Owner Name: Consolidated Freightways
 Owner Address: 16400 SE CF Way
 Owner City,St,Zip: Vancouver, WA 98683
 Tank ID: 9
 Tank Status: Currently In Use
 Installed Date: 11/1/1998 0:00:00
 Tank Capacity: 12000
 Product: Diesel

RGA LUST:

2012 OLD DOMINION FREIGHT LINE INC 1400 S SKYLINE DR.
 2011 OLD DOMINION FREIGHT LINE INC 1400 S SKYLINE DR.
 2010 OLD DOMINION FREIGHT LINE INC 1400 S SKYLINE DR.
 2009 OLD DOMINION FREIGHT LINE INC 1400 S SKYLINE DR.

19

**SAIA MOTOR FREIGHT LINES
 1715 S SKYLINE DRIVE
 OKLAHOMA CITY, OK 73117**

**LUST U001884609
 UST N/A
 HIST UST
 TIER 2
 RGA LUST**

LUST:

Facility ID: 5505434
 Case Number: 064-FD
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 01/17/1990
Close Date: 03/15/1990
 Lat/Long: 35.45400000 / -97.48050000
Status: Closed

UST:

Facility ID: 5505434
 Contact Name: SAIA Motor Freight Line
 Contact Address: 1625 CORPORATE PLACE
 Contact City,St,Zip: La Vergne, TN 37086
 Lat/Long: 35.4540000 / -97.4805000
 Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Diesel
 Date Installed: 04/18/1951
 Tank Type: UST

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

SAIA MOTOR FREIGHT LINES (Continued)

U001884609

Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 2
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Diesel
 Date Installed: 01/11/1990
 Tank Type: UST
 Tank Material: Steel With Fiberglass
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

HIST UST:

Facility ID: 5505434
 Owner Name: SAIA MOTOR FREIGHT LINE
 Owner Address: 104 WOODLAWN RANCH RD
 Owner City,St,Zip: Houma, LA 70363
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 4/18/1951 0:00:00
 Tank Capacity: 10000
 Product: Diesel

Facility ID: 5505434
 Owner Name: SAIA MOTOR FREIGHT LINE
 Owner Address: 104 WOODLAWN RANCH RD
 Owner City,St,Zip: Houma, LA 70363
 Tank ID: 2
 Tank Status: Currently in Use
 Installed Date: 1/1/1990 0:00:00
 Tank Capacity: 12000
 Product: Diesel

OK TIER 2:

Facility ID: FATR20118EQH72002QZ7
 Facility Country: USA
 All Chems. Same as Last Year: Not reported
 Date Tier 2 Signed: 2/21/2012
 Dike/Other Safeguards Employed: Not reported
 Facility Department: Saia
 Facility Date Modified: 6/20/2012
 State Fees Total: Not reported
 Facility Fire District: Not reported
 Mailing Address: 11465 JOHNS CREEK STE. 400
 Mailing City,St,Zip: JOHNS CREEK, GA 30097
 Mailing Country: USA
 Latitude: 35.454276
 Longitude: -97.481151
 Lat/Long Location Description: PG - Plant Entrance (General)
 Lat/Long Method: A2 - Address Matching (Block Face)
 Number of Employees on Site: Not reported
 Notes: Not reported
 Validation Report: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

SAIA MOTOR FREIGHT LINES (Continued)

U001884609

Reporting Year: 2011
 Site Coordinate Abbrvtions Submitted: Not reported
 State 1Require Contact: Not reported
 ID: 484122
 Facility Type: NAICS
 Facility Description: General Freight Trucking, Long-Distance, Less Than Truckload
 Facility Last Modified: 2/21/2012
 ID: 4213
 Facility Type: SIC
 Facility Description: TRUCKING, EXCEPT LOCAL
 Facility Last Modified: 2/21/2012
 ID: Not reported
 Facility Type: Dun & Bradstreet
 Facility Description: Not reported
 Facility Last Modified: 2/21/2012
 Contact Record ID: CTTR20118EQJC7006LMG
 Contact Name: Terminal Manager Bill Gaige
 Contact Email: BGaige@Saia.com
 Contact Mail Address: 1715 S. Skyline Drive
 Contact Mail City,St,Zip: Oklahoma City, OK 73129
 Contact Mail Country: USA
 Contact Type: Owner / Operator
 Contact Modified Date: 3/20/2012
 Contact Record ID: CTTR20117QV5HC00BASX
 Contact Name: Director of Safety Karla Staver
 Contact Email: KStaver@Saia.com
 Contact Mail Address: 11465 Johns Creek PKWY
 Contact Mail City,St,Zip: Duluth, GA 30097
 Contact Mail Country: USA
 Contact Type: Regulatory Point of Contact
 Contact Modified Date: 2/17/2011
 Contact Record ID: CTTR20118EPV2700RGM5
 Contact Name: Regional Safety Manager Darwin Brinkley
 Contact Email: Not reported
 Contact Mail Address: 1002 W. Oakdale
 Contact Mail City,St,Zip: Grand Prairie, TX 75050
 Contact Mail Country: USA
 Contact Type: Emergency Contact
 Contact Modified Date: 3/20/2012
 Acute Health Risks: Not reported
 Average Daily Amount: Not reported
 Average Daily Amount Code: Not reported
 Chemical Inventory Record ID: Not reported
 Chemical Same As Last Year: Not reported
 Chronic Heath Risks: Not reported
 CAS Number: Not reported
 EHS Substance: Not reported
 Last Modified: Not reported
 State Max Daily Amt Required: Not reported
 State Unit Required: Not reported
 Days on Site: Not reported
 Chemical Name: Not reported
 Fire Hazard: Not reported
 Gas: Not reported
 Liquid: Not reported
 Max Daily Amount: Not reported
 Max Daily Amount Code: Not reported

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

SAIA MOTOR FREIGHT LINES (Continued)

U001884609

Max Amount in Largest Container: Not reported
 Mixture Form: Not reported
 "Sudden Release of Pressure" Hazard: Not reported
 Pure Form: Not reported
 Reactive Hazard: Not reported
 Solid: Not reported
 State Contact Field: Not reported
 State Contact Comment: Not reported
 State EHS Comment: Not reported
 State Label Code: Not reported
 Max Daily Amount Required: Not reported
 State Mac Per Container Required: Not reported
 State Req Heading: Not reported
 Trade Secret: Not reported
 Mixture Chemical: Not reported
 Mixture Percentage: Not reported
 Mixture CAS: Not reported
 Mixture EHS: Not reported
 Mixture Last Modified: Not reported
 Amount of Substance: Not reported
 Amount Units: Not reported
 Type of Storage: Not reported
 Number Code for Storage Pressure: Not reported
 Number Code for Storage Temperature: Not reported
 Last Modified: Not reported
 Location: Not reported

RGALUST:

2012 SAIA MOTOR FREIGHT LINES 1715 S SKYLINE DRIVE

20

**ARMSTRONG TEXACO SERVICE
 1320 15TH ST SE
 OKLAHOMA CITY, OK 73129**

**EDR US Hist Auto Stat 1014179165
 N/A**

EDR Historical Auto Stations:

Name: ARMSTRONG TEXACO SERVICE
 Year: 1981
 Type: GASOLINE STATIONS

Name: FUEL AT THE FLAG
 Year: 2010
 Address: 1320 SE 15TH ST

Name: FUEL AT THE FLAG
 Year: 2011
 Address: 1320 SE 15TH ST

Name: FUEL AT THE FLAG
 Year: 2012
 Address: 1320 SE 15TH ST

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

**20 METRO MART
 1320 SE 15TH
 OKLAHOMA CITY, OK 73129**

**LUST U001885040
 UST N/A
 HIST UST
 RGA LUST**

LUST:

Facility ID: 5511727
 Case Number: 6S-1000
 Case Type: Suspicion of Release
Tank Type: UST
Release Date: 06/03/1996
Close Date: 07/18/1996
 Lat/Long: 35.4495 / -97.48669999
Status: Closed

UST:

Facility ID: 5511727
 Contact Name: Metro Mart Inc
 Contact Address: 1320 SE 15TH
 Contact City,St,Zip: Oklahoma City, OK 73129
 Lat/Long: 35.4495 / -97.486699

Tank ID: 1
 Tank Status: Currently In Use
 Total Capacity: 7943
 Substance: Gasoline
 Date Installed: 12/01/1983
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 2
 Tank Status: Currently In Use
 Total Capacity: 7943
 Substance: Gasoline
 Date Installed: 12/01/1983
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 3
 Tank Status: Currently In Use
 Total Capacity: 7943
 Substance: Diesel
 Date Installed: 12/01/1983
 Tank Type: UST
 Tank Material: Steel

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

METRO MART (Continued)

U001885040

Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

HIST UST:

Facility ID: 5511727
 Owner Name: Trishna Partnership
 Owner Address: 1103 Sovereign Row
 Owner City,St,Zip: Oklahoma City, OK 73108
 Tank ID: 1
 Tank Status: Currently in Use
 Installed Date: 12/1/1983 0:00:00
 Tank Capacity: 7943
 Product: Gasoline

Facility ID: 5511727
 Owner Name: Trishna Partnership
 Owner Address: 1103 Sovereign Row
 Owner City,St,Zip: Oklahoma City, OK 73108
 Tank ID: 2
 Tank Status: Currently in Use
 Installed Date: 12/1/1983 0:00:00
 Tank Capacity: 7943
 Product: Gasoline

Facility ID: 5511727
 Owner Name: Trishna Partnership
 Owner Address: 1103 Sovereign Row
 Owner City,St,Zip: Oklahoma City, OK 73108
 Tank ID: 3
 Tank Status: Currently in Use
 Installed Date: 12/1/1983 0:00:00
 Tank Capacity: 7943
 Product: Diesel

RGA LUST:

| | | |
|------|------------------|--------------|
| 2012 | METRO MART | 1320 SE 15TH |
| 2011 | METRO MART | 1320 SE 15TH |
| 2010 | METRO MART | 1320 SE 15TH |
| 2009 | METRO MART | 1320 SE 15TH |
| 2008 | FUEL AT THE FLAG | 1320 SE 15TH |
| 2007 | FUEL AT THE FLAG | 1320 SE 15TH |
| 2006 | FUEL AT THE FLAG | 1320 SE 15TH |
| 2005 | FUEL AT THE FLAG | 1320 SE 15TH |
| 2003 | FUEL AT THE FLAG | 1320 SE 15TH |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

METRO MART (Continued)

U001885040

| | | |
|------|------------------|--------------|
| 2002 | FUEL AT THE FLAG | 1320 SE 15TH |
| 2001 | FUEL AT THE FLAG | 1320 SE 15TH |
| 2000 | FUEL AT THE FLAG | 1320 SE 15TH |
| 1997 | FUEL AT THE FLAG | 1320 SE 15TH |

20

**SUNOCO GAS STATIONS
 1400 SE 15TH ST
 OKLAHOMA CITY, OK 73129**

**EDR US Hist Auto Stat 1014178156
 N/A**

EDR Historical Auto Stations:

| | |
|-------|---------------------|
| Name: | SUNOCO GAS STATIONS |
| Year: | 2000 |
| Type: | GASOLINE STATIONS |

20

**D X SERVICE STATION
 1400 15TH ST SE
 OKLAHOMA CITY, OK 73129**

**EDR US Hist Auto Stat 1014180175
 N/A**

EDR Historical Auto Stations:

| | |
|----------|-------------------------------------|
| Name: | JOLLYS CHAMPLIN SERVICE |
| Year: | 1965 |
| Type: | GASOLINE STATIONS |
| | |
| Name: | INTERSTATE CHAMPLIN SERVICE STATION |
| Year: | 1970 |
| Type: | GASOLINE STATIONS |
| | |
| Name: | MOORES CHAMPLIN SERVICE STATION |
| Year: | 1975 |
| Type: | GASOLINE STATIONS |
| | |
| Name: | D X SERVICE STATION |
| Year: | 1981 |
| Type: | GASOLINE STATIONS |
| | |
| Name: | D X SERVICE STATION |
| Year: | 1986 |
| Type: | GASOLINE STATIONS |
| | |
| Name: | FOUR SEASONS GAS |
| Year: | 2003 |
| Address: | 1400 SE 15TH ST |
| | |
| Name: | FOUR SEASONS GAS |
| Year: | 2004 |
| Address: | 1400 SE 15TH ST |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 Database(s)
 EPA ID Number

20 **EMBREE D X SERVICE** **EDR US Hist Auto Stat** **1014194752**
1300 15TH ST SE **N/A**
OKLAHOMA CITY, OK 73129

EDR Historical Auto Stations:

Name: LE ROYS D X SERVICE GAS
 Year: 1965
 Type: GASOLINE STATIONS

Name: EMBREE D X SERVICE
 Year: 1970
 Type: GASOLINE STATIONS

Name: LONNIES D X SERVICE
 Year: 1975
 Type: GASOLINE STATIONS

20 **FUEL AT THE FLAG NO 50 CONOCO** **EDR US Hist Auto Stat** **1014176004**
1300 15TH ST SE **N/A**
OKLAHOMA CITY S, OK 73129

EDR Historical Auto Stations:

Name: FUEL AT THE FLAG NO 50 CONOCO
 Year: 1993
 Type: GAS STATIONS

20 **CASEYS KERR MC GEE SERV STA** **EDR US Hist Auto Stat** **1014160908**
1301 15TH ST SE **N/A**
OKLAHOMA CITY, OK 73129

EDR Historical Auto Stations:

Name: K & M SERVICE STATION OR
 Year: 1965
 Type: GASOLINE STATIONS

Name: JIMS KERR MC GEE SERVICE STATION
 Year: 1970
 Type: GASOLINE STATIONS

Name: JOES KERR MC GEE SERVICE STATION
 Year: 1975
 Type: GASOLINE STATIONS

Name: RANDYS KERR MC GEE SERV STA E
 Year: 1981
 Type: GASOLINE STATIONS

Name: CASEYS KERR MC GEE SERV STA
 Year: 1986
 Type: GASOLINE STATIONS

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

20 **SUNOCO #23**
1400 SE 15TH
OKLAHOMA CITY, OK 73125

LUST **U001230463**
UST **N/A**
HIST UST
RGALUST

LUST:

Facility ID: 5511622
 Case Number: 064-2875
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 09/27/2004
Close Date: 08/21/2009
 Lat/Long: 35.4497 / -97.48569999
Status: Closed

UST:

Facility ID: 5511622
 Contact Name: French Distributing Co. Inc.
 Contact Address: P.O. Box 95635
 Contact City,St,Zip: Oklahoma City, OK 73143
 Lat/Long: 35.4497 / -97.485699

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 6000
 Substance: Gasoline
 Date Installed: 09/18/1989
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 6000
 Substance: Gasoline
 Date Installed: 09/18/1989
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 3
 Tank Status: Permanently Out Of Use
 Total Capacity: 8000
 Substance: Diesel
 Date Installed: 09/18/1989
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 4
 Tank Status: Permanently Out Of Use
 Total Capacity: 300
 Substance: Used Oil

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

SUNOCO #23 (Continued)

U001230463

Date Installed: Not reported
 Tank Type: UST

HIST UST:

Facility ID: 5511622
 Owner Name: French Distributing Co. Inc.
 Owner Address: P.O. Box 95635
 Owner City,St,Zip: Oklahoma City, OK 73143
 Tank ID: 1
 Tank Status: Currently in Use
 Installed Date: 9/18/1989 0:00:00
 Tank Capacity: 6000
 Product: Gasoline

Facility ID: 5511622
 Owner Name: French Distributing Co. Inc.
 Owner Address: P.O. Box 95635
 Owner City,St,Zip: Oklahoma City, OK 73143
 Tank ID: 2
 Tank Status: Currently in Use
 Installed Date: 9/18/1989 0:00:00
 Tank Capacity: 6000
 Product: Gasoline

Facility ID: 5511622
 Owner Name: French Distributing Co. Inc.
 Owner Address: P.O. Box 95635
 Owner City,St,Zip: Oklahoma City, OK 73143
 Tank ID: 3
 Tank Status: Currently in Use
 Installed Date: 9/18/1989 0:00:00
 Tank Capacity: 8000
 Product: Diesel

RGA LUST:

| | | |
|------|------------|--------------|
| 2012 | SUNOCO #23 | 1400 SE 15TH |
| 2011 | SUNOCO #23 | 1400 SE 15TH |
| 2010 | SUNOCO #23 | 1400 SE 15TH |
| 2009 | SUNOCO #23 | 1400 SE 15TH |
| 2008 | SUMOCO #23 | 1400 SE 15TH |
| 2007 | SUMOCO #23 | 1400 SE 15TH |
| 2006 | SUMOCO #23 | 1400 SE 15TH |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site
 Database(s)
 EPA ID Number
 EDR ID Number

20 **SUNOCO GAS ST** EDR US Hist Auto Stat 1014182805
1400 15TH ST SE
OKLAHOMA CITY S, OK 73129 N/A

EDR Historical Auto Stations:
 Name: SUNOCO GAS ST
 Year: 1993
 Type: GASOLINE STATIONS

20 **KERR MCGEE #121-6584** LUST U001228952
1301 SE 15TH UST N/A
OKLAHOMA CITY, OK 73129 HIST UST
 RGA LUST

LUST:
 Facility ID: 5500174
 Case Number: 064-0970
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 10/28/1993
Close Date: 03/09/2001
 Lat/Long: 35.449800000 / -97.48669999
Status: Closed

Facility ID: 5500174
 Case Number: 6C-167
 Case Type: Suspicion of Release
Tank Type: UST
Release Date: 09/29/1992
Close Date: 11/30/1992
 Lat/Long: 35.449800000 / -97.48669999
Status: Closed

UST:
 Facility ID: 5500174
 Contact Name: Kerr-Mcgee Refining Corporation
 Contact Address: P.O. Box 25861
 Contact City,St,Zip: Oklahoma City, OK 73125
 Lat/Long: 35.4498000 / -97.486699

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Diesel
 Date Installed: 12/07/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 12/07/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

KERR MCGEE #121-6584 (Continued)

U001228952

Pipe Material: Fiberglass

Tank ID: 3
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 12/07/1988
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

HIST UST:

Facility ID: 5500174
 Owner Name: Kerr-McGee Refining Corporation
 Owner Address: P.O. Box 25861
 Owner City,St,Zip: Oklahoma City, OK 73125
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: 12/7/1988 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5500174
 Owner Name: Kerr-McGee Refining Corporation
 Owner Address: P.O. Box 25861
 Owner City,St,Zip: Oklahoma City, OK 73125
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 12/7/1988 0:00:00
 Tank Capacity: 10000
 Product: Diesel

Facility ID: 5500174
 Owner Name: Kerr-McGee Refining Corporation
 Owner Address: P.O. Box 25861
 Owner City,St,Zip: Oklahoma City, OK 73125
 Tank ID: 3
 Tank Status: Permanently Out of Use
 Installed Date: 12/7/1988 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

RGA LUST:

| | | |
|------|----------------------|--------------|
| 2012 | KERR MCGEE #121-6584 | 1301 SE 15TH |
| 2011 | KERR MCGEE #121-6584 | 1301 SE 15TH |
| 2010 | KERR MCGEE #121-6584 | 1301 SE 15TH |
| 2009 | KERR MCGEE #121-6584 | 1301 SE 15TH |
| 2008 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2007 | KERR-MCGEE #121-6584 | 1301 SE 15TH |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

KERR MCGEE #121-6584 (Continued)

U001228952

| | | |
|------|------------------------------|--------------|
| 2006 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2006 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2005 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2005 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2003 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2003 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2002 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2002 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2001 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2001 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2000 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 2000 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 1997 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 1997 | KERR-MCGEE #121-6584 | 1301 SE 15TH |
| 1996 | KERR-MCGEE REFINING CORPORAT | 1301 SE 15TH |
| 1996 | KERR-MCGEE REFINING CORPORAT | 1301 SE 15TH |
| 1995 | KERR-MCGEE REFINING CORPORAT | 1301 SE 15TH |
| 1995 | KERR-MCGEE REFINING CORPORAT | 1301 SE 15TH |
| 1994 | KERR-MCGEE REFINING CORPORAT | 1301 SE 15TH |
| 1994 | KERR-MCGEE REFINING CORPORAT | 1301 SE 15TH |

21

**ROSEBUD COLN
 1141 15TH SE
 OKLAHOMA CITY, OK**

**EDR US Hist Cleaners 1014147845
 N/A**

EDR Historical Cleaners:

Name: ROSEBUD COLN
 Year: 1960
 Type: CLEANERS AND DYERS

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site Database(s) EPA ID Number
 EDR ID Number

21 TOP 0 THE TOWN INDRY A OR EDR US Hist Cleaners 1014143650
 1139 15TH SE N/A
 OKLAHOMA CITY, OK

EDR Historical Cleaners:
 Name: TOP 0 THE TOWN INDRY A OR
 Year: 1960
 Type: LAUNDRIES-SELF SERVE

22 HASTY LAUNDRY COIN EDR US Hist Cleaners 1014147051
 1212 15TH ST SE N/A
 OKLAHOMA CITY, OK 73129

EDR Historical Cleaners:
 Name: MAYTAG LAUNDRY COIN OPR
 Year: 1965
 Type: LAUNDRIES-SELF SERVE

 Name: HASTY LAUNDRY COIN
 Year: 1970
 Type: LAUNDRIES-SELF SERVICE

 Name: HASTY LAUNDRY COIN OPERATED
 Year: 1975
 Type: LAUNDRIES-SELF SERVE

23 7 ELEVEN #79 UST U003181959
 1000 SE 15TH ST HIST UST N/A
 OKLAHOMA CITY, OK 73129

UST:
 Facility ID: 5508069
 Contact Name: 7 Eleven LLC
 Contact Address: 2021 S MACARTHUR
 Contact City,St,Zip: Oklahoma City, OK 73128
 Lat/Long: 35.4495 / -97.494299

 Tank ID: 1
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 01/01/1986
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

 Tank ID: 2
 Tank Status: Currently In Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 01/01/1986
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number
 EPA ID Number

Database(s)

7 ELEVEN #79 (Continued)

U003181959

Tank ID: 3
 Tank Status: Currently In Use
 Total Capacity: 6000
 Substance: Gasoline
 Date Installed: 01/01/1986
 Tank Type: UST
 Tank Material: Fiberglass Reinforced Plastic
 Tank Construction: Single Walled
 Pipe Material: Fiberglass

HIST UST:

Facility ID: 5508069
 Owner Name: 7 ELEVEN INC
 Owner Address: 2021 S MACARTHUR
 Owner City,St,Zip: Oklahoma City, OK 73128
 Tank ID: 1
 Tank Status: Currently in Use
 Installed Date: 1/1/1986 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5508069
 Owner Name: 7 ELEVEN INC
 Owner Address: 2021 S MACARTHUR
 Owner City,St,Zip: Oklahoma City, OK 73128
 Tank ID: 3
 Tank Status: Currently in Use
 Installed Date: 1/1/1986 0:00:00
 Tank Capacity: 6000
 Product: Gasoline

Facility ID: 5508069
 Owner Name: 7 ELEVEN INC
 Owner Address: 2021 S MACARTHUR
 Owner City,St,Zip: Oklahoma City, OK 73128
 Tank ID: 2
 Tank Status: Currently in Use
 Installed Date: 1/1/1986 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

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**CENTRAL EXPLORATION CO., INC.
 1800 S JORDAN
 OKLAHOMA CITY, OK 73129**

**LUST U001885117
 UST N/A
 HIST UST
 RGA LUST**

LUST:

Facility ID: 5513045
 Case Number: 064-0815
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 03/01/1993
Close Date: 08/08/1995
 Lat/Long: 35.448300000 / -97.483800000
Status: Closed

Facility ID: 5513045

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CENTRAL EXPLORATION CO., INC. (Continued)

U001885117

Case Number: 064-1229
 Case Type: Confirmed Release
Tank Type: UST
Release Date: 10/20/1994
Close Date: 08/07/1995
 Lat/Long: 35.448300000 / -97.48380000
Status: Closed

UST:

Facility ID: 5513045
 Contact Name: Central Exploration Co., Inc.
 Contact Address: 7608 N. HUDSON
 Contact City,St,Zip: Oklahoma City, OK 73116
 Lat/Long: 35.4483000 / -97.483800

Tank ID: 1
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 12/01/1979
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 2
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Gasoline
 Date Installed: 12/01/1979
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 3
 Tank Status: Permanently Out Of Use
 Total Capacity: 10000
 Substance: Diesel
 Date Installed: 01/01/1979
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

Tank ID: 4
 Tank Status: Permanently Out Of Use
 Total Capacity: 500
 Substance: Used Oil
 Date Installed: 12/01/1979
 Tank Type: UST
 Tank Material: Steel
 Tank Construction: Single Walled
 Pipe Material: Steel

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CENTRAL EXPLORATION CO., INC. (Continued)

U001885117

HIST UST:

Facility ID: 5513045
 Owner Name: CENTRAL EXPLORATION CO., INC.
 Owner Address: 7608 N. HUDSON
 Owner City,St,Zip: Oklahoma City, OK 73116
 Tank ID: 1
 Tank Status: Permanently Out of Use
 Installed Date: 12/1/1979 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5513045
 Owner Name: CENTRAL EXPLORATION CO., INC.
 Owner Address: 7608 N. HUDSON
 Owner City,St,Zip: Oklahoma City, OK 73116
 Tank ID: 2
 Tank Status: Permanently Out of Use
 Installed Date: 12/1/1979 0:00:00
 Tank Capacity: 10000
 Product: Gasoline

Facility ID: 5513045
 Owner Name: CENTRAL EXPLORATION CO., INC.
 Owner Address: 7608 N. HUDSON
 Owner City,St,Zip: Oklahoma City, OK 73116
 Tank ID: 3
 Tank Status: Permanently Out of Use
 Installed Date: 1/1/1979 0:00:00
 Tank Capacity: 10000
 Product: Diesel

Facility ID: 5513045
 Owner Name: CENTRAL EXPLORATION CO., INC.
 Owner Address: 7608 N. HUDSON
 Owner City,St,Zip: Oklahoma City, OK 73116
 Tank ID: 4
 Tank Status: Permanently Out of Use
 Installed Date: 12/1/1979 0:00:00
 Tank Capacity: 500
 Product: Used Oil

RGA LUST:

| | | |
|------|-------------------------------|---------------|
| 2012 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2011 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2010 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2009 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2008 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2007 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2006 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |

MAP FINDINGS

Map ID
 Direction
 Distance
 Distance (ft.)Site

EDR ID Number

Database(s) EPA ID Number

CENTRAL EXPLORATION CO., INC. (Continued)

U001885117

| | | |
|------|-------------------------------|---------------|
| 2006 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2005 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2005 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2003 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2003 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2002 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2002 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2001 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2001 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2000 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 2000 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 1997 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 1997 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 1996 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 1996 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 1995 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |
| 1995 | CENTRAL EXPLORATION CO., INC. | 1800 S JORDAN |

ORPHAN SUMMARY

| City | EDR ID | Site Name | Site Address | Zip | Database(s) |
|---------------|------------|---|--|-------|-------------------|
| OKC | S109997332 | SWICK ALTERNATORS & STARTERS | 1300 N.E. 23RD | 73117 | SWRCY |
| OKC | S109997321 | METAL CHECK RECYCLER | S.E. 57TH AND HIGH. | 73129 | SWRCY |
| OKC | S109997333 | STANDARD IRON & METAL CO. INC. | 1501 E. RENO, MAILING P.O. BOX 302 73101 | 73117 | SWRCY |
| OKLAHOMA CITY | S114360456 | | 4801 N.W. 10 | | RGA LUST |
| OKLAHOMA CITY | 1003873966 | OKLAHOMA CITY DUMP-NE 10TH & GRAND BLVD | NE 10 AND GRAND BLVD | 73117 | CERC-NFRAP |
| OKLAHOMA CITY | S114361222 | | 841 S.W. 104TH | | RGA LUST |
| OKLAHOMA CITY | S114360333 | | 417 N.W. 106TH | | RGA LUST |
| OKLAHOMA CITY | S114358791 | | 1100 N.W. 108 | | RGA LUST |
| OKLAHOMA CITY | S114358774 | | 10TH AND PORTLAND | | RGA LUST |
| OKLAHOMA CITY | S114360469 | | 4912 N.W. 10TH | | RGA LUST |
| OKLAHOMA CITY | S114361907 | | NE 10TH AND VICKIE DR | | RGA LUST |
| OKLAHOMA CITY | S114360141 | | 3609 NW 10TH | | RGA LUST |
| OKLAHOMA CITY | S114361112 | | 7800 N.W. 10TH | | RGA LUST |
| OKLAHOMA CITY | S109376763 | MIDTOWN OKC | 318 NW 11TH ST. AND 1125 N. HARVEY AVE. | | VCP |
| OKLAHOMA CITY | S114360627 | | 5300 NE 122 STI-35 & NE 122 | | RGA LUST |
| OKLAHOMA CITY | S114358792 | | 1100 N.W. 122ND | | RGA LUST |
| OKLAHOMA CITY | S114362123 | | S.E. 13TH AND FAIRMONT | | RGA LUST |
| OKLAHOMA CITY | S114361331 | | 921 NE 13TH | | RGA LUST |
| OKLAHOMA CITY | S114361896 | | N.E. 13TH STR AT LINCOLN BLVD | | RGA LUST |
| OKLAHOMA CITY | S114361897 | | N.E. 13TH STR AT LINCOLN BLV | | RGA LUST |
| OKLAHOMA CITY | 1004578663 | PRESBYTERIAN HOSPITAL | NE 13TH & LINCOLN XRAY DEPT. | 73104 | RCRA-CESQG, FINDS |
| OKLAHOMA CITY | U004133015 | PRESBYTERIAN HOSPITAL, INC | NE 13TH STR AT LINCOLN BLVD | 73104 | UST |
| OKLAHOMA CITY | S110531937 | HYDROSTATIC TESTING | SE 13TH AND FAIRMONT | 73129 | LUST, RGA LUST |
| OKLAHOMA CITY | 1008341418 | OKC WAREHOUSE TIRE FIRE | 14TH ST. & ROBIN AVE. | | CERCLIS |
| OKLAHOMA CITY | U004132716 | S.W. TECHNICAL SCHOOL | 15TH AND S CENTRAL | 73104 | UST |
| OKLAHOMA CITY | S114359346 | | 1906 SW 15TH | | RGA LUST |
| OKLAHOMA CITY | S114361145 | | 8000 S.W. 15TH | | RGA LUST |
| OKLAHOMA CITY | S114359161 | | 1501 S.E. 15TH | | RGA LUST |
| OKLAHOMA CITY | S114359345 | | 1906 S.W. 15TH | | RGA LUST |
| OKLAHOMA CITY | S114359338 | | 1901 S.E. 15TH | | RGA LUST |
| OKLAHOMA CITY | S114359190 | | 15TH AND S. CENTRAL | | RGA LUST |
| OKLAHOMA CITY | S114359327 | | 18TH AND MERIDIAN | | RGA LUST |
| OKLAHOMA CITY | S114359611 | | 2211 NW 1ST STREET TERRACE | | RGA LUST |
| OKLAHOMA CITY | S114360451 | | 4720 S.W. 20TH | | RGA LUST |
| OKLAHOMA CITY | S114360154 | | 3644 S.W. 22ND | | RGA LUST |
| OKLAHOMA CITY | S114359642 | | 22ND & PENN SE CORNER | | RGA LUST |
| OKLAHOMA CITY | S114359551 | | 212 NW. 23RD | | RGA LUST |
| OKLAHOMA CITY | S114358577 | | 1000 NW 23RD STREET | | RGA LUST |
| OKLAHOMA CITY | S114360666 | | 5601 N.E. 23RD | | RGA LUST |
| OKLAHOMA CITY | S114358767 | | 10927 NE 23RD | | RGA LUST |
| OKLAHOMA CITY | S114359585 | | 218 S.E. 23RD | | RGA LUST |
| OKLAHOMA CITY | S114359087 | | 135 N.E. 23RD | | RGA LUST |
| OKLAHOMA CITY | S114359595 | | 2201 N.W. 23RD | | RGA LUST |

