

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED
STATE HIGHWAY
GRADE, DRAIN, BRIDGE AND SURFACE
(3R RESURFACE AND REHABILITATION PROJECT)
STATE HIGHWAY SH-7

ATOKA COUNTY

PROJECT NO. SSP-103C(09)SS
CONTROL SECTION NO. 07-03-14
STATE JOB PIECE NO. 24066(04)

BRIDGE 'A' EXISTING NBIS NO.06295 LOCATION NO.0314 0771 X

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STANDARDS:

ROADWAY	TRAFFIC SIGNING	TRAFFIC SAFETY	TRAFFIC CONTROL
SSS-1-1	SPB-1-4	PM1-1-02	TCS1-1-01
TSC2-3-2	FHTMPP-1-0	PM3-1-02	TCS2-1-00
TS2-2-0	FHTCP-3-1	PM6-1-00	TCS3-1-01
TRFD-1-2	SBI-4-2	PM7-1-00	TCS4-1-01
ASCD-5-2	PUD-3-2	DU1-1-00	TCS5-1-00
PSE-1-0	MI-3-0	DU2-1-00	TCS6-1-02
CET4S-3-2	RDI-3-1	RSD1-1-00	TCS7-1-02
CET6S-3-2	DC-3-2	WSD1-1-00	TCS8-1-00
CET4D-3-2	PDT-1-3	MSD1-1-00	TCS9-1-01
CET6D-3-2	RWF1-2-2	MSD2-1-00	TCS10-1-00
PCES-4-1	RWF2-2-1	MSD3-1-01	TCS11-1-01
SPI-4-1	SUEL1-3-2	SIS5-1-00	TCS12-1-00
FPI-3-3	SUEL4-3-2	SBS1-1-00	TCS13-1-00
		SBS2-1-00	TCS14-1-00
		SBS3-1-00	TCS15-1-00
		SBS4-1-00	TCS18-1-01
		GMS1-1-00	TCS19-1-01
		GMS2-1-00	TCS20-1-00
		SSP1-1-02	TCS21-1-02
			TCS22-1-00
			TCS24-1-02

STANDARDS:

BRIDGE
RCB-C1-20(2-10)-01E
RCB-E1-H10-0-1-01E
RCB-E1-H10-0-2-01E
RCB-CW1-D6-0-01E

JP NO. 24066(04)

DESIGN DATA

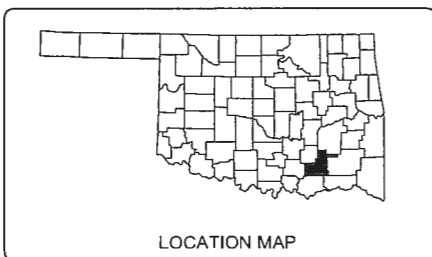
AADT 2020	=	1900
AADT 2050	=	3200
K (DHV/AADT)	=	11%
D	=	57%
T (% AADT)	=	22%
T (% DHV)	=	19%
T3 (% AADT)	=	14%
V	=	65 MPH
ESALS	=	2.50M

SEE SURVEY DATA SHEETS SWO 4522(1) & SWO 4523(1) FOR SURVEY CONTROL

SCALES	BRIDGE PLAN & ELEVATION
PLAN 1" = 50'	PLAN 1" = 10'
PROFILE HOR. 1" = 50'	PROFILE HOR. 1" = 10'
VERT. 1" = 5'	VER. 1" = 1'
LAYOUT MAP 1" = 3000'	

CONVENTIONAL SYMBOLS

[Symbol]	ASPHALT PAVEMENT - ROADWAY
[Symbol]	CONCRETE PAVEMENT - DRIVEWAY
[Symbol]	TRAFFIC BOUND SURFACE COURSE
[Symbol]	TEMP. PAVING
[Symbol]	PAVEMENT REMOVAL
[Symbol]	EARTH CHANNEL
[Symbol]	PAVED DITCH
[Symbol]	PROPOSED ROAD
[Symbol]	RAILROADS
[Symbol]	RANGE & TOWNSHIP
[Symbol]	SECTION LINES
[Symbol]	QUARTER SECTION LINES
[Symbol]	FENCES
[Symbol]	GROUND LINE
[Symbol]	EXISTING ROADS
[Symbol]	BASE LINE
[Symbol]	GRADE LINES
[Symbol]	TELEPHONE & TELEGRAPH
[Symbol]	POWER LINES
[Symbol]	BUILDINGS
[Symbol]	OIL WELL
[Symbol]	DRAINAGE STRUCTURES - IN PLACE
[Symbol]	DRAINAGE STRUCTURES - NEW
[Symbol]	RIGHT-OF-WAY LINES - EXISTING
[Symbol]	RIGHT-OF-WAY LINES - NEW
[Symbol]	CONTROLLED ACCESS
[Symbol]	RIGHT-OF-WAY FENCE

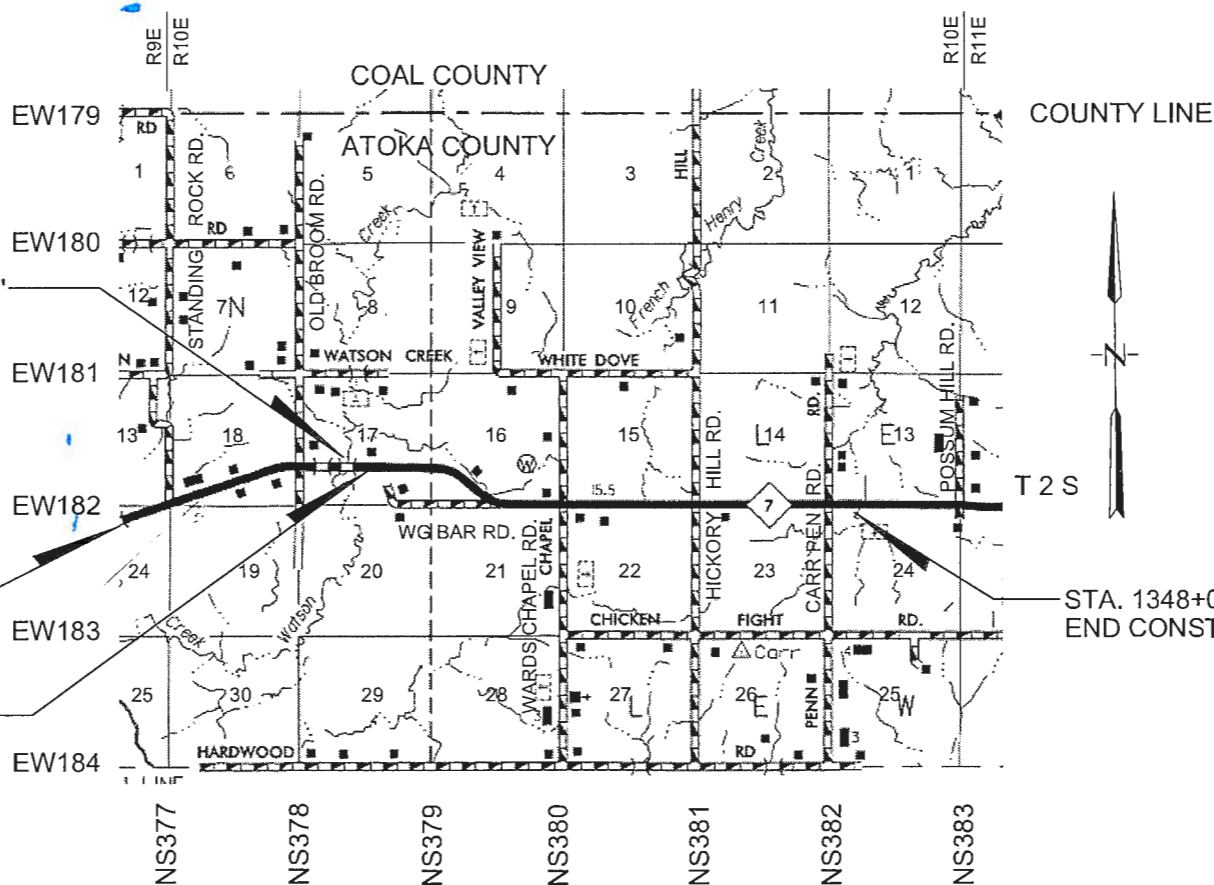


BRIDGE 'A'
Q STATION 1138+46.18
CONSTRUCT 3 - 10' X 7'
RCB EXTENSION

STA. 1061+49.80
BEGIN CONST. J/P NO. 24066(04)
SUBSECTION NO. 6.00.

PROJ. EXCEPTION
STA. 1139+17.75 TO
STA. 1146+48.78

STA. 1348+00.00
END CONST. J/P NO. 24066(04)



ROADWAY LENGTH	27885.17 FT.	5.281 MI.
BRIDGE LENGTH	34.00 FT.	0.006 MI.
PROJECT LENGTH		5.426 MI.

EQUATIONS : NONE

EXCEPTIONS : STA. 1139+17.75 TO STA. 1146+48.78
731.03'

BKL
incorporated
CA. 262 (PE) 6-30-2014
1623 E 6TH STREET
TULSA, OKLAHOMA 74120
TEL: 918-835-9588
FAX: 918-835-9119
MAIL@BKLINC.COM

PREPARED BY:
BKL, INC.
FOR THE OKLAHOMA
DEPARTMENT OF TRANSPORTATION

BKL 8/27/2018 *Kevin Belden*
DATE

OKLA. REG. NO. 20245

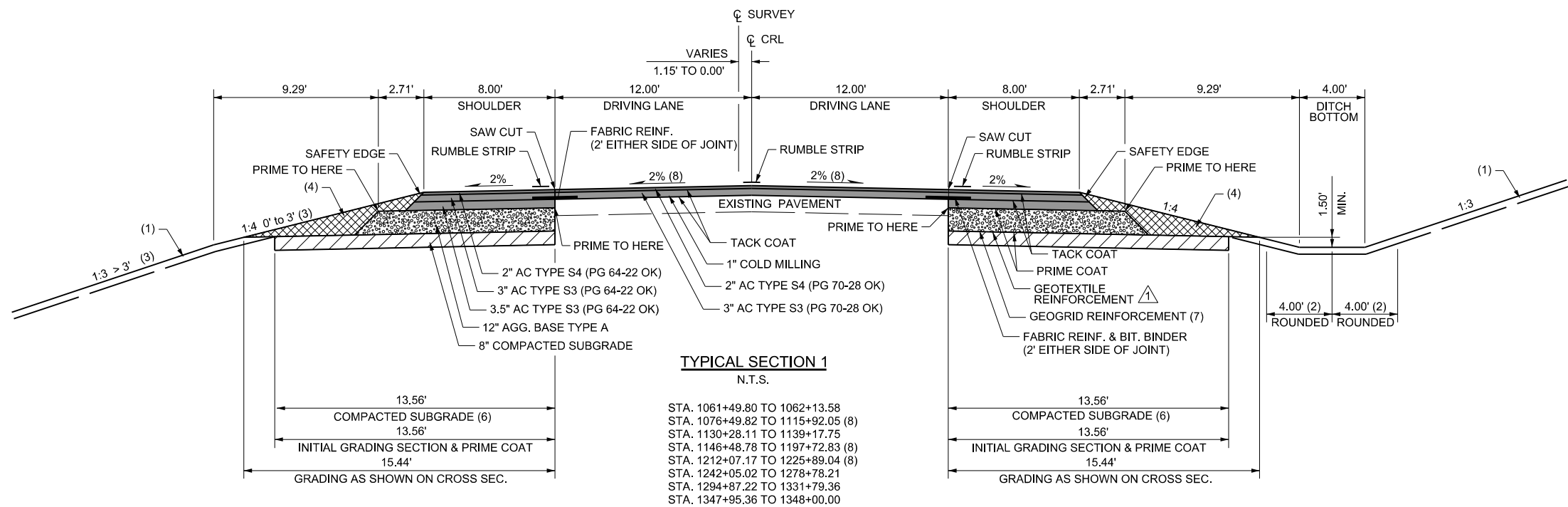
OKLAHOMA DEPARTMENT OF TRANSPORTATION	DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION
DATE APPROVED	DATE APPROVED
BY	BY
CHIEF ENGINEER	DIVISION ADMINISTRATOR
J.P. NO. 24066(04) SWO 4522(1) SWO 4523(1) SSP - 103C(09)SS	SHEET NO. 0001

REVISIONS		
REV. NO.	DESCRIPTION	DATE
1	GEOTEXTILE REINFORCEMENT REVISED	6-15-21

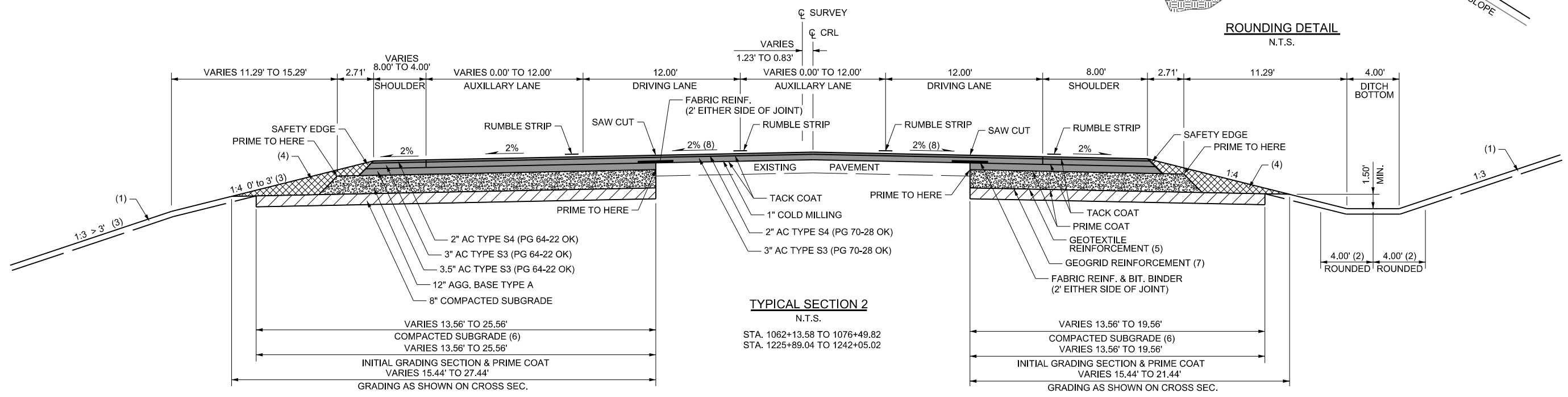
NOTES:

- (1) TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, APPROXIMATELY 5" THICK, STOCKPILE IT AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATION. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATION SHALL BE INCLUDED IN THE PAY ITEM FOR TYPE SALVAGED TOPSOIL. LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.
- (2) SEE ROUNDING DETAIL SHEET 0002.
- (3) DISTANCES ARE MEASURED VERTICALLY FROM THE EDGE OF THE FINISHED SHOULDER.
- (4) TO BE BACKFILLED WITH TBSC TYPE "E".
- (5) NOT USED.
- (6) SUBGRADE IS TO BE COMPACTED IN ACCORDANCE WITH THE OKLAHOMA DEPARTMENT OF TRANSPORTATION 2009 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION SECTION 202.04(5)(b)(2), EARTH FILL.
- (7) THE GEOGRID IS AN ESTIMATED QUANTITY, TO BE USED AS DIRECTED BY THE ENGINEER, AND WILL BE PLACED ON TOP OF THE 8" COMPACTED SUBGRADE BUT UNDERNEATH THE GEOTEXTILE REINFORCEMENT. THE MATERIAL WILL MEET THE MIN. REQUIREMENTS OF GEOGRID-TYPE 1. SEE GEOGRID NOTE ON THE "SUMMARY OF PAY QUANTITIES (ROADWAY)."
- (8) SEE SHEET 0004 FOR COLD MILLING AND SUPERELEVATION DETAILS, AND SEE SHEET 0005 FOR COLD MILLING DEPTH AND ASPHALT THICKNESS.



PAVEMENT REQUIREMENT		
8.5" PAVT. STRUCTURE	12.00' DRIVING LANES	8.00' PAVED SHOULDERS
SURFACE COURSE	2" AC TYPE S4 (PG 70-28 OK)	2" AC TYPE S4 (PG 64-22 OK)
BASE COURSE	3" AC TYPE S3 (PG 70-28 OK)	3" AC TYPE S3 (PG 64-22 OK)
		3.5" AC TYPE S3 (PG 64-22 OK)

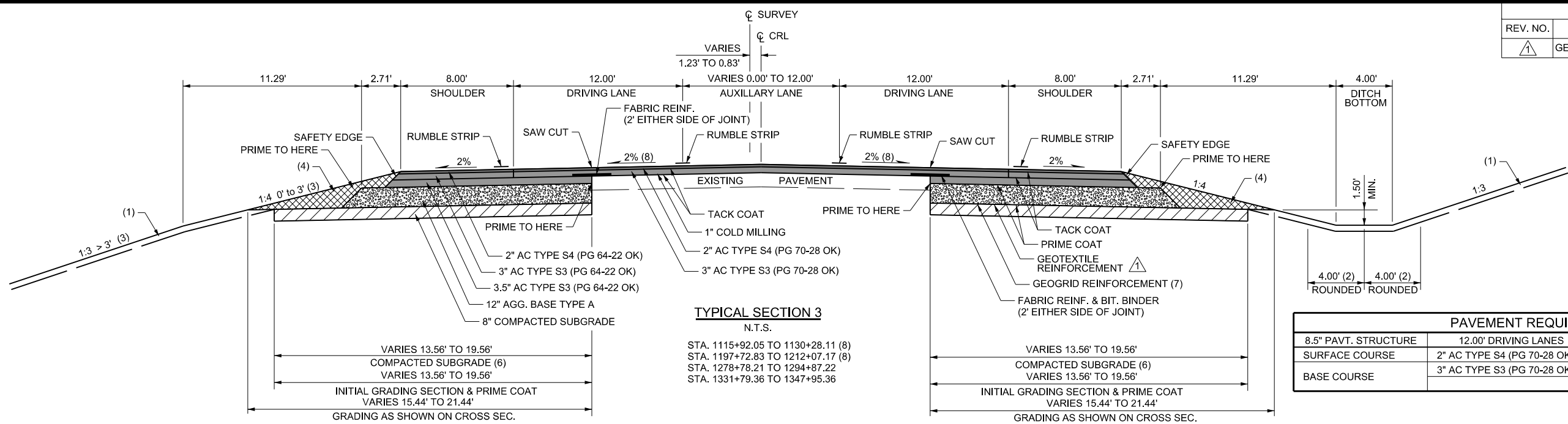


PAVEMENT REQUIREMENT		
8.5" PAVT. STRUCTURE	12.00' DRIVING LANES	8.00' PAVED SHOULDERS
SURFACE COURSE	2" AC TYPE S4 (PG 70-28 OK)	2" AC TYPE S4 (PG 64-22 OK)
BASE COURSE	3" AC TYPE S3 (PG 70-28 OK)	3" AC TYPE S3 (PG 64-22 OK)
		3.5" AC TYPE S3 (PG 64-22 OK)

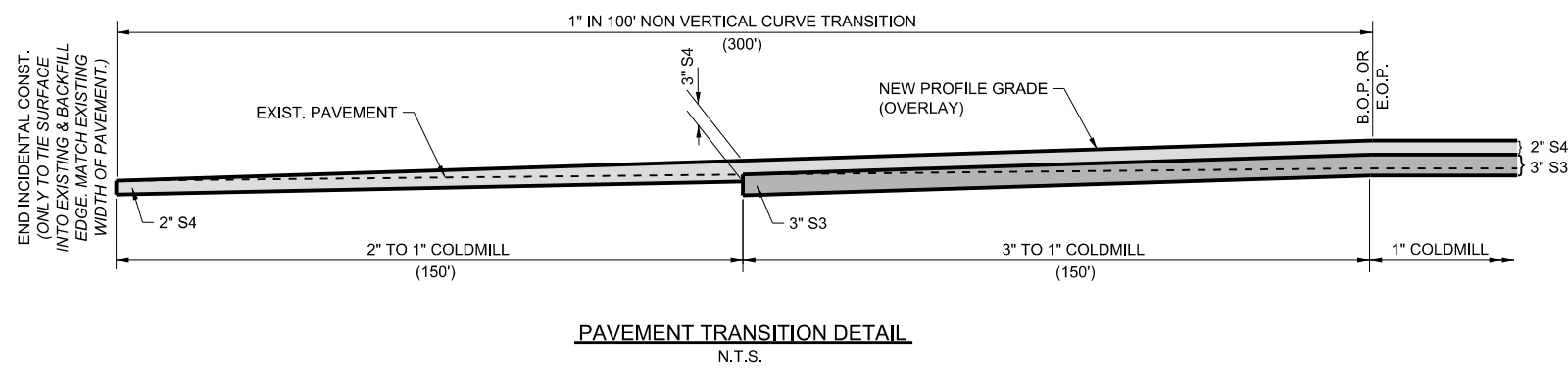
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DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 6/15/2021 C:\Projects\150-SHT Final Plans\002-24066(04)-Typical 1.dgn