ORPHAN SUMMARY

| City | EDR ID | Site Name | Site Address | Zip | Database(s) |
|---------------|------------|-----------------------------------|--|-------|-----------------------|
| OKLAHOMA CITY | S114359645 | | 230 SE 25TH | | RGA LUST |
| OKLAHOMA CITY | S114359644 | | 230 S.E. 25TH | | RGA LUST |
| OKLAHOMA CITY | S114362187 | | SW 2ND AND ROBINSON | | RGA LUST |
| OKLAHOMA CITY | S114358649 | | 1015 SW 2ND STREET | | RGA LUST |
| OKLAHOMA CITY | S114360200 | | 3949 N.W. 36TH | | RGA LUST |
| OKLAHOMA CITY | S114361143 | | 8000 N.W. 39TH EXPRESSWAY | | RGA LUST |
| OKLAHOMA CITY | S114360860 | | 6334 N.W. 39TH | | RGA LUST |
| OKLAHOMA CITY | S114360373 | | 4301 N.W. 39TH | | RGA LUST |
| OKLAHOMA CITY | S114359288 | | 1800 NW 3RD | | RGA LUST |
| OKLAHOMA CITY | S114359108 | | 1401 S.W. 3RD | | RGA LUST |
| OKLAHOMA CITY | S109376623 | ALPHA DRUM | 424 S.E. 3RD; NE/4, NW/4, SECT. 3, T11N, R3 W | | VCP |
| OKLAHOMA CITY | S114359099 | | 1400 NE 4TH STR | | RGA LUST |
| OKLAHOMA CITY | S114361902 | | N.W. 50TH & MERIDIAN | | RGA LUST |
| OKLAHOMA CITY | S114361077 | | 7400 N.W. 50TH | | RGA LUST |
| OKLAHOMA CITY | S114362124 | | S.E. 52ND & HIGH STREET | | RGA LUST |
| OKLAHOMA CITY | S114359830 | | 2900 S.W. 59TH | | RGA LUST |
| OKLAHOMA CITY | S114361201 | | 820 NW. 5TH | | RGA LUST |
| OKLAHOMA CITY | S114360485 | | 5 N.W. 5TH | | RGA LUST |
| OKLAHOMA CITY | S114360396 | | 4417 N.W. 63RD | | RGA LUST |
| OKLAHOMA CITY | S114359262 | | 1707 N.W. 6TH | | RGA LUST |
| OKLAHOMA CITY | S114360087 | | 325 NW 6TH | | RGA LUST |
| OKLAHOMA CITY | S114359923 | | 301 NW 6TH | | RGA LUST |
| OKLAHOMA CITY | S114361086 | | 74TH & S HIGH (1200 SE 74TH) | | RGA LUST |
| OKLAHOMA CITY | S114359528 | | 2100 S.W. 74TH | | RGA LUST |
| OKLAHOMA CITY | S114361087 | | 74TH & S. HIGH | | RGA LUST |
| OKLAHOMA CITY | S114360391 | | 441 S.W. 74TH | | RGA LUST |
| OKLAHOMA CITY | S114360852 | | 630 SW 7TH STR | | RGA LUST |
| OKLAHOMA CITY | S114361325 | | 915 SW 7TH | | RGA LUST |
| OKLAHOMA CITY | S114361900 | | N.E. 8TH & STONEWALL | | RGA LUST |
| OKLAHOMA CITY | S114361910 | | NE 8TH STREET AND STONEWALL | | RGA LUST |
| OKLAHOMA CITY | S114362188 | | SW 9TH & WALKER | | RGA LUST |
| OKLAHOMA CITY | S108489625 | ODOT/40/PARCEL 127 | SW 9TH AND HARVEY | | VCP |
| OKLAHOMA CITY | A100349429 | DERICHEBOURG RECYCLING USA, INC | 100 BATH STREET | 73117 | AST |
| OKLAHOMA CITY | S114361169 | | 801 W BRITTON | | RGA LUST |
| OKLAHOMA CITY | S114359878 | | 300 E. CALIFORNIA STR | | RGA LUST |
| OKLAHOMA CITY | S114359618 | | 2220 SOUTH CENTRAL | | RGA LUST |
| OKLAHOMA CITY | S114359664 | | 2311 N. CENTRAL | | RGA LUST |
| OKLAHOMA CITY | S114359659 | | 2307 NORTH CENTRAL | | RGA LUST |
| OKLAHOMA CITY | S114362190 | | SW CORN. OF W. RENO AND E.K. GAYLORD BLVD | | RGA LUST |
| OKLAHOMA CITY | U004132829 | DEEP DEUCE AREA | SE CORNER OF NE 3RD & N WALNUT | 73104 | UST |
| OKLAHOMA CITY | 1012291809 | 2ND & LOTTIE- OG&E POTENTIAL SITE | NE CORNER OF 2ND & LOTTIE | | US BROWNFIELDS, FINDS |
| OKLAHOMA CITY | U004132816 | UNREGISTERED ABANDONED GAS STA. | SW CORNER OF W RENO & EK GAYLORD BLVD | 73104 | UST |
| OKLAHOMA CITY | 1012134471 | CHILDREN'S INVESTMENT ZONE, INC. | SE CORNER OF NE 16TH STREET & N. BRYANT AVENUE | | US BROWNFIELDS, FINDS |
| OKLAHOMA CITY | S114362204 | | SW CORNER OF RENO & LINCOLN | | RGA LUST |
| OKLAHOMA CITY | S114362136 | | SE CORNER 10TH & BROADWAY | | RGA LUST |
| OKLAHOMA CITY | S114362140 | | SE CORNER OF NE 3RD & WALNUT | | RGA LUST |
| OKLAHOMA CITY | S114362199 | | SW CORNER OF E. RENO AND BYARS AVE. | | RGA LUST |

ORPHAN SUMMARY

| City | EDR ID | Site Name | Site Address | Zip | Database(s) |
|---------------|------------|--|---|-------|--------------------------|
| OKLAHOMA CITY | U004132824 | OKLAHOMA SCHOOL OF SCIENCE AND MATHEMATICS | CORNER OF PARK AND N WALNUT | 73104 | UST |
| OKLAHOMA CITY | S109376671 | COTPA/METRO TRANSIT TERMINAL | CORNER OF NW 4TH ST. AND HUDSON AVE. | | VCP |
| OKLAHOMA CITY | S114362125 | | S.E. CORNER N.W. 23 & ROCKWEE | | RGA LUST |
| OKLAHOMA CITY | S109417782 | UNREGISTERED ABANDONED GAS STA. | SW CORNER OF W RENO & EK GAYLORD BLVD | 73104 | LUST, RGA LUST |
| OKLAHOMA CITY | S109417715 | DEEP DEUCE AREA | SE CORNER OF NE 3RD & N WALNUT | 73104 | LUST, RGA LUST |
| OKLAHOMA CITY | S114361163 | | 801 S COUNCIL RD I-40 AND COUNCIL RD | | RGA LUST |
| OKLAHOMA CITY | S114361164 | | 801 S COUNCIL RD I-40 AND COUNCIL RD | | RGA LUST |
| OKLAHOMA CITY | U003429227 | WALNUT PROPERTIES | N E HARRISON & WALNUT STREEETS | 73104 | HIST UST |
| OKLAHOMA CITY | S107517614 | 1111 SE 25TH MAGIC SVCS | 1111 S E 25TH | 73129 | DRYCLEANERS, AIRS |
| OKLAHOMA CITY | S114362163 | | SOUTH E. TO 8TH & SANTA FE N | | RGA LUST |
| OKLAHOMA CITY | S114362109 | | S E. TO 8TH & SANTA FE N OF | | RGA LUST |
| OKLAHOMA CITY | S112431647 | CROWLEY TAR PRODUCTS | 1/2 MI. EAST AND 1/4 MI. SOUTH OF THE I-35/I-240; | | VCP |
| OKLAHOMA CITY | S114360679 | | 5707 SOUTH EASTERN | | RGA LUST |
| OKLAHOMA CITY | S114359802 | | 2800 N EASTERN | | RGA LUST |
| OKLAHOMA CITY | A100365100 | COMPARATIVE MEDICINE | 1020 N EVEREST | 73104 | AST |
| OKLAHOMA CITY | A100353419 | COMPARATIVE MEDICINE EPG | 1024 N EVEREST | 73104 | AST |
| OKLAHOMA CITY | S114359029 | | 1300 W. EXCHANGE | | RGA LUST |
| OKLAHOMA CITY | S114360100 | | 3300 N.W. EXPRESSWAY | | RGA LUST |
| OKLAHOMA CITY | S114360104 | | 3301 N.W. EXPRESSWAY | | RGA LUST |
| OKLAHOMA CITY | S114360172 | | 3751 N.W. EXPRESSWAY | | RGA LUST |
| OKLAHOMA CITY | U004132817 | UNION PACIFIC RAILROAD | 200 FEET SW OF RENO AND STILES | 73104 | UST |
| OKLAHOMA CITY | S114360877 | | 6401 W. FRONTAGE RD | | RGA LUST |
| OKLAHOMA CITY | S114360876 | | 6401 W FRONTAGE RD | | RGA LUST |
| OKLAHOMA CITY | S114361232 | | 8704 S. GATEWAY TERRACE | | RGA LUST |
| OKLAHOMA CITY | 1000705844 | BHP PETROLEUM INC | 5555 NW GRAND | | RCRA NonGen / NLR, FINDS |
| OKLAHOMA CITY | U003998026 | WALNUT PROPERTIES | NE HARRISON & N WALNUT STREEETS | 73104 | UST |
| OKLAHOMA CITY | 1015958608 | 11TH & HARVEY PARKING LOT | N HARVEY BETWEEN 11TH STREET AND WEST PARK PLACE | | US BROWNFIELDS, FINDS |
| OKLAHOMA CITY | S114360441 | | 4700 SOUTH HIGH | | RGA LUST |
| OKLAHOMA CITY | S114360102 | | 3300 S. HIGH ST | | RGA LUST |
| OKLAHOMA CITY | S114360629 | | 5301 S HIGH STR | | RGA LUST |
| OKLAHOMA CITY | S114360177 | | 3800 SOUTH HIGH | | RGA LUST |
| OKLAHOMA CITY | S114360695 | | 5859 N.W. HIGHWAY | | RGA LUST |
| OKLAHOMA CITY | S114358956 | | 12020 N.W. HWY | | RGA LUST |
| OKLAHOMA CITY | S112431662 | IMPERIAL CLEANERS | 5816 NW HWY | | VCP |
| OKLAHOMA CITY | S114360980 | | 7012 NW HWY | | RGA LUST |
| OKLAHOMA CITY | S114360696 | | 5859 N.W. HWY | | RGA LUST |
| OKLAHOMA CITY | S114360910 | | 6800 N. I-35 & INDUSTRIAL BLVD | | RGA LUST |
| OKLAHOMA CITY | S114360412 | | 4500 S I-35 SERVICE ROAD | | RGA LUST |
| OKLAHOMA CITY | S114360656 | | 5501 S. I-35 | | RGA LUST |
| OKLAHOMA CITY | S114360539 | | 5016 S I-35 SERVICE RD | | RGA LUST |
| OKLAHOMA CITY | S114358977 | | 12204 N I-35 SERVICE ROAD | | RGA LUST |
| OKLAHOMA CITY | U004132873 | DROVER'S INN | 3809 S I-35 | 73129 | UST |
| OKLAHOMA CITY | S114361230 | | 8600 SO I-35 | | RGA LUST |
| OKLAHOMA CITY | S114360413 | | 4500 S. I-35 | | RGA LUST |
| OKLAHOMA CITY | U004132929 | LEVITZ FURNITURE | 4901 S I-35 | 73129 | UST |
| OKLAHOMA CITY | S114360616 | | 5220 S. I-35 | | RGA LUST |
| OKLAHOMA CITY | S114361726 | | HWY I-35 & I-240 | | RGA LUST |

ORPHAN SUMMARY

| City | EDR ID | Site Name | Site Address | Zip | Database(s) |
|---------------|------------|-----------------------------|---|-------|----------------|
| OKLAHOMA CITY | S114361727 | | HWY I-35 AND I-240 | | RG LUST |
| OKLAHOMA CITY | S114360294 | | 4050 WEST I-40 | | RG LUST |
| OKLAHOMA CITY | 1014958936 | BRIDGE #S 18771 AND 18773 | I-44 | 73117 | RCRA-LQG |
| OKLAHOMA CITY | S114359651 | | 2301 E I-44 | | RG LUST |
| OKLAHOMA CITY | S106523188 | JOE BROWN | 5501 S IH 35 | 73129 | Ind. Haz Waste |
| OKLAHOMA CITY | S106895886 | CHARLIE O BUS. PARK | 2680 WEST INTERSTATE 40 | | VCP |
| OKLAHOMA CITY | S114361231 | | 8601 S INTERSTATE #35 | | RG LUST |
| OKLAHOMA CITY | S114359292 | | 1800 S. JORDAN | | RG LUST |
| OKLAHOMA CITY | 1014825585 | I-40/I-44 K INTERCHANGE | I-40/I-44 K INTERCHANGE, | | FINDS |
| OKLAHOMA CITY | S114361078 | | 7401 N. KELLEY | | RG LUST |
| OKLAHOMA CITY | S114360116 | | 3400 N. KELLEY | | RG LUST |
| OKLAHOMA CITY | S114360170 | | 3737 N. LINCOLN | | RG LUST |
| OKLAHOMA CITY | 1011860541 | BRICKTOWN FIRE STATION | 21 N LINCOLN | 73104 | US BROWNFIELDS |
| OKLAHOMA CITY | S114359691 | | 2401 N LINCOLN (TUNNEL-SEQUOYAH- WILL ROGERS) | | RG LUST |
| OKLAHOMA CITY | S114359903 | | 3001 N. LINCOLN | | RG LUST |
| OKLAHOMA CITY | S112431670 | OKC- BRICKTOWN FIRE STATION | 100 LINCOLN | | VCP |
| OKLAHOMA CITY | S114359692 | | 2401 N LINCOLN (TUNNEL-SEQUOYAH-WILL ROGERS) | | RG LUST |
| OKLAHOMA CITY | S114359690 | | 2401 N LINCOLN (TUNNEL-SEQUOYAH- WILL ROGERS) | | RG LUST |
| OKLAHOMA CITY | S114359689 | | 2401 N LINCOLN (TUNNEL-SEQUO | | RG LUST |
| OKLAHOMA CITY | S114360162 | | 3700 NORTH LINCOLN | | RG LUST |
| OKLAHOMA CITY | S114359819 | | 2827 W. LINDLEY STR | | RG LUST |
| OKLAHOMA CITY | S114359817 | | 2827 W. LINDLEY ST | | RG LUST |
| OKLAHOMA CITY | S114361288 | | 901 N LOTTIE | | RG LUST |
| OKLAHOMA CITY | S114358566 | | 100 S. LOTTIE | | RG LUST |
| OKLAHOMA CITY | S114359224 | | 1627 W. MAIN ST. | | RG LUST |
| OKLAHOMA CITY | S114359322 | | 1845 W. MAIN | | RG LUST |
| OKLAHOMA CITY | S114359321 | | 1840 W. MAIN | | RG LUST |
| OKLAHOMA CITY | S114359226 | | 1628 WEST MAIN STREET | | RG LUST |
| OKLAHOMA CITY | S114361315 | | 911-913 W. MAIN | | RG LUST |
| OKLAHOMA CITY | S114359089 | | 1350 WEST MAIN | | RG LUST |
| OKLAHOMA CITY | S114358568 | | 100 W MAIN | | RG LUST |
| OKLAHOMA CITY | S114359315 | | 1830 WEST MAIN ST | | RG LUST |
| OKLAHOMA CITY | S114359316 | | 1830 WEST MAIN STR | | RG LUST |
| OKLAHOMA CITY | S114359225 | | 1627 W. MAIN STR | | RG LUST |
| OKLAHOMA CITY | S114359227 | | 1628 WEST MAIN STR | | RG LUST |
| OKLAHOMA CITY | S114358805 | | 1101 N. MARTIN LUTHER KING | | RG LUST |
| OKLAHOMA CITY | S114359403 | | 20 S. MARTIN LUTHER KING | | RG LUST |
| OKLAHOMA CITY | S114359193 | | 1600 N. MARTIN LUTHER KING | | RG LUST |
| OKLAHOMA CITY | S114359760 | | 2620 N. MARTIN LUTHER KING BOULEVARD | | RG LUST |
| OKLAHOMA CITY | U004132984 | CATO OIL & GREASE CO. | 915 MARTIN LUTHER KING BLVD | 73117 | UST |
| OKLAHOMA CITY | S114360551 | | 5031 N MARTIN L. KING BLVD | | RG LUST |
| OKLAHOMA CITY | S114359800 | | 2800 MARTIN LUTHER KING | | RG LUST |
| OKLAHOMA CITY | S114359715 | | 2445 N MARTIN LUTHER KING BLVD | | RG LUST |
| OKLAHOMA CITY | S114359714 | | 2445 N MARTIN LUTHER KING BL | | RG LUST |
| OKLAHOMA CITY | S114361092 | | 7508 W MELROSE LANE | | RG LUST |
| OKLAHOMA CITY | S114358821 | | 1101 W. MEMORIAL | | RG LUST |
| OKLAHOMA CITY | S114358592 | | 1001 W. MEMORIAL | | RG LUST |

ORPHAN SUMMARY

| City | EDR ID | Site Name | Site Address | Zip | Database(s) |
|---------------|------------|------------------------------|---|-------|--------------------------|
| OKLAHOMA CITY | S114361872 | | MOTOR POOL DIVISION 2307 N CENTRAL | | RGALUST |
| OKLAHOMA CITY | S114361873 | | MOTOR POOL DIVISION 2307 N CENTRAL | | RGALUST |
| OKLAHOMA CITY | S109376733 | HARRISON LEVY CO. (REALTORS) | SW OF INTERSECTION OF NW 10TH AND CLASSEN | | VCP |
| OKLAHOMA CITY | 1000882451 | SUN OIL CO J M YOUNG | 3M W OF EDMOND | | RCRA NonGen / NLR, FINDS |
| OKLAHOMA CITY | S114360832 | | 621 N. OKLAHOMA ST. | | RGALUST |
| OKLAHOMA CITY | 1000882421 | SUN OIL CO JONES-HARN | IN OKLAHOMA CITY | | RCRA NonGen / NLR, FINDS |
| OKLAHOMA CITY | 1000882420 | SUN OIL CO HARDCASTLE | IN OKLAHOMA CITY | | RCRA NonGen / NLR, FINDS |
| OKLAHOMA CITY | 1000882444 | SUN OIL CO STATE LEASE UNIT | IN OKLAHOMA CITY | | RCRA NonGen / NLR, FINDS |
| OKLAHOMA CITY | S114359848 | | 2920 S. OKLAHOMA | | RGALUST |
| OKLAHOMA CITY | 1012127873 | BRICKTOWN FIRE STATION | NW QUADRANT OF NORTH LINCOLN BLVD & E SHERIDIAN AV SECTION 3 | 73104 | US BROWNFIELDS |
| OKLAHOMA CITY | S114362006 | | RELAY STATION (OKO) 1600 E RENO | | RGALUST |
| OKLAHOMA CITY | S114362005 | | RELAY STATION (OKO) 1600 E RENO | | RGALUST |
| OKLAHOMA CITY | S114362254 | | W. RENO & GAYLORD | | RGALUST |
| OKLAHOMA CITY | S114358675 | | 10220 W. RENO | | RGALUST |
| OKLAHOMA CITY | S114359342 | | 1903 EAST RENO | | RGALUST |
| OKLAHOMA CITY | S114359191 | | 1600 E. RENO | | RGALUST |
| OKLAHOMA CITY | S114362251 | | W RENO & GAYLORD | | RGALUST |
| OKLAHOMA CITY | S114359207 | | 1601 W. RENO | | RGALUST |
| OKLAHOMA CITY | S114358803 | | 1101 E. RENO | | RGALUST |
| OKLAHOMA CITY | S114360402 | | 4430 WEST RENO | | RGALUST |
| OKLAHOMA CITY | S114361342 | | 927 EAST RENO | | RGALUST |
| OKLAHOMA CITY | S114359901 | | 3000 WEST RENO | | RGALUST |
| OKLAHOMA CITY | S114358825 | | 1101 WEST RENO | | RGALUST |
| OKLAHOMA CITY | S114360004 | | 3105 EAST RENO | | RGALUST |
| OKLAHOMA CITY | S114359628 | | 2230 W RENO AT AGNEW | | RGALUST |
| OKLAHOMA CITY | S114360380 | | 4309 E. RENO | | RGALUST |
| OKLAHOMA CITY | S114359629 | | 2230 WEST RENO AT AGNEW | | RGALUST |
| OKLAHOMA CITY | S114362252 | | W RENO & GAYLORD | | RGALUST |
| OKLAHOMA CITY | S114358968 | | 12142 W. RENO | | RGALUST |
| OKLAHOMA CITY | S114359882 | | 300 EAST RENO | | RGALUST |
| OKLAHOMA CITY | S114359158 | | 1501 E. RENO P.O. BOX 302 | | RGALUST |
| OKLAHOMA CITY | S114359368 | | 1935 E. RENO | | RGALUST |
| OKLAHOMA CITY | S114360639 | | 5323 W. RENO | | RGALUST |
| OKLAHOMA CITY | S114360690 | | 5800 WEST RENO | | RGALUST |
| OKLAHOMA CITY | S114359731 | | 2501 W. RENO | | RGALUST |
| OKLAHOMA CITY | S114359163 | | 1501 W. RENO | | RGALUST |
| OKLAHOMA CITY | S114360354 | | 4220 WEST RENO | | RGALUST |
| OKLAHOMA CITY | S114359732 | | 2503 WEST RENO | | RGALUST |
| OKLAHOMA CITY | S114359765 | | 2627 WEST RENO | | RGALUST |
| OKLAHOMA CITY | U004132857 | SOUTHWESTERN BELL TELEPHONE | 600 E RENO | 73104 | UST |
| OKLAHOMA CITY | S114359341 | | 1903 E RENO | | RGALUST |
| OKLAHOMA CITY | S114358796 | | 1100 W. RENO | | RGALUST |
| OKLAHOMA CITY | S114359237 | | 1645 WEST RENO | | RGALUST |
| OKLAHOMA CITY | S114359371 | | 1948 E. RENO | | RGALUST |
| OKLAHOMA CITY | S114359162 | | 1501 W RENO | | RGALUST |
| OKLAHOMA CITY | A100384003 | PHF RESEARCH PARK | 865 RESEARCH PARK | 73104 | AST |

ORPHAN SUMMARY

| City | EDR ID | Site Name | Site Address | Zip | Database(s) |
|-----------------|------------|--|--|-------|-------------------|
| OKLAHOMA CITY | S114360677 | | 5700 N. ROCKWELL MAIN FUEL | | RGA LUST |
| OKLAHOMA CITY | S114360676 | | 5700 N. ROCKWELL MAIN FUEL | | RGA LUST |
| OKLAHOMA CITY | S114359633 | | 224 RUSSELL M. PERRY | | RGA LUST |
| OKLAHOMA CITY | S114361054 | | 726 W SHERIDAN | | RGA LUST |
| OKLAHOMA CITY | S114359267 | | 1715 E SKYLINE DR | | RGA LUST |
| OKLAHOMA CITY | S114359101 | | 1400 SE SKYLINE DR. | | RGA LUST |
| OKLAHOMA CITY | S114359344 | | 1905 SKYLINE DRIVE | | RGA LUST |
| OKLAHOMA CITY | S114359269 | | 1715 SKYLINE DRIVE | | RGA LUST |
| OKLAHOMA CITY | S114359268 | | 1715 E. SKYLINE DR | | RGA LUST |
| OKLAHOMA CITY | S114359300 | | 1801 SE SKYLINE DRIVE | | RGA LUST |
| OKLAHOMA CITY | S112431655 | EVANS TRANSPORT GASOLINE SPILL, I35S-I40W, OKC, ECLS# 121095 | I-35 SOUTH AT I-40 WEST | | VCP |
| OKLAHOMA CITY | S114360198 | | 3941 NORTH STILES | | RGA LUST |
| OKLAHOMA CITY | S112010760 | UNIVERSITY HOSPITAL HELIPAD | 804 N STONEWALL | 73104 | LUST, RGA LUST |
| OKLAHOMA CITY | S114362214 | | TINKER AFB | | RGA LUST |
| OKLAHOMA CITY | S114362144 | | SE TO 8TH & SANTA FE N OF ST | | RGA LUST |
| OKLAHOMA CITY | S114362143 | | SE TO 8TH & SANTA FE N OF STOCK | | RGA LUST |
| OKLAHOMA CITY | S114360680 | | 5708 N W EXPRESSWAY | | RGA LUST |
| OKLAHOMA CITY | S114359213 | | 1607 N. W. BRITTON ROAD | | RGA LUST |
| OKLAHOMA CITY | S114360335 | | 4179 N. W. 23RD | | RGA LUST |
| OKLAHOMA CITY | S114359820 | | 2828 N. W. 112TH | | RGA LUST |
| OKLAHOMA CITY | S114360664 | | 5600 S. W. 5TH | | RGA LUST |
| OKLAHOMA CITY | S114359837 | | 2901 N. W. 10TH | | RGA LUST |
| OKLAHOMA CITY | S114359533 | | 2101 S. W. 89TH | | RGA LUST |
| OKLAHOMA CITY | S114361291 | | 901 S. W. 74 | | RGA LUST |
| OKLAHOMA CITY | S110335579 | OKC EDUCARE REMED SITE - OKC MAPS SCHOOLS S.E. GRAND | WESTERN PARCEL, S SIDE OF S. BYERS AND S.E. GRAND | | INST CONTROL, VCP |
| OKLAHOMA COUNTY | S107030615 | TOWN OF HARRAH DISPOSAL SITE | CORNER OF HOLDEN STREET & COUNTY ROAD STREET | | SWF/LF |
| OKLAHOMA COUNTY | S106496826 | PICKERING ENVIRONMENTAL SOIL WASHERS | SW CORNER OF SE/4 OF S31 T12N R4W (8301 NW 2ND ST.) | | SWF/LF |
| OKLAHOMA COUNTY | S106496811 | RUFUS BRYANT & SON LANDFILL | GOVERNMENT LOTS 7 & 8 IN NW/4 OF S31 T12N R4W | | SWF/LF |
| OKLAHOMA COUNTY | S106496824 | AMERICAN MEDICAL DISPOSAL, INC. | LOTS 41 - 44, SWASTIKA ADDITION, A SUBDIVISION OF BLOCK 8, P | | SWF/LF |
| OKLAHOMA COUNTY | S106496805 | SPANN LANDFILL | LOTS 6, 7, 11 - 49, BLOCK 32, EAST OKLAHOMA CITY ADDITION, D | | SWF/LF |
| OKLAHOMA COUNTY | S106496818 | LAND RECLAIMERS, INC. LANDFILL | NW/4 NE/4 NW/4 & E/2 NE/4 NE/4 NW/4 OF S31 T12N R3W (NW 10TH | | SWF/LF |
| OKLAHOMA COUNTY | S107030616 | OKLAHOMA CITY SLUDGE LANDFILL #1 | N/2 NE/4 OF S21 T12N R2W | | SWF/LF |
| OKLAHOMA COUNTY | S106496825 | EDMOND YARD WASTE COMPOSTING FACILITY | NE/4 NW/4 OF S16 T14N R3W | | SWF/LF |
| OKLAHOMA COUNTY | S106496813 | TINKER AFB LANDFILL | NE/4 NW/4 OF S25 T11N R2W | | SWF/LF |
| OKLAHOMA COUNTY | S106474339 | EDMOND SOLID WASTE TRANSFER STATION | SE/4 OF S16 T14N R2W (1/4 MI EAST OF COVELL RD. & I-35) | | SWF/LF |
| OKLAHOMA COUNTY | S106496806 | CITY OF MIDWEST CITY LANDFILL | SW/4 OF S15 T12N R2W | | SWF/LF |
| OKLAHOMA COUNTY | S106496830 | DAYTON TIRE & RUBBER COMPANY | W/2 OF S8 T11N R4W | | SWF/LF |
| OKLAHOMA COUNTY | S106496821 | STOCKYARDS RECYCLERS, L.L.C. | SW/4 OF S6 T11N R3W | | SWF/LF |
| OKLAHOMA COUNTY | S106496828 | CITY OF OKLAHOMA CITY HOUSEHOLD HAZARDOUS WASTE COLLECTION F | NE/4 OF S11 T11N R4W | | SWF/LF |
| OKLAHOMA COUNTY | S106496820 | VILLA LANDFILL | NW/4 OF S31 T12N R3W (NW 10TH & VILLA) | | SWF/LF |
| OKLAHOMA COUNTY | S106496807 | CUSTOM EQUIPMENT COMPANY LANDFILL | SE/4 OF S25 T11N R3W | | SWF/LF |

ORPHAN SUMMARY

| City | EDR ID | Site Name | Site Address | Zip | Database(s) |
|-----------------|------------|---|---|-----|-------------|
| OKLAHOMA COUNTY | S106496819 | MIDWEST CID LANDFILL | NW/4 OF S24 T11N R2W | | SWF/LF |
| OKLAHOMA COUNTY | S108061238 | WM HEALTHCARE SOLUTIONS (FKA ENSERV MIDWEST, LLC; AND ERS) | S/2 OF S6 T11N R4W (8800 SW 8TH ST.) | | SWF/LF |
| OKLAHOMA COUNTY | S106496809 | CITY OF DEL CITY LANDFILL | NW/4 OF S32 T12N R2W LYING NORTH OF FRISCO RAILROAD (NE 10TH | | SWF/LF |
| OKLAHOMA COUNTY | S106496817 | OKLAHOMA CITY LANDFILL COMPANY | NE/4 OF S8 T11N R4W & GOVERNMENT LOTS 5, 6, 7, & SW/4 NW/4 O | | SWF/LF |
| OKLAHOMA COUNTY | S106496808 | A-1 SANITARY LANDFILL | NE/4 OF S21 T12N R2W | | SWF/LF |
| OKLAHOMA COUNTY | M300002665 | NOBLE MATERIALS INC | OKLAHOMA CITY PLANT | | US MINES |
| OKLAHOMA COUNTY | M300004197 | ACME BRICK COMPANY | OKLAHOMA CITY | | US MINES |
| OKLAHOMA COUNTY | S106496810 | MIDWEST MAINTENANCE & CONSTRUCTION COMPA LANDFILL | S/2 S/2 NW/4 OF S24 T11N R2W | | SWF/LF |
| OKLAHOMA COUNTY | S106496815 | CITY OF DEL CITY MUNICIPAL INCINERATOR | NE/4 SW/4 OF S32 T12N R2W(1/4 MI. E OF SUNNYLANE ON NE 4TH S | | SWF/LF |
| OKLAHOMA COUNTY | S106496827 | EDMOND SOLID WASTE TRANSFER STATION | S/2 SW/4 SW/4, & SW/4 SE/4 SW/4, & S 50' OF N/2 SW/4 SW/4 & | | SWF/LF |
| OKLAHOMA COUNTY | S106496822 | CMI ENERGY CONVERSION SYSTEMS WASTE-TO-ENERGY FACILITY | S6 T11N R3W (801 S. AGNEW) | | SWF/LF |
| OKLAHOMA COUNTY | S106496823 | SOUTHEAST LANDFILL | S25 T11N R3W | | SWF/LF |
| OKLAHOMA COUNTY | S106496812 | WMO-MOSLEY ROAD LANDFILL | S21 T12N R2W(MOSLEY RD. BETWEEN NE 23RD & 36TH) | | SWF/LF |
| OKLAHOMA COUNTY | S106496816 | NORTH CANADIAN WW TREATMENT PLANT | S16 T13N R1W | | SWF/LF |

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

To maintain currency of the following federal and state databases, EDR contacts the appropriate governmental agency on a monthly or quarterly basis, as required.

Number of Days to Update: Provides confirmation that EDR is reporting records that have been updated within 90 days from the date the government agency made the information available to the public.

FEDERAL RECORDS

NPL: National Priority List

National Priorities List (Superfund). The NPL is a subset of CERCLIS and identifies over 1,200 sites for priority cleanup under the Superfund Program. NPL sites may encompass relatively large areas. As such, EDR provides polygon coverage for over 1,000 NPL site boundaries produced by EPA's Environmental Photographic Interpretation Center (EPIC) and regional EPA offices.

| | |
|---|--|
| Date of Government Version: 04/26/2013 | Source: EPA |
| Date Data Arrived at EDR: 05/09/2013 | Telephone: N/A |
| Date Made Active in Reports: 07/10/2013 | Last EDR Contact: 11/11/2013 |
| Number of Days to Update: 62 | Next Scheduled EDR Contact: 01/20/2014 |
| | Data Release Frequency: Quarterly |

NPL Site Boundaries

Sources:

EPA's Environmental Photographic Interpretation Center (EPIC)
Telephone: 202-564-7333

EPA Region 1
Telephone 617-918-1143

EPA Region 6
Telephone: 214-655-6659

EPA Region 3
Telephone 215-814-5418

EPA Region 7
Telephone: 913-551-7247

EPA Region 4
Telephone 404-562-8033

EPA Region 8
Telephone: 303-312-6774

EPA Region 5
Telephone 312-886-6686

EPA Region 9
Telephone: 415-947-4246

EPA Region 10
Telephone 206-553-8665

Proposed NPL: Proposed National Priority List Sites

A site that has been proposed for listing on the National Priorities List through the issuance of a proposed rule in the Federal Register. EPA then accepts public comments on the site, responds to the comments, and places on the NPL those sites that continue to meet the requirements for listing.

| | |
|---|--|
| Date of Government Version: 04/26/2013 | Source: EPA |
| Date Data Arrived at EDR: 05/09/2013 | Telephone: N/A |
| Date Made Active in Reports: 07/10/2013 | Last EDR Contact: 11/11/2013 |
| Number of Days to Update: 62 | Next Scheduled EDR Contact: 01/20/2014 |
| | Data Release Frequency: Quarterly |

DELISTED NPL: National Priority List Deletions

The National Oil and Hazardous Substances Pollution Contingency Plan (NCP) establishes the criteria that the EPA uses to delete sites from the NPL. In accordance with 40 CFR 300.425.(e), sites may be deleted from the NPL where no further response is appropriate.

| | |
|---|--|
| Date of Government Version: 04/26/2013 | Source: EPA |
| Date Data Arrived at EDR: 05/09/2013 | Telephone: N/A |
| Date Made Active in Reports: 07/10/2013 | Last EDR Contact: 11/11/2013 |
| Number of Days to Update: 62 | Next Scheduled EDR Contact: 01/20/2014 |
| | Data Release Frequency: Quarterly |

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

NPL LIENS: Federal Superfund Liens

Federal Superfund Liens. Under the authority granted the USEPA by CERCLA of 1980, the USEPA has the authority to file liens against real property in order to recover remedial action expenditures or when the property owner received notification of potential liability. USEPA compiles a listing of filed notices of Superfund Liens.

| | |
|---|---|
| Date of Government Version: 10/15/1991 | Source: EPA |
| Date Data Arrived at EDR: 02/02/1994 | Telephone: 202-564-4267 |
| Date Made Active in Reports: 03/30/1994 | Last EDR Contact: 08/15/2011 |
| Number of Days to Update: 56 | Next Scheduled EDR Contact: 11/28/2011 |
| | Data Release Frequency: No Update Planned |

CERCLIS: Comprehensive Environmental Response, Compensation, and Liability Information System

CERCLIS contains data on potentially hazardous waste sites that have been reported to the USEPA by states, municipalities, private companies and private persons, pursuant to Section 103 of the Comprehensive Environmental Response, Compensation, and Liability Act (CERCLA). CERCLIS contains sites which are either proposed to or on the National Priorities List (NPL) and sites which are in the screening and assessment phase for possible inclusion on the NPL.

| | |
|---|--|
| Date of Government Version: 04/26/2013 | Source: EPA |
| Date Data Arrived at EDR: 05/29/2013 | Telephone: 703-412-9810 |
| Date Made Active in Reports: 08/09/2013 | Last EDR Contact: 11/11/2013 |
| Number of Days to Update: 72 | Next Scheduled EDR Contact: 03/10/2014 |
| | Data Release Frequency: Quarterly |

CERCLIS-NFRAP: CERCLIS No Further Remedial Action Planned

Archived sites are sites that have been removed and archived from the inventory of CERCLIS sites. Archived status indicates that, to the best of EPA's knowledge, assessment at a site has been completed and that EPA has determined no further steps will be taken to list this site on the National Priorities List (NPL), unless information indicates this decision was not appropriate or other considerations require a recommendation for listing at a later time. This decision does not necessarily mean that there is no hazard associated with a given site; it only means that, based upon available information, the location is not judged to be a potential NPL site.

| | |
|---|--|
| Date of Government Version: 04/26/2013 | Source: EPA |
| Date Data Arrived at EDR: 05/29/2013 | Telephone: 703-412-9810 |
| Date Made Active in Reports: 08/09/2013 | Last EDR Contact: 11/11/2013 |
| Number of Days to Update: 72 | Next Scheduled EDR Contact: 03/10/2014 |
| | Data Release Frequency: Quarterly |

LIENS 2: CERCLA Lien Information

A Federal CERCLA ('Superfund') lien can exist by operation of law at any site or property at which EPA has spent Superfund monies. These monies are spent to investigate and address releases and threatened releases of contamination. CERCLIS provides information as to the identity of these sites and properties.

| | |
|---|---|
| Date of Government Version: 02/06/2013 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 04/25/2013 | Telephone: 202-564-6023 |
| Date Made Active in Reports: 05/10/2013 | Last EDR Contact: 11/13/2013 |
| Number of Days to Update: 15 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Varies |

CORRACTS: Corrective Action Report

CORRACTS identifies hazardous waste handlers with RCRA corrective action activity.

| | |
|---|--|
| Date of Government Version: 09/10/2013 | Source: EPA |
| Date Data Arrived at EDR: 10/02/2013 | Telephone: 800-424-9346 |
| Date Made Active in Reports: 12/16/2013 | Last EDR Contact: 01/02/2014 |
| Number of Days to Update: 75 | Next Scheduled EDR Contact: 04/14/2014 |
| | Data Release Frequency: Quarterly |

RCRA-TSDF: RCRA - Treatment, Storage and Disposal

RCRAInfo is EPA's comprehensive information system, providing access to data supporting the Resource Conservation and Recovery Act (RCRA) of 1976 and the Hazardous and Solid Waste Amendments (HSWA) of 1984. The database includes selective information on sites which generate, transport, store, treat and/or dispose of hazardous waste as defined by the Resource Conservation and Recovery Act (RCRA). Transporters are individuals or entities that move hazardous waste from the generator offsite to a facility that can recycle, treat, store, or dispose of the waste. TSDFs treat, store, or dispose of the waste.

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

Date of Government Version: 09/10/2013
Date Data Arrived at EDR: 10/02/2013
Date Made Active in Reports: 12/16/2013
Number of Days to Update: 75

Source: Environmental Protection Agency
Telephone: 214-665-6444
Last EDR Contact: 01/02/2014
Next Scheduled EDR Contact: 04/14/2014
Data Release Frequency: Quarterly

RCRA-LQG: RCRA - Large Quantity Generators

RCRAInfo is EPA's comprehensive information system, providing access to data supporting the Resource Conservation and Recovery Act (RCRA) of 1976 and the Hazardous and Solid Waste Amendments (HSWA) of 1984. The database includes selective information on sites which generate, transport, store, treat and/or dispose of hazardous waste as defined by the Resource Conservation and Recovery Act (RCRA). Large quantity generators (LQGs) generate over 1,000 kilograms (kg) of hazardous waste, or over 1 kg of acutely hazardous waste per month.

Date of Government Version: 09/10/2013
Date Data Arrived at EDR: 10/02/2013
Date Made Active in Reports: 12/16/2013
Number of Days to Update: 75

Source: Environmental Protection Agency
Telephone: 214-665-6444
Last EDR Contact: 01/02/2014
Next Scheduled EDR Contact: 04/14/2014
Data Release Frequency: Quarterly

RCRA-SQG: RCRA - Small Quantity Generators

RCRAInfo is EPA's comprehensive information system, providing access to data supporting the Resource Conservation and Recovery Act (RCRA) of 1976 and the Hazardous and Solid Waste Amendments (HSWA) of 1984. The database includes selective information on sites which generate, transport, store, treat and/or dispose of hazardous waste as defined by the Resource Conservation and Recovery Act (RCRA). Small quantity generators (SQGs) generate between 100 kg and 1,000 kg of hazardous waste per month.

Date of Government Version: 09/10/2013
Date Data Arrived at EDR: 10/02/2013
Date Made Active in Reports: 12/16/2013
Number of Days to Update: 75

Source: Environmental Protection Agency
Telephone: 214-665-6444
Last EDR Contact: 01/02/2014
Next Scheduled EDR Contact: 04/14/2014
Data Release Frequency: Quarterly

RCRA-CESQG: RCRA - Conditionally Exempt Small Quantity Generators

RCRAInfo is EPA's comprehensive information system, providing access to data supporting the Resource Conservation and Recovery Act (RCRA) of 1976 and the Hazardous and Solid Waste Amendments (HSWA) of 1984. The database includes selective information on sites which generate, transport, store, treat and/or dispose of hazardous waste as defined by the Resource Conservation and Recovery Act (RCRA). Conditionally exempt small quantity generators (CESQGs) generate less than 100 kg of hazardous waste, or less than 1 kg of acutely hazardous waste per month.

Date of Government Version: 09/10/2013
Date Data Arrived at EDR: 10/02/2013
Date Made Active in Reports: 12/16/2013
Number of Days to Update: 75

Source: Environmental Protection Agency
Telephone: 214-665-6444
Last EDR Contact: 01/02/2014
Next Scheduled EDR Contact: 04/14/2014
Data Release Frequency: Varies

RCRA NonGen / NLR: RCRA - Non Generators

RCRAInfo is EPA's comprehensive information system, providing access to data supporting the Resource Conservation and Recovery Act (RCRA) of 1976 and the Hazardous and Solid Waste Amendments (HSWA) of 1984. The database includes selective information on sites which generate, transport, store, treat and/or dispose of hazardous waste as defined by the Resource Conservation and Recovery Act (RCRA). Non-Generators do not presently generate hazardous waste.

Date of Government Version: 09/10/2013
Date Data Arrived at EDR: 10/02/2013
Date Made Active in Reports: 12/16/2013
Number of Days to Update: 75

Source: Environmental Protection Agency
Telephone: 214-665-6444
Last EDR Contact: 01/02/2014
Next Scheduled EDR Contact: 04/14/2014
Data Release Frequency: Varies

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

US ENG CONTROLS: Engineering Controls Sites List

A listing of sites with engineering controls in place. Engineering controls include various forms of caps, building foundations, liners, and treatment methods to create pathway elimination for regulated substances to enter environmental media or effect human health.

| | |
|---|---|
| Date of Government Version: 06/17/2013 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 06/21/2013 | Telephone: 703-603-0695 |
| Date Made Active in Reports: 10/03/2013 | Last EDR Contact: 12/09/2013 |
| Number of Days to Update: 104 | Next Scheduled EDR Contact: 03/24/2014 |
| | Data Release Frequency: Varies |

US INST CONTROL: Sites with Institutional Controls

A listing of sites with institutional controls in place. Institutional controls include administrative measures, such as groundwater use restrictions, construction restrictions, property use restrictions, and post remediation care requirements intended to prevent exposure to contaminants remaining on site. Deed restrictions are generally required as part of the institutional controls.

| | |
|---|---|
| Date of Government Version: 06/17/2013 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 06/21/2013 | Telephone: 703-603-0695 |
| Date Made Active in Reports: 10/03/2013 | Last EDR Contact: 12/09/2013 |
| Number of Days to Update: 104 | Next Scheduled EDR Contact: 03/24/2014 |
| | Data Release Frequency: Varies |

ERNS: Emergency Response Notification System

Emergency Response Notification System. ERNS records and stores information on reported releases of oil and hazardous substances.

| | |
|---|---|
| Date of Government Version: 09/30/2013 | Source: National Response Center, United States Coast Guard |
| Date Data Arrived at EDR: 10/01/2013 | Telephone: 202-267-2180 |
| Date Made Active in Reports: 12/06/2013 | Last EDR Contact: 12/30/2013 |
| Number of Days to Update: 66 | Next Scheduled EDR Contact: 04/14/2014 |
| | Data Release Frequency: Annually |

HMIRS: Hazardous Materials Information Reporting System

Hazardous Materials Incident Report System. HMIRS contains hazardous material spill incidents reported to DOT.

| | |
|---|---|
| Date of Government Version: 09/30/2013 | Source: U.S. Department of Transportation |
| Date Data Arrived at EDR: 10/01/2013 | Telephone: 202-366-4555 |
| Date Made Active in Reports: 12/16/2013 | Last EDR Contact: 01/03/2014 |
| Number of Days to Update: 76 | Next Scheduled EDR Contact: 01/13/2014 |
| | Data Release Frequency: Annually |

DOT OPS: Incident and Accident Data

Department of Transportation, Office of Pipeline Safety Incident and Accident data.

| | |
|---|---|
| Date of Government Version: 07/31/2012 | Source: Department of Transportation, Office of Pipeline Safety |
| Date Data Arrived at EDR: 08/07/2012 | Telephone: 202-366-4595 |
| Date Made Active in Reports: 09/18/2012 | Last EDR Contact: 11/06/2013 |
| Number of Days to Update: 42 | Next Scheduled EDR Contact: 02/17/2014 |
| | Data Release Frequency: Varies |

US CDL: Clandestine Drug Labs

A listing of clandestine drug lab locations. The U.S. Department of Justice ("the Department") provides this web site as a public service. It contains addresses of some locations where law enforcement agencies reported they found chemicals or other items that indicated the presence of either clandestine drug laboratories or dumpsites. In most cases, the source of the entries is not the Department, and the Department has not verified the entry and does not guarantee its accuracy. Members of the public must verify the accuracy of all entries by, for example, contacting local law enforcement and local health departments.

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

Date of Government Version: 08/06/2013
Date Data Arrived at EDR: 09/11/2013
Date Made Active in Reports: 10/03/2013
Number of Days to Update: 22

Source: Drug Enforcement Administration
Telephone: 202-307-1000
Last EDR Contact: 12/05/2013
Next Scheduled EDR Contact: 03/17/2014
Data Release Frequency: Quarterly

US BROWNFIELDS: A Listing of Brownfields Sites

Brownfields are real property, the expansion, redevelopment, or reuse of which may be complicated by the presence or potential presence of a hazardous substance, pollutant, or contaminant. Cleaning up and reinvesting in these properties takes development pressures off of undeveloped, open land, and both improves and protects the environment. Assessment, Cleanup and Redevelopment Exchange System (ACRES) stores information reported by EPA Brownfields grant recipients on brownfields properties assessed or cleaned up with grant funding as well as information on Targeted Brownfields Assessments performed by EPA Regions. A listing of ACRES Brownfield sites is obtained from Cleanups in My Community. Cleanups in My Community provides information on Brownfields properties for which information is reported back to EPA, as well as areas served by Brownfields grant programs.

Date of Government Version: 09/24/2013
Date Data Arrived at EDR: 09/24/2013
Date Made Active in Reports: 12/06/2013
Number of Days to Update: 73

Source: Environmental Protection Agency
Telephone: 202-566-2777
Last EDR Contact: 12/24/2013
Next Scheduled EDR Contact: 04/07/2014
Data Release Frequency: Semi-Annually

DOD: Department of Defense Sites

This data set consists of federally owned or administered lands, administered by the Department of Defense, that have any area equal to or greater than 640 acres of the United States, Puerto Rico, and the U.S. Virgin Islands.

Date of Government Version: 12/31/2005
Date Data Arrived at EDR: 11/10/2006
Date Made Active in Reports: 01/11/2007
Number of Days to Update: 62

Source: USGS
Telephone: 888-275-8747
Last EDR Contact: 10/18/2013
Next Scheduled EDR Contact: 01/27/2014
Data Release Frequency: Semi-Annually

FUDS: Formerly Used Defense Sites

The listing includes locations of Formerly Used Defense Sites properties where the US Army Corps of Engineers is actively working or will take necessary cleanup actions.

Date of Government Version: 12/31/2011
Date Data Arrived at EDR: 02/26/2013
Date Made Active in Reports: 03/13/2013
Number of Days to Update: 15

Source: U.S. Army Corps of Engineers
Telephone: 202-528-4285
Last EDR Contact: 12/13/2013
Next Scheduled EDR Contact: 03/24/2014
Data Release Frequency: Varies

LUCIS: Land Use Control Information System

LUCIS contains records of land use control information pertaining to the former Navy Base Realignment and Closure properties.

Date of Government Version: 08/20/2013
Date Data Arrived at EDR: 08/23/2013
Date Made Active in Reports: 11/01/2013
Number of Days to Update: 70

Source: Department of the Navy
Telephone: 843-820-7326
Last EDR Contact: 11/18/2013
Next Scheduled EDR Contact: 03/03/2014
Data Release Frequency: Varies

CONSENT: Superfund (CERCLA) Consent Decrees

Major legal settlements that establish responsibility and standards for cleanup at NPL (Superfund) sites. Released periodically by United States District Courts after settlement by parties to litigation matters.

Date of Government Version: 06/30/2013
Date Data Arrived at EDR: 08/07/2013
Date Made Active in Reports: 10/03/2013
Number of Days to Update: 57

Source: Department of Justice, Consent Decree Library
Telephone: Varies
Last EDR Contact: 12/26/2013
Next Scheduled EDR Contact: 04/14/2014
Data Release Frequency: Varies

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

ROD: Records Of Decision

Record of Decision. ROD documents mandate a permanent remedy at an NPL (Superfund) site containing technical and health information to aid in the cleanup.

| | |
|---|--|
| Date of Government Version: 04/26/2013 | Source: EPA |
| Date Data Arrived at EDR: 06/11/2013 | Telephone: 703-416-0223 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 12/12/2013 |
| Number of Days to Update: 143 | Next Scheduled EDR Contact: 03/24/2014 |
| | Data Release Frequency: Annually |

UMTRA: Uranium Mill Tailings Sites

Uranium ore was mined by private companies for federal government use in national defense programs. When the mills shut down, large piles of the sand-like material (mill tailings) remain after uranium has been extracted from the ore. Levels of human exposure to radioactive materials from the piles are low; however, in some cases tailings were used as construction materials before the potential health hazards of the tailings were recognized.

| | |
|---|--|
| Date of Government Version: 09/14/2010 | Source: Department of Energy |
| Date Data Arrived at EDR: 10/07/2011 | Telephone: 505-845-0011 |
| Date Made Active in Reports: 03/01/2012 | Last EDR Contact: 11/26/2013 |
| Number of Days to Update: 146 | Next Scheduled EDR Contact: 03/10/2014 |
| | Data Release Frequency: Varies |

ODI: Open Dump Inventory

An open dump is defined as a disposal facility that does not comply with one or more of the Part 257 or Part 258 Subtitle D Criteria.

| | |
|---|---|
| Date of Government Version: 06/30/1985 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 08/09/2004 | Telephone: 800-424-9346 |
| Date Made Active in Reports: 09/17/2004 | Last EDR Contact: 06/09/2004 |
| Number of Days to Update: 39 | Next Scheduled EDR Contact: N/A |
| | Data Release Frequency: No Update Planned |

DEBRIS REGION 9: Torres Martinez Reservation Illegal Dump Site Locations

A listing of illegal dump sites location on the Torres Martinez Indian Reservation located in eastern Riverside County and northern Imperial County, California.

| | |
|---|---|
| Date of Government Version: 01/12/2009 | Source: EPA, Region 9 |
| Date Data Arrived at EDR: 05/07/2009 | Telephone: 415-947-4219 |
| Date Made Active in Reports: 09/21/2009 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 137 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: No Update Planned |

US MINES: Mines Master Index File

Contains all mine identification numbers issued for mines active or opened since 1971. The data also includes violation information.

| | |
|---|--|
| Date of Government Version: 08/01/2013 | Source: Department of Labor, Mine Safety and Health Administration |
| Date Data Arrived at EDR: 09/05/2013 | Telephone: 303-231-5959 |
| Date Made Active in Reports: 10/03/2013 | Last EDR Contact: 12/06/2013 |
| Number of Days to Update: 28 | Next Scheduled EDR Contact: 03/17/2014 |
| | Data Release Frequency: Semi-Annually |

TRIS: Toxic Chemical Release Inventory System

Toxic Release Inventory System. TRIS identifies facilities which release toxic chemicals to the air, water and land in reportable quantities under SARA Title III Section 313.

| | |
|---|--|
| Date of Government Version: 12/31/2011 | Source: EPA |
| Date Data Arrived at EDR: 07/31/2013 | Telephone: 202-566-0250 |
| Date Made Active in Reports: 09/13/2013 | Last EDR Contact: 11/27/2013 |
| Number of Days to Update: 44 | Next Scheduled EDR Contact: 03/10/2014 |
| | Data Release Frequency: Annually |

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

TSCA: Toxic Substances Control Act

Toxic Substances Control Act. TSCA identifies manufacturers and importers of chemical substances included on the TSCA Chemical Substance Inventory list. It includes data on the production volume of these substances by plant site.

| | |
|---|--|
| Date of Government Version: 12/31/2006 | Source: EPA |
| Date Data Arrived at EDR: 09/29/2010 | Telephone: 202-260-5521 |
| Date Made Active in Reports: 12/02/2010 | Last EDR Contact: 12/26/2013 |
| Number of Days to Update: 64 | Next Scheduled EDR Contact: 04/07/2014 |
| | Data Release Frequency: Every 4 Years |

FTTS: FIFRA/ TSCA Tracking System - FIFRA (Federal Insecticide, Fungicide, & Rodenticide Act)/TSCA (Toxic Substances Control Act)
FTTS tracks administrative cases and pesticide enforcement actions and compliance activities related to FIFRA, TSCA and EPCRA (Emergency Planning and Community Right-to-Know Act). To maintain currency, EDR contacts the Agency on a quarterly basis.

| | |
|---|---|
| Date of Government Version: 04/09/2009 | Source: EPA/Office of Prevention, Pesticides and Toxic Substances |
| Date Data Arrived at EDR: 04/16/2009 | Telephone: 202-566-1667 |
| Date Made Active in Reports: 05/11/2009 | Last EDR Contact: 11/21/2013 |
| Number of Days to Update: 25 | Next Scheduled EDR Contact: 03/10/2014 |
| | Data Release Frequency: Quarterly |

FTTS INSP: FIFRA/ TSCA Tracking System - FIFRA (Federal Insecticide, Fungicide, & Rodenticide Act)/TSCA (Toxic Substances Control Act)
A listing of FIFRA/TSCA Tracking System (FTTS) inspections and enforcements.

| | |
|---|--|
| Date of Government Version: 04/09/2009 | Source: EPA |
| Date Data Arrived at EDR: 04/16/2009 | Telephone: 202-566-1667 |
| Date Made Active in Reports: 05/11/2009 | Last EDR Contact: 11/21/2014 |
| Number of Days to Update: 25 | Next Scheduled EDR Contact: 03/10/2014 |
| | Data Release Frequency: Quarterly |

HIST FTTS: FIFRA/TSCA Tracking System Administrative Case Listing

A complete administrative case listing from the FIFRA/TSCA Tracking System (FTTS) for all ten EPA regions. The information was obtained from the National Compliance Database (NCDB). NCDB supports the implementation of FIFRA (Federal Insecticide, Fungicide, and Rodenticide Act) and TSCA (Toxic Substances Control Act). Some EPA regions are now closing out records. Because of that, and the fact that some EPA regions are not providing EPA Headquarters with updated records, it was decided to create a HIST FTTS database. It included records that may not be included in the newer FTTS database updates. This database is no longer updated.

| | |
|---|---|
| Date of Government Version: 10/19/2006 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 03/01/2007 | Telephone: 202-564-2501 |
| Date Made Active in Reports: 04/10/2007 | Last EDR Contact: 12/17/2007 |
| Number of Days to Update: 40 | Next Scheduled EDR Contact: 03/17/2008 |
| | Data Release Frequency: No Update Planned |

HIST FTTS INSP: FIFRA/TSCA Tracking System Inspection & Enforcement Case Listing

A complete inspection and enforcement case listing from the FIFRA/TSCA Tracking System (FTTS) for all ten EPA regions. The information was obtained from the National Compliance Database (NCDB). NCDB supports the implementation of FIFRA (Federal Insecticide, Fungicide, and Rodenticide Act) and TSCA (Toxic Substances Control Act). Some EPA regions are now closing out records. Because of that, and the fact that some EPA regions are not providing EPA Headquarters with updated records, it was decided to create a HIST FTTS database. It included records that may not be included in the newer FTTS database updates. This database is no longer updated.

| | |
|---|---|
| Date of Government Version: 10/19/2006 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 03/01/2007 | Telephone: 202-564-2501 |
| Date Made Active in Reports: 04/10/2007 | Last EDR Contact: 12/17/2008 |
| Number of Days to Update: 40 | Next Scheduled EDR Contact: 03/17/2008 |
| | Data Release Frequency: No Update Planned |

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

SSTS: Section 7 Tracking Systems

Section 7 of the Federal Insecticide, Fungicide and Rodenticide Act, as amended (92 Stat. 829) requires all registered pesticide-producing establishments to submit a report to the Environmental Protection Agency by March 1st each year. Each establishment must report the types and amounts of pesticides, active ingredients and devices being produced, and those having been produced and sold or distributed in the past year.

| | |
|---|--|
| Date of Government Version: 12/31/2009 | Source: EPA |
| Date Data Arrived at EDR: 12/10/2010 | Telephone: 202-564-4203 |
| Date Made Active in Reports: 02/25/2011 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 77 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Annually |

ICIS: Integrated Compliance Information System

The Integrated Compliance Information System (ICIS) supports the information needs of the national enforcement and compliance program as well as the unique needs of the National Pollutant Discharge Elimination System (NPDES) program.

| | |
|---|---|
| Date of Government Version: 07/20/2011 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 11/10/2011 | Telephone: 202-564-5088 |
| Date Made Active in Reports: 01/10/2012 | Last EDR Contact: 10/09/2014 |
| Number of Days to Update: 61 | Next Scheduled EDR Contact: 01/27/2014 |
| | Data Release Frequency: Quarterly |

PADS: PCB Activity Database System

PCB Activity Database. PADS Identifies generators, transporters, commercial storers and/or brokers and disposers of PCB's who are required to notify the EPA of such activities.

| | |
|---|--|
| Date of Government Version: 06/01/2013 | Source: EPA |
| Date Data Arrived at EDR: 07/17/2013 | Telephone: 202-566-0500 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 10/18/2013 |
| Number of Days to Update: 107 | Next Scheduled EDR Contact: 01/27/2014 |
| | Data Release Frequency: Annually |

MLTS: Material Licensing Tracking System

MLTS is maintained by the Nuclear Regulatory Commission and contains a list of approximately 8,100 sites which possess or use radioactive materials and which are subject to NRC licensing requirements. To maintain currency, EDR contacts the Agency on a quarterly basis.

| | |
|---|--|
| Date of Government Version: 07/22/2013 | Source: Nuclear Regulatory Commission |
| Date Data Arrived at EDR: 08/02/2013 | Telephone: 301-415-7169 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 12/09/2013 |
| Number of Days to Update: 91 | Next Scheduled EDR Contact: 03/24/2014 |
| | Data Release Frequency: Quarterly |

RADINFO: Radiation Information Database

The Radiation Information Database (RADINFO) contains information about facilities that are regulated by U.S. Environmental Protection Agency (EPA) regulations for radiation and radioactivity.

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|---|---|
| Date of Government Version: 09/30/2013 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 10/09/2013 | Telephone: 202-343-9775 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 10/09/2013 |
| Number of Days to Update: 23 | Next Scheduled EDR Contact: 01/20/2014 |
| | Data Release Frequency: Quarterly |

FINDS: Facility Index System/Facility Registry System

Facility Index System. FINDS contains both facility information and 'pointers' to other sources that contain more detail. EDR includes the following FINDS databases in this report: PCS (Permit Compliance System), AIRS (Aerometric Information Retrieval System), DOCKET (Enforcement Docket used to manage and track information on civil judicial enforcement cases for all environmental statutes), FURS (Federal Underground Injection Control), C-DOCKET (Criminal Docket System used to track criminal enforcement actions for all environmental statutes), FFIS (Federal Facilities Information System), STATE (State Environmental Laws and Statutes), and PADS (PCB Activity Data System).

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

Date of Government Version: 03/08/2013
Date Data Arrived at EDR: 03/21/2013
Date Made Active in Reports: 07/10/2013
Number of Days to Update: 111

Source: EPA
Telephone: (214) 665-2200
Last EDR Contact: 12/10/2013
Next Scheduled EDR Contact: 03/24/2014
Data Release Frequency: Quarterly

RAATS: RCRA Administrative Action Tracking System

RCRA Administration Action Tracking System. RAATS contains records based on enforcement actions issued under RCRA pertaining to major violators and includes administrative and civil actions brought by the EPA. For administration actions after September 30, 1995, data entry in the RAATS database was discontinued. EPA will retain a copy of the database for historical records. It was necessary to terminate RAATS because a decrease in agency resources made it impossible to continue to update the information contained in the database.

Date of Government Version: 04/17/1995
Date Data Arrived at EDR: 07/03/1995
Date Made Active in Reports: 08/07/1995
Number of Days to Update: 35

Source: EPA
Telephone: 202-564-4104
Last EDR Contact: 06/02/2008
Next Scheduled EDR Contact: 09/01/2008
Data Release Frequency: No Update Planned

RMP: Risk Management Plans

When Congress passed the Clean Air Act Amendments of 1990, it required EPA to publish regulations and guidance for chemical accident prevention at facilities using extremely hazardous substances. The Risk Management Program Rule (RMP Rule) was written to implement Section 112(r) of these amendments. The rule, which built upon existing industry codes and standards, requires companies of all sizes that use certain flammable and toxic substances to develop a Risk Management Program, which includes a(n): Hazard assessment that details the potential effects of an accidental release, an accident history of the last five years, and an evaluation of worst-case and alternative accidental releases; Prevention program that includes safety precautions and maintenance, monitoring, and employee training measures; and Emergency response program that spells out emergency health care, employee training measures and procedures for informing the public and response agencies (e.g the fire department) should an accident occur.

Date of Government Version: 05/08/2012
Date Data Arrived at EDR: 05/25/2012
Date Made Active in Reports: 07/10/2012
Number of Days to Update: 46

Source: Environmental Protection Agency
Telephone: 202-564-8600
Last EDR Contact: 10/28/2013
Next Scheduled EDR Contact: 02/11/2014
Data Release Frequency: Varies

BRS: Biennial Reporting System

The Biennial Reporting System is a national system administered by the EPA that collects data on the generation and management of hazardous waste. BRS captures detailed data from two groups: Large Quantity Generators (LQG) and Treatment, Storage, and Disposal Facilities.

Date of Government Version: 12/31/2011
Date Data Arrived at EDR: 02/26/2013
Date Made Active in Reports: 04/19/2013
Number of Days to Update: 52

Source: EPA/NTIS
Telephone: 800-424-9346
Last EDR Contact: 11/25/2013
Next Scheduled EDR Contact: 03/10/2014
Data Release Frequency: Biennially

PRP: Potentially Responsible Parties

A listing of verified Potentially Responsible Parties

Date of Government Version: 04/15/2013
Date Data Arrived at EDR: 07/03/2013
Date Made Active in Reports: 09/13/2013
Number of Days to Update: 72

Source: EPA
Telephone: 202-564-6023
Last EDR Contact: 01/02/2014
Next Scheduled EDR Contact: 04/14/2014
Data Release Frequency: Quarterly

LEAD SMELTER 2: Lead Smelter Sites

A list of several hundred sites in the U.S. where secondary lead smelting was done from 1931 and 1964. These sites may pose a threat to public health through ingestion or inhalation of contaminated soil or dust

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

Date of Government Version: 04/05/2001
Date Data Arrived at EDR: 10/27/2010
Date Made Active in Reports: 12/02/2010
Number of Days to Update: 36

Source: American Journal of Public Health
Telephone: 703-305-6451
Last EDR Contact: 12/02/2009
Next Scheduled EDR Contact: N/A
Data Release Frequency: No Update Planned

LEAD SMELTER 1: Lead Smelter Sites

A listing of former lead smelter site locations.

Date of Government Version: 01/29/2013
Date Data Arrived at EDR: 02/14/2013
Date Made Active in Reports: 02/27/2013
Number of Days to Update: 13

Source: Environmental Protection Agency
Telephone: 703-603-8787
Last EDR Contact: 01/03/2014
Next Scheduled EDR Contact: 04/21/2014
Data Release Frequency: Varies

2020 COR ACTION: 2020 Corrective Action Program List

The EPA has set ambitious goals for the RCRA Corrective Action program by creating the 2020 Corrective Action Universe. This RCRA cleanup baseline includes facilities expected to need corrective action. The 2020 universe contains a wide variety of sites. Some properties are heavily contaminated while others were contaminated but have since been cleaned up. Still others have not been fully investigated yet, and may require little or no remediation. Inclusion in the 2020 Universe does not necessarily imply failure on the part of a facility to meet its RCRA obligations.

Date of Government Version: 11/11/2011
Date Data Arrived at EDR: 05/18/2012
Date Made Active in Reports: 05/25/2012
Number of Days to Update: 7

Source: Environmental Protection Agency
Telephone: 703-308-4044
Last EDR Contact: 11/15/2013
Next Scheduled EDR Contact: 02/24/2014
Data Release Frequency: Varies

PCB TRANSFORMER: PCB Transformer Registration Database

The database of PCB transformer registrations that includes all PCB registration submittals.

Date of Government Version: 02/01/2011
Date Data Arrived at EDR: 10/19/2011
Date Made Active in Reports: 01/10/2012
Number of Days to Update: 83

Source: Environmental Protection Agency
Telephone: 202-566-0517
Last EDR Contact: 11/01/2013
Next Scheduled EDR Contact: 02/11/2014
Data Release Frequency: Varies

US HIST CDL: National Clandestine Laboratory Register

A listing of clandestine drug lab locations. The U.S. Department of Justice ("the Department") provides this web site as a public service. It contains addresses of some locations where law enforcement agencies reported they found chemicals or other items that indicated the presence of either clandestine drug laboratories or dumpsites. In most cases, the source of the entries is not the Department, and the Department has not verified the entry and does not guarantee its accuracy. Members of the public must verify the accuracy of all entries by, for example, contacting local law enforcement and local health departments.

Date of Government Version: 09/01/2007
Date Data Arrived at EDR: 11/19/2008
Date Made Active in Reports: 03/30/2009
Number of Days to Update: 131

Source: Drug Enforcement Administration
Telephone: 202-307-1000
Last EDR Contact: 03/23/2009
Next Scheduled EDR Contact: 06/22/2009
Data Release Frequency: No Update Planned

US AIRS MINOR: Air Facility System Data

A listing of minor source facilities.