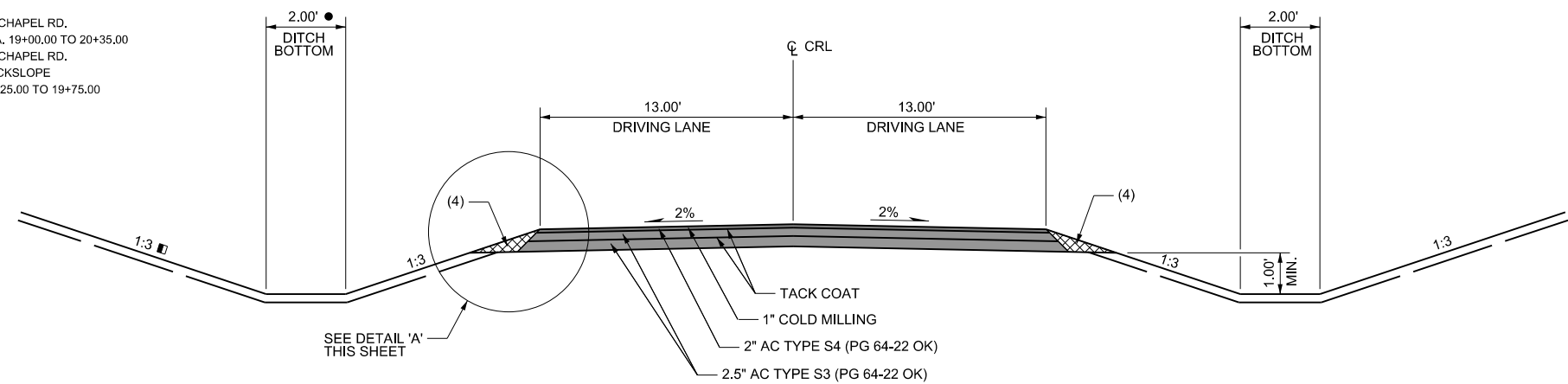
REVISIONS		
REV. NO.	DESCRIPTION	DATE
1	GEOTEXTILE REINFORCEMENT REVISED	6-15-21



PAVEMENT REQUIREMENT		
8.5" PAVT. STRUCTURE	12.00' DRIVING LANES	8.00' PAVED SHOULDERS
SURFACE COURSE	2" AC TYPE S4 (PG 70-28 OK)	2" AC TYPE S4 (PG 64-22 OK)
BASE COURSE	3" AC TYPE S3 (PG 70-28 OK)	3" AC TYPE S3 (PG 64-22 OK)



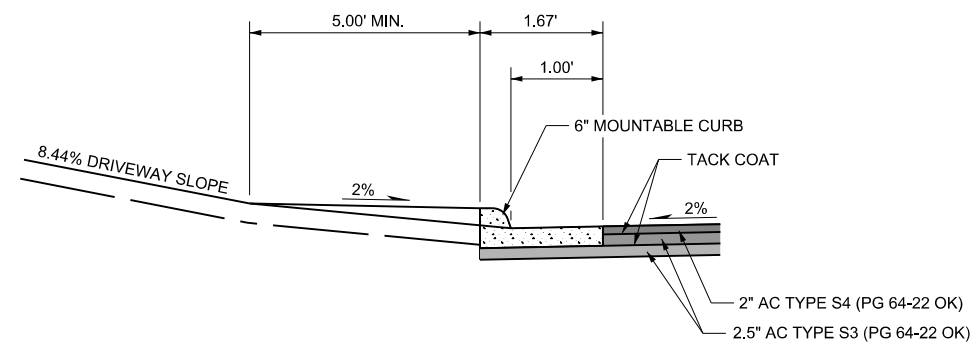
- WARDS CHAPEL RD. 1.00' STA. 19+00.00 TO 20+35.00
- WARDS CHAPEL RD. 1:2.5 BACKSLOPE STA. 19+25.00 TO 19+75.00



- STANDING ROCK RD. STA. 10+33.00 TO 11+24.43
- OLD BROOM RD. STA. 10+26.00 TO 11+71.17
- WG BAR RD. STA. 11+74.97 TO 15+40.13
- WARDS CHAPEL RD. STA. 13+45.25 TO 15+58.85
- STA. 16+18.85 TO 20+93.19
- HICKORY HILL RD. STA. 10+47.21 TO 11+51.53
- STA. 12+03.53 TO 13+21.99
- CARR PEN RD. STA. 12+95.71 TO 14+23.81
- STA. 14+75.81 TO 15+54.26

PAVEMENT REQUIREMENT		
7.0" PAVT. STRUCTURE	13.00' DRIVING LANES	
SURFACE COURSE	2" AC TYPE S4 (PG 64-22 OK)	
BASE COURSE	2.5" AC TYPE S3 (PG 64-22 OK)	

- NOTES:
- TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, APPROXIMATELY 5" THICK, STOCKPILE IT AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATION. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATION SHALL BE INCLUDED IN THE PAY ITEM FOR TYPE SALVAGED TOPSOIL. LUMP SUM.
 - SEE ROUNDING DETAIL SHEET 0002.
 - DISTANCES ARE MEASURED VERTICALLY FROM THE EDGE OF THE FINISHED SHOULDER.
 - TO BE BACKFILLED WITH TBSC TYPE "E".
 - NOT USED.
 - SUBGRADE IS TO BE COMPACTED IN ACCORDANCE WITH THE OKLAHOMA DEPARTMENT OF TRANSPORTATION 2009 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION SECTION 202.04(5)(b)(2), EARTH FILL.
 - THE GEOGRID IS AN ESTIMATED QUANTITY, TO BE USED AS DIRECTED BY THE ENGINEER, AND WILL BE PLACED ON TOP OF THE 8" COMPACTED SUBGRADE BUT UNDERNEATH THE GEOTEXTILE REINFORCEMENT. THE MATERIAL WILL MEET THE MIN. REQUIREMENTS OF GEOGRID-TYPE 1. SEE GEOGRID NOTE ON THE "SUMMARY OF PAY QUANTITIES (ROADWAY)."
 - SEE SHEET 0004 FOR COLD MILLING AND SUPER ELEVATION DETAILS, AND SEE SHEET 0005 FOR COLD MILLING DEPTH AND ASPHALT THICKNESS.



WARDS CHAPEL RD. STA. 20+30.00 TO 20+70.00

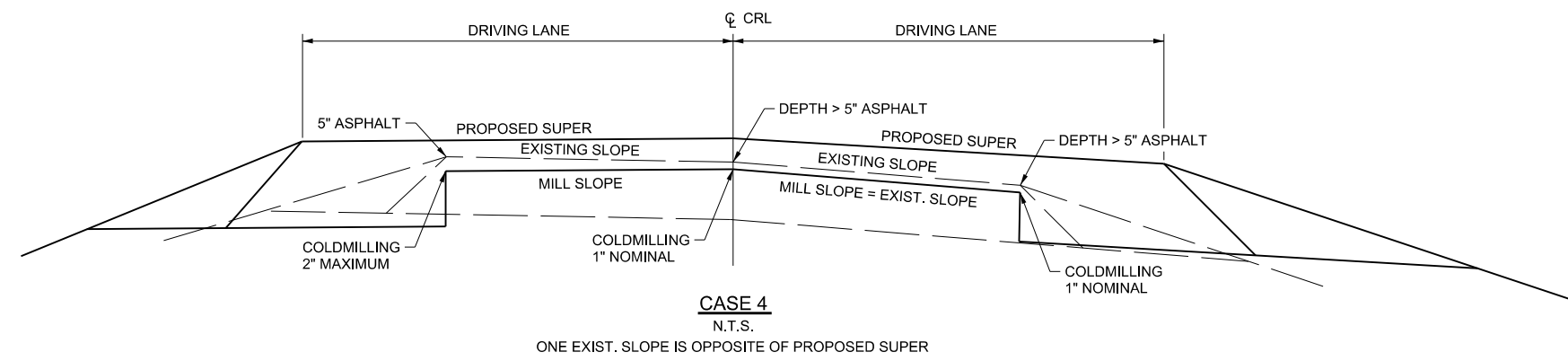
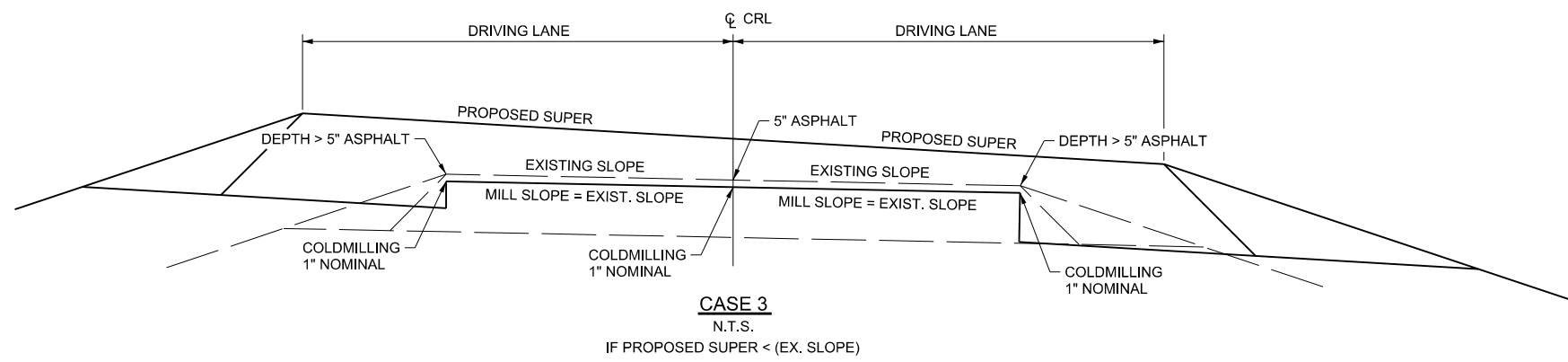
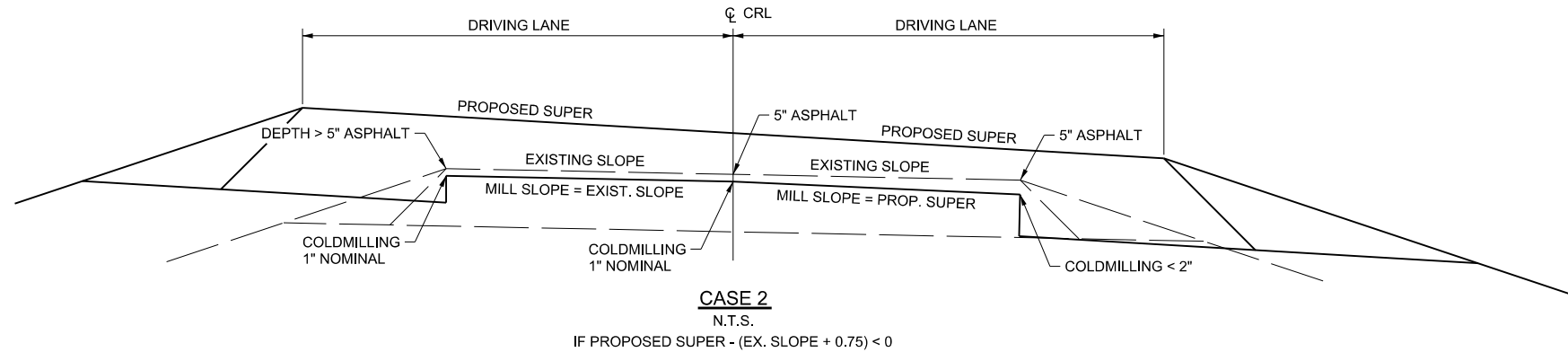
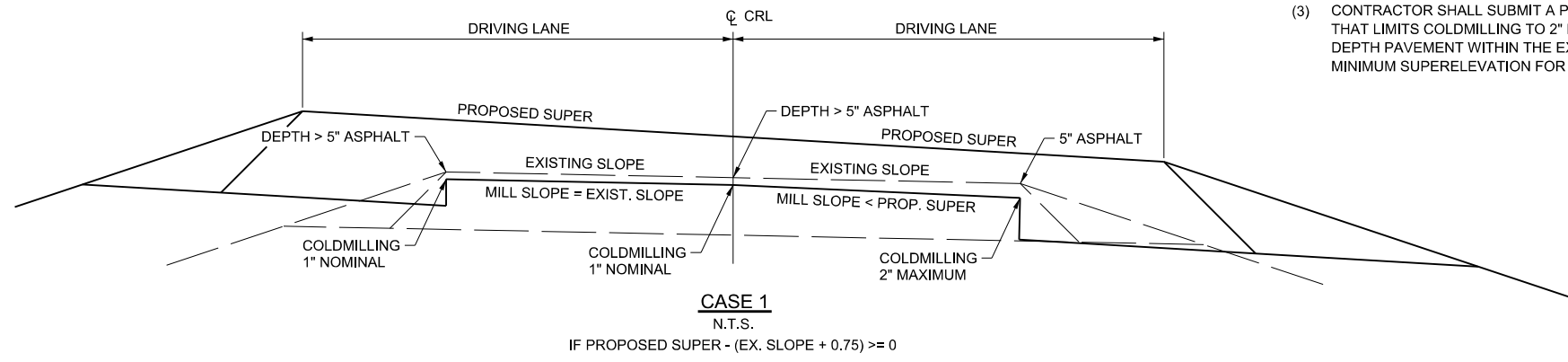
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DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

PRINT DATE: 6/15/2021 C:\Projects\650-SHT Final Plans\DWG\0003-24066(04)-Typical 2.dwg

PRINT DATE: 8/22/2018 C:\Projects\650-SHT Final Plans\04-24066(04)-SUPERELEVATION CORRECTION DETAIL.S.1.dgn

NOTES:

- (1) CONTRACTOR SHALL PROFILE CENTERLINE, RIGHT EDGE OF PAVEMENT AND LEFT EDGE OF PAVEMENT, IN ALL HORIZONTAL CURVES, AND SUBMIT TO THE ENGINEER FOR APPROVAL BEFORE WORK MAY BEGIN.
- (2) EXISTING PAVEMENT SHALL BE COLDMILLED 1" NOMINAL TO 2" MAX. TO BE APPROVED BY THE ENGINEER.
- (3) CONTRACTOR SHALL SUBMIT A PLAN TO THE ENGINEER FOR APPROVAL THAT LIMITS COLDMILLING TO 2" MAXIMUM, ELIMINATES FULL DEPTH PAVEMENT WITHIN THE EXISTING LANES, AND MEETS A MINIMUM SUPERELEVATION FOR 55 MPH.



SUPERELEVATION CASE LOCATIONS - J/P 24066(04)		
X-Sec Sta.	Case No.	Notes
Horizontal Curve No. 1		
1111+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1111+13.43	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1112+00.00	2	MILL VARIABLE 1" TO 2" TO MINIMIZE ASPH. WEDGE
1112+36.88	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1113+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1114+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1115+00.00	2	MILL VARIABLE 1" TO 2" TO MINIMIZE ASPH. WEDGE
1115+88.20	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1116+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1116+27.82	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1117+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1117+30.19	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1118+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1119+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1120+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1121+00.00	2	MILL VARIABLE 1" TO 2" TO MINIMIZE ASPH. WEDGE
1122+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1122+04.15	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1123+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
Horizontal Curve No. 2		
1176+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1177+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1177+04.96	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1177+39.57	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1178+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1179+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1180+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1181+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1182+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1182+79.19	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1183+00.00	2	MILL VARIABLE 1" TO 2" TO MINIMIZE ASPH. WEDGE
1184+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1185+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1185+86.10	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1185+95.48	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1186+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1187+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1188+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1189+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1190+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1190+41.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1191+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1192+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
Horizontal Curve No. 3		
1198+00.00	4	EX. SUPER IS OPPOSITE PROPOSED SUPERELEVATION
1199+00.00	4	EX. SUPER IS OPPOSITE PROPOSED SUPERELEVATION
1200+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1200+04.99	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1201+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1202+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1203+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1204+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1205+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1206+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1206+50.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1207+00.00	3	VARIABLE EX. SUPER WITH MIN. 5" ASPH WEDGE AT CRL
1207+66.52	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1208+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1209+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1210+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1211+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1212+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1213+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1213+19.37	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1214+00.00	1	MAX. 2" MILL TO MINIMIZE THICKNESS OF ASPH. WEDGE
1215+00.00	4	EX. SUPER IS OPPOSITE PROPOSED SUPERELEVATION

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

SUPERELEVATION CORRECTION DETAILS (1 OF 2)

JOB PIECE NO. 24066 (04) SHEET NO. 0004

GENERAL NOTES BRIDGE 'A'

SPECIFICATIONS

COMPLY WITH THE REQUIREMENTS OF THE 2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

VERIFICATION OF EXISTING CONDITIONS

ALL DIMENSIONS OF THE EXISTING BRIDGE COMPONENTS SHOWN ON THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS NECESSARY AND SHALL BE SOLELY RESPONSIBLE FOR THE ACCURACY THEREOF. BIDDERS SHALL FULLY INFORM THEMSELVES OF THE NATURE OF THE WORK AND CONDITIONS UNDER WHICH IT WILL BE PERFORMED. THE CONTRACTOR SHALL ADOPT METHODS CONSISTENT WITH GOOD CONSTRUCTION PRACTICES. CONSTRUCTION PLANS FOR THE EXISTING BRIDGE STRUCTURE MAY BE OBTAINED FROM THE REPRODUCTION BRANCH OF THE OKLAHOMA DEPARTMENT OF TRANSPORTATION.

PERFORATED PIPE UNDERDRAIN AT WINGWALLS

INSTALLATION OF THE PERFORATED PIPE, COARSE PIPE UNDERDRAIN COVER MATERIAL AND SEPARATOR FABRIC SHALL BE INSTALLED AT THE RETAINING WALLS AS SHOWN ON STANDARD PUD-3. RODENT SCREENS SHALL BE INSTALLED AT THE OUTLET END IN THE RCB WALL AS SHOWN ON STANDARD PUD-3. ALL COSTS TO INSTALL THE PERFORATED PIPE UNDERDRAIN INCLUDING THE COST OF ALL PERFORATED PIPE, PIPE UNDERDRAIN COVER MATERIAL, SEPARATOR FABRIC, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT OF " 6" PERFORATED PIPE UNDERDRAIN ROUND."

REMOVAL OF EXISTING BRIDGE STRUCTURE

THE PAY ITEM "REMOVAL OF EXISTING BRIDGE STRUCTURE" CONSISTS OF THE REMOVAL AND DISPOSAL OF THE FOLLOWING STRUCTURES AS SPECIFIED BELOW:

- 1) REMOVAL OF HEADWALL, WING WALLS, APRON AND CURTAIN WALL AND 18" OF THE BARREL STRUCTURE FROM EACH END OF EXISTING NBI NO. 06295. CENTERLINE STA. 1138+46.18, 3-10'X7' RCB, 0° SKEW.

FOR ADDITIONAL DETAILS AND INFORMATION, THE CONTRACTOR SHOULD CONSULT THE ORIGINAL CONSTRUCTION PLANS FOR THE EXISTING BRIDGE. THE ORIGINAL CONSTRUCTION PLANS FOR THE EXISTING BRIDGE MAY BE OBTAINED FROM THE PRINTING SERVICES BRANCH OF THE OKLAHOMA DEPARTMENT OF TRANSPORTATION BY REFERENCING THE ORIGINAL PROJECT NUMBER. THE EXISTING BRIDGE WAS CONSTRUCTED UNDER PROJECT NO. SAP-1043(1). THE REMOVAL AND DISPOSAL SHALL BE CONDUCTED IN ACCORDANCE WITH ODOT SPECIFICATION SECTION 619.04B (CODE OF FEDERAL REGULATIONS 29, CFR 1926.62) AND IN A MANNER APPROVED BY THE ENGINEER. ALL MATERIALS REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR. ALL COST TO REMOVE THE EXISTING BRIDGE INCLUDING THE COST OF ALL MATERIALS, LABOR, EQUIPMENT AND INCIDENTALS AND THE COST TO REMOVE ANY GUARD RAIL ON THE EXISTING BRIDGE SHALL BE INCLUDED IN THE UNIT PRICE BID PER LUMP SUM OF "REMOVAL OF EXISTING BRIDGE STRUCTURE".

CHAMFER REQUIREMENT

PROVIDE ALL EXPOSED CONCRETE EDGES WITH A 1½ INCH CHAMFER UNLESS OTHERWISE SHOWN OR NOTED. USE SIZED LUMBER FOR ALL CHAMFER STRIPS.

CURTAIN WALL INTO ROCK

IF ROCK IS ENCOUNTERED BEFORE THE TOTAL DEPTH OF THE CURTAIN WALL HAS BEEN REACHED, THE CURTAIN WALL SHALL BE KEYED SIX INCHES INTO ROCK.

MIGRATORY BIRD NOTE

REFER TO SHEET NO. AR02 FOR MIGRATORY BIRDS NOTE.

REINFORCING

UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS, ALL REINFORCING STEEL SHALL CONFORM TO AASHTO M31 (ASTM A615) GRADE 60. ALL REINFORCING STEEL SHALL HAVE A MINIMUM 2" COVER UNLESS OTHERWISE NOTED.

24066(04) 0200 BRIDGE 'A' 3 - 10' X 7' RCB LT. X 20.5 LF AND 3 - 10' X 7' RCB RT. X 19.2 LF					
ITEM		DESCRIPTION		UNIT	QUANTITY
202(A)	1301	UNCLASSIFIED EXCAVATION	(BR-1),(BR-2)	C.Y.	610
501(A)	1306	STRUCTURAL EXCAVATION UNCLASSIFIED	(BR-1),(BR-3)	C.Y.	153
509(A)	1326	CLASS AA CONCRETE	(BR-1)	C.Y.	290
511(A)	1332	REINFORCING STEEL	(BR-1)	LB.	43,849
601(B)	1353	TYPE 1A PLAIN RIPRAP	(BR-4)	TON	73
601(C)	1355	TYPE 1A FILTER BLANKET	(BR-5)	TON	34
613(H)	6204	6" PERFORATED PIPE UNDERDRAIN ROUND	(BR-1)	LF.	120
619(D)	1397	REMOVAL OF EXISTING BRIDGE STRUCTURE		LSUM	1.0

PAY ITEM NOTES (BRIDGE "A")

(BR-1) PAYMENT FOR THIS ITEM WILL BE BASED ON PLAN QUANTITIES ONLY. SEE SECTION 109.01B OF THE STANDARD SPECIFICATIONS.

(BR-2) "UNCLASSIFIED EXCAVATION" QUANTITY SHOWN INCLUDES A QUANTITY FOR EXCAVATION OF THE RCB IN ACCORDANCE WITH STANDARD SB1-4 PLUS QUANTITIES FOR WINGWALLS CALCULATED IN A SIMILAR MANNER.

(BR-3) "STRUCTURAL EXCAVATION UNCLASSIFIED" QUANTITY INCLUDES THE VOLUME OF THE BOTTOM SLAB OF THE RCB IN ACCORDANCE WITH STANDARD SB1-4 PLUS THE VOLUME OF THE FOOTINGS (APRONS) FOR WALLS.