Date of Government Version: 10/23/2013
Date Data Arrived at EDR: 11/06/2013
Date Made Active in Reports: 12/06/2013
Number of Days to Update: 30

Source: EPA
Telephone: 202-564-5962
Last EDR Contact: 12/26/2013
Next Scheduled EDR Contact: 04/14/2014
Data Release Frequency: Annually

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

US AIRS (AFS): Aerometric Information Retrieval System Facility Subsystem (AFS)

The database is a sub-system of Aerometric Information Retrieval System (AIRS). AFS contains compliance data on air pollution point sources regulated by the U.S. EPA and/or state and local air regulatory agencies. This information comes from source reports by various stationary sources of air pollution, such as electric power plants, steel mills, factories, and universities, and provides information about the air pollutants they produce. Action, air program, air program pollutant, and general level plant data. It is used to track emissions and compliance data from industrial plants.

| | |
|---|--|
| Date of Government Version: 10/23/2013 | Source: EPA |
| Date Data Arrived at EDR: 11/06/2013 | Telephone: 202-564-5962 |
| Date Made Active in Reports: 12/06/2013 | Last EDR Contact: 12/26/2013 |
| Number of Days to Update: 30 | Next Scheduled EDR Contact: 04/14/2014 |
| | Data Release Frequency: Annually |

FEDERAL FACILITY: Federal Facility Site Information listing

A listing of National Priority List (NPL) and Base Realignment and Closure (BRAC) sites found in the Comprehensive Environmental Response, Compensation and Liability Information System (CERCLIS) Database where EPA Federal Facilities Restoration and Reuse Office is involved in cleanup activities.

| | |
|---|---|
| Date of Government Version: 05/31/2013 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 07/08/2013 | Telephone: 703-603-8704 |
| Date Made Active in Reports: 12/06/2013 | Last EDR Contact: 10/11/2013 |
| Number of Days to Update: 151 | Next Scheduled EDR Contact: 01/20/2014 |
| | Data Release Frequency: Varies |

COAL ASH EPA: Coal Combustion Residues Surface Impoundments List

A listing of coal combustion residues surface impoundments with high hazard potential ratings.

| | |
|---|---|
| Date of Government Version: 08/17/2010 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 01/03/2011 | Telephone: N/A |
| Date Made Active in Reports: 03/21/2011 | Last EDR Contact: 12/13/2013 |
| Number of Days to Update: 77 | Next Scheduled EDR Contact: 03/24/2014 |
| | Data Release Frequency: Varies |

FEMA UST: Underground Storage Tank Listing

A listing of all FEMA owned underground storage tanks.

| | |
|---|--|
| Date of Government Version: 01/01/2010 | Source: FEMA |
| Date Data Arrived at EDR: 02/16/2010 | Telephone: 202-646-5797 |
| Date Made Active in Reports: 04/12/2010 | Last EDR Contact: 10/17/2013 |
| Number of Days to Update: 55 | Next Scheduled EDR Contact: 01/27/2014 |
| | Data Release Frequency: Varies |

US FIN ASSUR: Financial Assurance Information

All owners and operators of facilities that treat, store, or dispose of hazardous waste are required to provide proof that they will have sufficient funds to pay for the clean up, closure, and post-closure care of their facilities.

| | |
|---|---|
| Date of Government Version: 10/28/2013 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 10/29/2013 | Telephone: 202-566-1917 |
| Date Made Active in Reports: 12/06/2013 | Last EDR Contact: 11/18/2013 |
| Number of Days to Update: 38 | Next Scheduled EDR Contact: 03/03/2014 |
| | Data Release Frequency: Quarterly |

COAL ASH DOE: Steam-Electric Plan Operation Data

A listing of power plants that store ash in surface ponds.

| | |
|---|--|
| Date of Government Version: 12/31/2005 | Source: Department of Energy |
| Date Data Arrived at EDR: 08/07/2009 | Telephone: 202-586-8719 |
| Date Made Active in Reports: 10/22/2009 | Last EDR Contact: 12/10/2013 |
| Number of Days to Update: 76 | Next Scheduled EDR Contact: 03/24/2014 |
| | Data Release Frequency: Varies |

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

SCRD DRYCLEANERS: State Coalition for Remediation of Drycleaners Listing

The State Coalition for Remediation of Drycleaners was established in 1998, with support from the U.S. EPA Office of Superfund Remediation and Technology Innovation. It is comprised of representatives of states with established drycleaner remediation programs. Currently the member states are Alabama, Connecticut, Florida, Illinois, Kansas, Minnesota, Missouri, North Carolina, Oregon, South Carolina, Tennessee, Texas, and Wisconsin.

| | |
|---|---|
| Date of Government Version: 03/07/2011 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 03/09/2011 | Telephone: 615-532-8599 |
| Date Made Active in Reports: 05/02/2011 | Last EDR Contact: 11/18/2013 |
| Number of Days to Update: 54 | Next Scheduled EDR Contact: 02/03/2014 |
| | Data Release Frequency: Varies |

EPA WATCH LIST: EPA WATCH LIST

EPA maintains a "Watch List" to facilitate dialogue between EPA, state and local environmental agencies on enforcement matters relating to facilities with alleged violations identified as either significant or high priority. Being on the Watch List does not mean that the facility has actually violated the law only that an investigation by EPA or a state or local environmental agency has led those organizations to allege that an unproven violation has in fact occurred. Being on the Watch List does not represent a higher level of concern regarding the alleged violations that were detected, but instead indicates cases requiring additional dialogue between EPA, state and local agencies - primarily because of the length of time the alleged violation has gone unaddressed or unresolved.

| | |
|---|---|
| Date of Government Version: 06/30/2013 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 08/13/2013 | Telephone: 617-520-3000 |
| Date Made Active in Reports: 09/13/2013 | Last EDR Contact: 11/15/2013 |
| Number of Days to Update: 31 | Next Scheduled EDR Contact: 02/24/2014 |
| | Data Release Frequency: Quarterly |

STATE AND LOCAL RECORDS

SHWS: Voluntary Cleanup & Superfund Site Status Report

Land restoration projects carried out in several DEQ programs.

| | |
|---|---|
| Date of Government Version: 12/31/2009 | Source: Department of Environmental Quality |
| Date Data Arrived at EDR: 05/28/2010 | Telephone: 405-702-5100 |
| Date Made Active in Reports: 07/13/2010 | Last EDR Contact: 11/22/2013 |
| Number of Days to Update: 46 | Next Scheduled EDR Contact: 03/03/2014 |
| | Data Release Frequency: Varies |

SWF/LF: Permitted Solid Waste Disposal & Processing Facilities

Solid Waste Facilities/Landfill Sites. SWF/LF type records typically contain an inventory of solid waste disposal facilities or landfills in a particular state. Depending on the state, these may be active or inactive facilities or open dumps that failed to meet RCRA Subtitle D Section 4004 criteria for solid waste landfills or disposal sites.

| | |
|---|---|
| Date of Government Version: 03/18/2013 | Source: Department of Environmental Quality |
| Date Data Arrived at EDR: 11/08/2013 | Telephone: 405-702-5184 |
| Date Made Active in Reports: 12/27/2013 | Last EDR Contact: 11/04/2013 |
| Number of Days to Update: 49 | Next Scheduled EDR Contact: 02/17/2014 |
| | Data Release Frequency: Semi-Annually |

UIC: Underground Injection Wells Database Listing

Class I injection wells. CLASS I wells are used to inject liquid hazardous and non-hazardous wastes beneath the lower most Underground Sources of Drinking Water (USDW).

| | |
|---|---|
| Date of Government Version: 02/01/2013 | Source: Department of Environmental Quality |
| Date Data Arrived at EDR: 02/15/2013 | Telephone: 405-702-5188 |
| Date Made Active in Reports: 04/02/2013 | Last EDR Contact: 10/25/2013 |
| Number of Days to Update: 46 | Next Scheduled EDR Contact: 02/03/2014 |
| | Data Release Frequency: Varies |

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

SWRCY: Recycling Facilities

A listing of recycling facility locations.

Date of Government Version: 10/21/2013
Date Data Arrived at EDR: 10/25/2013
Date Made Active in Reports: 12/27/2013
Number of Days to Update: 63

Source: Department of Environmental Quality
Telephone: 405-702-5100
Last EDR Contact: 10/22/2013
Next Scheduled EDR Contact: 02/03/2014
Data Release Frequency: Varies

LUST: Leaking Underground Storage Tank List

Leaking Underground Storage Tank Incident Reports. LUST records contain an inventory of reported leaking underground storage tank incidents. Not all states maintain these records, and the information stored varies by state.

Date of Government Version: 11/01/2013
Date Data Arrived at EDR: 11/04/2013
Date Made Active in Reports: 12/27/2013
Number of Days to Update: 53

Source: Oklahoma Corporation Commission
Telephone: 405-521-3107
Last EDR Contact: 09/30/2013
Next Scheduled EDR Contact: 01/13/2014
Data Release Frequency: Varies

UST: Underground Storage Tank Listing

Registered Underground Storage Tanks. UST's are regulated under Subtitle I of the Resource Conservation and Recovery Act (RCRA) and must be registered with the state department responsible for administering the UST program. Available information varies by state program.

Date of Government Version: 11/05/2013
Date Data Arrived at EDR: 11/08/2013
Date Made Active in Reports: 12/26/2013
Number of Days to Update: 48

Source: Oklahoma Corporation Commission
Telephone: 405-521-3107
Last EDR Contact: 09/30/2013
Next Scheduled EDR Contact: 01/13/2014
Data Release Frequency: Varies

HIST UST: Underground Storage Tank List, List II Version

This underground storage tank listing includes tank information through March 2003. This listing is no longer updated by the Oklahoma Corporation Commission.

Date of Government Version: 03/21/2003
Date Data Arrived at EDR: 04/28/2003
Date Made Active in Reports: 05/27/2003
Number of Days to Update: 29

Source: Oklahoma Corporation Commission
Telephone: 405-521-3107
Last EDR Contact: 01/19/2009
Next Scheduled EDR Contact: 04/19/2009
Data Release Frequency: No Update Planned

LAST: Leaking Aboveground Storage Tanks List

Leaking aboveground storage tank site locations.

Date of Government Version: 11/01/2013
Date Data Arrived at EDR: 11/04/2013
Date Made Active in Reports: 12/27/2013
Number of Days to Update: 53

Source: Oklahoma Corporation Commission
Telephone: 405-522-4640
Last EDR Contact: 09/30/2013
Next Scheduled EDR Contact: 01/13/2014
Data Release Frequency: Varies

AST: Aboveground Storage Tanks

Registered Aboveground Storage Tanks.

Date of Government Version: 11/05/2013
Date Data Arrived at EDR: 11/08/2013
Date Made Active in Reports: 12/26/2013
Number of Days to Update: 48

Source: Oklahoma Corporation Commission
Telephone: 405-521-3107
Last EDR Contact: 09/30/2013
Next Scheduled EDR Contact: 01/13/2014
Data Release Frequency: Varies

INST CONTROL: Institutional Control Sites

Sites with institutional controls in place.

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

Date of Government Version: 11/07/2013
Date Data Arrived at EDR: 11/22/2013
Date Made Active in Reports: 12/27/2013
Number of Days to Update: 35

Source: Department of Environmental Quality
Telephone: 405-702-5100
Last EDR Contact: 11/22/2013
Next Scheduled EDR Contact: 03/03/2014
Data Release Frequency: Varies

VCP: Voluntary Cleanup Site Inventory

Investigations and cleanups by groups or individuals participating in the Voluntary Cleanup Program (VCP).

Date of Government Version: 11/07/2013
Date Data Arrived at EDR: 11/22/2013
Date Made Active in Reports: 12/27/2013
Number of Days to Update: 35

Source: Department of Environmental Quality
Telephone: 405-702-5100
Last EDR Contact: 11/22/2013
Next Scheduled EDR Contact: 03/03/2014
Data Release Frequency: Varies

DRYCLEANERS: Drycleaner Facilities

A listing of drycleaner facility locations.

Date of Government Version: 09/16/2013
Date Data Arrived at EDR: 09/18/2013
Date Made Active in Reports: 10/23/2013
Number of Days to Update: 35

Source: Department of Environmental Quality
Telephone: 405-702-9100
Last EDR Contact: 12/30/2013
Next Scheduled EDR Contact: 03/31/2014
Data Release Frequency: Varies

BROWNFIELDS: Brownfield Sites

Brownfields are defined by Oklahoma law as abandoned, idled or under used industrial or commercial facilities or other real property at which expansion or redevelopment of the real property is complicated by environmental contamination caused by regulated substances. This program provides a means for private parties and government entities to voluntarily investigate and if warranted, clean up properties that may be contaminated with hazardous wastes. The formal Brownfields Program provides specific state liability relief and protects the property from federal Superfund actions.

Date of Government Version: 09/07/2012
Date Data Arrived at EDR: 09/07/2012
Date Made Active in Reports: 10/10/2012
Number of Days to Update: 33

Source: Department of Environmental Quality
Telephone: 405-702-5100
Last EDR Contact: 11/18/2013
Next Scheduled EDR Contact: 03/03/2014
Data Release Frequency: No Update Planned

BROWNFIELDS 2: Brownfields Public Record Listing

The Brownfields program provides a means for private parties and government entities to voluntarily investigate and if warranted, clean up properties that may be contaminated with hazardous wastes. The formal Brownfields Program provides specific state liability relief and protects the property from federal Superfund actions.

Date of Government Version: 05/03/2013
Date Data Arrived at EDR: 05/22/2013
Date Made Active in Reports: 07/16/2013
Number of Days to Update: 55

Source: Department of Environmental Quality
Telephone: 405-702-5100
Last EDR Contact: 11/22/2013
Next Scheduled EDR Contact: 03/03/2014
Data Release Frequency: Varies

AIRS: Permitted AIRS Facility Listing

A listing of permitted AIRS facility locations.

Date of Government Version: 10/09/2013
Date Data Arrived at EDR: 10/10/2013
Date Made Active in Reports: 10/23/2013
Number of Days to Update: 13

Source: Department of Environmental Quality
Telephone: 405-702-4100
Last EDR Contact: 12/30/2013
Next Scheduled EDR Contact: 04/14/2014
Data Release Frequency: Varies

TIER 2: Tier 2 Data Listing

A listing of facilities which store or manufacture hazardous materials and submit a chemical inventory report.

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

Date of Government Version: 12/31/2011
Date Data Arrived at EDR: 06/29/2012
Date Made Active in Reports: 08/29/2012
Number of Days to Update: 61

Source: Department of Environmental Quality
Telephone: 405-702-1000
Last EDR Contact: 12/11/2013
Next Scheduled EDR Contact: 03/31/2014
Data Release Frequency: Varies

OK COMPLAINT: Oklahoma Complaint System Database

Environmental complaints reported to the Oklahoma Corporation Commission.

Date of Government Version: 09/30/2012
Date Data Arrived at EDR: 04/04/2013
Date Made Active in Reports: 04/19/2013
Number of Days to Update: 15

Source: Oklahoma Corporation Commission
Telephone: 405-521-2384
Last EDR Contact: 01/06/2014
Next Scheduled EDR Contact: 04/21/2014
Data Release Frequency: Varies

RGA LUST: Recovered Government Archive Leaking Underground Storage Tank

The EDR Recovered Government Archive Leaking Underground Storage Tank database provides a list of LUST incidents derived from historical databases and includes many records that no longer appear in current government lists.

Date of Government Version: N/A
Date Data Arrived at EDR: 07/01/2013
Date Made Active in Reports: 12/27/2013
Number of Days to Update: 179

Source: EDR
Telephone: N/A
Last EDR Contact: 06/01/2012
Next Scheduled EDR Contact: N/A
Data Release Frequency: Varies

RGA HWS: Recovered Government Archive State Hazardous Waste Facilities List

The EDR Recovered Government Archive State Hazardous Waste database provides a list of SHWS incidents derived from historical databases and includes many records that no longer appear in current government lists.

Date of Government Version: N/A
Date Data Arrived at EDR: 07/01/2013
Date Made Active in Reports: 01/03/2014
Number of Days to Update: 186

Source: EDR
Telephone: N/A
Last EDR Contact: 06/01/2012
Next Scheduled EDR Contact: N/A
Data Release Frequency: Varies

TRIBAL RECORDS

INDIAN RESERV: Indian Reservations

This map layer portrays Indian administered lands of the United States that have any area equal to or greater than 640 acres.

Date of Government Version: 12/31/2005
Date Data Arrived at EDR: 12/08/2006
Date Made Active in Reports: 01/11/2007
Number of Days to Update: 34

Source: USGS
Telephone: 202-208-3710
Last EDR Contact: 10/18/2013
Next Scheduled EDR Contact: 01/27/2014
Data Release Frequency: Semi-Annually

INDIAN ODI: Report on the Status of Open Dumps on Indian Lands

Location of open dumps on Indian land.

Date of Government Version: 12/31/1998
Date Data Arrived at EDR: 12/03/2007
Date Made Active in Reports: 01/24/2008
Number of Days to Update: 52

Source: Environmental Protection Agency
Telephone: 703-308-8245
Last EDR Contact: 11/04/2013
Next Scheduled EDR Contact: 02/17/2014
Data Release Frequency: Varies

INDIAN LUST R4: Leaking Underground Storage Tanks on Indian Land

LUSTs on Indian land in Florida, Mississippi and North Carolina.

Date of Government Version: 08/01/2013
Date Data Arrived at EDR: 08/02/2013
Date Made Active in Reports: 11/01/2013
Number of Days to Update: 91

Source: EPA Region 4
Telephone: 404-562-8677
Last EDR Contact: 10/28/2013
Next Scheduled EDR Contact: 02/11/2014
Data Release Frequency: Semi-Annually

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

INDIAN LUST R1: Leaking Underground Storage Tanks on Indian Land

A listing of leaking underground storage tank locations on Indian Land.

| | |
|---|--|
| Date of Government Version: 02/01/2013 | Source: EPA Region 1 |
| Date Data Arrived at EDR: 05/01/2013 | Telephone: 617-918-1313 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 11/01/2013 |
| Number of Days to Update: 184 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Varies |

INDIAN LUST R10: Leaking Underground Storage Tanks on Indian Land

LUSTs on Indian land in Alaska, Idaho, Oregon and Washington.

| | |
|---|--|
| Date of Government Version: 11/06/2013 | Source: EPA Region 10 |
| Date Data Arrived at EDR: 11/07/2013 | Telephone: 206-553-2857 |
| Date Made Active in Reports: 12/06/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 29 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Quarterly |

INDIAN LUST R8: Leaking Underground Storage Tanks on Indian Land

LUSTs on Indian land in Colorado, Montana, North Dakota, South Dakota, Utah and Wyoming.

| | |
|---|--|
| Date of Government Version: 08/27/2012 | Source: EPA Region 8 |
| Date Data Arrived at EDR: 08/28/2012 | Telephone: 303-312-6271 |
| Date Made Active in Reports: 10/16/2012 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 49 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Quarterly |

INDIAN LUST R5: Leaking Underground Storage Tanks on Indian Land

Leaking underground storage tanks located on Indian Land in Michigan, Minnesota and Wisconsin.

| | |
|---|--|
| Date of Government Version: 08/20/2013 | Source: EPA, Region 5 |
| Date Data Arrived at EDR: 08/23/2013 | Telephone: 312-886-7439 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 70 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Varies |

INDIAN LUST R6: Leaking Underground Storage Tanks on Indian Land

LUSTs on Indian land in New Mexico and Oklahoma.

| | |
|---|--|
| Date of Government Version: 09/12/2011 | Source: EPA Region 6 |
| Date Data Arrived at EDR: 09/13/2011 | Telephone: 214-665-6597 |
| Date Made Active in Reports: 11/11/2011 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 59 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Varies |

INDIAN LUST R7: Leaking Underground Storage Tanks on Indian Land

LUSTs on Indian land in Iowa, Kansas, and Nebraska

| | |
|---|--|
| Date of Government Version: 08/27/2013 | Source: EPA Region 7 |
| Date Data Arrived at EDR: 08/27/2013 | Telephone: 913-551-7003 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 66 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Varies |

INDIAN LUST R9: Leaking Underground Storage Tanks on Indian Land

LUSTs on Indian land in Arizona, California, New Mexico and Nevada

| | |
|---|---|
| Date of Government Version: 03/01/2013 | Source: Environmental Protection Agency |
| Date Data Arrived at EDR: 03/01/2013 | Telephone: 415-972-3372 |
| Date Made Active in Reports: 04/12/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 42 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Quarterly |

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

INDIAN UST R1: Underground Storage Tanks on Indian Land

The Indian Underground Storage Tank (UST) database provides information about underground storage tanks on Indian land in EPA Region 1 (Connecticut, Maine, Massachusetts, New Hampshire, Rhode Island, Vermont and ten Tribal Nations).

| | |
|---|--|
| Date of Government Version: 09/28/2012 | Source: EPA, Region 1 |
| Date Data Arrived at EDR: 11/07/2012 | Telephone: 617-918-1313 |
| Date Made Active in Reports: 04/12/2013 | Last EDR Contact: 11/01/2014 |
| Number of Days to Update: 156 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Varies |

INDIAN UST R4: Underground Storage Tanks on Indian Land

The Indian Underground Storage Tank (UST) database provides information about underground storage tanks on Indian land in EPA Region 4 (Alabama, Florida, Georgia, Kentucky, Mississippi, North Carolina, South Carolina, Tennessee and Tribal Nations)

| | |
|---|--|
| Date of Government Version: 08/01/2013 | Source: EPA Region 4 |
| Date Data Arrived at EDR: 08/02/2013 | Telephone: 404-562-9424 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 91 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Semi-Annually |

INDIAN UST R5: Underground Storage Tanks on Indian Land

The Indian Underground Storage Tank (UST) database provides information about underground storage tanks on Indian land in EPA Region 5 (Michigan, Minnesota and Wisconsin and Tribal Nations).

| | |
|---|--|
| Date of Government Version: 08/20/2013 | Source: EPA Region 5 |
| Date Data Arrived at EDR: 08/23/2013 | Telephone: 312-886-6136 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 70 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Varies |

INDIAN UST R7: Underground Storage Tanks on Indian Land

The Indian Underground Storage Tank (UST) database provides information about underground storage tanks on Indian land in EPA Region 7 (Iowa, Kansas, Missouri, Nebraska, and 9 Tribal Nations).

| | |
|---|--|
| Date of Government Version: 12/31/2012 | Source: EPA Region 7 |
| Date Data Arrived at EDR: 02/28/2013 | Telephone: 913-551-7003 |
| Date Made Active in Reports: 04/12/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 43 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Varies |

INDIAN UST R8: Underground Storage Tanks on Indian Land

The Indian Underground Storage Tank (UST) database provides information about underground storage tanks on Indian land in EPA Region 8 (Colorado, Montana, North Dakota, South Dakota, Utah, Wyoming and 27 Tribal Nations).

| | |
|---|--|
| Date of Government Version: 07/29/2013 | Source: EPA Region 8 |
| Date Data Arrived at EDR: 08/01/2013 | Telephone: 303-312-6137 |
| Date Made Active in Reports: 11/01/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 92 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Quarterly |

INDIAN UST R9: Underground Storage Tanks on Indian Land

The Indian Underground Storage Tank (UST) database provides information about underground storage tanks on Indian land in EPA Region 9 (Arizona, California, Hawaii, Nevada, the Pacific Islands, and Tribal Nations).

| | |
|---|--|
| Date of Government Version: 07/29/2013 | Source: EPA Region 9 |
| Date Data Arrived at EDR: 07/30/2013 | Telephone: 415-972-3368 |
| Date Made Active in Reports: 12/06/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 129 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Quarterly |

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

INDIAN UST R10: Underground Storage Tanks on Indian Land

The Indian Underground Storage Tank (UST) database provides information about underground storage tanks on Indian land in EPA Region 10 (Alaska, Idaho, Oregon, Washington, and Tribal Nations).

| | |
|---|--|
| Date of Government Version: 02/05/2013 | Source: EPA Region 10 |
| Date Data Arrived at EDR: 02/06/2013 | Telephone: 206-553-2857 |
| Date Made Active in Reports: 04/12/2013 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 65 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Quarterly |

INDIAN UST R6: Underground Storage Tanks on Indian Land

The Indian Underground Storage Tank (UST) database provides information about underground storage tanks on Indian land in EPA Region 6 (Louisiana, Arkansas, Oklahoma, New Mexico, Texas and 65 Tribes).

| | |
|---|--|
| Date of Government Version: 05/10/2011 | Source: EPA Region 6 |
| Date Data Arrived at EDR: 05/11/2011 | Telephone: 214-665-7591 |
| Date Made Active in Reports: 06/14/2011 | Last EDR Contact: 10/28/2013 |
| Number of Days to Update: 34 | Next Scheduled EDR Contact: 02/11/2014 |
| | Data Release Frequency: Semi-Annually |

INDIAN VCP R1: Voluntary Cleanup Priority Listing

A listing of voluntary cleanup priority sites located on Indian Land located in Region 1.

| | |
|---|--|
| Date of Government Version: 09/17/2013 | Source: EPA, Region 1 |
| Date Data Arrived at EDR: 10/01/2013 | Telephone: 617-918-1102 |
| Date Made Active in Reports: 12/06/2013 | Last EDR Contact: 01/03/2014 |
| Number of Days to Update: 66 | Next Scheduled EDR Contact: 04/14/2014 |
| | Data Release Frequency: Varies |

INDIAN VCP R7: Voluntary Cleanup Priority Listing

A listing of voluntary cleanup priority sites located on Indian Land located in Region 7.

| | |
|---|--|
| Date of Government Version: 03/20/2008 | Source: EPA, Region 7 |
| Date Data Arrived at EDR: 04/22/2008 | Telephone: 913-551-7365 |
| Date Made Active in Reports: 05/19/2008 | Last EDR Contact: 04/20/2009 |
| Number of Days to Update: 27 | Next Scheduled EDR Contact: 07/20/2009 |
| | Data Release Frequency: Varies |

EDR PROPRIETARY RECORDS

EDR MGP: EDR Proprietary Manufactured Gas Plants

The EDR Proprietary Manufactured Gas Plant Database includes records of coal gas plants (manufactured gas plants) compiled by EDR's researchers. Manufactured gas sites were used in the United States from the 1800's to 1950's to produce a gas that could be distributed and used as fuel. These plants used whale oil, rosin, coal, or a mixture of coal, oil, and water that also produced a significant amount of waste. Many of the byproducts of the gas production, such as coal tar (oily waste containing volatile and non-volatile chemicals), sludges, oils and other compounds are potentially hazardous to human health and the environment. The byproduct from this process was frequently disposed of directly at the plant site and can remain or spread slowly, serving as a continuous source of soil and groundwater contamination.

| | |
|----------------------------------|---|
| Date of Government Version: N/A | Source: EDR, Inc. |
| Date Data Arrived at EDR: N/A | Telephone: N/A |
| Date Made Active in Reports: N/A | Last EDR Contact: N/A |
| Number of Days to Update: N/A | Next Scheduled EDR Contact: N/A |
| | Data Release Frequency: No Update Planned |

EDR US Hist Auto Stat: EDR Exclusive Historic Gas Stations

EDR has searched selected national collections of business directories and has collected listings of potential gas station/filling station/service station sites that were available to EDR researchers. EDR's review was limited to those categories of sources that might, in EDR's opinion, include gas station/filling station/service station establishments. The categories reviewed included, but were not limited to gas, gas station, gasoline station, filling station, auto, automobile repair, auto service station, service station, etc. This database falls within a category of information EDR classifies as "High Risk Historical Records", or HRHR. EDR's HRHR effort presents unique and sometimes proprietary data about past sites and operations that typically create environmental concerns, but may not show up in current government records searches.

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

Date of Government Version: N/A
Date Data Arrived at EDR: N/A
Date Made Active in Reports: N/A
Number of Days to Update: N/A

Source: EDR, Inc.
Telephone: N/A
Last EDR Contact: N/A
Next Scheduled EDR Contact: N/A
Data Release Frequency: Varies

EDR US Hist Cleaners: EDR Exclusive Historic Dry Cleaners

EDR has searched selected national collections of business directories and has collected listings of potential dry cleaner sites that were available to EDR researchers. EDR's review was limited to those categories of sources that might, in EDR's opinion, include dry cleaning establishments. The categories reviewed included, but were not limited to dry cleaners, cleaners, laundry, laundromat, cleaning/laundry, wash & dry etc. This database falls within a category of information EDR classifies as "High Risk Historical Records", or HRHR. EDR's HRHR effort presents unique and sometimes proprietary data about past sites and operations that typically create environmental concerns, but may not show up in current government records searches.

Date of Government Version: N/A
Date Data Arrived at EDR: N/A
Date Made Active in Reports: N/A
Number of Days to Update: N/A

Source: EDR, Inc.
Telephone: N/A
Last EDR Contact: N/A
Next Scheduled EDR Contact: N/A
Data Release Frequency: Varies

EDR US Hist Auto Stat: EDR Proprietary Historic Gas Stations - Cole

Date of Government Version: N/A
Date Data Arrived at EDR: N/A
Date Made Active in Reports: N/A
Number of Days to Update: N/A

Source: N/A
Telephone: N/A
Last EDR Contact: N/A
Next Scheduled EDR Contact: N/A
Data Release Frequency: Varies

EDR US Hist Cleaners: EDR Proprietary Historic Dry Cleaners - Cole

Date of Government Version: N/A
Date Data Arrived at EDR: N/A
Date Made Active in Reports: N/A
Number of Days to Update: N/A

Source: N/A
Telephone: N/A
Last EDR Contact: N/A
Next Scheduled EDR Contact: N/A
Data Release Frequency: Varies

OTHER DATABASE(S)

Depending on the geographic area covered by this report, the data provided in these specialty databases may or may not be complete. For example, the existence of wetlands information data in a specific report does not mean that all wetlands in the area covered by the report are included. Moreover, the absence of any reported wetlands information does not necessarily mean that wetlands do not exist in the area covered by the report.

CT MANIFEST: Hazardous Waste Manifest Data

Facility and manifest data. Manifest is a document that lists and tracks hazardous waste from the generator through transporters to a tsd facility.

Date of Government Version: 07/30/2013
Date Data Arrived at EDR: 08/19/2013
Date Made Active in Reports: 10/03/2013
Number of Days to Update: 45

Source: Department of Energy & Environmental Protection
Telephone: 860-424-3375
Last EDR Contact: 11/22/2013
Next Scheduled EDR Contact: 03/03/2014
Data Release Frequency: Annually

NY MANIFEST: Facility and Manifest Data

Manifest is a document that lists and tracks hazardous waste from the generator through transporters to a TSD facility.

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

Date of Government Version: 11/01/2013
Date Data Arrived at EDR: 11/07/2013
Date Made Active in Reports: 11/18/2013
Number of Days to Update: 11

Source: Department of Environmental Conservation
Telephone: 518-402-8651
Last EDR Contact: 11/07/2013
Next Scheduled EDR Contact: 02/17/2014
Data Release Frequency: Annually

WI MANIFEST: Manifest Information

Hazardous waste manifest information.

Date of Government Version: 12/31/2012
Date Data Arrived at EDR: 08/09/2013
Date Made Active in Reports: 09/27/2013
Number of Days to Update: 49

Source: Department of Natural Resources
Telephone: N/A
Last EDR Contact: 12/11/2013
Next Scheduled EDR Contact: 03/31/2014
Data Release Frequency: Annually

Oil/Gas Pipelines: This data was obtained by EDR from the USGS in 1994. It is referred to by USGS as GeoData Digital Line Graphs from 1:100,000-Scale Maps. It was extracted from the transportation category including some oil, but primarily gas pipelines.

Sensitive Receptors: There are individuals deemed sensitive receptors due to their fragile immune systems and special sensitivity to environmental discharges. These sensitive receptors typically include the elderly, the sick, and children. While the location of all sensitive receptors cannot be determined, EDR indicates those buildings and facilities - schools, daycares, hospitals, medical centers, and nursing homes - where individuals who are sensitive receptors are likely to be located.

AHA Hospitals:

Source: American Hospital Association, Inc.
Telephone: 312-280-5991

The database includes a listing of hospitals based on the American Hospital Association's annual survey of hospitals.

Medical Centers: Provider of Services Listing

Source: Centers for Medicare & Medicaid Services
Telephone: 410-786-3000

A listing of hospitals with Medicare provider number, produced by Centers of Medicare & Medicaid Services, a federal agency within the U.S. Department of Health and Human Services.

Nursing Homes

Source: National Institutes of Health
Telephone: 301-594-6248

Information on Medicare and Medicaid certified nursing homes in the United States.

Public Schools

Source: National Center for Education Statistics
Telephone: 202-502-7300

The National Center for Education Statistics' primary database on elementary and secondary public education in the United States. It is a comprehensive, annual, national statistical database of all public elementary and secondary schools and school districts, which contains data that are comparable across all states.

Private Schools

Source: National Center for Education Statistics
Telephone: 202-502-7300

The National Center for Education Statistics' primary database on private school locations in the United States.

Daycare Centers: Day Care Centers

Source: Department of Human Services
Telephone: 405-521-3561

Flood Zone Data: This data, available in select counties across the country, was obtained by EDR in 2003 & 2011 from the Federal Emergency Management Agency (FEMA). Data depicts 100-year and 500-year flood zones as defined by FEMA.

NWI: National Wetlands Inventory. This data, available in select counties across the country, was obtained by EDR in 2002 and 2005 from the U.S. Fish and Wildlife Service.

GOVERNMENT RECORDS SEARCHED / DATA CURRENCY TRACKING

STREET AND ADDRESS INFORMATION

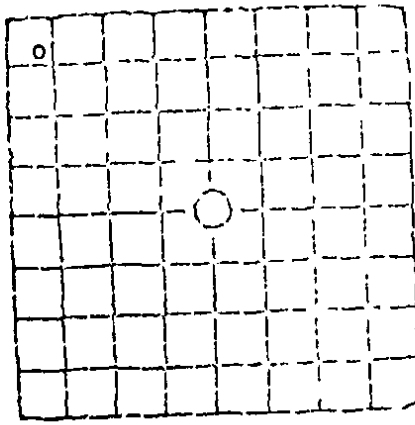
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CORPORATION COMMISSION
WELL LOG DIVISION
OKLAHOMA CITY, OKLAHOMA

PRODUCING

Well Oklahoma
Loyd sec
5-4-87

Still here



COMPANY— T. B. Slick,
 FARM— Baker #1
 LOCATION— NW NW NW
 COUNTY— Oklahoma
 TOTAL DEPTH— 6411'
 PRODUCTION— 2100 Bbls. 1st 24 hrs.; grav. 38
 ROCK PRESSURE—
 ELEVATION— 1162' ground
 DRILLING COMMENCED— 4-25-30
 DRILLING COMPLETED— 7-26-30

SEC. 1
 TWP. 11N
 RGE. 3W

ADDRESS CORRESPONDENCE REGARDING THIS WELL TO—

REMARKS: 2515' N of SL and
 125' E of WL of 1/4 sec.
 Record signed by J. D. Mugg.
 Rotary tools to 6411'; Rotary Rig.