(BR-4) RIPRAP QUANTITY ESTIMATED AT 110 LBS PER CUBIC FOOT FOR 15" TYPE 1A PLAIN RIPRAP.

(BR-5) FILTER BLANKET QUANTITY ESTIMATED AT 105 LBS PER CUBIC FOOT FOR 6" TYPE 1A FILTER BLANKET.

OKLAHOMA DEPARTMENT OF TRANSPORTATION STANDARDS

BRIDGE

RCB-C3-10(2-12)-02E
RCB-CW3-D6-0-01E
RCB-E3-H7-0-1-01E
RCB-E3-H7-0-2-01E
PUD-3-2
SB1-4-2

ROADWAY

PUD-3-2
SBI-4-2

SH7 OVER UNNAMED CREEK - BRIDGE "A"

DESIGN	SL		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION GENERAL NOTES & SUMMARY OF PAY QUANTITIES - BRIDGE 'A' JOB PIECE NO. 24066 (04) SHEET NO. AB01
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

REVISIONS		
REV. NO.	DESCRIPTION	DATE
A	GEOTEXTILE REINFORCEMENT REVISION	6-15-21

PAY QUANTITY NOTES

- (R-1) PAYMENT FOR THIS ITEM WILL BE BASED ON PLAN QUANTITY. SEE SECTION 109.01B OF THE STANDARD SPECIFICATIONS.
- (R-4) INCLUDES 1000 CU. YDS. FOR DRIVEWAYS, RETURNS, DIKES, AND MISCELLANEOUS EARTHWORK.
- (R-5) AN ESTIMATED QUANTITY OF 33299 C.Y. TOPSOIL TO BE RESERVED FOR REPLACEMENT OF APPROXIMATELY 5" ON COMPLETED FORESLOPES, DITCHES, AND BACKSLOPES. THIS QUANTITY IS INCLUDED IN THE EARTHWORK BALANCE. ANY ADDITIONAL EXCAVATION IN CUT SECTIONS TO ALLOW FOR PLACEMENT OF TOPSOIL TO FINAL GRADE, SHALL BE INCLUDED IN THE PRICE BID.
- (R-7) FOR 205(A) PRICE BID TO INCLUDE COST OF 18-46-0 FERTILIZER. ESTIMATED AT 150 POUNDS PER ACRE.
- (R-9) ESTIMATED AT 40 GALLONS PER SQ. YD. OF SODDING AND/OR SPRIGGING.
- (R-11) THE QUANTITY ESTIMATED FOR TEMPORARY EROSION AND SEDIMENT CONTROL IS 49.5 ACRES.
- (R-14) ESTIMATED AT 150 POUNDS OF 10-20-10 FERTILIZER PER ACRE OF PERMANENT SEEDING.
- (R-15) ESTIMATED AT 150 POUNDS OF 18-46-0 FERTILIZER PER ACRE OF AREA ON WHICH TOPSOIL IS TO BE REPLACED.
- (R-16) QUANTITY BASED ON TWO APPLICATIONS.
- (R-25) ESTIMATED AT 140 LBS. PER CU. FT.
- (R-28) PRIME COAT SHALL BE APPLIED AT AN ESTIMATED RATE OF 0.35 GAL. PER SQ. YD. WHEN APPLIED TO SUBGRADE, AND 0.25 GAL. PER SQ. YD. WHEN APPLIED TO AGGREGATE BASE. THE ACTUAL CUTBACK PRIME COAT REQUIRED FOR PLACEMENT OPERATIONS WILL BE DETERMINED BY THE CONTRACTOR, AND SHALL CONSIDER THE RESIDUE FROM DISTILLATION PERCENTAGE SHOWN IN SECTION 708.03 OF THE STANDARD SPECIFICATIONS.
- (R-32) ESTIMATED AT 112 LBS. PER SQ. YD. PER 1" THICK.
- (R-34) PRICE BID TO INCLUDE COST OF FOG SEAL, MEETING THE REQUIREMENTS OF SECTION 407 OF THE STANDARD SPECIFICATIONS.
- (R-48) INCLUDES REMOVAL OF ALL EXISTING ROADWAY DRAINAGE STRUCTURES, HEADWALLS (UNLESS OTHERWISE SPECIFIED), INLETS, FENCES, AND OTHER STRUCTURES WITHIN THE RIGHT OF WAY.
- (R-49) TO BECOME THE PROPERTY OF AND BE DISPOSED OF BY THE CONTRACTOR IN A MANNER APPROVED BY THE ENGINEER.
- (R-50) MATERIALS REMOVED SHALL NOT BE MEASURED FOR PAYMENT UNDER SECTION 202.06 UNCLASSIFIED EXCAVATION.
- (R-52) INCLUDES 2% FOR GROUND MEASUREMENT.
- (R-53) ALL GATES AND GATE END POSTS FOR STRANDED WIRE FENCE (SWF) SHALL BE CONSTRUCTED AT THE SAME WIDTH AS THE EXISTING, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

ROADWAY 0100				PAY QUANTITIES (ROADWAY)			
SPEC. NO.	DESCRIPTION		UNIT	QUANTITY			
201(A)	0102	CLEARING AND GRUBBING	(1) L.SUM	1.00			
202(A)	0183	UNCLASSIFIED EXCAVATION	(R-1) C.Y.	49,489.00			
202(C)	0182	ROCK EXCAVATION	(10) C.Y.	1,400.00			
202(D)	0184	UNCLASSIFIED BORROW	(R-1, R-4)(3) C.Y.	14,325.00			
202(E)	0186	SELECT BORROW	(3) C.Y.	200.00			
205(A)	4229	TYPE A-SALVAGED TOPSOIL	(R-5, R-7) L.SUM	1.00			
221(C)	2801	TEMPORARY SILT FENCE	(3,4) L.F.	8,800.00			
221(F)	0100	TEMPORARY SILT DIKE	(3,4) L.F.	4,660.00			
221(G)	0150	TEMPORARY ROCK FILTER DAM TYPE 1	(3,4) C.Y.	60.00			
230(A)	2806	SOLID SLAB SODDING	(R-7) S.Y.	179,276.00			
230(F)	2812	WATERING	(R-9) KGAL.	7,172.00			
232(B)	2814	SEEDING METHOD B	AC.	2.00			
233(A)	2817	VEGETATIVE MULCHING	(R-11) AC.	49.50			
234(A)	2824	FERTILIZING (10-20-10)	(R-14) TON	18.00			
234(A)	4409	FERTILIZING (18-46-0)	(R-15) TON	3.80			
241	2832	MOWING	(R-16) AC.	73.80			
303(A)	2100	AGGREGATE BASE TYPE A	C.Y.	26,625.00			
326(A)	0100	GEOTEXTILE REINFORCEMENT	A	94,454.00			
326(B)	0100	GEOGRID REINFORCEMENT	(2) S.Y.	18,891.00			
402(E)	0225	TRAFFIC BOUND SURFACE COURSE TYPE E	(R-25) TON	11,477.00			
407(B)	0250	TACK COAT	GAL.	7,490.00			
408	5774	PRIME COAT	(R-23) GAL.	56,382.00			
409(A)	4242	FABRIC REINFORCEMENT	S.Y.	24,818.00			
409(B)	4268	BITUMINOUS BINDER	GAL.	4,964.00			
411(B)	5940	SUPERPAVE, TYPE S3(PG 70-28 OK)	(R-32)(15) TON	15,306.00			
411(B)	5945	SUPERPAVE, TYPE S3(PG 64-22 OK)	(R-32) TON	23,156.00			
411(C)	5955	SUPERPAVE, TYPE S4(PG 70-28 OK)	(R-32) TON	9,491.00			
411(C)	5960	SUPERPAVE, TYPE S4(PG 64-22 OK)	(R-32) TON	6,402.00			
412	5267	COLD MILLING PAVEMENT	(R-34) S.Y.	73,588.00			
501(A)	0313	STRUCTURAL EXCAVATION UNCLASSIFIED	C.Y.	182.00			
509(A)	0319	CLASS AA CONCRETE	C.Y.	499.00			
509(D)	0325	CLASS C CONCRETE	C.Y.	3,209.00			
511(A)	0332	REINFORCING STEEL	LB.	75,863.00			
601(A)	0297	TYPE I PLAIN RIPRAP	TON	705.00			
609(B)	1526	2'-8" COMB. CURB & GUTTER (8" BARRIER)	L.F.	28.00			
613(A)	0491	18" R.C. PIPE CLASS III	L.F.	366.00			
613(A)	0492	24" R.C. PIPE CLASS III	L.F.	417.00			
613(A)	0494	36" R.C. PIPE CLASS III	L.F.	94.00			
613(B)	0689	18" CORR. GALV. STEEL PIPE	L.F.	1,470.00			
613(B)	0690	24" CORR. GALV. STEEL PIPE	L.F.	202.00			
613(B)	0691	30" CORR. GALV. STEEL PIPE	L.F.	128.00			
613(H)	0500	8" PERFORATED PIPE UNDERDRAIN ROUND	(3) L.F.	1,000.00			
613(I)	1097	8" NON-PERF. PIPE UNDERDRAIN RND.	(3) L.F.	300.00			
613(L)	5726	18" PREFAB. CULVERT AND SECTION, ROUND	EA.	9.00			
613(L)	5730	24" PREFAB. CULVERT AND SECTION, ROUND	EA.	6.00			
613(L)	5734	36" PREFAB. CULVERT AND SECTION, ROUND	EA.	2.00			
613(M)	7186	TYPE A4 CULVERT END TREATMENT	(7) EA.	24.00			
613(M)	7187	TYPE B4 CULVERT END TREATMENT	(7) EA.	3.00			
613(M)	7188	TYPE C4 CULVERT END TREATMENT	(7) EA.	2.00			
613(M)	7191	TYPE AA4 CULVERT END TREATMENT	(7) EA.	2.00			
613(M)	7192	TYPE BB4 CULVERT END TREATMENT	(7) EA.	1.00			
613(M)	7196	TYPE A6 CULVERT END TREATMENT	(7) EA.	24.00			
613(M)	7197	TYPE B6 CULVERT END TREATMENT	(7) EA.	3.00			
613(M)	7198	TYPE C6 CULVERT END TREATMENT	(7) EA.	2.00			
613(M)	7201	TYPE AA6 CULVERT END TREATMENT	(7) EA.	2.00			
613(M)	7202	TYPE BB6 CULVERT END TREATMENT	(7) EA.	1.00			
613(Q)	5946	OUTLET LATERAL HEADWALL	EA.	20.00			
613(S)	1186	STANDARD BEDDING MATERIAL, CLASS B	C.Y.	265.00			
613(U)	0530	BORE & JACK 36" STEEL CASING	(12) L.F.	228.00			
613(U)	0600	BORE & JACK 42" CASING	(13) L.F.	94.00			
613(V)	1180	TRENCH EXCAVATION	C.Y.	479.00			
616(J)	1085	BORE AND JACK 24" CASING	(14) L.F.	112.00			
619(A)	0920	REMOVAL OF STRUCTURES & OBSTRUCTIONS	(R-48, R-49)(6) L.SUM	1.00			
619(B)	4728	REMOVAL OF ASPHALT PAVEMENT	(R-49, R-50) S.Y.	74,452.00			
619(B)	8660	PLUGGING ABANDONED SEWER	(15) EA.	6.00			
619(C)	0924	SAWING PAVEMENT	(5) L.F.	56,134.00			
624(A)	4281	FENCE-STYLE WWF	L.F.	353.00			
624(B)	4464	GATES-STYLE WWF (4.5' HIGH X 14' LONG)	EA.	29.00			
624(C)	4458	FENCE-STYLE SWF (4 BARBED WIRE)	(R-52, R-53) L.F.	573.00			
624(C)	4459	FENCE-STYLE SWF (5 BARBED WIRE)	(R-52, R-53) L.F.	25,773.00			
624(C)	7181	FENCE-STYLE SWF (6 BARBED WIRE)	(R-52, R-53) L.F.	5,338.00			
624(E)	4288	FENCE-STYLE CLF (4' HIGH, CLASS A)	(R-52, R-53) L.F.	245.00			
629(A)	4958	MAILBOX INSTALLATION-SINGLE	EA.	10.00			
629(B)	4959	MAILBOX INSTALLATION-MULTIPLE	EA.	1.00			
629(C)	4960	MAILBOX	EA.	12.00			
629(D)	4961	REMOVAL OF MAILBOX INSTALLATION	EA.	11.00			

STAKING 600				PAY QUANTITIES (STAKING)			
SPEC. NO.	DESCRIPTION		UNIT	QUANTITY			
642(B)	0096	CONSTRUCTION STAKING LEVEL II	(9) L.SUM	1.00			

STAKING 640				PAY QUANTITIES (CONSTRUCTION)			
SPEC. NO.	DESCRIPTION		UNIT	QUANTITY			
220	2800	SWPPP DOCUMENTATION AND MANAGEMENT	L.SUM	1.00			
640(A)	1426	FIELD OFFICE	EA.	1.00			
641	1552	MOBILIZATION	L.SUM	1.00			

PAY ITEM NOTES

- (1) NO STUMPS OR OTHER DEBRIS SHALL BE BURIED OR DISPOSED OF ON THE RIGHT-OF-WAY UNLESS APPROVED BY THE ENGINEER. DO NOT DISTURB VEGETATION BACK OF BACKSLOPE UNLESS REQUIRED FOR UTILITY RELOCATION.
- (2) THIS GEOGRID REINFORCEMENT QUANTITY IS AN ESTIMATED QUANTITY THAT MAY BE REQUIRED WITHIN THE PROJECT EXTENTS, AS DIRECTED BY THE ENGINEER. THE GEOGRID REINFORCEMENT, WHEN LOCATION DETERMINED BY ENGINEER, WILL BE PLACED ON TOP OF THE 8" COMPACTED SUBGRADE, AND PLACED UNDERNEATH THE GEOTEXTILE REINFORCEMENT THAT LIES BELOW THE AGGREGATE BASE AS SHOWN ON THE TYPICAL SECTIONS. THIS GEOGRID REINFORCEMENT WILL FOLLOW ODOT SPECIFICATIONS, WITH MATERIAL MEETING THE MINIMUM REQUIREMENTS OF GEOGRID-TYPE 1. REFER TO ODOTS MATERIAL DIVISION APPROVED PRODUCTS LIST (APL).
- (3) ESTIMATED QUANTITY TO BE USED AS DIRECTED BY THE ENGINEER.
- (4) PRICE BID SHALL INCLUDE REMOVAL OF SEDIMENT WHEN DEPTH OF SEDIMENT REACHES ONE-THIRD HEIGHT OF THE TEMPORARY SILT DIKE OR TEMPORARY SILT FENCE.
- (5) SAWING PAVEMENT SHALL INCLUDE BOTH ASPHALT AND CONCRETE AND ANY COMBINATIONS THAT MAY BE ENCOUNTERED.
- (6) ADDITIONAL ITEMS OF WORK ARE TO BE INCLUDED IN OTHER ITEMS OF WORK. PARCEL NO 2. AT STA. 1340+50R 50' CLS SHOULD INCLUDE REMOVAL OF METAL AND WOOD POSTS, DEBRIS REMAINING FROM BURNED BARN. ITEMS TO BE REMOVED MAY OR MAY NOT BE PRESENT IN ANY SPECIFIED CONDITION.
- (7) C.E.T. SHALL BE GRATED (REFER TO DRAINAGE SUMMARY FOR MORE INFORMATION).
- (8) BORROW IS REQUIRED IN PHASE I.
- (9) INCLUDES STAKING OF SLOPE STAKES, BLUE TOPS AND BENCHMARKS AS DIRECTED BY THE ENGINEER.
- (10) ROCKY SOIL IS LOCATED ON THE NORTH SIDE OF THE HIGHWAY NEAR STA. 1157+00.
- (11) NOT USED
- (12) BORE AND JACK 36" CASING FOR STR. NO. C10 AND C29.
- (13) BORE AND JACK 42" CASING FOR STR. NO. C17.
- (14) BORE AND JACK 24" CASING FOR STR. NO. C18.
- (15) PLUG ABANDON DRAINAGE STRUCTURES AT:
STA 1115+88.20
STA 1207+00.00
STA 1234+19.23
STA 1286+51.86
STA 1301+39.50
STA 1339+56.05
- (16) INCLUDES ADDITIONAL AC TYPE S3 (PG 70-28 OK) THAT HAS BEEN ADDED TO THE QUANTITY DUE TO THE ASPHALT WEDGE THAT WILL BE PRESENT THROUGHOUT MOST SUPERELEVATIONS.

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION
DRAWN	MM		
CHECKED	DP		SUMMARY OF PAY QUANTITIES (ROADWAY)
APPROVED	SL		
SQUAD	BKL, Inc.		JOB PIECE NO. 24066 (04) SHEET NO. AR01

ENVIRONMENTAL MITIGATION NOTES

LOCATIONS OUTSIDE THE PROJECT AREA IN THE FOLLOWING AREA MUST NOT BE UTILIZED FOR BORROW, EQUIPMENT STAGING, HAUL ROADS, SPOIL DUMPS OR ANY OFF-SITE PROJECT-RELATED ACTIVITY.

T2S R10E

SECTION 19: SE1/4 SE1/4 NW1/4
SE1/4 SE1/4 SW1/4

SECTION 24: SW1/4 SW1/4 SE1/4

AMERICAN BURYING BEETLE (ABB) NOTE

THE AMERICAN BURYING BEETLE IS A LARGE CARRION BURYING BEETLE THAT OCCURS WITHIN THE PROJECT LIMITS. NO ARTIFICIAL LIGHTING SHALL BE USED DURING CONSTRUCTION. CARCASSES AND ALL FOOD TRASH SHALL BE REMOVED FROM THE PERMANENT AND TEMPORARY RIGHT-OF-WAY THROUGHOUT PROJECT ACTIVITIES. FOLLOWING CONSTRUCTION, 5 INCHES OF TOPSOIL SHALL BE PLACED ON TOP OF ALL AREAS OF GROUND DISTURBANCE, PRIOR TO RE-VEGETATION. RE-VEGETATION OF THE AREA OUTSIDE OF THE CLEAR ZONE TO THE EDGE OF RIGHT-OF-WAY OR THE EDGE OF DISTURBANCE SHALL BE WITH NATIVE SPECIES. RE-VEGETATION OF THE CLEAR ZONE AS SHOWN ON THE PLANS SHALL BE WITH BERMUDA SOD.

BAT BRIDGE SEASONAL RESTRICTION NOTE

THE NORTHERN LONG-EARED BATS ARE LISTED BAT SPECIES THAT OCCUR WITHIN THE PROJECT'S ACTION AREA. IN ORDER TO AVOID AND MINIMIZE ADVERSE IMPACTS TO LISTED BAT SPECIES, BRIDGE DEMOLITION SHALL BE RESTRICTED TO BETWEEN NOVEMBER 16, AND MARCH 31, OUTSIDE OF THE ACTIVE SEASON. IF BRIDGE DEMOLITION DURING THE ACTIVE SEASON (BETWEEN APRIL 1, AND NOVEMBER 15) CANNOT BE AVOIDED, THE RESIDENT ENGINEER SHALL CONTRACT THE ODOT BIOLOGIST AT 405-521-2515 TO SCHEDULE A BAT BRIDGE INSPECTION, PRIOR TO ANY BRIDGE WORK. INSPECTION SURVEYS CAN ONLY BE CONDUCTED BETWEEN MAY 15, AND AUGUST 15. IF THE SURVEY FINDS LISTED BAT SPECIES WITHIN THE PROJECT'S ACTION AREA, BRIDGE DEMOLITION SHALL ONLY BE PERMITTED BETWEEN NOVEMBER 16, AND MARCH 31 (WHEN BATS ARE HIBERNATING IN CAVES).

BAT TREE REMOVAL SEASONAL RESTRICTION NOTE

THE NORTHERN LONG-EARED BATS ARE LISTED BAT SPECIES THAT OCCUR WITHIN THE PROJECT'S ACTION AREA. IN ORDER TO AVOID AND MINIMIZE ADVERSE IMPACTS TO THESE SPECIES, ALL TREE REMOVAL SHALL BE RESTRICTED TO BETWEEN NOVEMBER 16, AND MARCH 21 (OUTSIDE THE BATS' ACTIVE SEASON). IF TREE REMOVAL DURING THE ACTIVE SEASON (BETWEEN APRIL 1, AND NOVEMBER 15) CANNOT BE AVOIDED, THE RESIDENT ENGINEER SHALL CONTACT ODOT BIOLOGIST AT 405-521-2515 TO SCHEDULE A BAT ACOUSTIC SURVEY, PRIOR TO ANY TREE REMOVAL. ACOUSTIC SURVEYS CAN ONLY BE CONDUCTED BETWEEN MAY 15, AND AUGUST 15. IF THE SURVEY FINDS LISTED BAT SPECIES WITHIN THE PROJECT'S ACTION AREA, TREE REMOVAL SHALL ONLY BE PERMITTED BETWEEN NOVEMBER 16, AND MARCH 31 (WHEN BATS ARE HIBERNATING IN CAVES).

BAT TREE REMOVAL LIMITS NOTE

THE NORTHERN LONG-EARED BATS ARE LISTED BAT SPECIES THAT OCCUR WITHIN THE PROJECT'S ACTION AREA. IN ORDER TO AVOID AND MINIMIZE ADVERSE IMPACTS TO THESE SPECIES, THE REMOVAL OF TREES AND SHRUBS SHALL BE RESTRICTED TO AREAS WITHIN THE ACTUAL LIMITS OF CONSTRUCTION (TOE OF SLOPE/TOP OF CUT). THE RESIDENT ENGINEER SHALL INSTALL BRIGHT-COLORED FLAGGING/FENCING TO INDICATE WHICH TREES ARE NOT TO BE REMOVED AND ENSURE LIMITS OF TREE REMOVAL ARE VISIBLY AND CLEARLY DEFINED FOR THE CONTRACTOR. THE RESIDENT ENGINEER SHALL ALSO PROVIDE BEFORE AND AFTER PHOTO-DOCUMENTATION TO THE ODOT BIOLOGIST OF EXTENT OF TREE CLEARING WITHIN THE PROJECT AREA.

BAT LIGHTING NOTE

THE NORTHERN LONG-EARED BAT IS A THREATENED BAT SPECIES. THE SPECIES OCCURS WITHIN THE PROJECT'S ACTION AREA. IN ORDER TO AVOID AND MINIMIZE ADVERSE IMPACTS TO THIS SPECIES, IF ANY PERMANENT LIGHTING IS INSTALLED OR REPLACED, DOWNWARD FACING FULL CUT-OFF LENS LIGHTS SHALL BE INSTALLED AND DIRECTED AWAY FROM WOODED AREAS AND STREAMS.

MIGRATORY BIRDS NOTE

MIGRATORY BIRDS ARE PROTECTED BY THE FEDERAL MIGRATORY BIRD TREATY ACT. MANY BIRDS COMMONLY USE BRIDGES AND CULVERTS FOR NESTING. THE NESTING SEASON FOR MOST MIGRATORY BIRD SPECIES EXTENDS FROM APRIL 1 TO AUGUST 31. SWALLOW USE AT THREE BRIDGES (NBI #06055, 06303, 24032) AND A CULVERT AT STATION 1534+40.87 HAVE BEEN OBSERVED DURING THE INITIAL SURVEYS CONDUCTED AS PART OF THE BIOLOGICAL STUDIES IN 2015. PAINTING, REPAIR, RETROFIT OR DEMOLITION OF THE EXISTING BRIDGE/STRUCTURES SHALL BE CONDUCTED BETWEEN SEPTEMBER 1, AND FEBRUARY 28, WHEN MIGRATORY BIRD NESTS ARE NOT OCCUPIED. IF PAINTING, REPAIR, RETROFIT, REHABILITATION OR DEMOLITION CANNOT BE COMPLETED BETWEEN SEPTEMBER 1 AND FEBRUARY 28, THE BRIDGE SHALL BE PROTECTED FROM NEW NEST ESTABLISHMENT PRIOR TO MARCH 1, BY MEANS THAT DO NOT RESULT IN BIRD DEATH OR INJURY. OPTIONS INCLUDE THE EXCLUSION OF ADULT BIRDS FROM SUITABLE NEST SITES ON OR WITHIN A STRUCTURE PLACEMENT OF WEATHER-RESISTANT POLYPROPYLENE NETTING WITH 0.25-INCH OR SMALLER OPENINGS, PRIOR TO MARCH 1. METHODS OTHER THAN NETTING MUST BE PRE-APPROVED BY THE ODOT BIOLOGISTS.

GENERAL CONSTRUCTION NOTES

THIS PROJECT SHALL BE CONSTRUCTED WITHOUT CLOSING THE EXISTING ROAD TO LOCAL AND THROUGH TRAFFIC. SEE STANDARD SPECIFICATIONS FOR MAINTENANCE OF LOCAL AND THROUGH TRAFFIC.

FOR PROJECTS THAT INCLUDE WIDENING AND/OR RESURFACING, THE CONTRACTOR SHALL SCHEDULE OPERATIONS TO MINIMIZE POTENTIAL DROP-OFF HAZARDS AND SHALL SUBMIT A SEQUENCE OF CONSTRUCTION OPERATIONS TO THE RESIDENT ENGINEER FOR APPROVAL BEFORE OPERATIONS BEGIN. ANY PORTION OF THE CONSTRUCTION OPERATIONS, SUCH AS SUPERPAVE LAYING OPERATIONS, EXCAVATION FOR PAVEMENT WIDENING, OR EXTENSION OF ROADWAY STRUCTURES, SHALL BE LIMITED TO ONE SIDE AT A TIME, AND THE PROCEDURES OUTLINED IN THE PAVEMENT DROP-OFF TREATMENT STANDARD PDT-1 (LATEST REVISION) SHALL BE IMPLEMENTED. ONLY THAT AMOUNT OF OPEN TRENCH WILL BE ALLOWED THAT CAN BE SURFACED IN 1 (ONE) DAY'S TIME WITHOUT APPROVAL BY THE ENGINEER. LIGHTS, SIGNS AND BARRICADES SHALL BE MOVED AS WORK PROGRESSES.

ALL TREES, BRUSH, AND OTHER DEBRIS THAT MIGHT INTERFERE WITH THE FLOW OF WATER SHALL BE CLEANED OUT TO THE RIGHT-OF-WAY LINE, AT EACH STRUCTURE AND BRIDGE, IN A MANNER APPROVED BY THE ENGINEER. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY RIGHT-OF-WAY FENCE AS REQUIRED. WHEN THE PORTION OF THE PROJECT THAT REQUIRED THIS FENCE IS COMPLETED, THE TEMPORARY FENCE SHALL BE REMOVED, AND PERMANENT RIGHT-OF-WAY FENCING SHALL BE RESTORED OR INSTALLED IN A MANNER APPROVED BY THE ENGINEER. ALL COST OF TEMPORARY FENCING SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

ALL FLOWLINES THAT ARE TO BE FILLED SHALL BE THOROUGHLY TAMPED BEFORE CONSTRUCTION OR EXTENSION OF DRAINAGE STRUCTURES. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

IN ORDER TO ALLEVIATE DUST CONDITIONS DURING GRADING OPERATIONS AND BEFORE PAVEMENT WORK IS COMPLETED, THE CONTRACTOR SHALL SPRINKLE GRADING AT INTERVALS APPROVED BY THE ENGINEER. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

THE CONTRACTOR SHALL NOT WASTE ANY EXCESS EXCAVATION UNTIL ALL PLANNED EMBANKMENTS AND BACKFILLS ARE COMPLETED. EXCESS UNCLASSIFIED EXCAVATION MATERIAL DETERMINED BY THE ENGINEER TO BE SUITABLE FOR BACKFILL SHALL BE USED TO REDUCE ANY UNCLASSIFIED BORROW NEEDED. COST OF SECOND HANDLING SHALL BE INCLUDED IN OTHER ITEMS OF WORK. ANY REMAINING EXCESS EXCAVATION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND BE DISPOSED OF IN A MANNER APPROVED BY THE ENGINEER.

THE CONTRACTOR SHALL KEEP THE OPEN TRENCH DRAINED. COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

AREAS ON WHICH SALVAGED TOPSOIL IS TO BE REPLACED SHALL HAVE 18-46-0 FERTILIZER APPLIED, AT THE RATE OF 150 POUNDS PER ACRE, JUST PRIOR TO THE REPLACEMENT OF SALVAGED TOPSOIL.

AT THE BEGINNING OF TURFING OPERATIONS, ANY AREAS INCLUDED IN PLANNED QUANTITIES THAT HAVE GROWN A SATISFACTORY VOLUNTEER TURF OF PERENNIAL GRASS, AS DETERMINED BY THE ENGINEER, SHALL BE FERTILIZED AND WATERED AS CALLED FOR ON THE PLANS, BUT SHALL NOT BE SEEDED, SODDED, OR SPRIGGED.

T.B.S.C. SURFACES SHALL BE SPRINKLED WITH WATER AND ROLLED WITH A PNEUMATIC ROLLER IN A MANNER APPROVED BY THE ENGINEER.

PRIOR TO FINAL ACCEPTANCE, ALL EXPOSED CURB SURFACES SHALL BE CLEANED OF ALL DISCOLORATION SUCH AS ASPHALT STAIN, TIRE MARKS, OR OTHER DISFIGUREMENT.

EXCESS ASPHALT AT JOINTS AND CRACKS IN EXISTING PAVEMENT SHALL BE REMOVED FLUSH TO TOP OF PAVING IN A MANNER APPROVED BY THE ENGINEER.

IN ACCORDANCE WITH THE OKLAHOMA UNDERGROUND FACILITIES DAMAGE PREVENTION ACT THE CONTRACTOR SHALL NOTIFY THE OKLAHOMA ONE-CALL SYSTEM, INC. 48 HOURS PRIOR TO BEGINNING EXCAVATION. OKLAHOMA ONE-CALL SYSTEM, INC. "CALL OKIE" 1-800-522-6543 OR 811

PLUGGING ABANDONED WATER WELLS WILL BE HANDELED IN ACCORDANCE WITH CURRENT REGULATIONS ESTABLISHED BY THE OKLAHOMA WATER RESOURCES BOARD BY A LICENSED WELL-DRILLER.

VEGETATIVE MULCHING: THE VEGETATIVE MULCH SHALL BE ANCHORED IN ACCORDANCE WITH THE "MULCHING-TILLER METHOD", AS SPECIFIED IN 233.04B(2) OF THE STANDARD SPECIFICATIONS.

PIPE UNDERDRAIN QUANTITIES ESTIMATED ONLY. LOCATION, IF AND WHERE REQUIRED, TO BE DETERMINED BY THE ENGINEER.

THE CONTRACTOR SHALL REMOVE AND RESET MAILBOXES AS NECESSARY. MAILBOXES ARE TO BE MAINTAINED IN AN UPRIGHT POSITION AND ACCESSIBLE TO MAIL CARRIER'S CAR DURING CONSTRUCTION. ANY DAMAGE TO BOXES OR SUPPORTS SHALL BE REPAIRED BY THE CONTRACTOR. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

COLD MILLED SURFACE TO BE IMMEDIATELY FOG SEALED OR WITHIN 24 HOURS, AS DIRECTED BY THE ENGINEER.

PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\04-GENERAL NOTES (ROADWAY).dgn

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION GENERAL NOTES (ROADWAY) JOB PIECE NO. 24066 (04) SHEET NO. AR02
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

TRAFFIC CONSTRUCTION PAY QUANTITY NOTES

- (TC-1) THE CONTRACTOR SHALL FURNISH AND INSTALL SUCH LIGHTS, SIGNS, BARRICADES, AND PROVIDE FLAGGERS NECESSARY FOR THE CONTROL, SAFETY, AND MAINTENANCE OF TRAFFIC WHEN INSTALLING, RELOCATING OR DELIVERING PORTABLE LONGITUDINAL BARRIER.
- (TC-2) QUANTITY INCLUDES SUFFICIENT LENGTH OF PORTABLE LONGITUDINAL BARRIER TO PROVIDE FOR THE LONGEST SECTION SHOWN ON THE PLANS. THIS SAME BARRIER WILL BE USED ON OTHER DETOUR PHASES.
- (TC-13) A PART, OR ALL, OF THIS ITEM IS INTENDED FOR REPLACEMENT OF REMOVED EXISTING CONFLICTING STRIPING.
- (TC-14) SEE STANDARD DRAWING PM1-1, PM2-1, PM3-1, PM4-1, PM5-1, PM6-1, PM7-1, PM8-1 (LATEST REVISION). A PART, OR ALL, OF THE QUANTITY SHOWN IS TO BE USED AS FINAL PAVEMENT MARKING.
- (TC-17) INCLUDES AN ESTIMATED 200 L.F. (PAINT)(4" WIDE) WHITE 10,000 L.F. (PAINT)(4" WIDE) YELLOW STRIPE.
- (TC-20) ALL STRIPING TO BE PLACED ON TEMPORARY SURFACES OR ON SURFACES SCHEDULED TO BE REMOVED SHALL BE DONE WITH PAINT UNLESS OTHERWISE NOTED ON THE PLANS OR STANDARD DRAWINGS. TEMPORARY PAVEMENT MARKINGS PLACED ON FINISHED PAVEMENT OR EXISTING PAVEMENT TO REMAIN IN PLACE SHALL USE ONE OF THE FOLLOWING METHODS:
REMOVABLE PAVEMENT MARKING TAPE
CLASS A PAVEMENT MARKERS
- (TC-21) INCLUDED IN THE COST OF THIS ITEM SHALL BE INSTALLATION, MAINTENANCE, AND REMOVAL. THIS ITEM SHALL BE BID ACCORDINGLY.
- (TC-22) AMOUNT SHOWN IS AN APPROXIMATION AND THE ACTUAL AMOUNT OF REMOVAL, IF NECESSARY, SHALL BE DETERMINED BY THE ENGINEER. PRICE BID FOR PAVEMENT MARKING REMOVAL SHALL INCLUDE THE COST OF REMOVING STRIPE, ARROWS, WORDS AND SYMBOLS, AS SHOWN IN THE PLANS. THESE ITEMS MAY CONSIST OF PLASTIC, PAINT OR NON-REMOVABLE MARKING TAPE.
- (TC-26) CONSTRUCTION TRAFFIC CONTROL WILL BE INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH CHAPTER VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, AND APPLICABLE O.D.O.T. STANDARD DRAWINGS. PRICE BID FOR THIS ITEM SHALL BE PAYMENT IN FULL FOR THE INSTALLATION, MAINTENANCE AND SUBSEQUENT REMOVAL OF ALL NECESSARY CONSTRUCTION TRAFFIC CONTROL DEVICES REQUIRED FOR COMPLETION OF THE PROJECT.

ALL SIGNS AND BARRICADES WHICH ARE SHOWN WITH TYPE 'A' LIGHTS IN THE STANDARD DRAWINGS SHALL HAVE THE CORRESPONDING LIGHT ATTACHED DURING NON-DAYLIGHT HOURS.
- (TC-28) INCLUDED IN THIS ITEM ARE ALL S.C.S. (SPECIAL CONSTRUCTION SIGNING) SIGNS WHICH ARE BETWEEN 0.00 S.F. AND 6.25 S.F. ALSO INCLUDED IN THIS ITEM SHALL BE THE COST OF INSTALLATION, MAINTENANCE, AND REMOVAL OF THESE SIGNS.
- (TC-29) INCLUDED IN THIS ITEM ARE ALL S.C.S. (SPECIAL CONSTRUCTION SIGNING) SIGNS WHICH ARE BETWEEN 6.26 S.F. AND 15.99 S.F. ALSO INCLUDED IN THIS ITEM SHALL BE THE COST OF INSTALLATION, MAINTENANCE, AND REMOVAL OF THESE SIGNS.
- (TC-30) INCLUDED IN THIS ITEM ARE ALL S.C.S. (SPECIAL CONSTRUCTION SIGNING) SIGNS WHICH ARE BETWEEN 16.00 S.F. AND 32.99 S.F. ALSO INCLUDED IN THIS ITEM SHALL BE THE COST OF INSTALLATION, MAINTENANCE, AND REMOVAL OF THESE SIGNS.
- (TC-33) ALL CONSTRUCTION WORK ZONE SIGNS SHALL HAVE FLUORESCENT SHEETING. THE FLUORESCENT SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956 (LATEST REVISION).

THE MANUFACTURER SHALL FURNISH A TYPE 'D' CERTIFICATION IN ACCORDANCE WITH O.D.O.T. STANDARD SPECIFICATIONS (CURRENT EDITION) SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON MATERIAL SUBMITTED FOR APPROVAL.
- (TC-52) ANY USED CHANGEABLE MESSAGE SIGN OR CONSTRUCTION ZONE IMPACT ATTENUATOR TO BE PLACED ON THIS PROJECT SHALL BE SUBJECT TO INSPECTION AND APPROVAL, BY THE OKLAHOMA DEPARTMENT OF TRANSPORTATION, TO ASSURE THAT THEY ARE IN GOOD WORKING CONDITION. PRIOR TO PLACEMENT ON THE PROJECT.
- (TC-61) DAMAGE TO A FINISHED OR EXISTING SURFACE RESULTING FROM THE CONTRACTOR'S NEGLIGENCE IN THE REMOVAL OF CONSTRUCTION ZONE PAVEMENT MARKERS OR CHANNELIZING DEVICES AND THE BITUMINOUS ADHESIVE USED IN THEIR INSTALLATION, SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE ENGINEER.

- (TC-70) THIS ITEM IS AND ESTIMATED QUANTITY TO BE USED AS DEEMED NECESSARY BY THE ENGINEER.
- (TC-73) QUANTITY SHOWN INCLUDES 400 EACH (WHITE) AND 800 EACH (YELLOW) CONSTRUCTION ZONE PAVEMENT MARKERS (FLEX TABS). THESE CONSTRUCTION ZONE PAVEMENT MARKERS SHALL BE EITHER "DAVIDSON PLASTIC, MODEL TOM", OR AN APPROVED EQUAL. PRICE BID FOR THIS ITEM SHALL INCLUDE THE INITIAL PLACEMENT, SUBSEQUENT REPLACEMENT, AND REMOVAL. THE CONSTRUCTION ZONE PAVEMENT MARKERS (FLEX TABS) SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND AS SHOWN ON STANDARD DRAWING TCS21-1-(LATEST REVISION).
- (TC-75) TEMPORARY PAVEMENT MARKINGS SHALL BE IN PLACE THE SAME DAY THAT EXISTING PAVEMENT MARKINGS ARE REMOVED FROM ANY ROADWAY OPEN TO TRAFFIC. ALSO, ALL TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED PRIOR TO THE INSTALLATION OF FINAL STRIPING.
- (TC-80) INCLUDED IN THIS ITEM SHALL BE ONE (1) ADDITIONAL UNIT TO BE USED AS A STAND-BY OR REPLACEMENT. THIS STAND-BY UNIT SHALL BE IMMEDIATELY ACCESSIBLE TO REPLACE A DAMAGED, STOLEN OR MALFUNCTIONING UNIT. THE AMOUNT OF TIME BETWEEN THE REMOVAL OF THE DAMAGED UNIT AND THE INSTALLATION OF THE STAND-BY UNIT SHALL BE NO MORE THAN TWENTY-FOUR (24) HOURS.
- (TC-84) 365 CONSTRUCTION CALENDAR DAYS WERE USED TO COMPUTE THE SIGN DAY PAY ITEMS. THE AMOUNT OF CALENDAR DAYS USED TO COMPUTE THE SIGN DAY PAY ITEMS IS AN ESTIMATED QUANTITY ONLY, BASED ON THE CURRENT O.D.O.T. STANDARDS AND SUGGESTED CONSTRUCTION SEQUENCE FOR THIS PROJECT. THESE ESTIMATED SIGN DAY QUANTITIES MAY CHANGE AS THE PROJECTS CONSTRUCTION TRAFFIC CONTROL IS MODIFIED DURING CONSTRUCTION.
- (TC-85) THESE SIGNS MUST BE ON THE OKLAHOMA DEPARTMENT OF TRANSPORTATION LIST OF APPROVED CHANGEABLE MESSAGE SIGNS. FOR A LITS OF THE APPROVED SIGNS GO TO THE OKLAHOMA DEPARTMENT OF TRANSPORTATION WEBSITE AT:
<http://www.okladot.state.ok.us/traffic/gpl/index.php>

TRAFFIC SIGNING PAY QUANTITY NOTES

- (TS-24) QUANTITY SHOWN INCLUDES 3,442.0 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(WHITE) AND 3,442.0 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF FOUR INCH (4") WIDE TRAFFIC STRIPE.
- (TS-25) QUANTITY SHOWN INCLUDES 57,855.0 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(WHITE) AND 57,353.0 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF SIX INCH (6") WIDE TRAFFIC STRIPE.
- (TS-27) QUANTITY SHOWN INCLUDES 0 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(WHITE) AND 3,126.0 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF SIX INCH (12") WIDE TRAFFIC STRIPE.
- (TS-28) QUANTITY SHOWN INCLUDES 180.0 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(WHITE) AND AND WILL BE MEASURED BY THE LINEAR FOOT OF TWENTY-FOUR INCH (24") WIDE TRAFFIC STRIPE.
- (TS-33) INCLUDED IN THIS PAY ITEM IS ALL HARDWARE ASSOCIATED WITH PROPERLY ANCHORING AND MOUNTING THE HIGHWAY SIGN IN ACCORDANCE WITH O.D.O.T. PLANS AND STANDARD DRAWINGS SSA1-1 AND SSP1-1-(LATEST REVISION).
- (TS-34) INCLUDED IN THIS PAY ITEM IS THE REMOVAL OF ANY EXISTING SIGNS TO BE REPLACED BY NEW ASSEMBLIES AND THE REMOVAL OF ANY EXISTING SIGNS THAT WILL BE IN CONFLICT WITH THE NEW ROADWAY OR NEW SIGNAGE.