T. B. Slick
 Box 1146
 Okla. City, Okla.

CASING RECORD:

| | | | | |
|---------|-------|-----|------|--------|
| 20" | 41' | Set | None | Pulled |
| 15 1/2" | 260' | " | " | " |
| 9" | 5345' | " | " | " |
| 6 5/8" | 6128' | " | " | " |

| FORMATION | TOP | BOTTOM |
|----------------|-------|--------|
| slate & shells | 6290 | 6305 |
| sand | 6305 | 6330 |
| sand | 6330 | 6411 |
| TOTAL DEPTH | 6411' | |

(9-18-30 G)

| FORMATION | TOP | BOTTOM | FORMATION | TOP | BOTTOM |
|--------------------------|------|--------|------------------------|------|--------|
| Sand | 1 | 32 | sandy lime hard | 3990 | 4034 |
| clay | 32 | 37 | sandy lime & shale | 4034 | 4104 |
| sand and clay | 37 | 260 | sticky shale and lime | 4104 | 4154 |
| Set 260' of 15 1/2" csg. | 260 | | gumbo | 4154 | 4194 |
| shale and shells | 260 | 290 | sticky shale and lime | 4194 | 4290 |
| sand, shells & shale | 290 | 1150 | sandy lime | 4290 | 4301 |
| sand & shale | 1150 | 1300 | lime and shale | 4301 | 4341 |
| shale & shells | 1300 | 1620 | lime | 4341 | 4391 |
| sand | 1620 | 1635 | hard lime | 4391 | 4447 |
| shale and red bed | 1635 | 1770 | lime and shale | 4447 | 4507 |
| shale & lime shells | 1770 | 1970 | lime | 4507 | 4536 |
| shale and shells | 1970 | 2090 | hard lime | 4636 | 4660 |
| shells & shale | 2090 | 2190 | lime and shale | 4660 | 4800 |
| shells & shale | 2190 | 2290 | shale, sand and shells | 4800 | 4860 |
| lime and shale broken | 2290 | 2420 | shale | 4360 | 4973 |
| lime and shale | 2420 | 2570 | sand hard | 4973 | 5023 |
| shale and shells | 2570 | 3075 | sand | 5023 | 5070 |
| lime | 3075 | 3090 | shale | 5070 | 5170 |
| lime and shale broken | 3090 | 3182 | shale and lime | 5170 | 5260 |
| sand | 3182 | 3238 | lime | 5260 | 5350 |
| sticky shale | 3238 | 3272 | shale | 5350 | 5589 |
| lime and shale | 3272 | 3275 | lime and shale | 5589 | 5659 |
| sticky shale | 3275 | 3370 | shale | 5659 | 5799 |
| sand | 3370 | 3495 | shale and sand | 5799 | 5854 |
| sticky shale | 3495 | 3565 | shale | 5854 | 5874 |
| sticky shale | 3565 | 3615 | sand | 5874 | 5914 |
| shale and sand | 3615 | 3680 | lime | 5914 | 5981 |
| sand and shale | 3680 | 3753 | lime and shale | 5981 | 6021 |
| sticky shale | 3753 | 3868 | lime & shale broken | 6021 | 6054 |
| sticky shale & lime | 3868 | 3940 | shale | 6054 | 6214 |
| sticky shale | 3940 | 3990 | sand | 6214 | 6290 |

(See top)

Oklahoma Corporation Commission
 Oil & Gas Conservation Division
 Post Office Box 52000
 Oklahoma City, Oklahoma 73152-2000
 Rule 165: 10-3-25

Form 1002A

API No.: 35109358410001

Completion Report

Spud Date: September 17, 2012

OTC Prod. Unit No.: 109-14666

Drilling Finished Date: October 05, 2012

1st Prod Date:

Completion Date: October 22, 2012

Amended

Amend Reason: RE-ENTRY / DRILL DEEPER

Drill Type: STRAIGHT HOLE

Well Name: YOUNT "A" (CLAUDE YOUNT 1) 1

Purchaser/Measurer:

Location: OKLAHOMA 2 11N 3W
 NW NW SE SW
 1170 FSL 1485 FWL of 1/4 SEC
 Derrick Elevation: 1210 Ground Elevation: 1201

First Sales Date:

Operator: WHITE OPERATING COMPANY 8891

1627 SW 96TH ST
 OKLAHOMA CITY, OK 73159-7136

| Completion Type | |
|-----------------|---------------|
| X | Single Zone |
| | Multiple Zone |
| | Commingled |

| Location Exception | |
|---|--|
| Order No | |
| There are no Location Exception records to display. | |

| Increased Density | |
|--|--|
| Order No | |
| There are no Increased Density records to display. | |

| Casing and Cement | | | | | | | |
|-------------------|-------|--------|-------|------|-----|-----|------------|
| Type | Size | Weight | Grade | Feet | PSI | SAX | Top of CMT |
| SURFACE | 10.75 | 60 | J-55 | 1026 | | 350 | SURFACE |
| INTERMEDIATE | 7 | 24 | J-55 | 6654 | | 290 | 3972 |
| PRODUCTION | 5.5 | 15.5 | J-55 | 6868 | | 150 | 5218 |

| Liner | | | | | | | | |
|--|------|--------|-------|--------|-----|-----|-----------|--------------|
| Type | Size | Weight | Grade | Length | PSI | SAX | Top Depth | Bottom Depth |
| There are no Liner records to display. | | | | | | | | |

Total Depth: 8172

| Packer | |
|--------|--------------|
| Depth | Brand & Type |
| 6752 | ARROW AS1 |

| Plug | |
|---------------------------------------|-----------|
| Depth | Plug Type |
| There are no Plug records to display. | |

| Initial Test Data | | | | | | | | | | |
|---|-----------|-------------|-------------------|-------------|-------------------------|---------------|-------------------|--------------------------|------------|----------------------|
| Test Date | Formation | Oil BBL/Day | Oil-Gravity (API) | Gas MCF/Day | Gas-Oil Ratio Cu FT/BBL | Water BBL/Day | Pumpin or Flowing | Initial Shut-In Pressure | Choke Size | Flow Tubing Pressure |
| There are no Initial Data records to display. | | | | | | | | | | |

| Completion and Test Data by Producing Formation | | | | | | | | | | |
|---|--|------------------|---------------|--|--|--|-------------|-----------|--|--|
| Formation Name: ARBUCKLE | | | Code: 169ABCK | | | | Class: DISP | | | |
| Spacing Orders | | | | | Perforated Intervals | | | | | |
| Order No | | Unit Size | | | From | | | To | | |
| There are no Spacing Order records to display. | | | | | 6820 | | | 8172 | | |
| Acid Volumes | | | | | Fracture Treatments | | | | | |
| There are no Acid Volume records to display. | | | | | There are no Fracture Treatments records to display. | | | | | |

| Formation | Top |
|-----------|------|
| ARBUCKLE | 6820 |

Were open hole logs run? No

Date last log run:

Were unusual drilling circumstances encountered? No

Explanation:

| Other Remarks |
|--|
| OCC - THIS WELL IS AN OPEN HOLE COMPLETION FROM THE BOTTOM OF THE 5 1/2" AT 6,861' TO TOTAL DEPTH 8,172'. UIC PERMIT NUMBER 1206470025 |

| FOR COMMISSION USE ONLY | |
|-------------------------|---------|
| Status: Accepted | 1116802 |

AS SUBMITTED

RECEIVED

Form 1002A
Rev. 2009

OCT 25 2012

OKLAHOMA CORPORATION
COMMISSION

API NO. 109-35841
OTC PROD. UNIT NO. 109-14666

PLEASE TYPE OR USE BLACK
NOTE:

Attach copy of original 1
if recompletion or reer

Rule 165:10-3-25

ORIGINAL
 AMENDED (Reason)

Recentry / Drill Deeper

COMPLETION REPORT

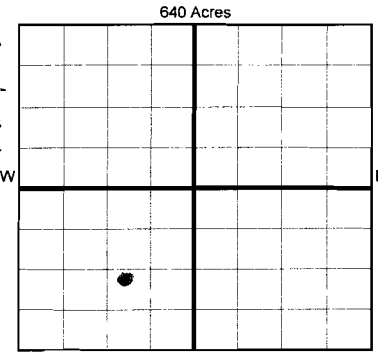
TYPE OF DRILLING OPERATION

STRAIGHT HOLE DIRECTIONAL HOLE HORIZONTAL HOLE
 SERVICE WELL

If directional or horizontal, see reverse for bottom hole location.

SPUD DATE *9-17-12* 11/5/57
DRLG FINISHED DATE *10-3-12* 12/28/57
DATE OF WELL COMPLETION *10-22-12* 1/25/58
1st PROD DATE

see Lloyd White



COUNTY **Oklahoma** SEC **2** TWP **11N** RGE **3W**
LEASE NAME **Yount "A"** WELL NO. **1**
NW 1/4 NW 1/4 SE 1/4 SW 1/4 FSL OF 1/4 SEC **1170** FWL OF 1/4 SEC **1485** RECOMP DATE
ELEVATION N Derrick **1210** Ground **1201** Latitude (if known) Longitude (if known)
OPERATOR NAME **White Operating Co.** OTC/OCC OPERATOR NO. **08891**
ADDRESS **1627 SW 96th**
CITY **Oklahoma City** STATE **OK** ZIP **73159**

COMPLETION TYPE

SINGLE ZONE
 MULTIPLE ZONE
Application Date
 COMMINGLED
Application Date
LOCATION
EXCEPTION ORDER
INCREASED DENSITY
ORDER NO.

CASING & CEMENT (Form 1002C must be attached)

| TYPE | SIZE | WEIGHT | GRADE | FEET | PSI | SAX | TOP OF CMT | |
|--------------|-------|--------|-------|------|-----|-----|-------------|------|
| CONDUCTOR | | | | | | | | |
| SURFACE | 10.75 | 60 | J55 | 1026 | | 350 | SURF. | |
| INTERMEDIATE | 7 | 24 | J55 | 6654 | | 290 | 3972 | |
| PRODUCTION | 5.5 | 15.5 | J55 | 6861 | | 150 | 5218 | |
| LINER | | | | | | | | |
| | | | | | | | TOTAL DEPTH | 8172 |

PACKER @ **6752** BRAND & TYPE **Arrow AS1** PLUG @ _____ TYPE _____ PLUG @ _____ TYPE _____
PACKER @ _____ BRAND & TYPE _____ PLUG @ _____ TYPE _____ PLUG @ _____ TYPE _____

COMPLETION & TEST DATA BY PRODUCING FORMATION *169 ARBUCKLE*

| | |
|---|------------------|
| FORMATION | Arbuckle |
| SPACING & SPACING ORDER NUMBER | |
| CLASS: Oil, Gas, Dry, Inj, Disp, Comm Disp, Svc | DISP. |
| | 6820-8172 |
| PERFORATED INTERVALS | <i>open hole</i> |
| ACID/VOLUME | |
| FRACTURE TREATMENT (Fluids/Prop Amounts) | |

Min Gas Allowable (165:10-17-7) OR Oil Allowable (165:10-13-3) Gas Purchaser/Measurer _____ First Sales Date _____

INITIAL TEST DATA

| | |
|--------------------------|------------|
| INITIAL TEST DATE | SWD |
| OIL-BBL/DAY | |
| OIL-GRAVITY (API) | |
| GAS-MCF/DAY | |
| GAS-OIL RATIO CU FT/BBL | |
| WATER-BBL/DAY | |
| PUMPING OR FLOWING | |
| INITIAL SHUT-IN PRESSURE | |
| CHOKE SIZE | |
| FLOW TUBING PRESSURE | |

SCANNED

A record of the formations drilled through, and pertinent remarks are presented on the reverse. I declare that I have knowledge of the contents of this report and am authorized by my organization to make this report, which was prepared by me or under my supervision and direction, with the data and facts stated herein to be true, correct, and complete to the best of my knowledge and belief.

SIGNATURE *Lloyd White* NAME (PRINT OR TYPE) **Lloyd White** DATE **10/23/2012** PHONE NUMBER **(405)-239-6001**
ADDRESS **1627 SW 96th** CITY **Oklahoma City** STATE **OK** ZIP **73159** EMAIL ADDRESS **woclrw@coxinet.net**

PLEASE TYPE OR USE BLACK INK ONLY
FORMATION RECORD

Give formation names and tops, if available, or descriptions and thickness of formations drilled through. Show intervals cored or drillstem tested.

LEASE NAME Yount "A" WELL NO. 1

| NAMES OF FORMATIONS | TOP |
|---------------------|------|
| Arbuckle | 6820 |

FOR COMMISSION USE ONLY

ITD on file YES NO

APPROVED _____ DISAPPROVED _____

2) Reject Codes _____

Were open hole logs run? yes no

Date Last log was run _____

Was CO₂ encountered? yes no at what depths? _____

Was H₂S encountered? yes no at what depths? _____

Were unusual drilling circumstances encountered? yes no
If yes, briefly explain below _____

Other remarks: *OCC - This well is an open hole completion from the bottom of the 3 1/2" at 6861' to total depth 8172' UIC permit # 1206470025*

640 Acres

| | | | | | | | |
|--|--|--|--|--|--|--|--|
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |

If more than three drainholes are proposed, attach a separate sheet indicating the necessary information.

Direction must be stated in degrees azimuth.
Please note, the horizontal drainhole and its end point must be located within the boundaries of the lease or spacing unit.

Directional surveys are required for all drainholes and directional wells.

640 Acres

| | | | | | | | |
|--|--|--|--|--|--|--|--|
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |

BOTTOM HOLE LOCATION FOR DIRECTIONAL HOLE

| SEC | TWP | RGE | COUNTY | | | | |
|----------------------|---------------------|-----|---|-----|-------------------------|-----|-----|
| Spot Location | 1/4 | 1/4 | 1/4 | 1/4 | Feet From 1/4 Sec Lines | FSL | FWL |
| Measured Total Depth | True Vertical Depth | | BHL From Lease, Unit, or Property Line: | | | | |

BOTTOM HOLE LOCATION FOR HORIZONTAL HOLE: (LATERALS)

LATERAL #1

| SEC | TWP | RGE | COUNTY | | | | |
|----------------------|---------------------|-----|---|-----|-------------------------|-----|-----|
| Spot Location | 1/4 | 1/4 | 1/4 | 1/4 | Feet From 1/4 Sec Lines | FSL | FWL |
| Depth of Deviation | Radius of Turn | | Direction | | Total Length | | |
| Measured Total Depth | True Vertical Depth | | BHL From Lease, Unit, or Property Line: | | | | |

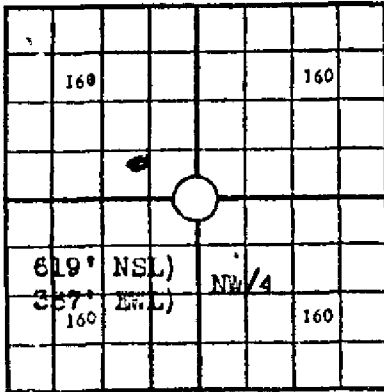
LATERAL #2

| SEC | TWP | RGE | COUNTY | | | | |
|----------------------|---------------------|-----|---|-----|-------------------------|-----|-----|
| Spot Location | 1/4 | 1/4 | 1/4 | 1/4 | Feet From 1/4 Sec Lines | FSL | FWL |
| Depth of Deviation | Radius of Turn | | Direction | | Total Length | | |
| Measured Total Depth | True Vertical Depth | | BHL From Lease, Unit, or Property Line: | | | | |

LATERAL #3

| SEC | TWP | RGE | COUNTY | | | | |
|----------------------|---------------------|-----|---|-----|-------------------------|-----|-----|
| Spot Location | 1/4 | 1/4 | 1/4 | 1/4 | Feet From 1/4 Sec Lines | FSL | FWL |
| Depth of Deviation | Radius of Turn | | Direction | | Total Length | | |
| Measured Total Depth | True Vertical Depth | | BHL From Lease, Unit, or Property Line: | | | | |

640 Acres
N



Locate Well Correctly

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION OKLAHOMA CITY, OKLAHOMA

NOTICE: All questions on this form must be satisfactorily answered

Company Operating Slick-Urschel Oil Company
 Office Address Box 1146, Oklahoma City, Okla.
 County Oklahoma Sec 2 Twp 11 Range 3 W
 Farm Name Dannreuther Well No 3 Field Okla. City
 Character of Well (whether oil, gas or dry) oil

Commenced Plugging September 23, 1935 Finished October 16, 1936 Total Depth 6517'

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Howard

Name of producing sand Wilcox, Depth top 6215', Bottom 6265'

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------|---------|------|------|-------|--------|------------|
| Wilcox | Oil | 6198 | 6240 | 6 5/8 | 6270 | 5588 |
| | | | | 9" | 5400 | 938 |

Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed. Hole filled with mud and cap cemented

Does the above conform strictly to the oil and gas regulations? Yes.

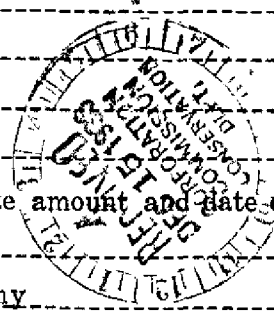
The Law requires that adjacent lease, royalty and land-owners be notified; give their names with their address below:

Skelly Oil Company - Tulsa, Oklahoma

REMARKS: Why plugged? Abandoned If abandoned oil or gas well, state amount and date of last production October, 1935 - 2 Bbls.

Correspondence regarding this well should be addressed to Slick-Urschel Oil Company

Address Box 1146, Oklahoma City, Okla.



I, H. T. King, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct.

Subscribed and sworn to before me this 14 day of December, 1936.

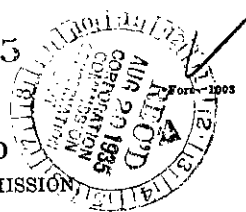
My Commission expires April 16, 1939

R. Price
Notary Public.

109 00 652

00011060002

60575

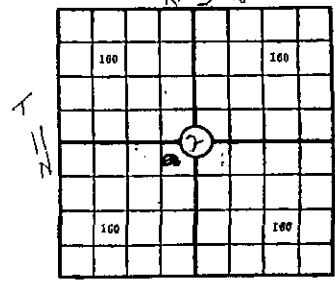


PLUGGING RECORD

MAIL TO CORPORATION COMMISSION

Oklahoma City, Oklahoma

640 Acres
R-3-W



LOCATE WELL CORRECTLY

NOTICE: All questions on this form must be satisfactorily answered.

Company Operating Skelly Oil Company
Office Address Box 4246, Oklahoma City, Oklahoma
County Oklahoma Sec. 2 Twp. 11 N Range 3 West
Farm Name Ed Dillon Community Well No. 1 Field Oklahoma City
Character of Well (whether oil, gas or dry) oil

Commenced Plugging July 21st, Finished July 31st, Total Depth 6545'

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? YES

Name of Conservation Officer who supervised plugging of this well Mr. Thomas

Name of producing sand Dimpson, Depth top 6360', Bottom 6545'

Show depth and thickness of all fresh water, oil and gas formations.

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------------|---------|-------|-------|-----------|----------|------------|
| Pawhuska Lime | | 3150 | | 20" | 40' 5" | none |
| Oread Lime | | 4110 | | 15-1/2" | 250' 10" | " |
| Oswego Lime | | 6020 | | 9" OD | 6451' | 775' 6" |
| Little Oil Sand | | 6200' | 6230' | 6-5/8" OD | 6231' | 4158' 5" |
| Broken Oil Sand | | 6230' | 6310' | | | |
| Little Oil Sand | | 6330' | 6360' | | | |
| Oil Sand | | 6360' | 6440' | | | |
| Broken Oil Sand | | 6460' | 6545' | | | |

Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed:
Filled hole with mud to 6020' and set Lane-Wellis Packer Plug. Perforated casing from 5560' to 5532' to test sand but no results. Mudded to 5271' and shot casing to part and pull. Shot again at 4994', another shot at 4782', another at 4550' but unable to start casing. Mudded to 4158' and run ripper and parted pipe and started pulling. Mudded hole to 2000' & shot 9" csg. shot again at 1500' and again at 776' and pulled 9" mudded hole to top and cemented.
Does the above conform strictly to the oil and gas regulations?

The Law requires that adjacent lease, royalty and land-owners be notified; give their names with their address below:

REMARKS: Why plugged? quit producing abandoned oil or gas well, state amount and date of last production.
March 19th, 1934.

Correspondence regarding this well should be addressed to Skelly Oil Company, Box 4246, Okla
Address Box 4246, Oklahoma City, Oklahoma.

I, A. A. Hunt, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct.

Subscribed and sworn to before me this 12th day August 1935

My Commission expires August 28th, 1938

Notary Public.

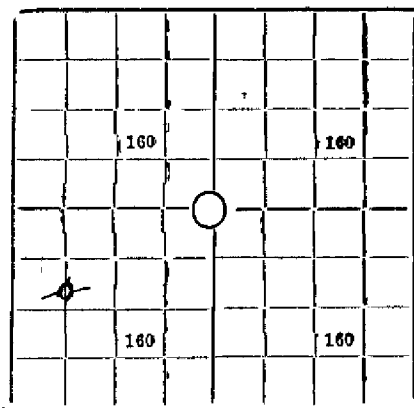
00011060002

640 Acres
N

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION,
Oklahoma City, Oklahoma

60597



Locate Well Correctly

NOTICE All questions on this form must be satisfactorily answered

Company Operating Champlin Refining Company
 Office Address Enid, Oklahoma.
 County Oklahoma Sec 2 Twp 11 Range 3
 Farm Name Seal Well No 1 Field Okla.Ct.
 Character of Well (whether oil, gas or dry) Oil

Commenced Plugging Mar. 19-1935 Finished May 5-1935 Total Depth 6500'

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well? W. J. Armstrong

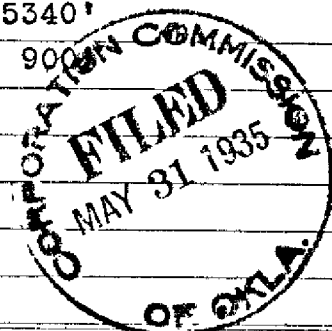
Name of producing sand Simpson Depth top 6270' Bottom 6508'

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|--------------|---------|------|------|---------|--------|------------|
| Simpson sand | Oil | 6270 | 6508 | 7" O.D. | 6215 | 5340' |
| | | | | 9 5/8" | 5523 | |
| | | | | 15 1/2" | 272 | |
| | | | | 20" | 40' | |



Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole If cement or other plugs were used state the character of same and depth placed

Mud filled up to 5340'. Run tubing back in, and run 100 sacks cement on the sand. Verified by Dan Howard, Federal Conservation Officer.

Intend to run 2500 sacks cement and plug off for bottom to make a deep, fresh water well.

Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below

| | | |
|-------------------------------------|-----------------------------|-----------------------|
| <u>Kedburg Oil Company</u> | <u>Oklahoma City, Okla.</u> | <u>Lease Owner</u> |
| <u>Harper & Turner</u> | <u>"</u> | <u>Royalty owners</u> |
| <u>C. W. & Anna Corley Seal</u> | <u>"</u> | <u>Land owners</u> |
| <u>Sunray Oil Company</u> | <u>Tulsa, Oklahoma</u> | <u>Lease owner</u> |
| <u>Chas. B. Key</u> | <u>Oklahoma City, Okla.</u> | <u>Land owner</u> |

REMARKS Why plugged? Exhausted If abandoned oil or gas well, state amount and date of last production
5.24 bbls on 2/15/35.

Correspondence regarding this well should be addressed to Champlin Refining Company
 Address Enid, Oklahoma.

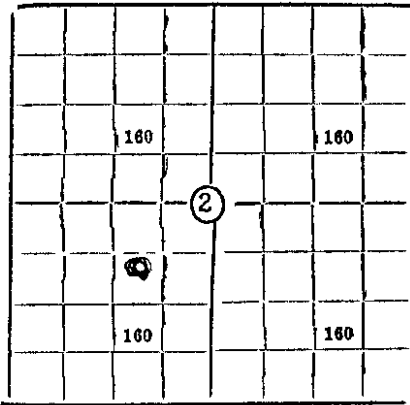
I, A. N. Corkery, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct to the best of my knowledge and belief.
 Subscribed and sworn to before me this 29th day of May, 1935
 My Commission expires August 26, 1937
Manietta Marsh
 Notary Public

640 Acres
N

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION,
Oklahoma City, Oklahoma

60593



Locate Well Correctly

NOTICE All questions on this form must be satisfactorily answered

Company Operating The Carter Oil Company
 Office Address Box 801, Tulsa, Oklahoma
 County Oklahoma Sec 2 Twp 11N Range 3W
 Farm Name A. G. Moore Well No 1 Field GC
 Character of Well (whether oil, gas or dry) Oil

Commenced Plugging 8-2-35 Finished 8-16-35 Total Depth 6515
 Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes
 Name of Conservation Officer who supervised plugging of this well? Dan Howard
 Name of producing sand Simpson Depth top 6302, Bottom 6505
 Show depth and thickness of all fresh water, oil and gas formations

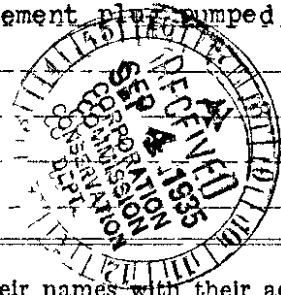
SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------|---------|------|------|---------|--------|------------|
| Simpson | Oil | 6302 | 6505 | 20" | 66 | 0 |
| | | | | 15 1/2" | 292 | 0 |
| | | | | 9" | 5453 | 1152 |
| | | | | 6-5/8" | 6318 | 4367 |

Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used state the character of same and depth placed.
Hole filled with mud to within 4' of top of surface pipe. 4' cement plug pumped in on top of mud. 2" high pressure valve welded into top of surface pipe.

Does the above conform strictly to the oil and gas regulations? Yes



The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below
Skelly Oil Company - Tulsa, Oklahoma
Larkins & Warr - Oklahoma City, Oklahoma

REMARKS Why plugged? Abandoned If abandoned oil or gas well, state amount and date of last production
5 bbl. - Jan. 8, 1935

Correspondence regarding this well should be addressed to R. L. Clifton
 Address Box 978, Oklahoma City, Oklahoma

I, W. T. Brown, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct
 Subscribed and sworn to before me this 3rd day September, 1935
 My Commission expires January 19, 1935
Edward P. Glub
 Notary Public

OKLAHOMA CORPORATION COMMISSION
Oil and Gas Conservation Division
Post Office Box 52000
Oklahoma City, Oklahoma 73152-2000

AFFIDAVIT OF OLD WELL STATUS


| | | | | | | |
|---|--|-----------------|-------------------|-------------------|-----------------------------|--|
| Lease Name / Well No. Burruss 1 | | | | | API No. 109-35736 | |
| Location 1/4 NE 1/4 SW 1/4 SW 1/4 | | Sec 2 | Twp 11N | Rge 03W | County Oklahoma | |


The Oklahoma Corporation Commission deems it to be in the best interest of land owners and mineral owners to clear mineral titles where possible for surface lands where old records show there has been drilled oil and/or gas wells but for which no record has been filed for the plugging of the well. The absence of an official plugging record prevents a clear mineral title in cases of recorded drilling activity.

This document, when properly completed, will serve as a substitute for a plugging record in Oklahoma Corporation Commission well log record files.

A physical search of the above described land was made and the findings are:

- [1] No evidence of any well.
- [2] Evidence of the well and found it to be plugged in accordance with rules in force at the time of plugging.
- [3] Evidence of unplugged well; Producing Non-Producing

| | | |
|--|---------------------------------------|---|
| Attested | | |
|  <small>02/18/2011 08:42:12 am</small> | _____ SigPlus1 | _____ Date |
| Attested | | |
| _____ Signature | _____ Individual requesting search | _____ Date |
| Reason for request | | |
| _____ | | RECEIVED FEB 18 2011 <small>OKLAHOMA CORPORATION COMMISSION KINGFISHER OFFICE</small> |

| | | |
|--------------------------|---|-------------------------|
| District No. 2 |  Signature OCC District Manager | 1 Mar 11 Date |
|--------------------------|---|-------------------------|

0 0 0 1 1 0

640 Acres
N



PLUG BACK
PLUGGING RECORD

MAIL TO CORPORATION COMMISSION,
Oklahoma City, Oklahoma

| | | | | | |
|-----|--|---|--|--|-----|
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| 160 | | | | | 160 |
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LOCATE WELL CORRECTLY

NOTICE All questions on this form must be satisfactorily answered

Company Operating The Carter Oil Company

Office Address Box No. 801, Tulsa, Oklahoma

County Oklahoma Sec 2 Twp 11N Range 3W

Farm Name Classen Co. Well No 1 Field OC

Character of Well (whether oil, gas or dry) Oil

Commenced Plugging 7-2-35 Finished 8-20-35 Total Depth 6307

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Dan Howard

Name of producing sand Simpson Depth top 6435, Bottom 6598

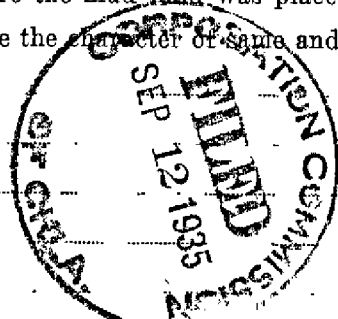
Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|----------------|----------------|-------------|-------------|------|--------|------------|
| <u>Simpson</u> | <u>Oil-Gas</u> | <u>6435</u> | <u>6598</u> | | | |
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed
53 sacks cement run in hole.



Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below

REMARKS Cement off Simpson Why plugged? If abandoned oil or gas well, state amount and date of last production.

Correspondence regarding this well should be addressed to R. L. Clifton

Address Box No. 978, Oklahoma City, Oklahoma

I, W. T. Brown, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct

Subscribed and sworn to before me this 10th day September, 1935

My Commission expires January 19, 1939

W. T. Brown
Edward E. Galuk
Notary Public

OKLAHOMA CORPORATION COMMISSION
Oil and Gas Conservation Division
Post Office Box 52000
Oklahoma City, Oklahoma 73152-2000

AFFIDAVIT OF OLD WELL STATUS



| | | | | | |
|--|-----------------|-------------------|-------------------|---------------------------|-----------------------------|
| Lease Name / Well No. Claude Yount 1 | | | | | API No, 109-35841 |
| Location 1/4 NW 1/4 SE 1/4 SW 1/4 | Sec 2 | Twp 11N | Rge 03W | County Oklahoma | |

The Oklahoma Corporation Commission deems it to be in the best interest of land owners and mineral owners to clear mineral titles where possible for surface lands where old records show there has been drilled oil and/or gas wells but for which no record has been filed for the plugging of the well. The absence of an official plugging record prevents a clear mineral title in cases of recorded drilling activity.


This document, when properly completed, will serve as a substitute for a plugging record in Oklahoma Corporation Commission well log record files.

A physical search of the above described land was made and the findings are:

- [1] No evidence of any well.
- [2] Evidence of the well and found it to be plugged in accordance with rules in force at the time of plugging.
- [3] Evidence of unplugged well; Producing Non-Producing

| | | |
|--|---|---------------------------|
| Attested | | |
|  <small>02/18/2011 09:13:52 am</small> | <hr/> <small>SigPlus1</small> | <hr/> <small>Date</small> |
| Attested | | |
| <hr/> <small>Signature</small> | <hr/> <small>Individual requesting search</small> | <hr/> <small>Date</small> |
| Reason for request | | |
| <hr/> <hr/> <hr/> | | |
|  RECEIVED FEB 18 2011 | | |

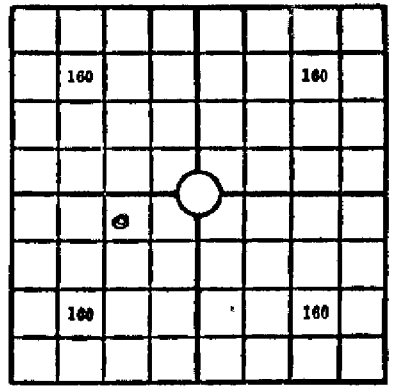
OKLAHOMA
CORPORATION COMMISSION
KINGFISHER OFFICE

| | | |
|--------------------------|---|---------------------------|
| District No. 2 |  <hr/> <small>Signature</small> | <hr/> <small>Date</small> |
|--------------------------|---|---------------------------|

00011060002

640 Acres

N



LOCATE WELL CORRECTLY

PLUGGING RECORD

60591

MAIL TO CORPORATION COMMISSION
 Oil & Gas Conservation Department
 Oklahoma City, Oklahoma

NOTICE All questions on this form must be satisfactorily answered

Company Operating **Transwestern Oil Co.**
 Office Address **Box 88, San Antonio, Texas.**
 County **Oklahoma** Sec **2** Twp **11N** Range **3W**
 Farm Name **Clegg** Well No **1** Field **O.C.**
 Character of Well (whether oil, gas or dry) **Oil**

Commenced Plugging **April 12, 1938** Finished **April 24, 1938** Total Depth **6570'**

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? **Yes**

Name of Conservation Officer who supervised plugging of this well **Dan Howard**

Name of producing sand **Wilcox** Depth top **6241'**, Bottom **6420'**

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------|---------|------|----|------|--------|------------|
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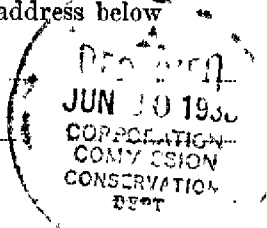
Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole If cement or other plugs were used, state the character of same and depth placed

Hole cemented back to 6 5/8" casing, shot 6 5/8 off mt and pulled mudded 9" back to 3000'
shot and mudded as we pulled 9" let mud settle for several days and than run a cap with
5 sacks of cement.