TRAFFIC 0300		SUMMARY OF PERMANENT QUANTITIES (TRAFFIC)		
SPEC. NO.		DESCRIPTION	UNIT	QUANTITY
413(A)	4870	RUMBLE STRIP - CENTERLINE HMA-CON	L.F.	35,610.00
413(B)	4863	RUMBLE STRIP - METHOD HMA-CYC	L.F.	57,300.00
805(D)	8756	(PL) REMOVE & RESET EXISTING SIGNS	EA.	6.00
850(A)	8110	SHEET ALUMINUM SIGNS	(TS-34) S.F.	167.00
851(C)	8324	2" SQUARE TUBE POST	(TS-33) L.F.	187.00
853	9039	DELINEATORS (TYPE 2, CODE 3)	(1) EA.	50.00
856(A)	8530	TRAFFIC STRIPE (MULTI-POLYMER) (4" WIDE)	(TS-24) L.F.	6,884.00
856(A)	8535	TRAFFIC STRIPE (MULTI-POLY.) (6" WIDE)	(TC-13,14)(TS-25)(5) L.F.	1,5208.00
856(A)	8548	TRAFFIC STRIPE (MULTI-POLY.) (12" WIDE)	(TS-27) L.F.	3,126.00
856(A)	8555	TRAFFIC STRIPE (MULTI-POLY.) (24" WIDE)	(TS-28) L.F.	180.00
856(B)	8860	TRAFFIC STRIPE (MULTI-POLY.) (ARROWS)	L.F.	39.00

TRAFFIC 0301		SUMMARY OF CONSTRUCTION QUANTITIES (TRAFFIC)		
SPEC. NO.		DESCRIPTION	UNIT	QUANTITY
823	8478	(SP) PORTABLE TRAFFIC SIGNAL SYSTEM	(TC-80,84) S.D.	365.00
857(A)	8839	CONSTRUCTION TRAFFIC STRIPE (PAINT)(4" WIDE)	(TC-17,20,70,75) L.F.	10,200.00
857(E)	8890	(PL) CONST ZONE PAV MKRS (FLEX TAB) TYPE 2-2	(TC-21,61,70,73,75) EA.	1,200.00
857(F)	8006	PAVEMENT MARKING REMOVAL (TRAFFIC STRIPE)	(TC-22,70,75) L.F.	10,200.00
858(A)	8956	PAVEMENT MARKERS CLASS D TYPE 2	EA.	2,000.00
871(B)	8705	(SP) CONST. ZONE IMPACT ATTEN.	(TC-80,84) S.D.	730.00
877(B)	8484	DELIVER PORTABLE LONGITUDINAL BARRIER	(TC-1,2) L.F.	440.00
877(C)	8486	RELOCATION OF PORTABLE LONGITUDINAL BARRIER	(TC-1) L.F.	880.00
880(B)	8818	CONSTRUCTION SIGNS 0 TO 6.25 SF	(TC-26,28,33,84) S.D.	6,600.00
880(B)	8821	CONSTRUCTION SIGNS 6.26 SF TO 15.99 SF	(TC-26,29,33,84) S.D.	30,360.00
880(B)	8824	CONSTRUCTION SIGNS 16.0 SF TO 32.99 SF	(TC-26,30,33,84) S.D.	11,880.00
880(C)	8842	CONSTRUCTION BARRICADES (TYPE III)	(TC-26,84) S.D.	74,580.00
880(C)	8848	WING BARRICADES	(TC-26,84) S.D.	2,640.00
880(D)	8854	VERTICAL PANELS	(TC-26,84)(2) S.D.	36,000.00
880(E)	8860	WARNING LIGHTS (TYPE A)	(TC-26,84) S.D.	159,720.00
880(F)	8878	DRUMS	(TC-26,84)(2) S.D.	374,880.00
880(I)	8902	FLAGGER	S.D.	200.00
882(A)	8303	PORTABLE CHANGEABLE MESSAGE SIGN	(TC-52,80,84,85)(3,4) EA.	2.00

TRAFFIC SIGNING GENERAL CONSTRUCTION NOTES

ALL REGULATORY SIGNS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR TYPE III SHEETING.

ALL WARNING SIGNS SHALL HAVE FLUORESCENT YELLOW SHEETING. THE FLUORESCENT YELLOW SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION); REQUIREMENTS FOR TYPE VIII SHEETING.

ALL GREEN AND BLUE SIGNS ON CONVENTIONAL HIGHWAYS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR THE TYPE III SHEETING.

THE MANUFACTURER SHALL FURNISH A TYPE 'A' CERTIFICATION IN ACCORDANCE WITH ODOT STANDARD SPECIFICATIONS, LATEST EDITION, SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON THE MATERIAL SUBMITTED FOR APPROVAL.

THE CONTRACTOR SHALL PROVIDE A PERSON TO BE ON 24 HOUR CALL AS DETERMINED BY THE ENGINEER. THIS PERSON SHALL HOLD A CERTIFICATION FROM THE AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA) OR THE OKLAHOMA TRAFFIC ENGINEERING ASSOCIATION (OTEA) AS A TRAFFIC CONTROL TECHNICIAN OR TRAFFIC CONTROL SUPERVISOR.

- (1) INCLUDED FOR USE AT NEW DRAINAGE STRUCTURE HEADWALLS, ONE PER HEADWALL.
- (2) WARNING LIGHTS TYPE C ARE NOT REQUIRED.
- (3) SIGN PLACEMENT LOCATIONS TO BE DETERMINED BY THE ENGINEER.
- (4) SIGNS TO BE IN PLACE 14 DAYS BEFORE THE BEGINNING OF CONSTRUCTION.
- (5) QUANTITY SHOWN INCLUDES 48,586 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(YELLOW) WILL BE MEASURED BY THE LINEAR FOOT OF SIX INCH (6") WIDE TRAFFIC STRIPE.

PRINT DATE: 8/23/2018 C:\Programs\590-SHT Final Plans\OGNAV101-24066(04)-SUMMARY OF PAY QUANTITIES (TRAFFIC).dgn

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

SUMMARY OF PAY QUANTITIES (TRAFFIC)
JOB PIECE NO. 24066 (04) SHEET NO. AT01

PRINT DATE: 8/25/2018 C:\Projects\150-SHT Final Plans\DNVAU01-24066\04-SUMMARY OF ROADWAY QUANTITIES SHEET 1.dwg

SUMMARY OF SURFACING														
Station to Station	Aggregate Base Type A	Geotextile Reinforcement	Traffic Bound Surface Course Type E	Tack Coat	Prime Coat	Fabric Reinf.	Bituminous Binder	Superpave, Type S3 (PG 70-28 OK)	* ASPH. WEDGE Superpave, Type S3 (PG 70-28 OK)	Superpave, Type S3 (PG 64-22 OK)	Superpave Type S4 (PG 70-28 OK)	Superpave Type S4 (PG 64-22 OK)	Cold Milling Pavement	Sawing Pavement
	303(A)	326(A)	420(E)	407(B)	408	409(A)	409(B)	411(B)	411(B)	411(B)	411(C)	411(C)	412	619(C)
	C.Y.	S.Y.	TON	GAL.	GAL.	S.Y.	GAL.	TON	TON	TON	TON	TON	S.Y.	L.F.
1061+49.80 to 1069+00.00	840.2	2,912.4	302.6	212.1	1,649.5	666.8	133.4	482.3	0.0	649.5	321.6	153.6	2,248.0	1,526.4
1069+00.00 to 1082+00.00	1,425.9	4,956.8	524.4	343.2	2,804.3	1,155.6	231.1	807.5	0.0	1,044.2	538.3	230.4	3,489.6	2,621.0
1082+00.00 to 1095+00.00	1,099.3	3,976.9	524.4	290.0	2,216.4	1,155.6	231.1	582.4	0.0	898.2	388.3	261.4	3,407.1	2,600.0
1095+00.00 to 1108+00.00	1,105.2	3,994.7	524.4	290.0	2,227.1	1,155.6	231.1	582.4	0.0	902.2	388.3	261.4	3,389.3	2,600.0
1108+00.00 to 1121+00.00	1,229.0	4,367.5	524.4	307.7	2,450.4	1,155.6	231.1	642.7	160.5	975.3	428.5	260.7	3,374.8	2,600.0
1121+00.00 to 1134+00.00	1,428.2	4,963.6	524.4	338.5	2,808.4	1,155.6	231.1	746.3	10.9	1,092.1	497.5	260.7	3,394.2	2,620.0
1134+00.00 to 1139+17.75	412.7	1,507.0	208.8	115.4	837.0	460.2	92.0	232.0	0.0	345.4	154.6	103.8	1,433.8	1,083.5
PROJECT EXCEPTION														
1146+48.78 to 1147+00.00	14.8	70.1	20.7	11.4	35.6	45.5	9.1	22.9	0.0	20.4	15.3	10.3	220.8	150.4
1147+00.00 to 1160+00.00	1,113.5	4,019.4	524.4	290.0	2,241.9	1,155.6	231.1	582.4	0.0	907.0	388.3	261.4	3,364.6	2,600.0
1160+00.00 to 1173+00.00	1,105.4	3,995.1	524.4	290.0	2,227.4	1,155.6	231.1	582.4	0.0	902.3	388.3	261.4	3,388.9	2,600.0
1173+00.00 to 1186+00.00	1,105.2	3,994.6	524.4	290.0	2,227.0	1,155.6	231.1	582.4	169.5	902.3	388.3	261.4	3,391.7	2,600.0
1186+00.00 to 1199+00.00	1,109.0	4,005.8	524.4	291.1	2,233.8	1,155.6	231.1	587.0	177.1	904.4	391.4	260.7	3,405.8	2,600.0
1199+00.00 to 1212+00.00	1,534.5	5,282.4	524.4	354.9	2,999.7	1,155.6	231.1	801.5	467.0	1,154.6	534.4	260.7	3,406.8	2,620.0
1212+00.00 to 1225+00.00	1,101.7	3,985.2	524.4	290.0	2,221.1	1,155.6	231.1	582.4	85.0	900.3	388.3	261.4	3,399.7	2,600.0
1225+00.00 to 1238+00.00	1,668.7	5,685.1	524.4	375.3	3,241.3	1,155.6	231.1	944.0	0.0	1,157.2	629.4	211.4	3,399.0	2,637.0
1238+00.00 to 1251+00.00	1,144.5	4,112.4	524.4	295.6	2,297.7	1,155.6	231.1	622.2	0.0	904.7	414.8	247.4	3,383.4	2,600.0
1251+00.00 to 1264+00.00	1,100.9	3,981.5	524.4	290.0	2,219.2	1,155.6	231.1	582.4	0.0	899.6	388.3	261.4	3,402.5	2,600.0
1264+00.00 to 1277+00.00	1,098.3	3,973.7	524.4	290.0	2,214.5	1,155.6	231.1	582.4	0.0	898.1	388.3	261.4	3,410.7	2,600.0
1277+00.00 to 1290+00.00	1,486.2	5,137.6	524.4	348.3	2,912.9	1,155.6	231.1	779.3	0.0	1,126.2	519.5	260.7	3,418.3	2,642.0
1290+00.00 to 1303+00.00	1,208.7	4,305.1	524.4	306.4	2,413.3	1,155.6	231.1	638.5	0.0	963.1	425.6	260.7	3,412.2	2,600.0
1303+00.00 to 1316+00.00	1,099.7	3,978.1	524.4	290.0	2,217.1	1,155.6	231.1	582.4	0.0	898.9	388.3	261.4	3,405.9	2,600.0
1316+00.00 to 1329+00.00	1,092.1	3,955.2	524.4	290.0	2,203.4	1,155.6	231.1	582.4	0.0	894.4	388.3	261.4	3,428.9	2,600.0
1329+00.00 to 1342+00.00	1,436.2	4,987.5	524.4	341.7	2,822.8	1,155.6	231.1	757.1	0.0	1,096.8	504.7	260.7	3,435.7	2,633.0
1342+00.00 to 1348+00.00	664.1	2,305.5	242.0	157.4	1,305.0	533.3	106.7	348.4	0.0	506.9	232.3	120.3	1,576.0	1,200.0
Totals =	26,624.1	94,453.4	11,261.3	6,699.6	53,026.8	24,817.0	4,963.4	14,235.8	1,069.9	20,944.2	9,490.6	5,516.6	73,587.7	56,133.3

* ASPHALT S3 WEDGE IS FOR LEVELING OF THE SUPER CORRECTION ON THE HORIZONTAL CURVES

SUMMARY OF SEDIMENT CONTROL						
Location Station to Station	Type A Salvedged Topsoil	Solid Slab Sodding	Vegetative Mulching	Fertilizing (10-20-10)	Fertilizing (18-46-0)	Mowing**
	205(A)	230(A)	233(A)	234(A)	234(A)	241
	C.Y.*	S.Y.	AC	TON	TON	AC
1060+00.00 to 1069+00.00	757.7	3328.2	1.1	0.3	0.1	1.4
1069+00.00 to 1082+00.00	1,700.9	8752.3	2.5	0.9	0.2	3.6
1082+00.00 to 1095+00.00	1,681.6	9736.8	2.5	1.0	0.2	4.0
1095+00.00 to 1108+00.00	1,437.1	7760.1	2.1	0.8	0.2	3.2
1108+00.00 to 1121+00.00	1,385.4	7481.3	2.1	0.7	0.2	3.0
1121+00.00 to 1134+00.00	1,356.4	7324.6	2.0	0.7	0.2	3.0
1134+00.00 to 1139+17.75	743.4	4014.4	1.1	0.4	0.1	1.6
PROJECTION EXCEPTION						
1146+48.78 to 1147+00.00	52.5	283.4	0.1	0.0	0.0	0.2
1147+00.00 to 1160+00.00	1,673.4	9036.5	2.5	0.9	0.2	3.8
1160+00.00 to 1173+00.00	1,636.3	8335.8	2.4	0.9	0.2	3.6
1173+00.00 to 1186+00.00	1,782.5	9525.8	2.7	1.0	0.2	4.0
1186+00.00 to 1199+00.00	1,271.3	6364.8	1.9	0.7	0.1	2.8
1199+00.00 to 1212+00.00	2,089.5	11283.3	3.1	1.1	0.2	4.6
1212+00.00 to 1225+00.00	1,496.5	8081.3	2.2	0.8	0.2	3.4
1225+00.00 to 1238+00.00	2,462.9	13299.9	3.7	1.3	0.3	5.4
1238+00.00 to 1251+00.00	1,543.1	8332.5	2.3	0.8	0.2	3.4
1251+00.00 to 1264+00.00	1,560.7	8427.6	2.3	0.8	0.2	3.4
1264+00.00 to 1277+00.00	1,447.9	7318.6	2.2	0.8	0.2	3.2
1277+00.00 to 1290+00.00	1,486.4	8026.3	2.2	0.8	0.2	3.4
1290+00.00 to 1303+00.00	1,206.5	6515.0	1.8	0.7	0.1	2.6
1303+00.00 to 1316+00.00	1,024.8	5533.8	1.5	0.6	0.1	2.2
1316+00.00 to 1329+00.00	1,121.9	6558.3	1.7	0.6	0.1	2.6
1329+00.00 to 1342+00.00	1,601.4	8547.5	2.4	0.9	0.2	3.6
1342+00.00 to 1348+00.00	779.1	4207.1	1.2	0.4	0.1	1.8
TOTAL	33,299.1	179,275.3	49.5	17.9	3.8	73.8

* Estimated quantity to be paid as lump sum.

** Two applications.

SUMMARY OF EROSION CONTROL				
Location Station to Station	Temporary Silt Fence	Temporary Silt Dike	Temp. Rock Filter Dam Type 1	Watering
	221(C)	221(F)	221(G)	230(F)
	L.F.	L.F.	C.Y.	KGal
1060+00.00 to 1069+00.00	1000.0	60.0		133.1
1069+00.00 to 1082+00.00		200.0		350.1
1082+00.00 to 1095+00.00		240.0		369.5
1095+00.00 to 1108+00.00		260.0		310.4
1108+00.00 to 1121+00.00	400.0	280.0		299.3
1121+00.00 to 1134+00.00	800.0	200.0		293.0
1134+00.00 to 1139+17.75	300.0	80.0	60.0	160.6
PROJECTION EXCEPTION				
1146+48.78 to 1147+00.00	1500.0	140.0		11.3
1147+00.00 to 1160+00.00		240.0		361.5
1160+00.00 to 1173+00.00		240.0		353.4
1173+00.00 to 1186+00.00		260.0		365.0
1186+00.00 to 1199+00.00		240.0		274.6
1199+00.00 to 1212+00.00		240.0		451.3
1212+00.00 to 1225+00.00		200.0		323.3
1225+00.00 to 1238+00.00	1300.0	180.0		532.0
1238+00.00 to 1251+00.00		260.0		333.3
1251+00.00 to 1264+00.00		180.0		337.1
1264+00.00 to 1277+00.00		240.0		312.7
1277+00.00 to 1290+00.00	200.0	220.0		321.1
1290+00.00 to 1303+00.00	200.0	200.0		260.6
1303+00.00 to 1316+00.00	1300.0	60.0		221.4
1316+00.00 to 1329+00.00	1100.0	140.0		242.3
1329+00.00 to 1342+00.00	600.0	180.0		345.9
1342+00.00 to 1348+00.00	100.0	120.0		168.3
TOTAL	8,800.0	4,660.0	60.0	7,171.1

DESIGN	RB
DRAWN	MM
CHECKED	DP
APPROVED	SL
SQUAD	BKL, Inc.

SUMMARY OF DRIVEWAYS

Location			Type	Notes	Length	Width	Radii		Surface Area		Tack Coat	Prime Coat	Superpave, Type S3 (PG 64-22 OK)	Depth	Superpave Type S4 (PG 64-22 OK)	Depth	Traffic Bound Surface Course Type E	2'-8" COMB. CURB & GUTTER (6" BARRIER)	
Station	LT.	RT.					FT.	FT.	LT.	RT.									Asph.
					S.Y.	S.Y.					GAL.	GAL.	TON						
1067+63.61		X	Field Ent.	Asphalt To End of Return then TBSC	84.0	14.0	15	15	34.1	107.4	3.4	49.5		9.5	5	3.8	2	14.5	3
1070+20.77		X	Field Ent.	Asphalt To End of Return then TBSC	62.0	20.0	15	15	44.1	104.8	4.4	52.1		12.3	5	4.9	2	14.1	3
1070+21.30	X		S/L Return	Standing Rock Road	90.0	26.0	25	25	282.7		28.3	98.9		79.2	5	31.7	2		
1074+54.08	X		Pvt. Dr.	Asphalt Drive	58.0	12.0	15	15	89.4		8.9	31.3		25.0	5	10.0	2		
1097+75.00	X		Field Ent.	Asphalt To End of Return then TBSC	55.0	15.0	15	15	35.7	67.2	3.6	36.0		10.0	5	4.0	2	9.1	3
1101+03.42		X	Field Ent.	Asphalt To End of Return then TBSC	55.0	12.0	15	15	30.7	53.0	3.1	29.3		8.6	5	3.4	2	7.2	3
1101+58.32	X		Field Ent.	Asphalt To End of Return then TBSC	56.0	12.0	15	15	30.9	54.2	3.1	29.8		8.7	5	3.5	2	7.3	3
1111+13.43	X		Pvt. Dr.	Asphalt Drive	56.0	12.0	15	15	85.3		8.5	29.9		23.9	5	9.6	2		
1115+00.00	X		Field Ent.	Asphalt To End of Return then TBSC	55.0	16.0	15	15	37.7	70.5	3.8	37.9		10.6	5	4.2	2	9.5	3
1116+27.82	X		Field Ent.	Asphalt To End of Return then TBSC	51.0	12.0	15	15	30.5	48.4	3.1	27.6		8.5	5	3.4	2	6.5	3
1124+00.00	X		S/L Return	Old Broom Road	146.0	26.0	25	25	435.4		43.5	152.4		121.9	5	48.8	2		
1124+00.00	X		Pvt. Dr.	Asphalt Drive	54.0	15.0	15	15	100.8		10.1	35.3		28.2	5	11.3	2		
1130+50.00	X		Field Ent.	Asphalt To End of Return then TBSC	50.0	16.0	15	15	37.4	62.2	3.7	34.9		10.5	5	4.2	2	8.4	3
1154+04.14	X		Pvt. Dr.	Asphalt Drive	50.0	12.0	15	15	91.3		9.1	32.0		25.6	5	10.2	2		
1154+54.12	X		Field Ent.	Asphalt To End of Return then TBSC	70.0	12.0	15	15	30.8	79.7	3.1	38.7		8.6	5	3.4	2	10.8	3
1166+00.00	X		Field Ent.	Asphalt To End of Return then TBSC	59.0	12.0	15	15	30.8	58.0	3.1	31.1		8.6	5	3.4	2	7.8	3
1170+87.57	X		Field Ent.	Asphalt To End of Return then TBSC	47.0	12.0	15	15	30.8	45.3	3.1	26.6		8.6	5	3.4	2	6.1	3
1170+90.86	X		Pvt. Dr.	Asphalt Drive	72.0	14.0	15	15	132.9		13.3	46.5		37.2	5	14.9	2		
1177+04.96	X		Field Ent.	Asphalt To End of Return then TBSC	70.0	14.0	15	15	34.1	85.6	3.4	41.9		9.5	5	3.8	2	11.6	3
1182+26.20	X		Pvt. Dr.	Asphalt Drive	45.5	20.0	15	15	112.0		11.2	39.2		31.4	5	12.5	2		
1182+79.19	X		Field Ent.	Asphalt To End of Return then TBSC	53.0	12.0	15	15	29.3	54.2	2.9	29.2		8.2	5	3.3	2	7.3	3
1204+00.00	X		S/L Return	WG Bar Road	1347.9	26.0	25	25	1322.2		132.2	462.8		370.2	5	148.1	2		
1207+66.52	X		Field Ent.	Asphalt To End of Return then TBSC	82.3	16.0	15	15	37.0	120.9	3.7	55.3		10.4	5	4.1	2	16.3	3
1219+98.11	X		Pvt. Dr.	Asphalt Drive	60.0	12.0	15	15	90.8		9.1	31.8		25.4	5	10.2	2		
1222+45.27	X		Field Ent.	Asphalt To End of Return then TBSC	58.0	12.0	15	15	30.8	57.0	3.1	30.7		8.6	5	3.4	2	7.7	3
1223+83.66	X		Field Ent.	Asphalt To End of Return then TBSC	51.0	16.0	15	15	37.4	63.7	3.7	35.4		10.5	5	4.2	2	8.6	3
1225+45.20	X		Field Ent.	Asphalt To End of Return then TBSC	65.0	14.0	15	15	34.1	77.8	3.4	39.2		9.5	5	3.8	2	10.5	3
1228+55.63	X		Pvt. Dr.	Asphalt Drive	50.0	12.0	15	15	77.4		7.7	27.1		21.7	5	8.7	2		
1231+33.49	X		Field Ent.	Asphalt To End of Return then TBSC	51.3	12.0	15	15	33.1	48.0	3.3	28.4		9.3	5	3.7	2	6.5	3
1233+97.26	X		S/L Return	Wards Chapel Road (North)	1412.7	26.0	25	25	1386.1		138.6	485.1		388.1	5	155.2	2		
20+50.00	X		Field Ent.	Asphalt Drive (Wards Chapel Road)	41.0	12.0	11	15	34.6	92.9	3.5	44.6		9.7	5	3.9	2		28.0
1233+97.26	X		S/L Return	Wards Chapel Road (South)	636.9	26.0	25	25	625.0		62.5	218.8		175.0	5	70.0	2		
1241+38.57	X		Pvt. Dr.	Asphalt Drive	54.3	14.0	15	15	95.2		9.5	33.3		26.7	5	10.7	2		
1244+59.08	X		Pvt. Dr.	Asphalt Drive	50.0	16.0	15	15	99.6		10.0	34.9		27.9	5	11.2	2		
1245+69.65	X		Field Ent.	Asphalt To End of Return then TBSC	55.0	12.0	15	15	30.8	53.3	3.1	29.4		8.6	5	3.4	2	7.2	3
1251+06.00	X		Pvt. Dr.	Asphalt Drive	60.0	12.0	15	15	90.7		9.1	31.7		25.4	5	10.2	2		
1261+86.54	X		Pvt. Dr.	Asphalt Drive	55.0	12.0	15	15	84.1		8.4	29.4		23.5	5	9.4	2		
1263+33.42	X		Field Ent.	Asphalt To End of Return then TBSC	50.0	12.0	15	15	30.8	46.7	3.1	27.1		8.6	5	3.4	2	6.3	3
1269+00.00	X		Field Ent.	Asphalt To End of Return then TBSC	60.0	12.0	15	15	30.8	60.0	3.1	31.8		8.6	5	3.4	2	8.1	3
1275+06.00	X		Field Ent.	Asphalt To End of Return then TBSC	50.0	12.0	15	15	30.7	46.7	3.1	27.1		8.6	5	3.4	2	6.3	3
1286+85.79	X		S/L Return	Hickory Hill Road (North)	356.8	26.0	25	25	350.3		35.0	122.6		98.1	5	39.2	2		
1286+85.79	X		S/L Return	Hickory Hill Road (South)	315.6	26.0	25	25	309.6		31.0	108.4		86.7	5	34.7	2		
10+81.50	X		Field Ent.	Asphalt Drive (Hickory Hill Road)	27.0	12.0	15	15	30.1	16.4	3.0	16.3		8.4	5	3.4	2	2.2	3
1290+94.80	X		Pvt. Dr.	Asphalt Drive	51.0	12.0	15	15	78.7		7.9	27.5		22.0	5	8.8	2		
1304+80.31	X		Comm. Dr.	Asphalt Drive	30.0	32.0	15	15	117.4		11.7	41.1		32.9	5	13.1	2		
1306+85.43	X		Comm. Dr.	Asphalt Drive	30.0	28.0	15	15	104.1		10.4	36.4		29.1	5	11.7	2		
1310+14.37	X		Comm. Dr.	Asphalt Drive	30.0	30.0	15	15	110.8		11.1	38.8		31.0	5	12.4	2		
1311+90.90	X		Field Ent.	Asphalt To End of Return then TBSC	50.0	12.0	15	15	30.8	46.4	3.1	27.0		8.6	5	3.4	2	6.3	3
1313+00.00	X		Comm. Dr.	Asphalt Drive	30.0	34.0	15	15	124.1		12.4	43.4		34.7	5	13.9	2		
1313+40.00	X		Field Ent.	Asphalt To End of Return then TBSC	50.0	12.0	15	15	30.8	46.7	3.1	27.1		8.6	5	3.4	2	6.3	3
1339+87.50	X		S/L Return	Carr Pen Road (North)	239.3	26.0	25	25	234.8		23.5	82.2		65.7	5	26.3	2		
1339+87.50	X		S/L Return	Carr Pen Road (South)	385.2	26.0	25	25	378.0		37.8	132.3		105.8	5	42.3	2		
13+50.00	X		Field Ent.	Asphalt Drive (Carr Pen Road)	29.0	12.0	15	15	30.7	18.5	3.1	17.2		8.6	5	3.4	2	2.5	3
TOTALS =											789.8	3354.3	2211.5	884.6	215.0		28.0		

PRINT DATE: 8/25/2018 C:\Projects\650-SHT Final Plans\DNVA\04-24066(04)-SUMMARY OF ROADWAY QUANTITIES SHEET 4.dwg

SUMMARY OF PAVED DITCHES							
Location Station to Station	Design No.	Bottom Width	Side Slope Ratio	Length	No. of Curtain Walls	Class C Conc. 509(D)	Remarks
		FT.	XX:1	L.F.	EA.	C.Y.	
E.B. DITCH							
Sta. 1067+00.00 to Sta. 1073+00.00, EB	2A	4	3:1	600.0	7	77.7	
Sta. 1077+00.00 to Sta. 1083+30.00, EB	2A	4	3:1	630.0	7	81.5	
Sta. 1083+55.00 to Sta. 1091+00.00, EB	2A	4	3:1	745.0	8	96.3	
Sta. 1095+15.00 to Sta. 1101+23.00, EB	2A	4	3:1	608.0	7	78.7	
Sta. 1108+00.00 to Sta. 1109+20.00, EB	2A	4	3:1	120.0	2	15.6	
Sta. 1115+00.00 to Sta. 1115+96.50, EB	2A	4	3:1	96.5	1	12.5	
Sta. 1133+00.00 to Sta. 1138+25.00, EB	2A	4	3:1	525.0	6	68.0	
Sta. 1138+70.00 to Sta. 1139+18.00, EB	2A	4	3:1	48.0	1	6.3	
Sta. 1154+04.14 to Sta. 1164+00.00, EB	2A	4	3:1	995.9	10	128.7	
Sta. 1164+00.00 to Sta. 1174+35.00, EB	2A	4	3:1	1035.0	11	133.8	
Sta. 1174+55.00 to Sta. 1180+00.00, EB	2A	4	3:1	545.0	6	70.5	
Sta. 1191+00.00 to Sta. 1192+00.00, EB	2A	4	3:1	100.0	2	13.1	
Sta. 1196+00.00 to Sta. 1198+00.00, EB	2A	4	3:1	200.0	3	26.0	
Sta. 1217+00.00 to Sta. 1225+00.00, EB	2A	4	3:1	800.0	9	103.5	
Sta. 1234+55.00 to Sta. 1237+00.00, EB	2A	4	3:1	245.0	3	31.8	
Sta. 1239+50.00 to Sta. 1240+00.00, EB	2A	4	3:1	50.0	1	6.5	
Sta. 1241+00.00 to Sta. 1259+00.00, EB	2A	4	3:1	1800.0	19	232.7	
Sta. 1265+00.00 to Sta. 1265+55.00, EB	2A	4	3:1	55.0	1	7.2	
Sta. 1265+80.00 to Sta. 1266+00.00, EB	2A	4	3:1	20.0	1	2.7	
Sta. 1268+00.00 to Sta. 1273+00.00, EB	2A	4	3:1	500.0	6	64.8	
Sta. 1277+00.00 to Sta. 1278+00.00, EB	2A	4	3:1	100.0	2	13.1	
Sta. 1286+00.00 to Sta. 1286+30.00, EB	2A	4	3:1	30.0	1	4.0	
Sta. 1332+00.00 to Sta. 1339+40.00, EB	2A	4	3:1	740.0	8	95.7	
Sta. 1343+00.00 to Sta. 1344+00.00, EB	2A	4	3:1	100.0	2	13.1	
Sta. 1347+00.00 to Sta. 1348+00.00, EB	2A	4	3:1	100.0	2	13.1	
W.B. DITCH							
Sta. 1067+00.00 to Sta. 1074+40.00, WB	2A	4	3:1	740.0	8	95.7	
Sta. 1074+70.00 to Sta. 1075+00.00, WB	2A	4	3:1	30.0	1	4.0	
Sta. 1077+00.00 to Sta. 1083+30.00, WB	2A	4	3:1	630.0	7	81.5	
Sta. 1083+55.00 to Sta. 1097+60.00, WB	2A	4	3:1	1405.0	15	181.7	
Sta. 1098+00.00 to Sta. 1100+00.00, WB	2A	4	3:1	200.0	3	26.0	
Sta. 1109+00.00 to Sta. 1111+00.00, WB	2A	4	3:1	200.0	3	26.0	
Sta. 1115+00.00 to Sta. 1115+96.50, WB	2A	4	3:1	96.5	1	12.5	
Sta. 1117+00.00 to Sta. 1128+00.00, WB	2A	4	3:1	1100.0	12	142.3	
Sta. 1138+70.00 to Sta. 1139+18.00, WB	2A	4	3:1	48.0	1	6.3	
Sta. 1153+30.00 to Sta. 1154+30.00, WB	2A	4	3:1	100.0	2	13.1	
Sta. 1154+65.00 to Sta. 1160+10.00, WB	2A	4	3:1	545.0	6	70.5	
Sta. 1166+00.00 to Sta. 1174+35.00, WB	2A	4	3:1	835.0	9	108.0	
Sta. 1174+55.00 to Sta. 1181+00.00, WB	2A	4	3:1	645.0	7	83.4	
Sta. 1192+00.00 to Sta. 1194+00.00, WB	2A	4	3:1	200.0	3	26.0	
Sta. 1206+60.00 to Sta. 1208+00.00, WB	2A	4	3:1	140.0	2	18.2	
Sta. 1219+00.00 to Sta. 1223+50.00, WB	2A	4	3:1	450.0	5	58.2	
Sta. 1227+75.00 to Sta. 1233+60.00, WB	2A	4	3:1	585.0	6	75.6	
Sta. 1234+55.00 to Sta. 1237+00.00, WB	2A	4	3:1	245.0	3	31.8	
Sta. 1238+00.00 to Sta. 1239+00.00, WB	2A	4	3:1	100.0	2	13.1	
Sta. 1242+00.00 to Sta. 1262+00.00, WB	2A	4	3:1	2000.0	21	258.6	
Sta. 1265+00.00 to Sta. 1265+55.00, WB	2A	4	3:1	55.0	1	7.2	
Sta. 1265+80.00 to Sta. 1273+09.00, WB	2A	4	3:1	729.0	8	94.3	
Sta. 1276+00.00 to Sta. 1279+00.00, WB	2A	4	3:1	300.0	4	38.9	
Sta. 1287+10.00 to Sta. 1288+05.00, WB	2A	4	3:1	95.0	1	12.3	
Sta. 1301+00.00 to Sta. 1301+22.00, WB	2A	4	3:1	22.0	1	3.0	
Sta. 1308+00.00 to Sta. 1309+00.00, WB	2A	4	3:1	100.0	2	13.1	
Sta. 1314+00.00 to Sta. 1315+00.00, WB	2A	4	3:1	100.0	2	13.1	
Sta. 1339+00.00 to Sta. 1339+40.00, WB	2A	4	3:1	40.0	1	5.3	
Sta. 1340+00.00 to Sta. 1341+00.00, WB	2A	4	3:1	100.0	2	13.1	
Sta. 1343+00.00 to Sta. 1346+00.00, WB	2A	4	3:1	300.0	4	38.9	
TOTAL						2,968.7	

SUMMARY OF SIGNS							
Facility	Approx. Station	Type	ODOT Std.	Sign Blank Std.	Sheet Alum. Signs	Remove & Reset Signs	Square Posts
					805(A)	805(D)	2" Sq.
					S.F.	EA.	851(C)
SH-7	1067+00.00 LT.	W1-4E(L) (LEFT REVERSE CURVE)	WSD1-1	B-30(D)	9.00		
SH-7	1070+50.00 LT.	STREET (STANDINGROCK RD)					1
SH-7	1124+40.00 LT.	STREET (OLD BROOM RD)					1
SH-7	1134+00.00 RT.	W8-13E(BRIDGE ICES BEFORE ROAD)	WSD3-1	B-36(D)	9.00		11.0
SH-7	1152+50.00 LT.	W8-13E(BRIDGE ICES BEFORE ROAD)	WSD3-1	B-36(D)	9.00		11.0
SH-7	1169+50.00 RT.	W1-2E(R)	WSD1-1	B-36(D)	9.00		11.0
		W13-1PE (55)	WSD3-1	B-24(S)	4.00		
SH-7	1192+00.00 RT.	W1-2E(R)	WSD1-1	B-36(D)	9.00		11.0
		W13-1PE (55)	WSD3-1	B-24(S)	4.00		
SH-7	1198+00.00 LT.	W1-2E(R)	WSD1-1	B-36(D)	9.00		11.0
		W13-1PE (55)	WSD3-1	B-24(S)	4.00		
SH-7	1203+80.00 RT.	STREET (WG BAR RD)					1
SH-7	1218+50.00 LT.	W1-2E(R)	WSD1-1	B-36(D)	9.00		11.0
		W13-1PE (55)	WSD3-1	B-24(S)	4.00		
SH-7	1234+30.00 RT.	STREET (WARDS CHAPEL RD)					1
SH-7	1240+00.00 LT.	W2-1E	WSD1-1	B-36(D)	9.00		11.0
		W13-1PE (45)	WSD3-1	B-24(S)	4.00		
SH-7	1287+10.00 LT.	STREET (HICKORY HILL RD)					1
SH-7	1339+65.00 RT.	STREET (CARR PEN RD)					1
COUNTY ROADS							
STANDING ROCK RD.	10+60.00 LT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
OLD BROOM RD.	9+60.00 RT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
OLD BROOM RD.	10+50.00 LT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
WG BAR RD.	15+20.00 RT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
WARDS CHAPEL RD.	15+45.00 RT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
WARDS CHAPEL RD.	16+32.00 LT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
HICKORY HILL RD.	11+40.00 RT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
HICKORY HILL RD.	12+20.00 LT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
CARR PEN RD.	14+05.00 RT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
CARR PEN RD.	14+90.00 LT.	R1-1E(STOP)	SBS1-1	B36(O)	7.46		11.0
TOTAL =					166.6	6	187.0

SUMMARY OF PAVED DITCHES							
Location Station to Station	Design No.	Bottom Width	Side Slope Ratio	Length	No. of Curtain Walls	Class C Conc. 509(D)	Remarks
		FT.	XX:1	L.F.	EA.	C.Y.	
COUNTY ROADS							
Sta. 10+54.00 to Sta. 11+24.23, NB	2A	4	3:1	70.2	1	9.1	STANDING ROCK ROAD
Sta. 10+50.00 to Sta. 11+24.23, SB	2A	4	3:1	74.2	1	9.6	STANDING ROCK ROAD
Sta. 11+00.00 to Sta. 11+71.23, NB	2A	4	3:1	71.2	1	9.2	OLD BROOM ROAD
Sta. 10+40.00 to Sta. 11+00.00, SB	2A	4	3:1	60.0	1	7.8	OLD BROOM ROAD
Sta. 11+50.00 to Sta. 11+71.23, SB	2A	4	3:1	21.2	1	2.9	OLD BROOM ROAD
Sta. 10+50.00 to Sta. 12+50.00, EB	2A	4	3:1	200.0	3	26.0	G BAR ROAD
Sta. 14+00.00 to Sta. 15+25.00, EB	2A	4	3:1	125.0	2	16.3	G BAR ROAD
Sta. 11+50.00 to Sta. 15+25.00, WB	2A	4	3:1	375.0	4	48.5	G BAR ROAD
Sta. 13+45.25 to Sta. 15+00.00, NB	2A	8	3:1	154.8	2	27.8	WARD CHAPEL ROAD
Sta. 15+00.00 to Sta. 15+50.00, NB	2A	4	3:1	50.0	1	6.5	WARD CHAPEL ROAD
Sta. 16+45.00 to Sta. 19+00.00, NB	2A	4	3:1	255.0	3	33.0	WARD CHAPEL ROAD
Sta. 16+40.00 to Sta. 19+00.00, SB	2A	4	3:1	260.0	3	33.7	WARD CHAPEL ROAD
Sta. 10+47.23 to Sta. 10+60.00, SB	2A	4	3:1	12.8	1	1.8	HICKORY HILL ROAD
Sta. 15+00.00 to Sta. 15+54.35, SB	2A	4	3:1	54.3	1	7.1	S CARR PEN ROAD
TOTAL						239.5	

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION SUMMARY SHEET (4)
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	
JOB PIECE NO. 24066 (04) SHEET NO. AU04		

SUMMARY OF TRAFFIC STRIPE QUANTITIES															
Facility	Station to Station	Distance Measured from Station to Station (May Not Be Actual Stripping Quantity)				PERMANENT							CONSTRUCTION		
		Passing Lanes				Traffic Stripe (Multi-Poly.) (4" White)	Traffic Stripe (Multi-Poly.) (4" Yellow)	Traffic Stripe (Multi-Poly.) (6" White)	Traffic Stripe (Multi-Poly.) (6" Yellow)	Traffic Stripe (Multi-Poly.) (12" Yellow)	Traffic Stripe (Multi-Poly.) (24" Wide)	Traffic Stripe (Multi-Poly.) (Arrows)	Const. Trf. Stripe (Paint) (White)	Const. Trf. Stripe (Paint) (Yellow)	Removal Trf. Stripe (Paint)
		6" White Edge Line	6" Dashed Yellow Lane Line	6" Solid Yellow Barrier Line	6" Double Yellow Barrier Line										
		L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.
SH-7 (Intersection)	1061+49.80 to 1076+49.92	3398.00					3398.00	4720.00	532.00						
SH-7 (No Passing)	1076+49.92 to 1115+92.05	3943.00				3943.00		7886.00	7886.00						
SH-7 (Intersection)	1115+92.05 to 1130+28.20	2998.00				2998.00		5116.00	530.00	3.00					
SH-7 (2-Lane Passing)	1130+28.20 to 1139+17.75	890.00	890.00					1780.00	238.00						
PROJECT EXCEPTION															
SH-7 (2-Lane Passing)	1146+48.78 to 1149+00.00	252.00	252.00					504.00	68.00						
SH-7 (No Passing)	1149+00.00 to 1157+00.00	800.00			800.00			1600.00	1600.00						
SH-7 (1-Lane Passing)	1157+00.00 to 1163+00.00	600.00	600.00	600.00				1200.00	760.00						
SH-7 (No Passing)	1163+00.00 to 1197+72.74	3473.00			3473.00			6946.00	6946.00						
SH-7 (Intersection)	1197+72.74 to 1212+07.26	2993.00			2993.00			5116.00	522.00	3.00					
SH-7 (No Passing)	1212+07.26 to 1225+88.98	1382.00			1382.00			2764.00	2764.00						
SH-7 (Intersection)	1225+88.98 to 1242+05.11	3888.00			3888.00			5364.00	510.00	12.00					
SH-7 (1-Lane Passing)	1242+05.11 to 1252+00.00	995.00	995.00	995.00				1990.00	1261.00						
SH-7 (2-Lane Passing)	1252+00.00 to 1263+00.00	1100.00			1100.00			2200.00	294.00						
SH-7 (1-Lane Passing)	1263+00.00 to 1279+78.15	1679.00	1679.00	1679.00				3358.00	2127.00						
SH-7 (Intersection)	1279+78.15 to 1294+87.31	3480.00			3480.00			5362.00	512.00	6.00					
SH-7 (2-Lane Passing)	1294+87.31 to 1318+00.00	2313.00	2313.00					4626.00	617.00						
SH-7 (1-Lane Passing)	1318+00.00 to 1331+79.33	1380.00	1380.00	1380.00				2760.00	1748.00						
SH-7 (Intersection)	1331+79.33 to 1348+00.00	3484.00			3484.00			5366.00	520.00	6.00					
COUNTY ROADS															
Standing Rock Rd.	10+33.00 to 11+24.43						183.00	183.00			20.00				
Old Broom Rd.	10+26.00 to 11+71.17						291.00	291.00			20.00				
WG Bar Rd.	11+74.97 to 15+40.13						731.00	731.00			20.00				
Wards Chapel Rd. (South)	13+45.25 to 15+58.85						428.00	428.00			20.00				
Wards Chapel Rd. (North)	16+18.85 to 20+93.19						949.00	949.00			20.00				
Hickory Hill Rd. (South)	10+47.21 to 11+51.53						209.00	209.00			20.00				
Hickory Hill Rd. (North)	12+03.53 to 13+21.99						237.00	237.00			20.00				
Carr Pen Rd. (South)	12+95.71 to 14+23.81						257.00	257.00			20.00				
Carr Pen Rd. (North)	14+75.81 to 15+54.26						157.00	157.00			20.00				
CONSTRUCTION STRIPING															
Phase I											100.0	5000.0			
Phase II											100.0	5000.0	5100.0		
Phase II													5100.0		
TOTAL =	n/a	n/a	n/a	n/a	n/a	3,442.00	3,442.00	57,855.00	57,353.00	3,126.00	180.00	39.00	200.00	10,000.00	10,200.00

SUMMARY OF MISC. TRAFFIC QUANTITIES												
Location Station to Station	(SP) Portable Traffic Signal System	Delineators Type 2 Code 1	Delineators Type 2 Code 3	Flex Tab Pvmt Mkr (White)	Flex Tab Pvmt Mkr (Yellow)	Pvmt Mkrs Class D Type 2	Barricades (Type III)	Wing Barricades	Vertical Panels	Type "A" Lights	Drums	C.Z Impact Attenuator
	823 SD	853 EA	853 EA	857(E) EA	857(E) EA	858(A) EA	880(C) S.D	880(C) S.D	880(D) S.D	880(E) S.D	880(F) S.D	871(B) S.D
PHASE I	185			200	400	1000	6780	240	18000	14520	34080	2
PHASE II	180			200	400	1000	67800	2400	18000	145200	340800	2
EACH HEADWALL			50									
TOTAL	365	0	50	400	800	2000	74580	2640	36000	159720	374880	730

Total Signr Days (S.D.) = 365

SUMMARY OF MAILBOXES								
Facility	Location			Mailbox Installation - Single	Mailbox Installation - Multiple	Mailbox	Removal of Mailbox Installation	Note
	Station	LT.	RT.					
SH-7	1111+45.00	X		1.0		1.0	1.0	
SH-7	1123+75.00		X	1.0		1.0	1.0	
SH-7	1154+50.00	X		1.0		1.0	1.0	
SH-7	1171+06.00		X	1.0		1.0	1.0	
SH-7	1186+20.00		X	1.0		1.0	1.0	
SH-7	1204+30.00	X			1.0	2.0	1.0	REMOVED FROM STA. 1210+60.00
SH-7	1228+25.00	X		1.0		1.0	1.0	
SH-7	1241+13.00		X	1.0		1.0	1.0	
SH-7	1261+64.00		X	1.0		1.0	1.0	
SH-7	1291+11.00		X	1.0		1.0	1.0	
SH-7	1309+85.00		X	1.0		1.0	1.0	
Totals =				10.0	1.0	12.0	11.0	n/a