Does the above conform strictly to the oil and gas regulations? **Yes**

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below

All off-sets to this property have been plugged and abandoned.



REMARKS Why plugged? **exhausted** If abandoned oil or gas well, state amount and date of last production.....

Correspondence regarding this well should be addressed to **Transwestern Oil Co**

Address **Box 88, San Antonio, Texas.**

I the undersigned, being first duly sworn upon oath state that this well record is true correct and complete according to the records of this office and to the best of my knowledge and belief

Subscribed and sworn to before me this the **9** day of **June**

J.A. Langhob
 Name and title of representative of company
 1938

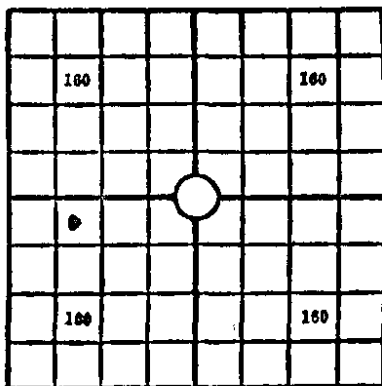
My Commission expires **December 4, 1941**

Harry D. Mork
 Notary Public

0 0 0 1 1 0 6 0 0 0 2

640 Acres

N



LOCATE WELL CORRECTLY

PLUGGING RECORD

60590

MAIL TO CORPORATION COMMISSION
Oil & Gas Conservation Department
Oklahoma City, Oklahoma

NOTICE All questions on this form must be satisfactorily answered.

Company Operating **Transwestern Oil Co.**

Office Address **Box 88, San Antonio, Texas**

County **Oklahoma** Sec **2** Twp **11N** Range **3W**

Farm Name **Clegg** NE NW SW Well No **2** Field **O.C.**

Character of Well (whether oil, gas or dry) **Oil**

Commenced Plugging **1-14-37** and **April 26, 1938** Finished **May 6, 1938** Total Depth **6564'**

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? **Yes**

Name of Conservation Officer who supervised plugging of this well **Dan Howard**

Name of producing sand **Wilcox** Depth top **6222'** Bottom **6385'**

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

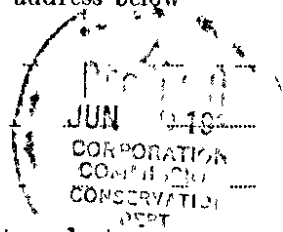
| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------|---------|------|----|------|--------|------------|
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed

Hole cemented back to 6 5/8" casing, shot 6 5/8" off and pulled, mudded 9" back to 3500' and shot off and pulled and mudded as pulled 9" let mud settle for several days and then ran cap with 5 sacks of cement.

Does the above conform strictly to the oil and gas regulations? **Yes**

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below



REMARKS Why plugged? **Exhausted** If abandoned oil or gas well, state amount and date of last production

Correspondence regarding this well should be addressed to **Transwestern Oil Co.**

Address **Box 88, San Antonio, Texas.**

I the undersigned being first duly sworn upon oath, state that this well record is true, correct and complete according to the records of this office and to the best of my knowledge and belief

Subscribed and sworn to before me this the **9** day of **June**

J. A. Traughber
Name and title of representative of company

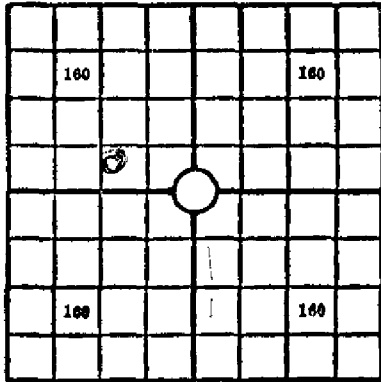
My Commission expires **December 4, 1941**

Harry D. Mark
Notary Public

0 0 0 1 1 0 6 0 0 0 2

640 Acres

N



LOCATE WELL CORRECTLY

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION **60636**
 Oil & Gas Conservation Department
 Oklahoma City, Oklahoma

NOTICE All questions on this form must be satisfactorily answered

Company Operating **Transwestern Oil Co**
 Office Address **Box 88, San Antonio, Texas**
 County **Oklahoma** Sec **2** Twp **11N** Range **3W**
 Farm Name **Daugherty** Well No **1** Field **0 C**
 Character of Well (whether oil, gas or dry) **Oil Well**

Commenced Plugging **4-16-38** Finished **4-26-38** Total Depth **6559'**

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? **Yes**

Name of Conservation Officer who supervised plugging of this well **Dan Howard**

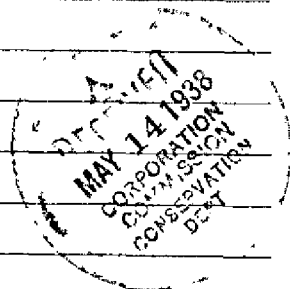
Name of producing sand **Wilcox & Simpson** Depth top **6275'** Bottom **6559'**

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------|---------|------|----|------|--------|------------|
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed

Filled with water to top of 6 5/8 Casing. Then shot @ 5700' second shot @ 5540'. Then ran heavy mud in 9" casing within 900' of top. Shot 9" casing @ 3500' 3300' 3200' and 3171' Started mudding hole 1000' from top. Ran cap with 5 sacks of cement

Does the above conform strictly to the oil and gas regulations? **Yes By Dan Howard**

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below

REMARKS Why plugged? **Dry** If abandoned oil or gas well, state amount and date of last production

Correspondence regarding this well should be addressed to **Box 88, San Antonio, Texas**

Address

I, the undersigned being first duly sworn upon oath, state that this well record is true, correct and complete according to the records of this office and to the best of my knowledge and belief

Subscribed and sworn to before me this **13** day of **May**

My Commission expires **December 14, 1941**

J. A. Young - Div. Supt.
 Name and title of representative of company
Harry D. March
 Notary Public

V

0001106

60592

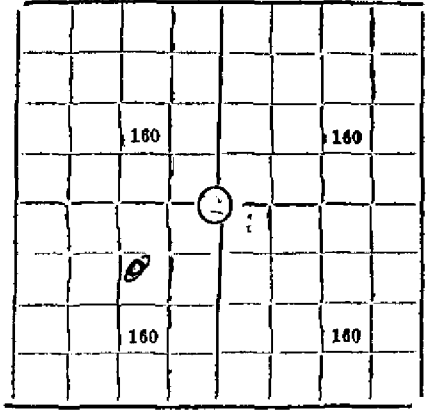
Form 1003

640 Acres
N



PLUGGING RECORD

MAIL TO CORPORATION COMMISSION,
Oklahoma City, Oklahoma



Locate Well Correctly

NOTICE All questions on this form must be satisfactorily answered

Company Operating The Carter Oil Company
Office Address Box No. 801, Tulsa, Oklahoma
County Oklahoma Sec 2 Twp 11N Range 3W
Farm Name D. L. Houston Well No 1 Field OC
Character of Well (whether oil, gas or dry) Oil

Commenced Plugging 8-17-35 Finished 8-28-35 Total Depth 6505

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well? Dan Howard

Name of producing sand Simpson Depth top 6360 Bottom 6504

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

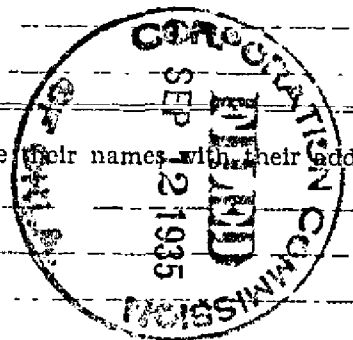
CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------|---------|------|------|---------|--------|------------|
| Simpson | Gas-Oil | 6360 | 6504 | 20" | 54 | 0 |
| | | | | 15 1/2" | 267 | 0 |
| | | | | 9" | 5418 | 1992 |
| | | | | 6-5/8" | 6295 | 5579 |
| | | | | | | |
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used state the character of same and depth placed. Hole filled with mud to within 4' of top of surface pipe. 4' cement plug pumped in on top of mud. 2" high pressure valve welded into top of surface pipe.

Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below. Skelly Oil Company - Tulsa, Oklahoma



REMARKS Why plugged? Abandoned If abandoned oil or gas well, state amount and date of last production

Correspondence regarding this well should be addressed to R. L. Clifton

Address Box No. 978, Oklahoma City, Oklahoma

I, W. T. Brown, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct

Subscribed and sworn to before me this this 10th day September, 1935

My Commission expires January 19, 1939

Edward G. Galus
Notary Public

INSTRUCTIONS

- 1 Form must be completed in its entirety and mailed to the appropriate District Office within 30 days after plugging is completed
- 2 Send original and one (1) copy
- 3 Type or use BLACK ink only
- 4 API No must be on form To get an API No call Petroleum Information at (405) 848 9824
- 5 In specifying the type of plug use the following notations CIBP - cast iron bridge plug CEM - cement plug CIBP + CEM - cast iron bridge plug and cement Packer packer If other abbreviations are used please define
- 6 Cement plugs shall be placed in the well bore as required by the Rules of the Commission plus any additional plugs as may be specified by the District Manager or his representative where unusual local conditions exist (OAC 165 10 11 6)
- 7 The minimum amount of cement normally used in each plug in the open hole shall be a slurry volume equal to the amount necessary to fill the calculated volume of 100 feet of the hole in which the plug is placed
- 8 A 30 feet cement plug is required to be placed near the top of the well (OAC 165 10 11-6)

DISTRICT I
115 W 6th St., Box 779
Bristow, OK 74010
(918) 367-3396

DISTRICT II
101 S 6th Box 1107
Kingfisher, OK 73750
(405) 375 5570

DISTRICT III
1016 Maple, Box 1525
Duncan, OK 73533
(405) 255 0103

DISTRICT IV
703 N Broadway
Ada, OK 74820
(405) 332-3441

FOR COMMISSION USE ONLY

| Approved | Rejected | | Approved | Rejected | |
|----------|----------|--|----------|----------|--|
| _____ | _____ | 1 API No invalid | _____ | _____ | 10 Record of pipe pulled incomplete |
| _____ | _____ | 2 Legal Description invalid for County | _____ | _____ | 11. Well location does not match plat |
| _____ | _____ | 3 Operator No missing/invalid | _____ | _____ | 12 Treatable water depth missing |
| _____ | _____ | 4. Well location missing/invalid | _____ | _____ | 13 Perforation depths missing |
| _____ | _____ | 5 Lease name missing | _____ | _____ | 14. Information on plug - operator |
| _____ | _____ | 6 Well No missing | _____ | _____ | 15 Plugging description missing |
| _____ | _____ | 7. Plugging date invalid | _____ | _____ | 16 Plugging contractors name missing |
| _____ | _____ | 8 Well type missing/invalid | _____ | _____ | 17 Information on plug - cementing company |
| _____ | _____ | 9 Total depth missing/invalid | _____ | _____ | 18 Other _____ |

By signing this form, the District Manager has approved the contents thereof as to form only. Said District Manager does not warrant that the facts provided by the operator are true or that the operator has properly plugged the described well.

[Signature]

Signature of District Manager

[Signature]

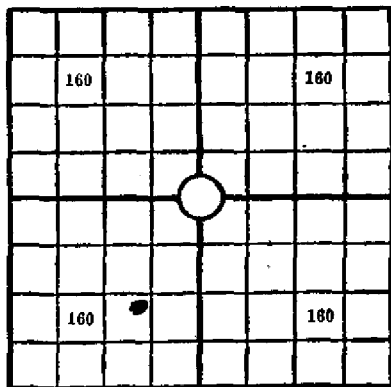
Field Inspector



640 Acres N

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION,
Oklahoma City, Oklahoma



NOTICE: All questions on this form must be satisfactorily answered.

Company Operating The Carter Oil Company
 Office Address Box #801, Tulsa, Oklahoma
 County Oklahoma Sec. 2 Twp. 11N Range 3W
 Farm Name A. L. Foote Well No. #3 Field OC
 Character of Well (whether oil, gas or dry) Oil
 NW SE SW

Commenced Plugging 4-18-35 Finished 5-9-35 Total Depth 6566

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Dan Howard

Name of producing sand Simpson Depth top 6220, Bottom 6422

Show depth and thickness of all fresh water, oil and gas formations.

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|----------------|----------------|-------------|-------------|---------------|-------------|-------------|
| <u>Simpson</u> | <u>Oil-Gas</u> | <u>6220</u> | <u>6422</u> | <u>15 1/2</u> | <u>255</u> | <u>-0-</u> |
| | | | | <u>9</u> | <u>5365</u> | <u>1487</u> |
| | | | | <u>6-5/8</u> | <u>6276</u> | <u>5192</u> |
| | | | | <u>4-3/4</u> | <u>339</u> | <u>-0-</u> |

Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed:
Hole filled with mud to within 4' of top of surface pipe. 4' cement plug pumped in on top of mud. 2" high pressure gate valve welded into top of surface pipe.

Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified; give their names with their address below:

Marco Oil Company, Oklahoma City, Oklahoma
Wirt Franklin Petroleum Corp., Ardmore, Oklahoma

REMARKS: Why plugged? Abandoned If abandoned oil or gas well, state amount and date of last production...
5 bbl. Jan. 5, 1935.

Correspondence regarding this well should be addressed to R. L. Clifton
 Address Box #978, Oklahoma City, Oklahoma

I, W. T. Brown, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct.

Subscribed and sworn to before me this 22nd day May, 1935

My Commission expires January 19, 1939

W. T. Brown
Edward F. Galik
 Notary Public.

TYPE OR USE BLACK INK
See Reverse
for Instructions

OKLAHOMA CORPORATION COMMISSION RECEIVED
Oil and Gas Conservation Division
Jim Thorpe Office Building
Oklahoma City, Oklahoma 73105-1993
Form 1003/1003C
(Rev 1993)

406309287

API No : 35-109-35871
 Lease Name/No.: Foote #4
 Location: 1/4 NE 1/4 SE 1/4 SW 1/4 Total Depth: 6577
 Sec: 2 Twp: 11N Rge: 3W County: Oklahoma
 Ft. from 1/4 sec: 990 FSL 330 FWL Plugging Date: 6/18/94
 Well Class: Oil Treatable Water Depth: 775

OKLAHOMA CORPORATION COMMISSION
 JUL 05 1994
 OIL & GAS CONSERVATION
 X

Operator
 Name: Combined Resources Corporation OCC/OTC No.: 07775
 Address: 14000 Quail Springs Parkway, Suite 3300 Phone: (405) 748-7300
 City: Oklahoma City State: OK Zip: 73134

Pipe Record

| Size | Run (ft) | Pulled (ft) | |
|--------|----------|-------------|-----------|
| 13 3/8 | 25 | | Conductor |
| 9-5/8 | 997 | None | Surface |
| | | | I. C. |
| | | | I. C. |
| 7" | 6577 | 1114 | P. C. |

Perforation Depths

Set 1 - From 6022 To 6045
 Set 2 - From To
 Set 3 - From To
 Set 4 - From To



INSTRUCTIONS

- 1 Form must be completed in its entirety and mailed to the appropriate District Office within 30 days after plugging is completed
- 2 Send original and one (1) copy
- 3 Type or use BLACK ink only
- 4 API No must be on form To get an API No call Petroleum Information at (405) 848 9824
- 5 In specifying the type of plug use the following notations CIBP - cast iron bridge plug, CEM - cement plug CIBP + CEM - cast iron bridge plug and cement Packer - packer If other abbreviations are used please define
- 6 Cement plugs shall be placed in the well bore as required by the Rules of the Commission plus any additional plugs as may be specified by the District Manager or his representative, where unusual local conditions exist (OAC 165 10 11 6)
- 7 The minimum amount of cement normally used in each plug in the open hole shall be a slurry volume equal to the amount necessary to fill the calculated volume of 100 feet of the hole in which the plug is placed
- 8 A 30 feet cement plug is required to be placed near the top of the well (OAC 165 10 11-6)

DISTRICT I
115 W 6th St, Box 779
Bristow OK 74010
(918) 367 3396

DISTRICT II
101 S 6th, Box 1107
Kingfisher, OK 73750
(405) 375 5570

DISTRICT III
1016 Maple, Box 1525
Duncan, OK 73533
(405) 255 0103

DISTRICT IV
703 N Broadway
Ada OK 74820
(405) 332 3441

FOR COMMISSION USE ONLY

| Approved | Rejected | | Approved | Rejected | |
|----------|----------|--|----------|----------|---|
| _____ | _____ | 1 API No invalid | _____ | _____ | 10 Record of pipe pulled incomplete |
| _____ | _____ | 2 Legal Description invalid for County | _____ | _____ | 11. Well location does not match plat |
| _____ | _____ | 3 Operator No missing/invalid | _____ | _____ | 12 Treatable water depth missing |
| _____ | _____ | 4 Well location missing/invalid | _____ | _____ | 13 Perforation depths missing |
| _____ | _____ | 5. Lease name missing | _____ | _____ | 14. Information on plug - operator |
| _____ | _____ | 6 Well No missing | _____ | _____ | 15. Plugging description missing |
| _____ | _____ | 7 Plugging date invalid | _____ | _____ | 16 Plugging contractors name missing |
| _____ | _____ | 8 Well type missing/invalid | _____ | _____ | 17. Information on plug - cementing company |
| _____ | _____ | 9 Total depth missing/invalid | _____ | _____ | 18. Other _____ |



By signing this form, the District Manager has approved the contents thereof as to form only. Said District Manager does not warrant that the facts provided by the operator are true or that the operator has properly plugged the described well.

Signature of District Manager

Field Inspector

Tommy L. Adams

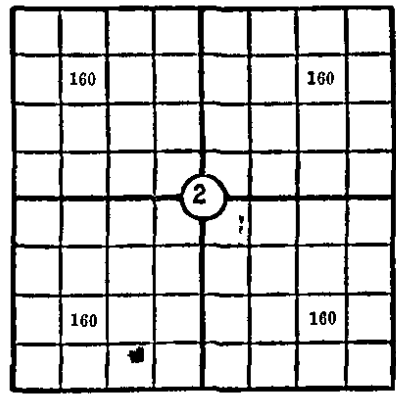
00011060002

60584 Form 1003

640 Acres
N

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION,
Oklahoma City, Oklahoma



LOCATE WELL CORRECTLY

NOTICE All questions on this form must be satisfactorily answered

Company Operating The Carter Oil Company
 Office Address Box #801, Tulsa, Oklahoma
 County Oklahoma Sec 2 Twp 11N Range 37
 Farm Name Foots-Brisco Well No 1 Field OC
 Character of Well (whether oil, gas or dry) Oil

Commenced Plugging 6-9-35 Finished 6-20-35 Total Depth 6565

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Dan Howard

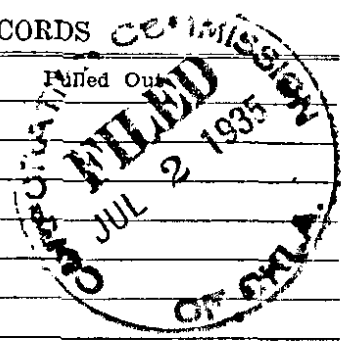
Name of producing sand Simpson Depth top 6205, Bottom 6565

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Runned Out |
|----------------|------------|-------------|-------------|----------------|-------------|-------------|
| <u>Simpson</u> | <u>Oil</u> | <u>6205</u> | <u>6265</u> | <u>15 1/2"</u> | <u>257</u> | <u>0</u> |
| | | | | <u>9"</u> | <u>5498</u> | <u>1994</u> |
| | | | | <u>6-5/8"</u> | <u>6219</u> | <u>5622</u> |
| | | | | | | |
| | | | | | | |
| | | | | | | |



Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed.
Hole filled with mud to within 4' of top of surface pipe. 4' cement plug pumped in on top of mud. 2" high pressure valve welded into top of surface pipe.

Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below
Marco Oil Company - 402 Savings Bldg., Oklahoma City, Oklahoma

REMARKS Why plugged? Abandoned If abandoned oil or gas well, state amount and date of last production.
Well last produced in August, 1934--Opened 6 days and made 7 bbls.

Correspondence regarding this well should be addressed to R. L. Clifton
 Address Box #978, Oklahoma City, Oklahoma

I, W. T. Brown, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct

W. T. Brown

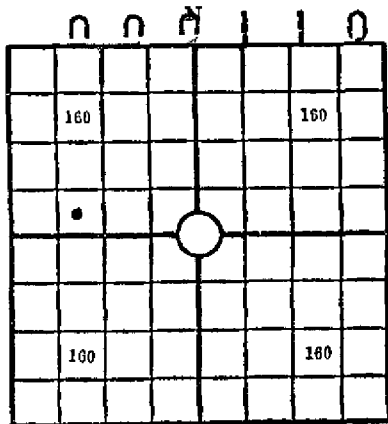
Subscribed and sworn to before me this 1st day July, 1935

My Commission expires January 19th, 1939
Edward P. Edlin
 Notary Public

640 Acres

PLUGGING ~~RECORD~~

MAIL TO CORPORATION COMMISSION,
Oklahoma City, Oklahoma



LOCATE WELL CORRECTLY

NOTICE All questions on this form must be satisfactorily answered
 Company Operating Anderson & Kerr Drilling Company.
 Office Address 2009 1 St Natl Bldg, Okla City, Okla.
 County Oklahoma. Sec 2 Twp 11 Range 3
 Farm Name Hathaway. Well No #1. Field OklaCity.
 Character of Well (whether oil, gas or dry) Cil.

Commenced Plugging 11-14-35 Finished 11-15-35 Total Depth 6400 Ft.

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes.

Name of Conservation Officer who supervised plugging of this well Dan Howard, Okla City, Okla.

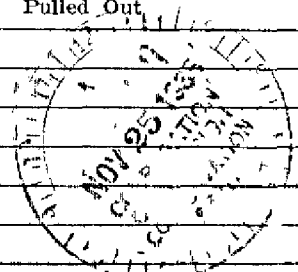
Name of producing sand Wilcox. Depth top 6230 Ft, Bottom 6400 Ft.

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------|---------|------|------|---------|--------|------------|
| | Same as | | well | record. | | |
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed

Plugged back with 13 sacks of cement.

Does the above conform strictly to the oil and gas regulations? Yes.

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below

REMARKS Why plugged? Water. If abandoned oil or gas well, state amount and date of last production

Correspondence regarding this well should be addressed to Anderson & Kerr Drilling Company,
Address 2009 1 St Natl Bldg, Okla City, Okla.

I, John J. Lawry, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct

Subscribed and sworn to before me this 21 day Nov 1935

My Commission expires Mar 17. 1937., 193

ANDERSON & KERR DRILLING CO.

BY John Lawry

Wallace Neff

Notary Public

OKLAHOMA CORPORATION COMMISSION
Oil and Gas Conservation Division
Post Office Box 52000
Oklahoma City, Oklahoma 73152-2000

AFFIDAVIT OF OLD WELL STATUS

| | | | | | | |
|--------------------------------------|--|----------|------------|------------|----------------------|--|
| Lease Name / Well No. Holder 2 | | | | | API No, 109-35822 | |
| Location 1/4 SW 1/4 SE 1/4 SW 1/4 | | Sec 2 | Twp 11N | Rge 03W | County Oklahoma | |

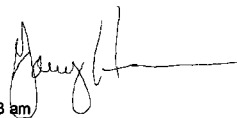
The Oklahoma Corporation Commission deems it to be in the best interest of land owners and mineral owners to clear mineral titles where possible for surface lands where old records show there has been drilled oil and/or gas wells but for which no record has been filed for the plugging of the well. The absence of an official plugging record prevents a clear mineral title in cases of recorded drilling activity.

This document, when properly completed, will serve as a substitute for a plugging record in Oklahoma Corporation Commission well log record files.

A physical search of the above described land was made and the findings are:

- [1] No evidence of any well.
 [2] Evidence of the well and found it to be plugged in accordance with rules in force at the time of plugging.
 [3] Evidence of unplugged well; Producing Non-Producing

Attested

02/18/2011 09:09:03 am  _____ Date _____
SigPlus1

Attested

Signature Individual requesting search Date

Reason for request

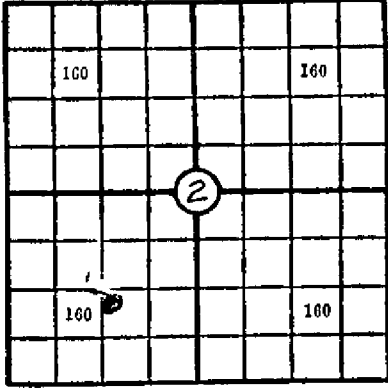
RECEIVED
FEB 18 2011
OKLAHOMA CORPORATION COMMISSION
KINGFISHER OFFICE

District No. 2

Signature OCC District Manager Date 1 Mar 11

0 0 0 1 1 0 6 0 0 0 2

640 Acres
N



LOCATE WELL CORRECTLY

~~BACK~~
PLUGGING RECORD
MAIL TO CORPORATION COMMISSION,
Oklahoma City, Oklahoma

NOTICE All questions on this form must be satisfactorily answered

Company Operating Slick-Urschel Oil Co
Office Address 403 Colcord Bldg, Okla City
County Okla Sec 2 Twp 11N Range 3E
Farm Name Key Well No 1 Field Okla City
Character of Well (whether oil, gas or dry) Oil

Commenced Plugging Back 3-8-34 Finished 3-8-34 Total Depth 6114

Was permission obtained from the Corporation Commission or its agents before plugging ^{back} was commenced? Yes

Name of Conservation Officer who supervised plugging of this well _____

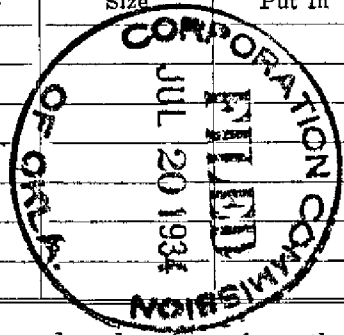
Name of producing sand Lower Simpson Depth top 6215, Bottom 6425

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------|---------|------|----|------|--------|------------|
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed.

Cemented back from 6231 - 6114

Does the above conform strictly to the oil and gas regulations? _____

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below:

REMARKS: Why plugged? _____ If abandoned oil or gas well, state amount and date of last production _____

Correspondence regarding this well should be addressed to _____
Address _____

I, _____, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct

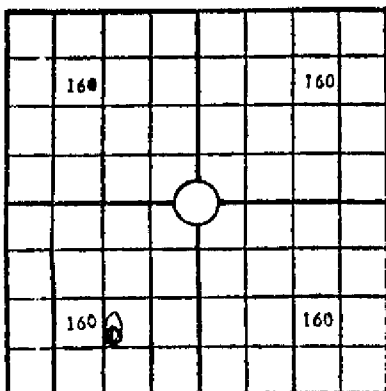
Subscribed and sworn to before me this _____ day _____, 193 _____

My Commission expires _____, 193 _____

Notary Public

00011060002

640 Acres
N



Locate Well Correctly

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION OKLAHOMA CITY, OKLAHOMA.

NOTICE All questions on this form must be satisfactorily answered.

Company Operating Slick-Urschel Oil Co.

Office Address Box 1146, Okla. City, Okla.

County Okla. Sec 2 Twp 11 Range 3W

Farm Name Key Well No 2 Field Okla. City

Character of Well (whether oil, gas or dry) Oil

Commenced Plugging April 1936 Finished April 1936 Total Depth 6553

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Mr. Howard

Name of producing sand Simpson, Depth top 6208, Bottom 6502

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|---------------------|------------|-------------|-------------|-----------|-------------|--------------|
| <u>Simpson sand</u> | <u>Oil</u> | <u>6208</u> | <u>6502</u> | <u>9"</u> | <u>5504</u> | <u>1700'</u> |
| | | | | <u>6"</u> | <u>6199</u> | <u>5265'</u> |
| | | | | | | |
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed: Hole pumped full of mud and a 2 1/2" pipe with valve cemented in top.

Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below

Wirt Franklin Oil Co. Okla City

Chaplin R & O. Co. Enid, Okla.

REMARKS: Why plugged? Abandoned If abandoned oil or gas well, state amount and date of last production 5 bbla. Sept. 1935.

Correspondence regarding this well should be addressed to Slick-Urschel Oil Co.

Address Box 1146, Okla. City, Okla.

I, H.F. King, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct

Subscribed and sworn to before me this 30 day of April, 1936

My Commission expires April 16, 1939, 19

R. Price
Notary Public

OKLAHOMA CORPORATION COMMISSION
Oil and Gas Conservation Division
Post Office Box 52000
Oklahoma City, Oklahoma 73152-2000

AFFIDAVIT OF OLD WELL STATUS


| | | | | | | |
|--|--|-----------------|-------------------|-------------------|---------------------------|--|
| Lease Name / Well No. Leala Rumley 1 | | | | | API No. 109-769 | |
| Location 1/4 NE 1/4 SW 1/4 SW 1/4 | | Sec 2 | Twp 11N | Rge 03W | County Oklahoma | |



The Oklahoma Corporation Commission deems it to be in the best interest of land owners and mineral owners to clear mineral titles where possible for surface lands where old records show there has been drilled oil and/or gas wells but for which no record has been filed for the plugging of the well. The absence of an official plugging record prevents a clear mineral title in cases of recorded drilling activity.

This document, when properly completed, will serve as a substitute for a plugging record in Oklahoma Corporation Commission well log record files.

A physical search of the above described land was made and the findings are:

- [1] No evidence of any well.
- [2] Evidence of the well and found it to be plugged in accordance with rules in force at the time of plugging.
- [3] Evidence of unplugged well; Producing Non-Producing

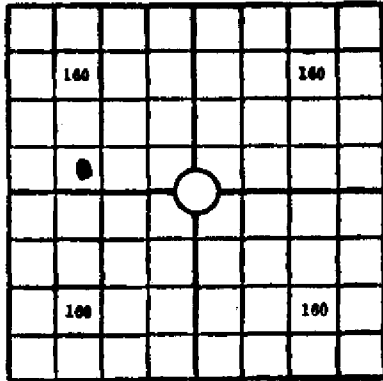
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| Attested | | |
|  <small>02/18/2011 08:33:57 am</small> <small>SigPlus1</small> | _____ | Date |
| Attested | | |
| _____ | Individual requesting search | Date |
| Reason for request | | |
| _____ | | RECEIVED |
| _____ | | FEB 18 2011 |
| _____ | | OKLAHOMA CORPORATION COMMISSION KINGFISHER OFFICE |

| | | |
|--------------------------|--|---|
| District No. 2 |  Signature OCC District Manager |  Date 1 Mar 11 |
|--------------------------|--|---|

0 0 0 1 1 0 5 0 0 0 2

640 Acres

N



LOCATE WELL CORRECTLY

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION **60615**
Oil & Gas Conservation Department
Oklahoma City, Oklahoma

NOTICE: All questions on this form must be satisfactorily answered.

Company Operating Chatham Oil Company
Office Address Philtower Bldg., Tulsa, Oklahoma.
County Oklahoma Sec. 2 Twp. 11N Range 3W
Oklahoma
Farm Name Lindsay Well No. 3 Field City
Character of Well (whether oil, gas or dry) Oil

Commenced Plugging Oct. 6, 1936 Finished November 5, 1936 Total Depth 6500

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Don Howard Bill Bradley

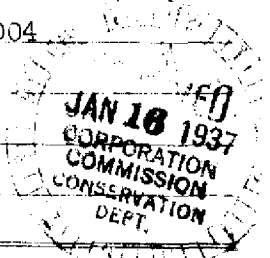
Name of producing sand Wilcox Depth top 6223; Bottom 6500

Show depth and thickness of all fresh water, oil and gas formations.

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-------------|-----------|------|------|-------|--------|------------|
| Wilcox Sand | Oil & gas | 6223 | 6500 | 15" | 236 | None |
| Oswego Lime | No show | 6088 | 6113 | 9 | 4974 | 2300 |
| Sand | No show | 5128 | 5150 | 6 5/8 | 6212 | 5004 |
| | | | | | | |
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed:

Set a Lane Wells bridging plug at 6175'. Tested with Lane Wells perforating gun from 6088 to 6113 and from 5128 to 5150 - no show. Shot 6 5/8" casing loose at 5004 and 9 inch casing loose at 2300' and pulled casing. Filled hole full of mud and placed a 25' cement plug in top.

Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified; give their names with their address below:

Slick-Urschel Oil Company, Oklahoma City, Oklahoma.
Davon Oil Company, Oklahoma City, Oklahoma.
Phillips Petroleum Company, Bartlesville, Oklahoma.

REMARKS: Why plugged? Non-profitable abandoned oil or gas well, state amount and date of last production
August 1, 1936. pumped 5 bbls. oil and 30 bbls. of water in 24 hours.

Correspondence regarding this well should be addressed to Chatham Oil Company, Philtower Bldg., Tulsa, Okla.

Address _____

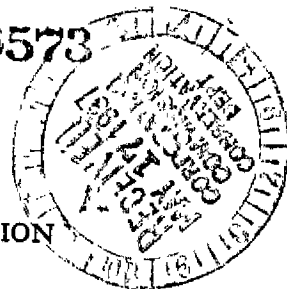
I, the undersigned, being first duly sworn upon oath, state that this well record is true, correct and complete according to the records of this office and to the best of my knowledge and belief.

Subscribed and sworn to before me this the 15 day of January, 1937.
William Reynolds Vice President.
My Commission expires Oct. 4, 1937
William Reynolds Notary Public.

00011060002

Form 1003

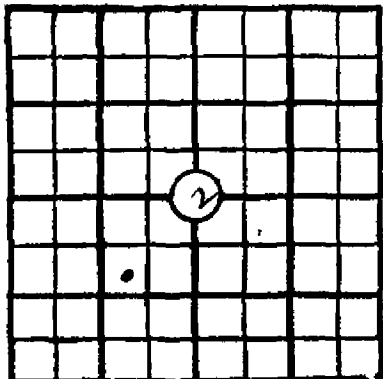
60573



640 Acres
N

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION
Oil & Gas Conservation Department
Oklahoma City, Oklahoma



LOCATE WELL CORRECTLY

NOTICE All questions on this form must be satisfactorily answered

Company Operating The Carter Oil Company
Office Address Box 978, Oklahoma City.
County Oklahoma Sec 2 Twp. 11N Range 3W
Farm Name M. E. Alexander Well No 2 Field O.G.
SW NE SW
Character of Well (whether oil, gas or dry) Oil

Commenced Plugging 4/10/37 Finished 4/27/37 Total Depth 6548'

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Dan Howard

Name of producing sand Depth top , Bottom

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|----------------|------------|--------------|--------------|----------------|--------------|--------------|
| <u>Simpson</u> | <u>Oil</u> | <u>6222'</u> | <u>6475'</u> | <u>6-5/8"</u> | <u>6239'</u> | <u>4286'</u> |
| | | | | <u>9"</u> | <u>5422'</u> | <u>1524'</u> |
| | | | | <u>15-1/2"</u> | <u>254'</u> | <u>None</u> |
| | | | | <u>20"</u> | <u>54'</u> | <u>None</u> |
| | | | | | | |
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed

Set bridge at 5304'. Cemented with 1 bushel of chat and 5 sacks of cement.
Hole filled with mud to within 4' of top of surface pipe. 4' cement plug put in on
top of mud. 2" high pressure valve welded into top of surface pipe.

Does the above conform strictly to the oil and gas regulations? Yes.

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below

Skelly Oil Company, Tulsa, Oklahoma

REMARKS Why plugged? abandoned. If abandoned oil or gas well, state amount and date of last production

8 bbls. oil and 5 bbls. water March 12, 1937.

Correspondence regarding this well should be addressed to W. A. Jones

Box 978

Address Oklahoma City, Okla.

I, the undersigned being first duly sworn upon oath state that this well record is true, correct and complete according to the records of this office and to best of my knowledge and belief

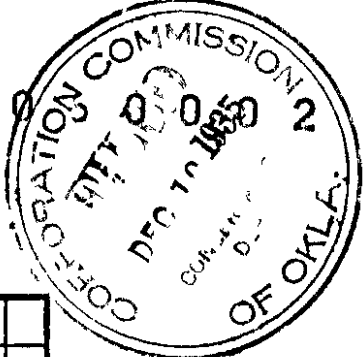
B. M. Shultz
Name and title of representative of company

Subscribed and sworn to before me this the 15th day of May, 1937

My Commission expires 1-19-39

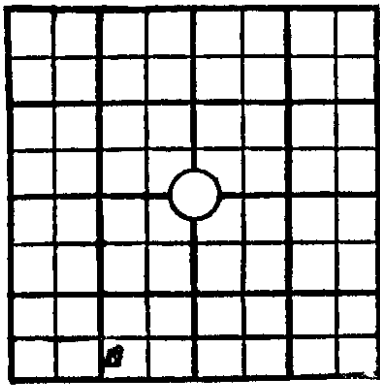
Edward P. Eulick
Notary Public

0 0 0 1 1 2



60570

640 Acres
N



LOCATE WELL CORRECTLY

PLUGGING RECORD
MAIL TO CORPORATION COMMISSION
Oil & Gas Conservation Department
Oklahoma City, Oklahoma

NOTICE All questions on this form must be satisfactorily answered

Company Operating The Carter Oil Company
Office Address Box 801, Tulsa, Oklahoma
County Oklahoma Sec 2 Twp 11N Range 3W
Farm Name J. W. Miller Well No 1 Field Q.C.
SWSW SE SW
Character of Well (whether oil, gas or dry) Oil

Commenced Plugging 11-6-35 Finished 11-21-35 Total Depth 6507

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Dan Howard

Name of producing sand Simpson Depth top 6350 , Bottom 6420

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|----------------|----------------|-------------|-------------|----------------|-------------|-------------|
| <u>Simpson</u> | <u>Gas-Oil</u> | <u>6198</u> | <u>6507</u> | <u>15 1/2"</u> | <u>257</u> | <u>0</u> |
| | | | | <u>9"</u> | <u>5468</u> | <u>497</u> |
| | | | | <u>6-5/8"</u> | <u>6312</u> | <u>4079</u> |
| | | | | | | |
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed

Hole filled with mud to within 4' of top of surface pipe. 4' cement plug pumped in on top of mud. 2" high pressure gate valve welded into top of surface pipe.

Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below.

- Wirt Franklin Petroleum Co., Ardmore, Oklahoma
- Indian Territory Illum. Oil Co., Bartlesville, Oklahoma
- Marco Oil Company, Oklahoma City, Oklahoma

REMARKS Why plugged? Abandoned If abandoned oil or gas well, state amount and date of last production 20 bbl. October 25, 1935.

Correspondence regarding this well should be addressed to Mr. R. L. Clifton
Box 978

Address Oklahoma City, Oklahoma

I, the undersigned, being first duly sworn upon oath, state that this well record is true, correct and complete according to the records of this office and to best of my knowledge and belief

A. P. Cook
Name and title of representative of company

Subscribed and sworn to before me this the 9th day of December, 1935

My Commission expires January 19, 1935

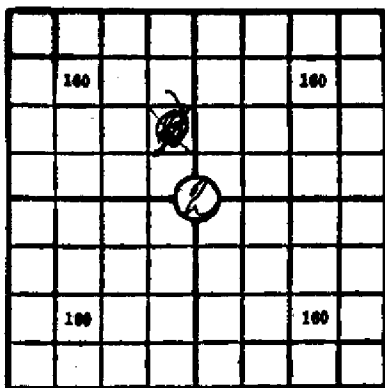
Edward E. Edick
Notary Public

8 POTTED

640 Acres
0 0 0 N 1 1 0 6 0 0 0 2

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION
Oil & Gas Conservation Department
Oklahoma City, Oklahoma



LOCATE WELL CORRECTLY

NOTICE: All questions on this form must be satisfactorily answered.

Company Operating Anderson Richard Oil Corp.
Office Address 1000 Ramsey Tower
County Oklahoma Sec. 2 Twp. 11 N Range 3 W
Farm Name Osphange Well No. 1 Field O.E.
Character of Well (whether oil, gas or dry) Gas

Commenced Plugging 6255 Finished 6140 Total Depth 6140

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Ian Howard

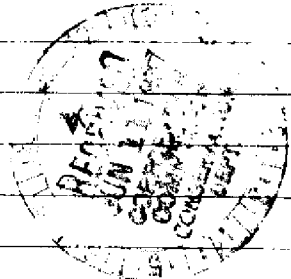
Name of producing sand Oswego Depth top 6040 Bottom 6140

Show depth and thickness of all fresh water, oil and gas formations.

SAND OR ZONE RECORDS

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------|---------|------|----|------|--------|------------|
| | | | | | | |
| | | | | | | |
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Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed:

Well was cleaned out to 6255 and plugged back with cement to 6140. Cement was dumped in the hole with a dump boiler.

Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified; give their names with their address below:

REMARKS: Why plugged? For Gas. If abandoned oil or gas well, state amount and date of last production

Correspondence regarding this well should be addressed to S.S. Reames - 1000 Ramsey Tower, Oklahoma City, Okla.

Address

I, the undersigned, being first duly sworn upon oath, state that this well record is true, correct and complete according to the records of this office and to the best of my knowledge and belief.

Subscribed and sworn to before me this the 9 day of June, 1937.
My Commission expires 6-27-37
S.S. Reames Field Supt.
Name and title of representative of company.
Clair B. Chedee
Notary Public.

OKLAHOMA CORPORATION COMMISSION
Oil and Gas Conservation Division
Post Office Box 52000
Oklahoma City, Oklahoma 73152-2000

AFFIDAVIT OF OLD WELL STATUS

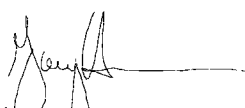
| | | | | | | |
|---|--|-----------------|-------------------|-------------------|-----------------------------|--|
| Lease Name / Well No. Sahan 1 | | | | | API No. 109-35756 | |
| Location 1/4 SE 1/4 SW 1/4 SW 1/4 | | Sec 2 | Twp 11N | Rge 03W | County Oklahoma | |

The Oklahoma Corporation Commission deems it to be in the best interest of land owners and mineral owners to clear mineral titles where possible for surface lands where old records show there has been drilled oil and/or gas wells but for which no record has been filed for the plugging of the well. The absence of an official plugging record prevents a clear mineral title in cases of recorded drilling activity.

This document, when properly completed, will serve as a substitute for a plugging record in Oklahoma Corporation Commission well log record files.

A physical search of the above described land was made and the findings are:

- [1] No evidence of any well.
- [2] Evidence of the well and found it to be plugged in accordance with rules in force at the time of plugging.
- [3] Evidence of unplugged well; Producing Non-Producing

| | | |
|--|---------------------------------------|---|
| Attested | | |
|  <small>02/18/2011 08:46:31 am</small> | | _____ Date |
| SigPlus1 | | |
| Attested | | |
| _____ Signature | _____ Individual requesting search | _____ Date |
| Reason for request | | |
| _____ _____ _____ | | RECEIVED FEB 18 2011 <small>OKLAHOMA CORPORATION COMMISSION KINGFISHER OFFICE</small> |

| | | |
|--------------------------|--|----------------------------------|
| District No. 2 | | |
| _____ Signature | T. Cupp OCC District Manager | _____ Date 1 Mar 11 |

OKLAHOMA CORPORATION COMMISSION
Oil and Gas Conservation Division
Post Office Box 52000
Oklahoma City, Oklahoma 73152-2000

AFFIDAVIT OF OLD WELL STATUS

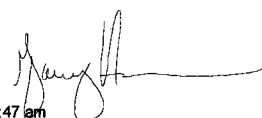

| | | | | | | |
|---|--|-----------------|-------------------|-------------------|-----------------------------|--|
| Lease Name / Well No. Smith 2 | | | | | API No. 109-35757 | |
| Location 1/4 SE 1/4 NW 1/4 SW 1/4 | | Sec 2 | Twp 11N | Rge 03W | County Oklahoma | |

The Oklahoma Corporation Commission deems it to be in the best interest of land owners and mineral owners to clear mineral titles where possible for surface lands where old records show there has been drilled oil and/or gas wells but for which no record has been filed for the plugging of the well. The absence of an official plugging record prevents a clear mineral title in cases of recorded drilling activity.


This document, when properly completed, will serve as a substitute for a plugging record in Oklahoma Corporation Commission well log record files.

A physical search of the above described land was made and the findings are:

- [1] No evidence of any well.
 [2] Evidence of the well and found it to be plugged in accordance with rules in force at the time of plugging.
 [3] Evidence of unplugged well; Producing Non-Producing

| | | |
|--|---------------------------------------|---|
| Attested | | |
|  <small>02/18/2011 08:47:47 am</small> | _____ SigPlus1 | _____ Date |
| Attested | | |
| _____ Signature | _____ Individual requesting search | _____ Date |
| Reason for request | | |
| _____ _____ _____ | | |
| | |  FEB 18 2011 |

OKLAHOMA CORPORATION COMMISSION
KINGFISHER OFFICE

| | | |
|---|---|--|
| District No. 2 | | |
|  Signature OCC District Manager |  Date | |

60524



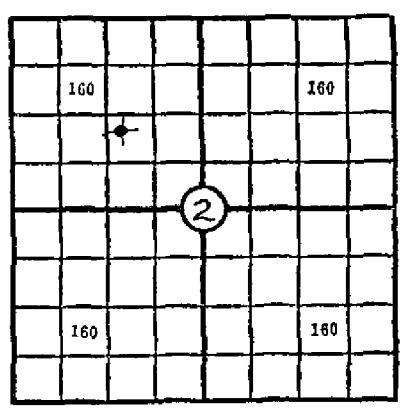
0 0 0 1 1 0 6 0 0 0 2

640 Acres
N

gr

PLUGGING RECORD

MAIL TO CORPORATION COMMISSION,
Oklahoma City, Oklahoma



LOCATE WELL CORRECTLY

NOTICE All questions on this form must be satisfactorily answered

Company Operating Morgan Pet. Corp.
Office Address Lucy Products Corp. 1st Natl. Bldg. Oklahoma City
County Okla Sec 2 Twp 11 N Range 3W
Farm Name Stout Well No 1 Field D.C.

Character of Well (whether oil, gas or dry) Oil & Gas

Commenced Plugging April 19 1937 Finished April 27 Total Depth 6510'

Was permission obtained from the Corporation Commission or its agents before plugging was commenced? Yes

Name of Conservation Officer who supervised plugging of this well Dan Howard

Name of producing sand Wilcox Depth top 6205' Bottom 6265'

Show depth and thickness of all fresh water, oil and gas formations

SAND OR ZONE RECORDS *Log Shows*

CASING RECORDS

| Formation | Content | From | To | Size | Put In | Pulled Out |
|-----------------------|----------------------|-------------|-------------|----------------|-------------|------------|
| <u>Wilcox Sand</u> | <u>Oil & Gas</u> | <u>6205</u> | <u>6265</u> | <u>20"</u> | <u>50'</u> | |
| <u>School Land 5d</u> | <u>"</u> | <u>6475</u> | <u>6510</u> | <u>15 1/2"</u> | <u>231'</u> | |
| | | | | <u>9" O.D.</u> | <u>5600</u> | |
| | | | | <u>6 5/8"</u> | <u>6205</u> | |

Describe in detail the manner in which the well was plugged, indicating where the mud fluid was placed and the methods used in introducing it into the hole. If cement or other plugs were used, state the character of same and depth placed

This well was cleaned out with sand pump to 6265' in green shale below Wilcox sand 10 sks or cement was dumped with dump bailer which plugged back to 6248'. This conforms with CC Order #10946. Sand was mudded and cement bridge set in casing at 6010'. Hole filled with mud to surface and cemented with 5 sacks in 15 1/2" at surface.
Does the above conform strictly to the oil and gas regulations? Yes

The Law requires that adjacent lease, royalty and land-owners be notified, give their names with their address below

The Garter Oil Co. Okla. City.
Sunray Oil Co. "
(Slick (Urschel) Transwestern Oil "
Anderson - Prichard Oil Corp. "

REMARKS. Why plugged? Exhausted If abandoned oil or gas well, state amount and date of last production.
1935 October

Correspondence regarding this well should be addressed to Lucy Products Corp
Address 910 S Boston Tulsa Oklahoma

I, C.C. Kimberley, being first duly sworn, on oath state that I have knowledge of the facts and matter herein set forth and that the same are true and correct

Subscribed and sworn to before me this 23 day April
My Commission expires March 27, 1938

C.C. Kimberley mgr
1707 1st Nat Bank Bldg
1937
J. Chapman

701060621

Form 1073 Rev. 2001

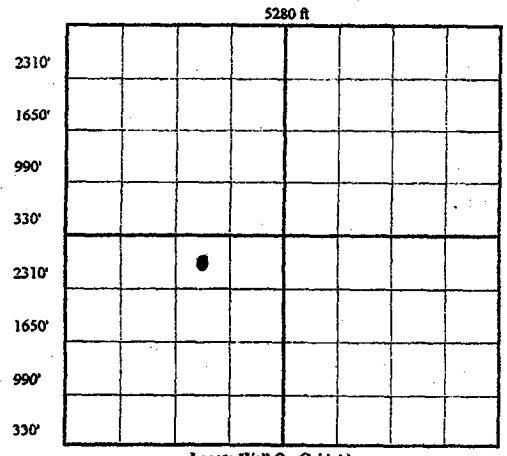
Instructions

- A. Please type or print using black ink.
- B. Form must be signed by former operator and new operator.
- C. Outline boundaries of lease and spot well being transferred.
- D. Attach 1002A for well.
- E. Questions should be directed to Well Records (405) 521-2275.

OKLAHOMA CORPORATION COMMISSION

Oil & Gas Conservation Division
 Post Office Box 52000
 Oklahoma City, OK 73152-2000

Transfer of Operator
 OAC 165:10-1-15



| | | | | | |
|---------------------------|--------------------------|-------------------|----------|--------------------|----------|
| API No. | 109-00736 | | | OTC Prod. Unit No. | |
| Location | 1/4 NW 1/4 NE 1/4 SW 1/4 | Sec. 2 | Twp. 11N | Rge. 3W | |
| Ft FSL of Qtr Sec | 2300 FSL | Ft FWL of Qtr Sec | 1800 FWL | County | Oklahoma |
| Current Well Name/No. | Ed Dillon COMMUNITY # 2 | | | | |
| Original Well Name/No. | Ed Dillon COMMUNITY # 2 | | | | |
| Unit Name (if applicable) | | | | | |

Well Class: Oil Gas Dry Plugged

| | | | |
|---------------------------|--|---------|-------|
| Producing formation(s) | Wilcox, Prue, Oswego, Oolitic, Cleveland, Layton, Checkboard, Hoover, Pawhuska | | |
| Oil Transporter/Purchaser | Sunoco | OTC No. | 21108 |
| Gas Measurer | Duke Energy Field Ser. | OTC No. | 16132 |

The effective date of transfer of this well for the purposes of Commission records, is the date the transfer is approved by the Commission.

CURRENT OPERATOR

| | | | | |
|---|--------------------------|---------|--------------|-----------|
| Name | Bent Twig Operating Corp | | OCC No. | 05934 |
| Address | PO Box 94716 | | | |
| City | OKC | State | OK | Zip 73143 |
| Phone No. | 405 671 3453 | FAX No. | 405 677 5156 | |
| I verify that I am the legal operator of record with authority to transfer operatorship of this well. | | | | |
| Signature | Byron R Neher | | | |
| Name & Title (Typed or Printed) | Byron Neher Pres | | | |
| Signed and sworn to before me this | 9 | day of | JAN | 2007 |
| Commission # | 00000918 | | | |
| Expires | Jan. 14, 2008 | | | |
| My commission expires: | | | | |

NEW OPERATOR

| | | | | |
|--|-----------------------|---------|--------------|-----------|
| Name | TNT Operating Co. Inc | | OCC No. | 19087 |
| Address | PO Box 94716 | | | |
| City | OKC | State | OK | Zip 73143 |
| Phone No. | 405 671 3453 | FAX No. | 405 677 5156 | |
| Being the new operator, as of the effective date and time of transfer accept the facts presented as being true and correct and accept the operational responsibility for the well on the described property. | | | | |
| Signature | Byron R Neher | | | |
| Name & Title (Typed or Printed) | Byron Neher Pres | | | |
| Signed and sworn to before me this | 9 | day of | JAN | 2007 |
| Commission # | 00000918 | | | |
| Expires | Jan. 14, 2008 | | | |
| My commission expires: | | | | |

I verify under oath that I have exercised due diligence in attempting to locate the current operator of record according to OCC records, who has abandoned the above well/lease and cannot be located to obtain signature. I have attached a copy of the certified recorded assignment of lease.

Signature

Signed and sworn to before me this _____ day of _____

Notary Public

My commission expires: _____

FOR OCC USE ONLY

Surety Dept. Approved Rejected Date JAN 22 2007

Well Records Dept. Approved Rejected Date 1-22-2007

NOTE: By processing this Form 1073, the Oklahoma Corporation Commission has approved the contents thereof as to form only. Oklahoma Corporation Commission does not warrant that the facts provided by the operator are true. Form is not approved until approved by Well Records.

WELL RECORD APPROVED

RECEIVED

OKLAHOMA CORPORATION COMMISSION
OIL & GAS CONSERVATION DIVISION
Jim Thorpe Office Building
Oklahoma City, Oklahoma 73105-4993

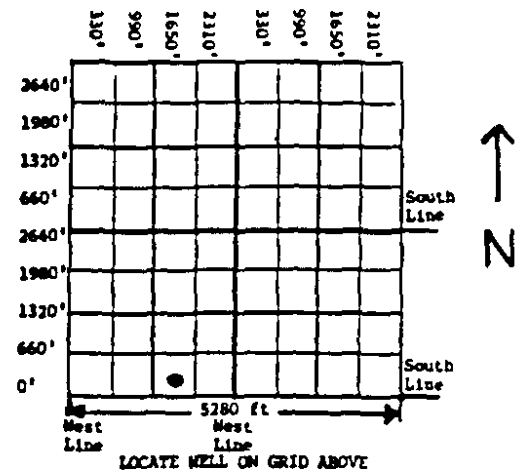
10313629

MAR 13 1991

INSTRUCTIONS

OKLAHOMA CORPORATION COMMISSION

- A. PLEASE TYPE OR USE BLACK INK.
- B. Form must be signed by current and new operator.
- C. Reverse side MUST be completed in its entirety. All wells being transferred MUST be listed.
- D. Outline boundaries of lease and spot all wells being transferred (Separate map no larger than 8 1/2" x 11", may be attached).
- E. Attach 1002A for each well.
- F. Transfer of disposal and injection wells subject to OCC Rule 3-309.



Name of Lease or Unit Holder B

Total Number of Wells Being Transferred 1 OTC Prod Unit No. 109-14664

Lease Description (Fully Define) Sec. 2-11N-3W County Oklahoma

Oil Purchaser Phillips 66 Company Gas Measurer _____

Current Operator Phillips Petroleum Company OTC/OCC Operator No. 01701

Address P. O. Box 358 City Borger State TX Zip 79008

Phillips Petroleum Company, Being the current operator, certifies that the facts presented are true and correct as to the area covered and the specific wells being transferred are being transferred to White Operating Company OTC/OCC Operator No. 08891

Effective on 3-1-91 at 7:00 AM Date Time

D. R. Warner (806) 273-2831, X 3406 Agent of Current Operator Phone (AC/No.)

D. R. Warner Proration Engineer Name (Print or type) Title



New Operator White Operating Company OTC/OCC Operator No. 08891

Address 100 N. Broadway, Suite 2160 City Okla City State OK Zip 73102

Type of Surety FIN. STMT. Expiration date of surety 12/31/91

White Operating Company, Being the new operator, as of the effective date and time of transfer accepts the facts presented as being true and correct and accept the operational responsibility for the specific wells on the described property.

Lloyd White (405) 843-0395 Agent of New Operator Phone (AC/No.)

Lloyd White PRES. Name (Print or type) Title

FOR O.C.C. USE ONLY

| | Approved Date | Rejected Date |
|-------------------------|---------------|---------------|
| Surety Department | MAR 13 1991 | — |
| Production Department | _____ | _____ |
| Well Records Department | MAR 19 1991 | _____ |
| UIC Department | _____ | _____ |
| Reason for Rejection: | _____ | _____ |

2
1N
3W
Kge.

Instructions

- A. Please type or print using black ink.
- B. Form must be signed by former operator and new operator.
- C. Outline boundaries of lease and spot well being transferred.
- D. Attach 1002A for well.
- E. Direct questions to Well Records (405) 521-2275.

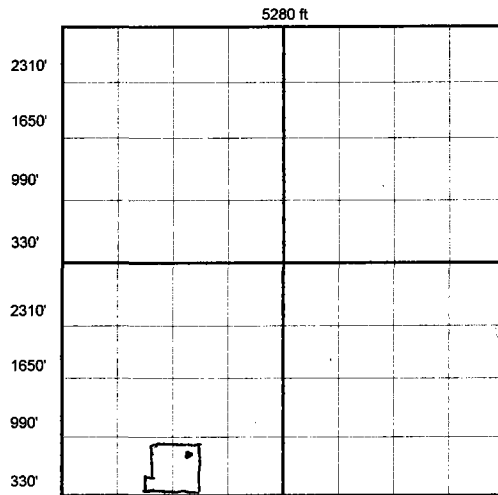
OKLAHOMA CORPORATION COMMISSION

Oil & Gas Conservation Division
 Post Office Box 52000
 Oklahoma City, OK 73152-2000

Form 1073
 Rev. 2009

Transfer of Operator
 OAC 165:10-1-15

| | | | |
|---------------------------|-----------------------------|--------------------|-----------------|
| API No | 109-35839 | OTC Prod. Unit No. | 109-14667 |
| Location | NW 1/4 SW 1/4 SE 1/4 SW 1/4 | Sec. 2 | Twp 11N Rge. 3W |
| Ft FSL of Qtr Sec | 495 | Ft FWL of Qtr Sec | 1,485 |
| County | Oklahoma | | |
| Current Well Name/Number | Yount "B" #1 | | |
| Original Well Name/Number | Maggie Yount #1 | | |
| Unit Name (if applicable) | | | |



Locate Well On Grid Above

Well Class: OIL GAS DRY **BK-5911 PG-1603**

Producing formation(s) Simpson

The effective date of transfer of this well, for the purposes of Commission records, is the date that the transfer is approved by the Commission.

| | |
|---|----------------|
| CURRENT OPERATOR | OCC No. |
| Name | None of Record |
| Address | |
| City | State Zip |
| Phone No. | FAX No./E-mail |
| I verify that I am the legal operator of record with authority to transfer operatorship of this well. | |
| Signature | |
| Name & Title (Typed or Printed) | |
| Signed and sworn to before me this _____ day of _____, _____. | |
| Notary Public | |
| My commission expires: _____ | |

| | |
|---|--|
| NEW OPERATOR | OCC No. |
| Name | White Operating Co. |
| Address | 1627 SW 96th |
| City | Oklahoma City State OK Zip 73159 |
| Phone No. | (405)-239-6001 FAX No./E-mail woclrw@coxinet.net |
| I verify that I am the legal operator of record with authority to transfer operatorship of this well. | |
| Signature <i>Lloyd White</i> | |
| Lloyd White, President | |
| Name & Title (Typed or Printed) | |
| Signed and sworn to before me this <u>14</u> day of <u>November</u> , <u>2012</u> | |
| Notary Public <i>Vickie White</i> | |
| My commission expires: _____ | |

I verify under oath that I have exercised due diligence in attempting to locate the current operator of record according to OCC records, who has abandoned the above well/lease and cannot be located to obtain signature.

I have attached a copy of the certified recorded assignment of lease.

* APPROVED COPY AVAILABLE ON OCC WEBSITE. *

Signed and sworn to before me this 14 *Vickie White* November, 2012
 Signature _____
 Notary Public _____

My commission expires: _____
FOR OCC USE ONLY

Surety Dept. Approved Rejected Date NOV 20 2012 Well Records Dept. Approved Rejected Date NOV 20 2012

NOTE: By processing this Form 1073, the Oklahoma Corporation Commission has approved the contents thereof as to form only. Oklahoma Corporation Commission does not warrant that the facts provided by the operator are true. Form is not approved until approved by Well Records.

Per Dennis 1002A In-House Issue DBN/wr

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 EC-1405D – TASK 01
 I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26926, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

SECTION 09 – NATURAL RESOURCES

Enercon Services conducted a natural resources review of the study area.

9.1 Threatened and Endangered Species

Federally-listed threatened and endangered species in Oklahoma County, according to the U.S. Fish and Wildlife Service (USFWS) website, are listed below:

| Species | Classification | Critical Habitat |
|--------------------------|----------------|--------------------|
| Piping Plover | Threatened | None in Study Area |
| Whooping Crane | Endangered | None in Study Area |
| Interior Least Tern | Endangered | None in Study Area |
| Arkansas River Shiner | Threatened | None in Study Area |

None of the above species were observed within the project area during the initial site reconnaissance. The proposed project is not located within a federally listed aquatic dependent species watershed (Exhibit 9-4) or a federally listed aquatic species watershed (Exhibit 9-5). According to the Oklahoma Natural Heritage Inventory Database, no documented occurrences of federally listed species have been recorded within the project study area or the immediate vicinity.

The North Canadian River may exhibit the characteristics of suitable habitat for the Arkansas River shiner; however, based on the initial project scoping review conducted using the USFWS Information, Planning, and Conservation System (IPaC) tool, this species is considered unlikely to be impacted by the proposed project.

Depending on the water level and availability of sand bar habitat, the North Canadian River may provide suitable habitat for interior least terns and foraging habitat for migrating piping plovers and whooping cranes within the project study area and the immediate vicinity. However, due to the encroachment of the highway and adjacent development, the whooping crane is unlikely to utilize habitat within the project area.

None of the above species were observed within the project area during the initial site reconnaissance.

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26926, 27869
OKLAHOMA COUNTY
J/P 30444(04)

9.2 Potentially Jurisdictional Streams and Wetlands

Prior to site reconnaissance, Enercon Services, Inc. (ENERCON) reviewed the National Wetland Inventory (NWI) maps, USGS 7.5 Minute Topographic Map Quad Maps (Midwest City Quad), Natural Resources Conservation Services (NRCS) soil survey maps (Exhibit 9-3), aerial photography (Exhibit 9-1), and the NRCS hydric soils list for Oklahoma. The attached maps depict the NWI overlaid with the USGS map (Exhibit 9-2).

Based upon review of the above materials and the visual assessment of the study area conducted on January 9, 2014, the following features were observed within the project study area:

- The North Canadian River
- One (1) mapped intermittent stream
- Three (3) unmapped, potentially jurisdictional features in the following locations:
 - Southwest quarter of Section 2, Township 11 North, Range 3 West.
 - Northwest quarter of Section 2, Township 11 North, Range 3 West.
 - Northeast quarter of Section 2, Township 11 North, Range 3 West.
- The NWI map depicts one (1) riverine wetland system (R2USC) within the North Canadian River. This feature is likely associated with a natural sandbar within the North Canadian River channel.

It should be recognized that this survey is based on limited visual observations only and did not include the standard data collection effort to confirm all three wetlands parameters. Access to the survey corridor was limited to which may have been visible from the current ODOT right-of-way. It is understood that further studies will be conducted in the future to ensure the study area conforms to the USACE 1987 manual and Midwest Regional Supplement (August 2010).

9.3 Critical Resource Waters, Section 10 Waters, Scenic Rivers, and Protected Aquifers

Critical Resource Waters listed by the USACE as Outstanding Resource Waters or High Quality Waters do not occur within the project study. Additionally, the proposed project is not located within a Critical Resource Water watershed. None of the waters within the project study area are subject to regulation under Section 10 of the Clean Water Act.

Oklahoma Scenic Rivers are located in Adair, Cherokee, Delaware, LeFlore, McCurtain, and Sequoyah Counties. The proposed project is located in Oklahoma County; therefore, none of the streams within the project study area are listed for protection under the Oklahoma Scenic Rivers Act or the Wild and Scenic Rivers Act. The proposed project is located within the Garber-Wellington Aquifer (Exhibit 9-6).

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26926, 27869
OKLAHOMA COUNTY
J/P 30444(04)

9.4 Impaired Waters

Within the project study area, The North Canadian River is included on the Oklahoma 2012 303(d) list of impaired waters for impairments due to bacteria (*Escherichia coli*) and dissolved oxygen levels. A total maximum daily load (TMDL) is scheduled to be completed in 2014. A TMDL for *Enterococcus* bacteria and fecal coliform was established in 2010. All waters within the project study area drain directly into the North Canadian River.

9.5 Oklahoma Sensitive Waters and Watersheds for Storm Water Permit

No Sensitive Waters for Federal and State listed Species are located within Oklahoma County.