SUMMARY OF PORTABLE MEDIAN BARRIER			
Location Station to Station	Deliver Portable Type Precast Median Barrier	Relocate Portable Type Precast Median Barrier	Notes
	877(B) L.F.	877(C) L.F.	
PHASE I	440		
PHASE II		440	
TOTAL	440	440	RETURN REMAINING BARRIER

SUMMARY OF DRAINAGE STRUCTURES (1 OF 2)

Str. No.	CRL Station	Facility	Description	Design	Top Of Cover/ Grate Elev.	Flow Line Elevation	Structural Installation		Class AA Conc	Reinf Steel	RCP Class III					PCES		CET						Std. Bedding Material, Class B	Trench Excevation	
							Fill Height	Uncl. Excav.			Struct. Excav.	16 Ga. CGSP					18"	24"	A4	B4	C4	A6	B6			C6
												202(A)	501(A)	509(A)	511(A)	18"										
FT.	C.Y.	C.Y.	C.Y.	LBS	L.F.	L.F.	L.F.	L.F.	L.F.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	C.Y.	C.Y.					
C1	1062+55.60	LT SH-7	EXTEND 4'X3' RCB 18 L.F. WITH HEADWALL	SBI-4, RCB-C1-3&4&5(2-20), RCB-E1-H3-0-1	550.61	33.03' LT. UP 544.28 15.64' LT. DN 544.23	2.15	1	6	16	2352															
C2	1062+55.60	RT SH-7	EXTEND 4'X3' RCB 18 L.F. WITH HEADWALL	SBI-4, RCB-C1-3&4&5(2-20), RCB-E1-H3-0-1	550.61	15.61' RT. UP 543.87 32.96' RT. DN 543.67	2.36	3	6	16	2352															
SD1	1067+63.61	RT SH-7	CONST. 18" CGSP 65 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	555.05	1067+24.00 DN 549.18 1067+89.00 UP 551.11	2.87						65			1			1							
SD2	1070+20.77	RT SH-7	CONST. 18" CGSP 73 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	564.00	1067+74.00 DN 557.99 1070+47.00 UP 560.09	2.40						73			1			1							
SD3	1070+21.30	LT SH-7	CONST. 18" RCP 66 L.F. W/ CET'S	CET4S-3, CET6S-3, SPI-4, SPB-1, FHTCP-3	564.56	1069+90.00 DN 559.74 1070+56.00 UP 561.01	2.39					66				1			1							
C3	1083+43.10	LT SH-7	EXTEND 10'X7' RCB 19 L.F. WITH HEADWALL	SBI-4, RCB-C1-10(2-14), RCB-E1-H7-0-1	567.94	33.12' LT. UP 556.75 14.39' LT. DN 556.46	2.03	27	13	64	10858															
C4	1083+43.10	RT SH-7	EXTEND 10'X7' RCB 16 L.F. WITH HEADWALL	SBI-4, RCB-C1-10(2-14), RCB-E1-H7-0-1	567.94	16.80' RT. UP 555.99 32.90' RT. DN 555.80	2.31	32	11	59	10038															
C5	1095+12.40	LT SH-7	CONST. 18" RCP 19 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	589.16	38.04' LT. UP 589.57 19.46' LT. DN 589.35	3.10									1				5.21	4.46					
C6	1095+12.40	RT SH-7	CONST. 18" RCP 23 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	588.67	23.34' RT. UP 587.44 46.44' RT. DN 586.21	4.73									1				6.30	5.40					
C7	1098+03.84	RT SH-7	CONST. 18" RCP 37 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	601.83	22.47' RT. UP 594.01 59.57' RT. DN 591.73	6.16									1				10.14	8.69					
SD4	1101+03.42	RT SH-7	CONST. 18" CGSP 51 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	612.62	1100+72.68 DN 608.44 1101+22.36 UP 609.94	1.50						51			1			1							
SD5	1101+58.32	RT SH-7	CONST. 18" CGSP 44 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	613.29	1101+33.76 DN 610.14 1101+76.88 UP 610.71	1.30						44			1			1							
C8	1109+22.85	LT SH-7	CONST. 18" RCP 19 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	609.88	34.78' LT. UP 604.99 15.88' LT. DN 604.92	1.90									1				5.21	4.46					
C9	1109+22.85	RT SH-7	CONST. 18" RCP 23 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	609.47	15.26' RT. UP 604.31 38.42' RT. DN 603.25	2.40									1				6.30	2.70					
SD6	1111+13.43	LT SH-7	CONST. 18" CGSP 44 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	608.32	1110+96.86 UP 605.14 1111+39.56 DN 604.57	1.20						44			1			1							
SD6A	1115+00.00	RT SH-7	CONST. 18" CGSP 63 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	604.30	1114+66.96 UP 600.47 1115+29.24 DN 599.39	2.90						63			1			1							
C10	1116+00.00	SH-7	CONST. 2 - 24" RCP 74 L.F. W/ HEADWALL	PCES-4, SPI-4, SPB-1, FHTCP-3, BORE & JACK 2 - 36" CASINGS	603.61	37.44' LT. UP 598.97 36.75' RT. DN 598.04	2.40			2	126		148							57.13	131.56					
SD7	1124+00.00	LT SH-7	CONST. 30" CGSP 70 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	586.87	1123+69.00 UP 583.51 1124+38.23 DN 582.00	0.60											1		1						
SD8	1130+50.00	LT SH-7	CONST. 30" CGSP 58 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	575.00	1130+25.80 UP 571.66 1130+82.59 DN 570.93	0.90											1		1						
SD9	1154+04.14	LT SH-7	CONST. 18" CGSP 43 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	598.87	1153+83.35 DN 594.28 1154+25.60 UP 596.41	1.20						43			1			1							
C11	1154+94.56	LT SH-7	CONST. 24" RCP 16 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	603.25	29.91' LT. UP 599.46 13.90' LT. DN 599.11	0.40							15				1		6.18	3.56					
C12	1154+94.56	RT SH-7	CONST. 24" RCP 24 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	603.44	20.00' RT. UP 597.72 43.29' RT. DN 595.21	2.50							24				1		9.26	5.33					
C13	1160+57.54	RT SH-7	EXTEND 3'X3' RCB 13 L.F. WITH HEADWALL	SBI-4, RCB-C1-3&4&5(2-20), RCB-E1-H3-0-1	614.00	35.92' RT. DN 615.01 48.94' RT. UP 615.36	5.40	2	4	12	1989															
C14	1160+57.54	LT SH-7	EXTEND 3'X3' RCB 25 L.F. WITH HEADWALL	SBI-4, RCB-C1-3&4&5(2-20), RCB-E1-H3-0-1	615.00	60.55' LT. DN 612.43 36.26' LT. UP 613.08	7.60	5	7	16	2478															
SD10	1166+00.00	LT SH-7	CONST. 18" CGSP 52 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	635.42	1165+80.28 UP 632.54 1166+31.42 DN 631.13	1.40						52			1			1							
SD11	1170+87.57	LT SH-7	CONST. 18" CGSP 60 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	617.12	1170+67.07 UP 614.05 1171+26.21 DN 611.63	1.60						60			1			1							
SD12	1170+90.86	RT SH-7	CONST. 18" CGSP 55 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	618.29	1170+65.42 UP 615.18 1171+19.95 DN 612.58	1.50						55													
C15	1174+44.62	RT SH-7	EXTEND 5'X7' RCB 22 L.F. WITH HEADWALL	SBI-4, RCB-C1-3&4&5(2-20), RCB-E1-H7-0-1	607.38	23.40' RT. DN 592.54 44.26' RT. UP 592.66	6.40	8	9	50	7510															
C16	1174+44.62	LT SH-7	EXTEND 5'X7' RCB 25 L.F. WITH HEADWALL	SBI-4, RCB-C1-3&4&5(2-20), RCB-E1-H7-0-1	607.02	48.95' LT. DN 590.87 24.80' LT. UP 591.48	7.50	1	10	53	7843															
SD13	1177+04.96	LT SH-7	CONST. 24" CGSP 66 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	611.04	1176+73.69 DN 604.53 1177+38.57 UP 606.31	2.40						66					1			1					
SD14	1182+79.19	LT SH-7	CONST. 18" CGSP 55 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	624.52	1182+55.87 DN 620.38 1183+09.62 UP 620.75	2.00						55			1			1							
SD15	1182+26.20	RT SH-7	CONST. 18" CGSP 64 L.F. W/ CET'S	CET4S-3, CET6S-3, FPI-3, FHTMP-5	621.79	1181+88.76 DN 616.96 1182+52.18 UP 618.12	1.90						64			1			1							
			TOTAL				n/a	79.0	64.2	288.7	45545.7	187.0	188.0	669.0	66.0	128.0	5.0	2.0	12.0	1.0	2.0	12.0	1.0	2.0	105.7	166.2

PRINT DATE: 8/25/2018 C:\Pro\as150-SHT Final Plans\DRAWING\06-24\066(04)-DRAIN SUMMARY 1.dgn

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION SUMMARY OF DRAINAGE STRUCTURES (1) JOB PIECE NO. 24066 (04) SHEET NO. AU06
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

SUMMARY OF DRAINAGE STRUCTURES (2 OF 2)

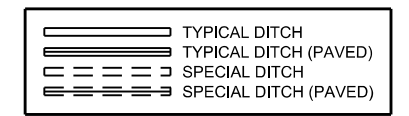
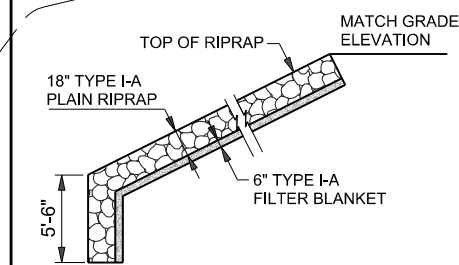
Str. No.	CRL Station	Facility	Description	Design	Top Of Cover/Grate Elev.	Flow Line Elevation		Structural Installation			Class AA Conc	Reinf. Steel	RCP Class III			16 Ga. CGSP Round			PCES			CET						Std. Bedding Material, Class B	Trench Excavation				
								Fill Height	Uncl. Excav. 202(A) C.Y.	Struct. Excav. 501(A) C.Y.			18"	24"	36"	18"	24"	36"	18"	24"	36"	A4	B4	AA4	BB4	A6	B6			AA6	BB6		
																						613(B)	613(B)	613(L)	613(M)	RS-GR	RS-GR			RS-GR	RS-GR	RS-GR	RS-GR
SD16	1204+00.00 RT	SH-7	CONST. 18" CGSP 55 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	638.86	1203+70.95 UP 1204+25.97 DN 635.29	635.61	0.90														1											
C17	1206+50.00	SH-7	CONST. 36" RCP 94 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3, BORE & JACK 42" RCP CASING	640.05	48.92' LT. DN 44.62' RT. UP 633.89	631.17	3.40																							70.59	128.88	
SD17	1207+66.52 RT	SH-7	CONST. 24" CGSP 80 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	641.36	1207+19.64 DN 1207+99.22 UP 635.74	634.75	3.60						80										1				1					
SD18	1219+98.11 LT	SH-7	CONST. 18" CGSP 61 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	658.82	1219+69.00 DN 1220+29.46 UP 654.56	652.90	2.70															1				1						
SD19	1222+45.27 LT	SH-7	CONST. 18" CGSP 53 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	667.72	1222+17.07 DN 1222+69.48 UP 664.99	662.37	1.50															1				1						
SD20	1228+55.63 LT	SH-7	CONST. 24" CGSP 56 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	679.82	1228+27.30 DN 1228+82.53 UP 676.30	674.09	1.40							56										1				1				
SD21	1231+33.49 LT	SH-7	CONST. 18" CGSP 55 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	691.12	1231+02.98 DN 1231+56.66 UP 688.56	685.19	1.35															1				1						
C18	1234+50.00	SH-7	CONST. 18" RCP 112 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3, BORE & JACK 24" RCP CASING	709.90	56.78' LT. UP 55.09' RT. DN 700.06	701.95	5.80															2								30.69	105.20	
C19	1239+45.67 RT	SH-7	CONST. 18" RCP 17 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	720.76	24.65' RT. DN 41.21' RT. UP 714.67	714.49	3.20															1								4.66	3.99	
C20	1239+45.67 LT	SH-7	CONST. 18" RCP 12 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	720.76	42.55' LT. DN 30.25' LT. UP 713.36	713.16	3.10															1								3.29	2.11	
SD22	DELETED																																
SD23	1244+59.08 LT	SH-7	CONST. 18" CGSP 63 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	710.42	1244+35.89 UP 1244+97.79 DN 704.91	706.67	2.30															1				1						
SD24	1245+69.65 RT	SH-7	CONST. 18" CGSP 58 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	707.40	1245+40.66 UP 1245+97.16 DN 702.22	703.87	2.25															1				1						
SD24A	1251+06.00 LT	SH-7	CONST. 18" CGSP 64 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	693.98	1250+19.00 UP 1251+45.00 DN 688.53	689.91	2.75															1				1						
SD25	1262+86.54 RT	SH-7	CONST. 18" CGSP 45 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	669.82	1261+62.90 UP 1262+06.19 DN 666.26	666.84	1.00															1				1						
SD26	1263+33.42 LT	SH-7	CONST. 18" CGSP 52 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	668.02	1263+11.41 UP 1263+62.33 DN 664.15	664.57	1.60															1				1						
C21	1265+67.72 RT	SH-7	EXTEND 2 - 6'X3' RCB 19 L.F. WITH HEADWALL	SBI-4, RCB-C2-6(2-12), RCB-E2-H3-0-1	666.84	14.80' RT. DN 32.98' RT. UP 560.95	560.85	0.80	40	16	35	5227																					
C22	1265+67.72 LT	SH-7	EXTEND 2 - 6'X3' RCB 17 L.F. WITH HEADWALL	SBI-4, RCB-C2-6(2-12), RCB-E2-H3-0-1	666.84	32.90' LT. DN 15.70' LT. UP 560.08	559.48	1.70	17	14	33	4890																					
SD27	DELETED																																
SD28	1269+00.00 RT	SH-7	CONST. 18" CGSP 58 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	672.12	1268+72.54 DN 1269+29.21 UP 668.56	666.95	2.30															1				1						
C23	1286+37.00	SH-7	CONSTRUCT 5'X2' RCB 78 L.F. WITH 2 HEADWALLS	SBI-4, RCB-C1-3&4&5(2-20), RCB-E2-H2-0-1	674.33	38.96' LT. DN 39.04' RT. UP 669.87	669.06	0.90	62	30	46	6718																					
SD29	10+81.50 LT.	HICKORY HILL ROAD	CONST. 18" RCP 38 L.F. W/ CETS	CET4S-3, CET6S-3, SPI-4, SPB-1, FHTCP-3	672.56	10+65.53 UP 11+02.05 DN 670.14	670.32	0.50						38									1				1						
C24	1288+14.60 RT	SH-7	CONSTRUCT 6'X2' RCB 27 L.F. WITH HEADWALL	SBI-4, SEE DETAIL SHEET B002.	674.13	11.74' RT. DN 39.01' RT. UP 669.46	669.33	0.70	12	12	18	2648																					
C25	1288+14.60 LT	SH-7	CONSTRUCT 6'X2' RCB 24 L.F. WITH HEADWALL	SBI-4, SEE DETAIL SHEET B002.	674.13	38.78' LT. DN 15.23' LT. UP 668.92	667.59	1.20	15	11	15	2296																					
SD30	1290+94.80 RT	SH-7	CONST. 18" CGSP 45 L.F. W/ CETS	CET4S-3, CET6S-3, FPI-3, FHTMP-5	674.92	1290+69.00 DN 1291+14.00 UP 672.05	671.63	0.70															1				1						
C26	1301+27.00	SH-7	CONSTRUCT 4'X2' RCB 66 L.F. WITH HEADWALL	SBI-4, RCB-C1-3&4&5(2-20), RCB-E1-H2-0-1	672.50	33.02' LT. UP 32.95' RT. DN 667.12	668.47	0.50	73	22	37	4931																					
SD31	1311+90.90 LT	SH-7	CONST. 2 - 18" CGSP 47 L.F. W/ CETS	CET4D-3, CET6D-3, FPI-3, FHTMP-5	674.83	1311+70.00 UP 1212+17.00 DN 672.68	673.01	0.50																	1				1				
SD32	1313+40.00 LT.	SH-7	CONST. 2 - 18" CGSP 49 L.F. W/ CETS	CET4D-3, CET6D-3, FPI-3, FHTMP-5	673.88	1313+18.74 UP 1313+66.69 DN 671.63	671.97	0.50																	1				1				
C27	1323+65.90 LT	SH-7	CONSTRUCT 4'X2' RCB 18 L.F. WITH HEADWALL	SBI-4, RCB-C1-3&4&5(2-20), RCB-E1-H2-0-1	668.92	32.01' LT. UP 13.70' LT. DN 664.65	664.78	0.50	5	6	13	1804																					
C28	1323+65.90 RT	SH-7	CONSTRUCT 4'X2' RCB 18 L.F. WITH HEADWALL	SBI-4, RCB-C1-3&4&5(2-20), RCB-E1-H2-0-1	668.92	14.54' RT. UP 32.65' RT. DN 662.39	664.14	1.50	7	6	13	1804																					
C29	1339+45.00	SH-7	CONST. 24" RCP 80 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3, BORE & JACK 36" CASING	669.09	36.76' LT. UP 43.19' RT. DN 636.51	638.39	1.00																							30.88	59.26	
SD33	13+50.00 LT.	CARR PEN ROAD	CONST. 2 - 24" RCP 50 L.F. W/ CETS	CET4D-3, CET6D-3, SPI-4, SPB-1, FHTCP-3	638.67	13+26.00 DN 13+76.00 UP 636.24	635.90	0.50																					1				
C30	1342+40.20 LT.	SH-7	CONST. 24" RCP 24 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	640.87	42.27' LT. UP 18.23' LT. DN 634.71	634.97	2.00															1							9.26	5.56		
C31	1342+40.20 RT.	SH-7	CONST. 24" RCP 25 L.F. W/ PCES	PCES-4, SPI-4, SPB-1, FHTCP-3	640.87	19.57' RT. UP 44.69' RT. DN 634.04	634.30	2.50															1							9.26	7.41		
				TOTAL					n/a	231.0	117.0	210.3	30317.0	179.0	229.0	94.0	801.0	136.0	4.0	4.0	2.0	12.0	2.0	2.0	1.0	12.0	2.0	2.0	1.0	158.6	312.4		

PRINT DATE: 8/25/2018 C:\projects\50-SHT Final Plans\50-SHT-24066(04)-DRAIN SUMMARY 2.dgn

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION SUMMARY OF DRAINAGE STRUCTURES (2) JOB PIECE NO. 24066 (04) SHEET NO. AU07
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

STANDARDS:

BRIDGE	ROADWAY
RCB-C3-10(2-12)-02E	PUD-3-2
RCB-CW3-D6-0-01E	SB1-4-2
RCB-E3-H7-0-1-01E	
RCB-E3-H7-0-2-01E	



DITCH LEGEND

DESIGN DATA LRFD

DESIGN SPECIFICATIONS: 2007 EDITION OF AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS WITH INTERIMS FROM 2008

LOADING: HL93 AND ODOT OVERLOAD TRUCK

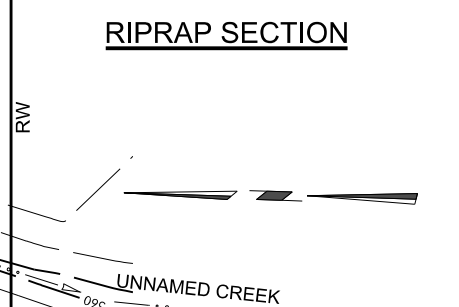
CLASS AA CONCRETE F'C = 4,000 psi
 REINFORCING STEEL (GR. 60) FY = 60,000 psi

HYDRAULIC SUMMARY

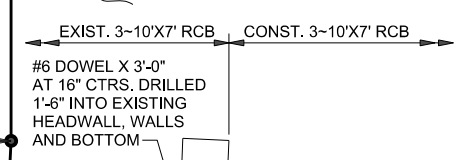
PROPOSED STRUCTURE: CRL STATION 1138+46.18
 3 - 10' X 7' RCB, 40'-0" CLEAR ROADWAY
 ROADWAY ELEVATION = 572.30

TOTAL DRAINAGE AREA = 0.145 SQ. MI.
 CONTROLLED DA = 0.00 SQ. MI.
 EFFECTIVE DA = 0.145 SQ. MI.

Q2 = 112 cfs	Q50 = 582 cfs
V2 = 2.10 fps	V50 = 7.29 fps
CHW2 = 562.11 ft.	CHW50 = 569.42 ft.
Q5 = 220 cfs	Q100 = 720 cfs
V5 = 3.03 fps	V100 = 18.27 fps
CHW5 = 565.50 ft.	CHW100 = 570.47 ft.
Q10 = 312 cfs	Q500 = 1,119 cfs
V10 = 4.11 fps	V500 = 21.10 fps
CHW10 = 566.40 ft.	CHW500 = 572.84 ft.
Q25 = 455 cfs	QOT = 203
V25 = 6.28 fps	
CHW25 = 568.11 ft.	