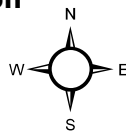
Legend

 Project Footprint



Prepared for:
Oklahoma Department of Transportation

Subject Property:
I 35 Reconstruction Additional Lanes Project
J/P #309444(04)
Section 2, T11N R3W
Oklahoma County, Oklahoma



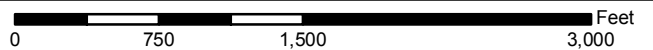
1:12,000





Exhibit 9-1: Aerial Photography

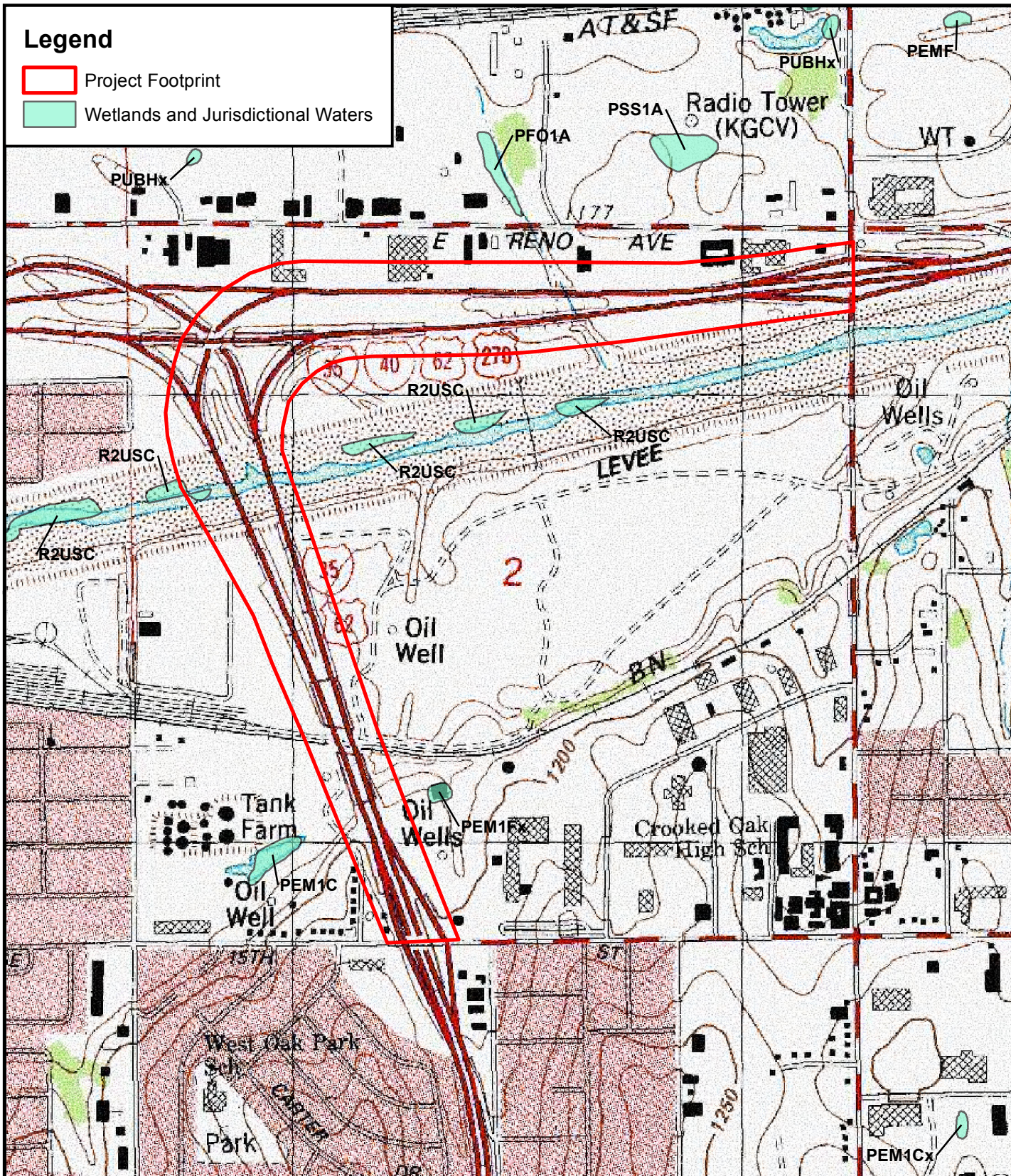
Source: 2013 USDA NAIP
Oklahoma County, Oklahoma

Prepared by: C. Seck; January 31, 2014



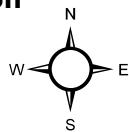
Legend

-  Project Footprint
-  Wetlands and Jurisdictional Waters



Prepared for:
Oklahoma Department of Transportation

Subject Property:
I 35 Reconstruction Additional Lanes Project
J/P #309444(04)
Section 2, T11N R3W
Oklahoma County, Oklahoma

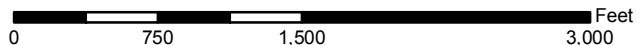


1:12,000





Exhibit 9-2: Footprint with NWI Wetlands
Source: US Fish and Wildlife Service - NWI
USGS 7.5 Minute Series - Midwest City, OK Quadrangle








Prepared by: C. Seck; January 31, 2014

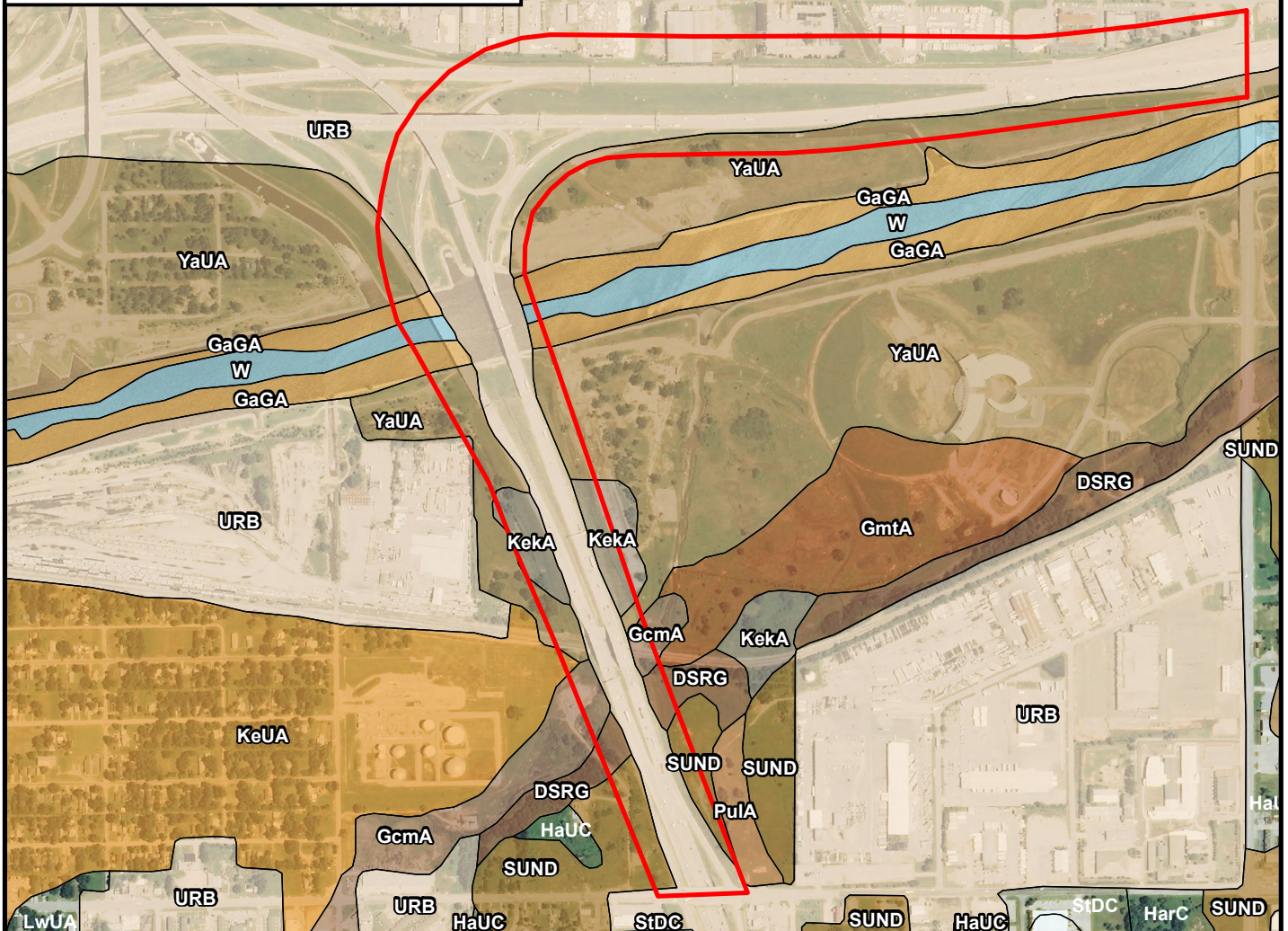


Legend

-  Project Footprint
-  Area Soils

Soils within Project Footprint

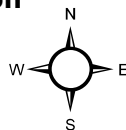
-  DSRG - Darsil-Stephenville-Rock outcrop complex
-  GaGA - Gaddy-Gracemore complex
-  GcmA - Gracemont silty clay
-  GmtA - Gracemont fine sandy loam
-  KeUA - Keokuk-Urban land complex
-  KekA - Keokuk very fine sandy loam
-  PuIA - Pulaski fine sandy loam
-  SUND - Stephenville-Urban land-Newalla complex
-  URB - Urban land
-  W - Water
-  YaUA - Yahola-Urban land complex



Prepared for:
Oklahoma Department of Transportation

Subject Property:

I 35 Reconstruction Additional Lanes Project
 J/P #309444(04)
 Section 2, T11N R3W
 Oklahoma County, Oklahoma



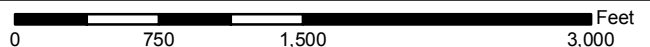
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Exhibit 9-3: Soil Survey Map

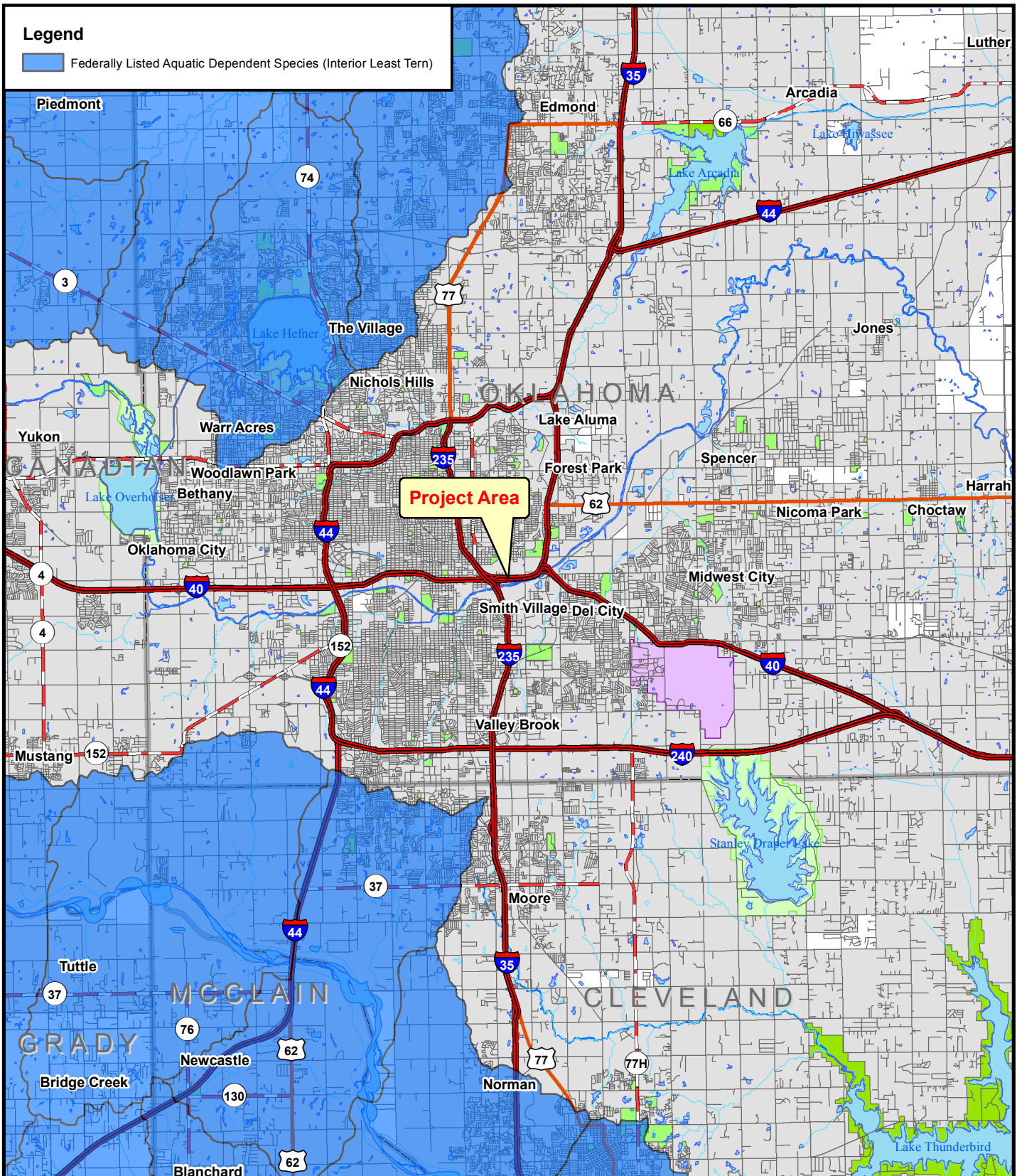
Source: USDA NRCS Soil Survey Geographic Database
 Oklahoma County, Oklahoma; 2013 USDA NAIP

Prepared by: C. Seck; January 31, 2014



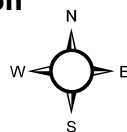
Legend

 Federally Listed Aquatic Dependent Species (Interior Least Tern)



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J/P #309444(04)
Section 2, T11N R3W
Oklahoma County, Oklahoma

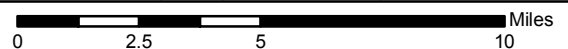


1:250,000



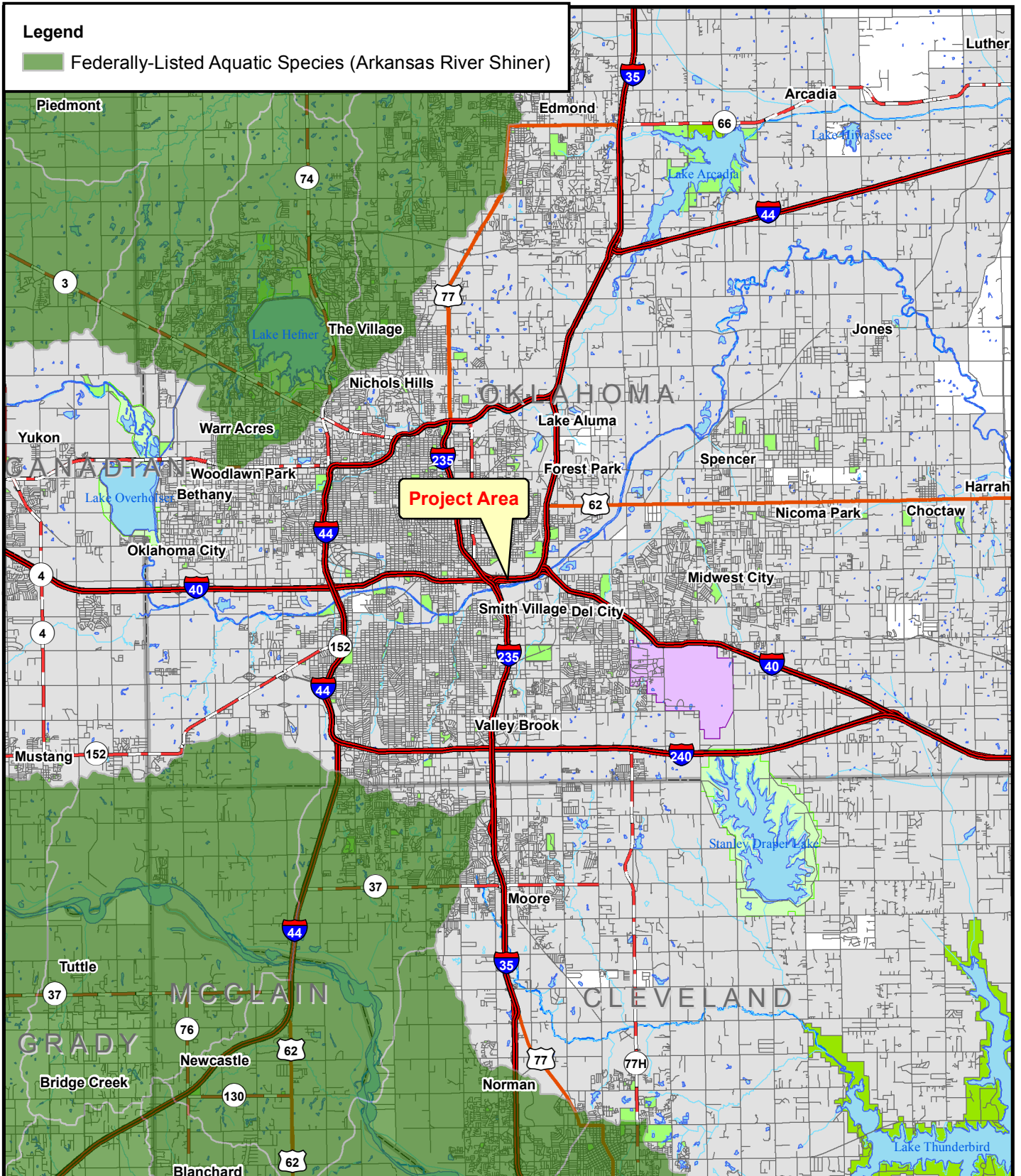
Exhibit 9-4: Federally-Listed Aquatic Dependent Species Watersheds Map

Source: USFWS - Oklahoma Ecological Services
Field Office Federally Listed Aquatic Dependent Species Watershed of Oklahoma Map (PDF); OU
Prepared by: C. Seck; February 04, 2014



Legend

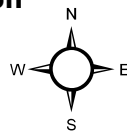
 Federally-Listed Aquatic Species (Arkansas River Shiner)



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Subject Property:

I 35 Reconstruction Additional Lanes Project
J/P #309444(04)
Section 2, T11N R3W
Oklahoma County, Oklahoma

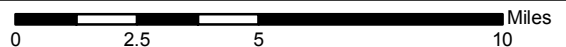


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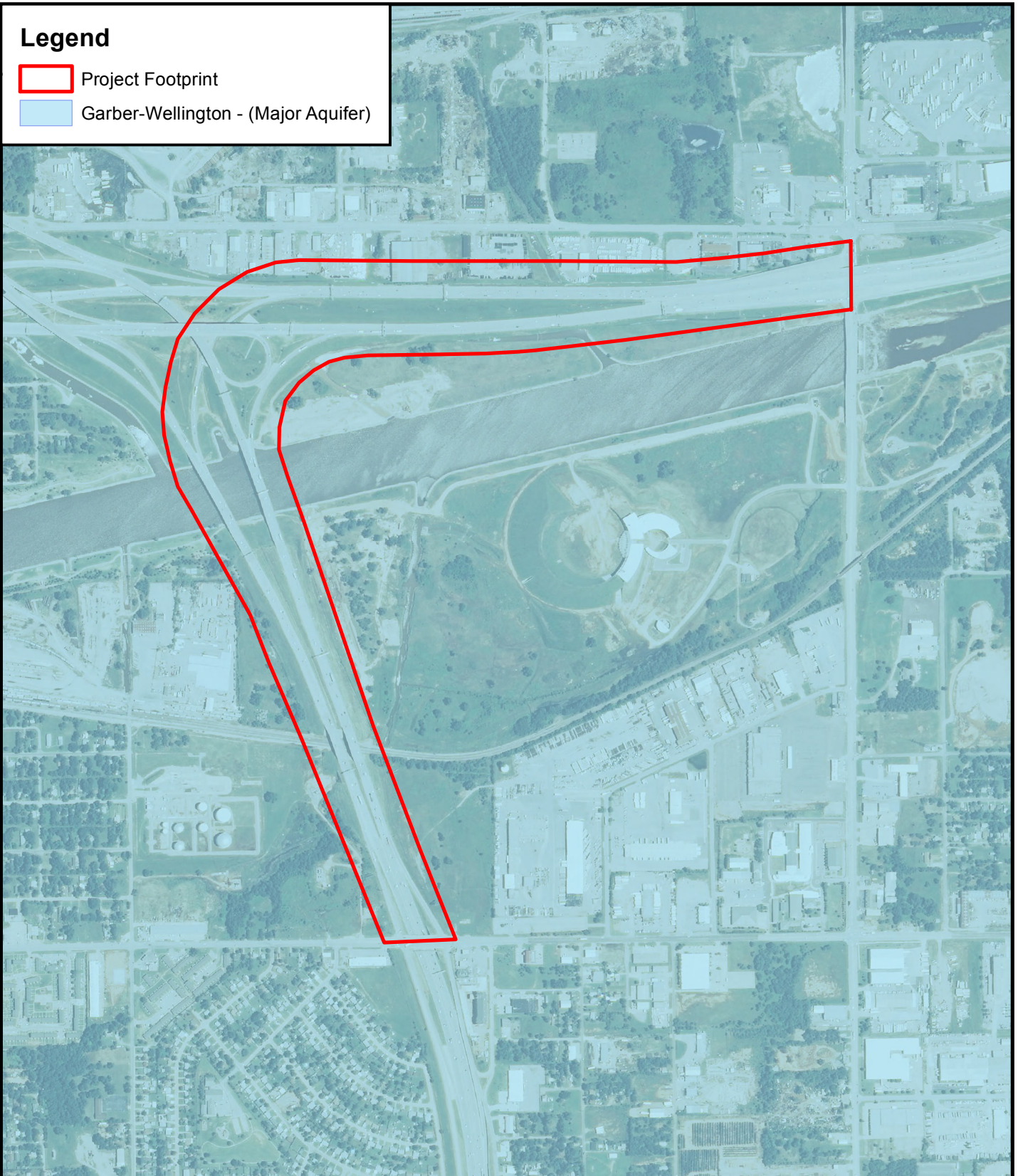
Exhibit 9-5: Federally-Listed Aquatic Species Watersheds Map

Source: USFWS - Oklahoma Ecological Services
Field Office Federally Listed Aquatic Species
Watershed of Oklahoma Map (PDF); OU
Prepared by: C. Seck; February 04, 2014



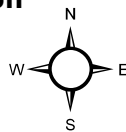
Legend

-  Project Footprint
-  Garber-Wellington - (Major Aquifer)



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Subject Property:
I 35 Reconstruction Additional Lanes Project
J/P #309444(04)
Section 2, T11N R3W
Oklahoma County, Oklahoma



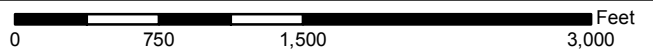
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**Exhibit 9-6: Footprint over Groundwater
Aquifer Map**

Source: Oklahoma Water Resources Board Aquifer
Data; 2013 USDA NAIP Oklahoma County, Oklahoma

Prepared by: C. Seck; February 03, 2014



OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
I-35: ADD ADDITIONAL RAMPS FOR I-35 NB & SB AT THE I-40/I-35 JCT.
NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
OKLAHOMA COUNTY
J/P 30444(04)

SECTION 10 – EXISTING FACILITY DATA

10.1 Functional Classification

- 10.1.1 Area Type: **O.K.C. Urbanized**
- 10.1.2 Terrain Type: **Outlying Business District**
- 10.1.3 Access Control: **Full Access Control**
- 10.1.4 Highway Type: **Interstate; NHS**

10.2 Traffic Data within Study Area

- 10.2.1 Current Average Annual Daily Traffic (AADT): **See attached map.**
- 10.2.2 Projected AADT: **See attached map.**
- 10.2.3 Percentage of Truck Traffic: **See attached e-mail.**

10.3 Roadway Characteristics within Study Area

- 10.3.1 Number and Width of Lanes: **4-lanes; 12' each**
- 10.3.2 Outside Shoulder Widths: **10 foot**
- 10.3.3 Open Section, Curb & Gutter, Divided (with median width): **Divided; 40' Width**
- 10.3.4 Pavement Material Type and Condition: **P.C. Concrete; Good**
Shoulder Material Type and Condition: **Paved; Good**
- 10.3.5 Storm Sewer Identification and Condition: **None**
- 10.3.6 Sidewalks Identification and Width: **None**

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 OKLAHOMA COUNTY
 J/P 30444(04)

10.4 Bridge Characteristics For Each Bridge within Study Area

| NBI 14239 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | Unknown Creek |
| 10.4.2 | NBI Number | 14239 |
| | Structure Number | 5515 0526 X |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 2 Span; Concrete Culvert |
| 10.4.4 | Sufficiency Rating | 83.0 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 1959 |
| 10.4.6 | Bridge Width | 136.0 feet |
| | Bridge Length | 22.6 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 68.0 feet; Unlimited Not posted |
| 10.4.8 | Health Index | 100.0 |

| NBI 21335 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | B.N. & S.F. Railroad |
| 10.4.2 | NBI Number | 21335 |
| | Structure Number | 5515 0534EX |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 3 Span; Pre-stressed Conc. Girder |
| 10.4.4 | Sufficiency Rating | 86.3 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 1986 |
| 10.4.6 | Bridge Width | 70.0 feet |
| | Bridge Length | 250.0 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 68.0 feet; Unlimited Not posted |
| 10.4.8 | Health Index | 90.7 |

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 OKLAHOMA COUNTY
 J/P 30444(04)

| NBI 21356 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | Oklahoma River |
| 10.4.2 | NBI Number | 21356 |
| | Structure Number | 5515 0566EX |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 8 Span; Pre-stressed Conc. Girder |
| 10.4.4 | Sufficiency Rating | 86.3 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 1986 |
| 10.4.6 | Bridge Width | 71.0 feet |
| | Bridge Length | 802.0 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 68.0 feet; Unlimited Not posted |
| 10.4.8 | Health Index | 92.4 |

| NBI 21708 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | B.N. & S.F. Railroad |
| 10.4.2 | NBI Number | 21708 |
| | Structure Number | 5515 0534WX |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 3 Span; Pre-stressed Conc. Girder |
| 10.4.4 | Sufficiency Rating | 86.3 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 1987 |
| 10.4.6 | Bridge Width | 71.0 feet |
| | Bridge Length | 250.0 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 68.0 feet; Unlimited Not posted |
| 10.4.8 | Health Index | 91.0 |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
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 NBI 14239, 21335, 21356, 21708, 21722, 21723, 22096, 23635, 26296, 27869
 OKLAHOMA COUNTY
 J/P 30444(04)

| NBI 21722 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | I-35 S-W, I-35 E-S Under |
| 10.4.2 | NBI Number | 21722 |
| | Structure Number | 5569 1022 X |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 4 Span; Steel Continuous Girder |
| 10.4.4 | Sufficiency Rating | 82.3 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 1987 |
| 10.4.6 | Bridge Width | 95.2 feet |
| | Bridge Length | 712.0 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 92.2 feet; Unlimited Not posted |
| 10.4.8 | Health Index | 81.6 |

| NBI 21723 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | Oklahoma River |
| 10.4.2 | NBI Number | 21723 |
| | Structure Number | 5515 0566WX |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 8 Span; Pre-stressed Conc. Girder |
| 10.4.4 | Sufficiency Rating | 97.4 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 1987 |
| 10.4.6 | Bridge Width | 71.0 feet |
| | Bridge Length | 802.0 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 68.0 feet; Unlimited Not posted |
| 10.4.8 | Health Index | 93.8 |

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 OKLAHOMA COUNTY
 J/P 30444(04)

| NBI 22096 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | I-35 E-S, I-40 WB |
| 10.4.2 | NBI Number | 22096 |
| | Structure Number | 5515 0585WXR |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 5 Span; Steel Continuous Girder |
| 10.4.4 | Sufficiency Rating | 85.0 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 1988 |
| 10.4.6 | Bridge Width | 71.0 feet |
| | Bridge Length | 967.8 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 68.0 feet; 17.3 feet Not posted |
| 10.4.8 | Health Index | 91.2 |

| NBI 23635 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | I-35 |
| 10.4.2 | NBI Number | 23635 |
| | Structure Number | 5515 0506 X |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 2 Span; Steel Continuous Girder |
| 10.4.4 | Sufficiency Rating | 93.1 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 1994 |
| 10.4.6 | Bridge Width | 96.0 feet |
| | Bridge Length | 172.0 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 74.0 feet; Unlimited Not posted |
| 10.4.8 | Health Index | 92.5 |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
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 OKLAHOMA COUNTY
 J/P 30444(04)

| NBI 26296 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | Eastern Avenue |
| 10.4.2 | NBI Number | 26296 |
| | Structure Number | 5515 0671SX |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 3 Span; Pre-stressed Conc. Girder |
| 10.4.4 | Sufficiency Rating | 96.4 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 2000 |
| 10.4.6 | Bridge Width | 86.0 feet |
| | Bridge Length | 192.5 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 83.0 feet; Unlimited Not posted |
| 10.4.8 | Health Index | 98.3 |

| NBI 27869 | | |
|------------------|--|--|
| 10.4.1 | Feature Intersected | Eastern Avenue |
| 10.4.2 | NBI Number | 27869 |
| | Structure Number | 5515 0671NX |
| | Local ID | -1 |
| | Approximate Study Station | |
| 10.4.3 | Span and Material Description | 3 Span; Pre-stressed Conc. Girder |
| 10.4.4 | Sufficiency Rating | 96.4 |
| | SD/FO Description | Non Deficient |
| 10.4.5 | Year Built | 2000 |
| 10.4.6 | Bridge Width | 86.0 feet |
| | Bridge Length | 192.5 feet |
| 10.4.7 | Horizontal and Vertical Clearances, Measured and Posted | 83.0 feet; Unlimited Not posted |
| 10.4.8 | Health Index | 98.2 |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
EC-1405D – TASK 01
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OKLAHOMA COUNTY
J/P 30444(04)

10.5 Alternative Agency Impacts Associated with Study Area

10.5.1 The project study area lies within the jurisdiction of the Association of Central Oklahoma Governments.

10.6 Existing Agreements:

10.6.1 No existing agreements were found.

From: SSundaram@odot.org
Sent: Wednesday, February 19, 2014 8:07 AM
To: Cyganovich, Bill
Cc: David_Saulsberry/ODOT@fd9ns01.okladot.state.ok.us
Subject: Fw: Design Traffic Data for I-35 Ramps at I-40/I-235 JCT, Oklahoma Co
Attachments: I-35 Ramps at I-40 & I-235 JCT, Oklahoma Co.pdf

----- Forwarded by Siv Sundaram/ODOT on 02/19/2014 08:06 AM -----

Daryl Johnson/ODOT

02/19/2014 07:35 AM

To Siv Sundaram/ODOT@fd9ns01.okladot.state.ok.us

cc

Subject Fw: Design Traffic Data for I-35 Ramps at I-40/I-235 JCT, Oklahoma Co

----- Forwarded by Daryl Johnson/ODOT on 02/18/2014 03:24 PM -----

Matthew Blakeslee/ODOT

02/18/2014 03:22 PM

To Daryl Johnson/ODOT@fd9ns01.okladot.state.ok.us

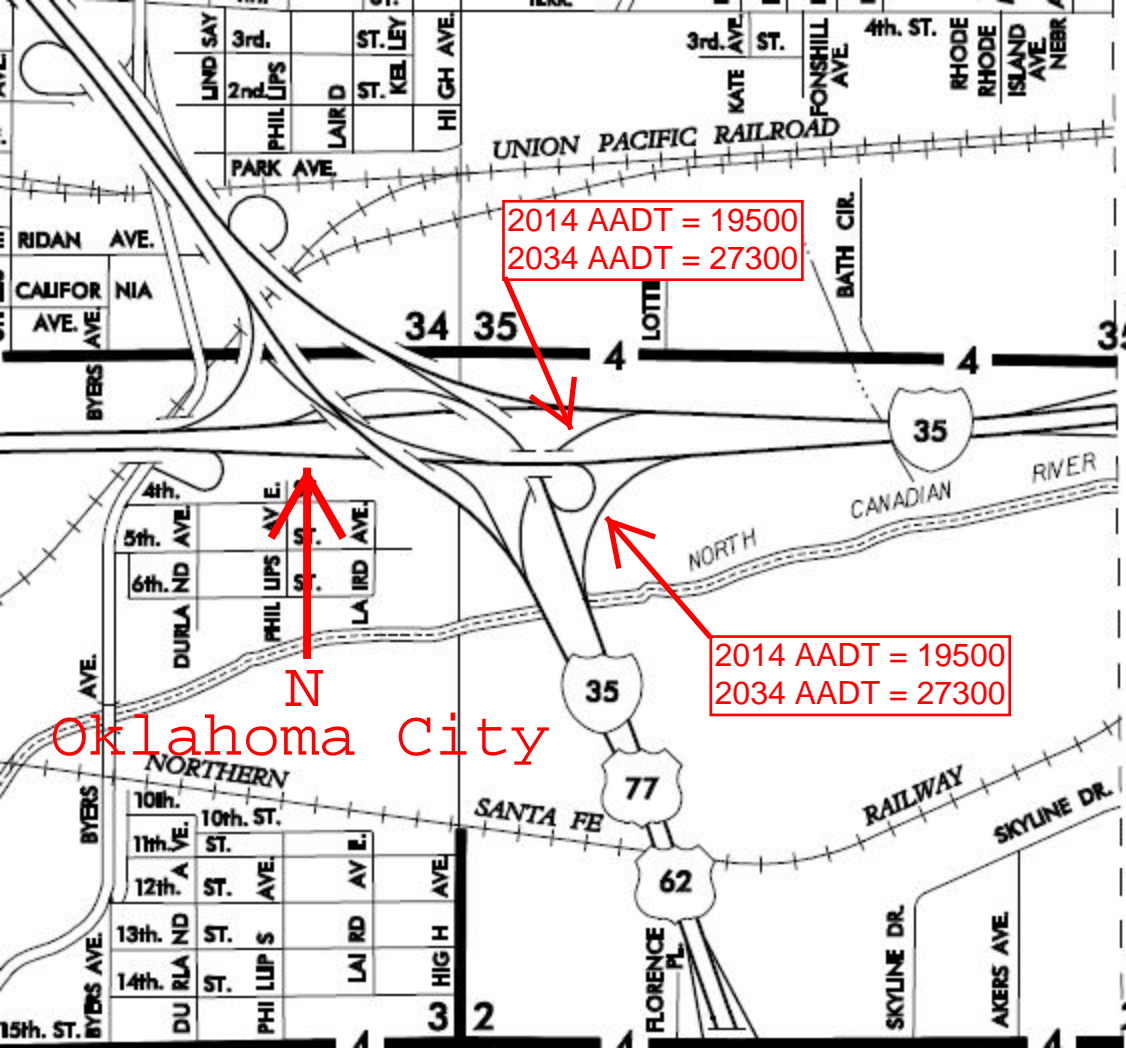
cc

Subject Design Traffic Data for I-35 Ramps at I-40/I-235 JCT, Oklahoma Co

Design Traffic Data for:

County: Oklahoma
JP No.: 30444(04)
Highway: I-35 Ramp

2014 AADT = 19500
2034 AADT = 27300
K = 9 %
D =
T (AADT) = 20 %
T (DHV) = 14 %
T3 = 14 %



2014 AADT = 19500
2034 AADT = 27300

2014 AADT = 19500
2034 AADT = 27300

Oklahoma City

N