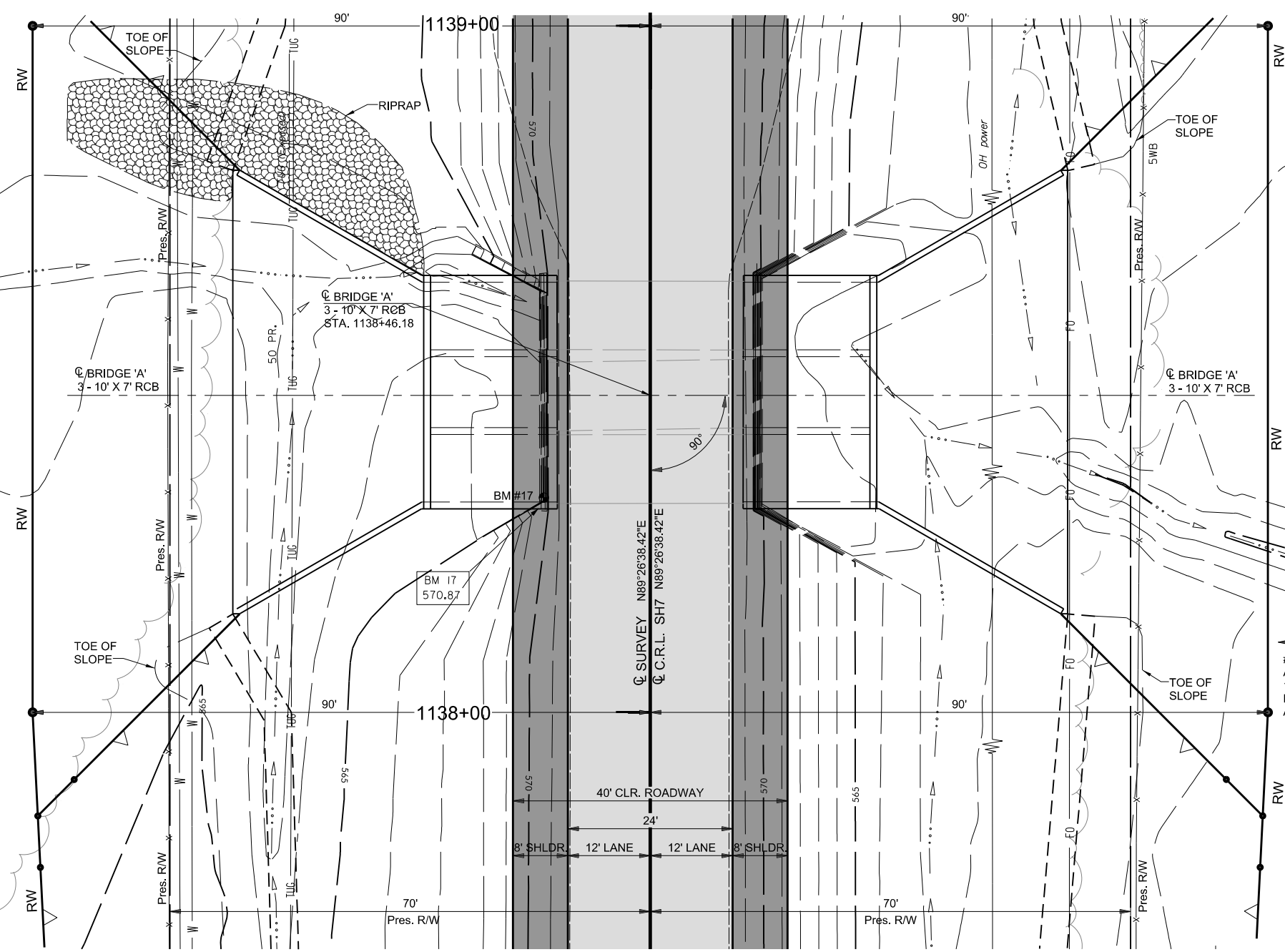


RIPRAP SECTION



DETAIL 'A'

NOTE: DOWELING INTO EXISTING RCB SHALL MEET THE ODOT SPECIFICATION SECTION 509.04 D.(3). ALL COST OF CONNECTING THE NEW RCB TO THE EXISTING RCB INCLUDING ALL CONSTRUCTION MATERIALS LABOR AND APPURTENANCES SHALL BE INCLUDED IN OTHER ITEMS OF WORK. 78 #6 DOWEL BARS X 3'-0" LONG WILL BE REQUIRED.



PLAN

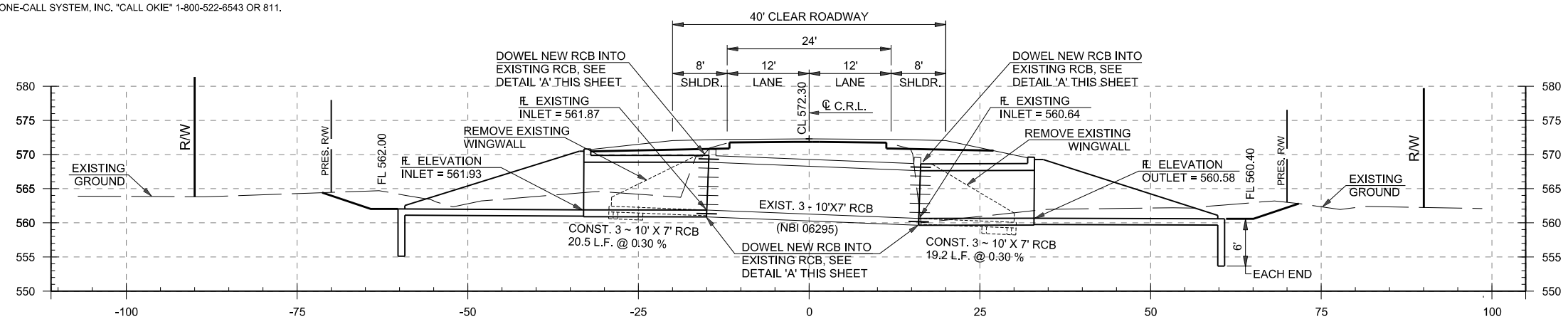
SCALE: 1" = 10'

SEC. 17, T-2-S, R-10-E

SEC. 17, T-2-S, R-10-E

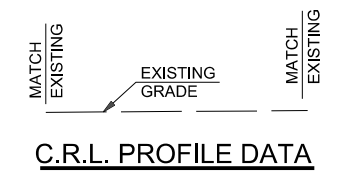
B.M. # 17
 BOX ON N.W. WINGWALL
 ELEV. = 570.87
 STA. 1138+29.14, 17.50' LT.

IN ACCORDANCE WITH THE OKLAHOMA UNDERGROUND FACILITIES DAMAGE PREVENTION ACT THE CONTRACTOR SHALL NOTIFY THE OKLAHOMA ONE-CALL SYSTEM, INC. 48 HOURS PRIOR TO BEGINNING EXCAVATION. OKLAHOMA ONE-CALL SYSTEM, INC. "CALL OKIE" 1-800-522-6543 OR 811.



PROFILE - STA. 1138+46.18

SCALE: 1" = 10'



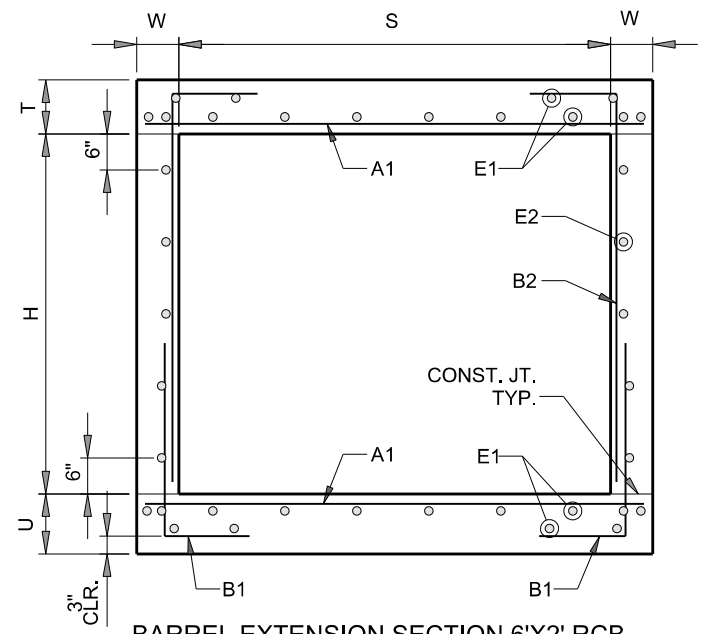
C.R.L. PROFILE DATA

SH7 OVER UNNAMED CREEK - BRIDGE "A"		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION	
DESIGN	SL	GENERAL PLAN AND ELEVATION C STATION 1138+46.18 CONSTRUCT 3 - 10' X 7' RCB OVER UNNAMED CREEK 40' CLR. ROADWAY JOB PIECE NO. 24066 (04) SHEET NO. B001	
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\50-SHT Final Plans\04-13PE BRIDGE A.dwg

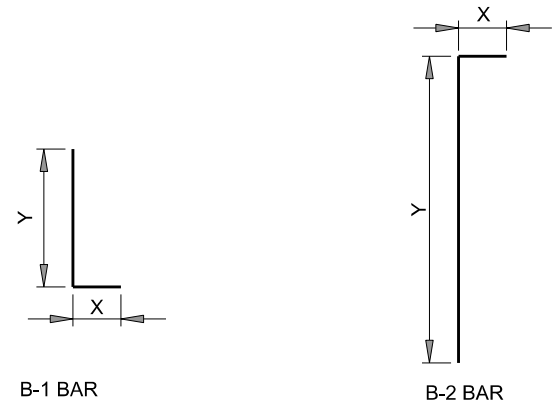
REINFORCING STEEL

SECTION DIMENSIONS					A1-BARS				B1-BARS				B2-BARS				E1-BARS AT 12" MAX.			E2-BARS			QUANTITIES PER FOOT OF BARREL					
S	H	T	U	W	SIZE	SPA.	LENGTH	WEIGHT PER FT.	SIZE	SPA.	"X"	"Y"	LENGTH	WEIGHT PER FT.	SIZE	SPA.	"X"	"Y"	LENGTH	WEIGHT PER FT.	NO.	SIZE	WEIGHT PER FT.	NO.	SIZE	WEIGHT PER FT.	CONC. (C.Y.)	REINF. (LB.)
6'	2'	9"	10"	7"	#6	6"	6'-10"	41.1	#4	12"	0'-8"	1'-11"	2'-7"	3.5	#4	12"	0'-8"	2'-7"	3'-3"	4.32	24.0	#4	16.0	4.0	#4	2.67	0.51	67.59



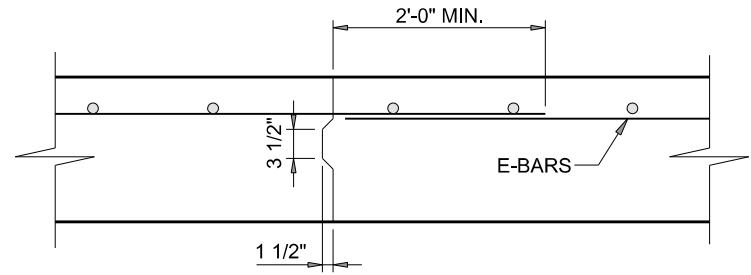
BARREL EXTENSION SECTION 6'X2' RCB

NOTE: NUMBER AND SPACING OF E-BARS SHOWN MAY NOT BE REPRESENTATIVE OF ACTUAL CULVERT SECTIONS. SEE SCHEDULE ABOVE FOR NUMBER AND SPACING OF E-BARS.



BAR BEND DIAGRAMS

NOTE: ALL "X" DIMENSIONS ARE HORIZONTAL. IN BARREL SECTION
ALL "Y" DIMENSIONS ARE VERTICAL. IN BARREL SECTION.



TRANS. CONST. JOINT

DESIGN DATA

- DESIGNED IN ACCORDANCE WITH 2007 AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS AND INTERIM SPECIFICATIONS FROM 2008.
- DESIGNED FOR HL-93 LOADING AND ODOT OVERLOAD TRUCK.
- MATERIALS:
CONCRETE (CLASS AA) $f_c = 4$ KSI
REINFORCING STEEL $f_y = 60$ KSI

GENERAL NOTES

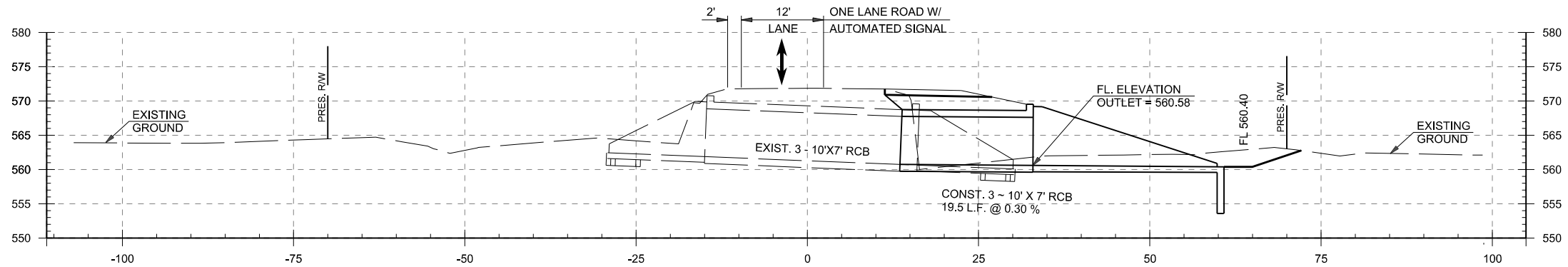
- ALL CONSTRUCTION AND MATERIAL REQUIREMENTS SHALL BE IN ACCORDANCE WITH 2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.
- ALL CONCRETE EDGED SHALL HAVE A 1 1/2" CHAMFER UNLESS OTHERWISE SHOWN OR NOTED. ALL CHAMFER STRIPS SHALL BE SIZED LUMBER.
- ALL REINFORCING STEEL SHALL HAVE A 2" MINIMUM CLEAR COVER UNLESS OTHERWISE SHOWN.
- THE QUANTITY FOR REINFORCING STEEL DOES NOT INCLUDE LAP SPLICES OF E1-BARS OR E2-BARS IN THE LENGTH OF THE BARREL OR AT TRANSVERSE CONSTRUCTION JOINTS. THE SPLICE LENGTH FOR E-BARS SHALL BE 24" MINIMUM. THE NUMBER OF SPLICES USED IS TO BE APPROVED BY THE ENGINEER. REINFORCING STEEL FOR SPLICES SHALL NOT BE MEASURED FOR PAYMENT, AND ALL COSTS WILL BE INCLUDED IN THE UNIT BID PRICE FOR REINFORCING STEEL.
- TRANSVERSE CONSTRUCTION JOINTS SHALL BE PLACED IN ALL CULVERTS 100 FT. OR MORE IN LENGTH. JOINTS SHALL BE SPACED AT 60 FT. MAX.
- REINFORCING STEEL SHALL BE CONTINUOUS THROUGH THE TRANSVERSE CONSTRUCTION JOINT AND EXTEND A MIN. OF 24" INTO ADJACENT SECTION.

BASIS OF PAYMENT		
ITEM NO.	ITEM	UNIT
509.06	CLASS AA CONCRETE	C.Y.
511.06	REINFORCING STEEL	LB.

RCB BARREL DETAILS
STR. NO. C24 & C25
STA. 1288+14.60 LT. & RT.

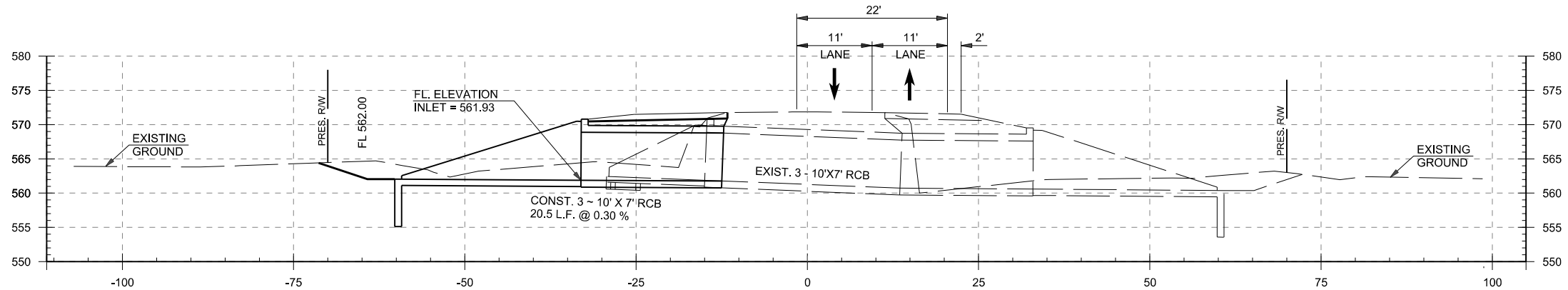
DESIGN	SL		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION RCB - CULVERTS - BARREL DETAILS 6'-0" SPAN - SINGLE CELL 2 FT. TO 14 FT. FILL JOB PIECE NO. 24066 (04) SHEET NO. B002
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/17/2018 C:\Projects\150-SHT Final Plans\04-RCB\CULVERT DETAILS.dgn



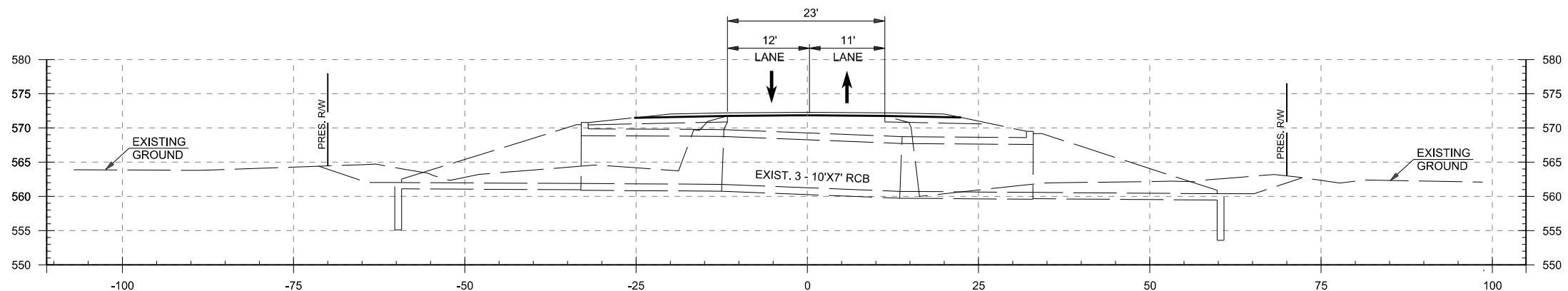
CONSTRUCTION SEQUENCE - PHASE 1

SCALE: 1" = 10'



CONSTRUCTION SEQUENCE - PHASE 2

SCALE: 1" = 10'



CONSTRUCTION SEQUENCE - PHASE 3

SCALE: 1" = 10'

PRINT DATE: 5/17/2018 C:\Projects\150-SHT Final Plans\04\BRIDGE A CONST SEQ.dwg

SH7 OVER UNNAMED CREEK - BRIDGE "A"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE PHASING BRIDGE 'A' JOB PIECE NO. 24066 (04) SHEET NO. B003
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

U.S. ARMY CORPS OF ENGINEERS SECTION 404 PERMIT CONDITIONS

404 PERMIT INFORMATION

NATIONWIDE PERMIT NO. _____

TO BE PROVIDED AT A LATER DATE

SECTION 404 OF THE CLEAN WATER ACT REQUIRES PRIOR AUTHORIZATION FROM SECRETARY OF THE ARMY (CORPS) FOR THE DISCHARGE OF DREDGED OR FILL MATERIAL INTO WATERS OF THE UNITED STATES.

NO PRE-CONSTRUCTION NOTIFICATION REQUIRED: PROJECT DOES NOT REQUIRE NOTIFICATION TO THE US ARMY CORPS OF ENGINEERS (USACE) IN ORDER TO COMMENCE.

PRE-CONSTRUCTION NOTIFICATION REQUIRED: RESIDENT ENGINEER MUST NOTIFY THE USACE WITHIN 30 DAYS OF THE START OF CONSTRUCTION AND 30 DAYS PRIOR TO COMPLETION OF CONSTRUCTION, FORMS LOCATED IN THE CONTRACT.

INDIVIDUAL PERMIT: WILL BE MONITORED CLOSELY BY THE USACE.

GENERAL PERMIT: PROJECT WITHIN A DESIGNATED CRITICAL RESOURCE WATER AND WILL REQUIRE PRE-CONSTRUCTION NOTIFICATION SEE ABOVE FOR EXPLANATION OF PRE-CONSTRUCTION NOTIFICATION.

NO PERMIT REQUIRED

SWT TRACKING NO. _____

SPECIAL CONDITIONS

NAVIGABLE WATER OF THE U.S.

ON-SITE MITIGATION

ENDANGERED SPECIES PRESENT

HISTORIC PROPERTIES PRESENT

DESIGNATED CRITICAL RESOURCE WATERS

PERMIT GENERAL CONDITIONS

THE CONTRACTOR SHALL BE RESPONSIBLE BUT NOT LIMITED TO THE FOLLOWING HIGHLIGHTS OF THE 404 PERMIT (SEE CONTRACT FOR COMPLETE LIST):

TEMPORARY FILLS:

APPROPRIATE MEASURES MUST BE TAKEN TO MAINTAIN NORMAL DOWNSTREAM FLOWS AND MINIMIZE FLOODING TO THE MAXIMUM EXTENT PRACTICABLE. WHEN TEMPORARY STRUCTURES (WORK ROADS, WORKPADS, ETC..) WORK, AND DISCHARGES, INCLUDING COFFERDAMS, ARE NECESSARY FOR CONSTRUCTION ACTIVITIES, ACCESS FILLS, OR DEWATERING OF CONSTRUCTION SITES, TEMPORARY FILLS MUST CONSIST OF MATERIALS, AND BE PLACED IN A MANNER, THAT WILL NOT BE ERODED BY EXPECTED HIGH FLOWS. TEMPORARY FILLS MUST BE REMOVED IN THEIR ENTIRETY AND THE AFFECTED AREAS RETURNED TO PRE-CONSTRUCTION ELEVATIONS. THE AREAS AFFECTED BY TEMPORARY FILLS MUST BE RE VEGETATED, AS APPROPRIATE.

NAVIGATION:

NO ACTIVITY MAY CAUSE MORE THAN A MINIMAL ADVERSE EFFECT ON NAVIGATION WITHIN A NAVIGABLE WATER OF THE U.S. IF THIS PROJECT IS LOCATED WITHIN A NAVIGABLE WATER OF THE U.S., IT WILL BE IDENTIFIED IN THE SPECIAL CONDITIONS.

AQUATIC LIFE MOVEMENTS & ADVERSE EFFECTS FROM IMPOUNDMENTS:

NO ACTIVITY MAY LARGELY DISRUPT THE NECESSARY LIFE CYCLE MOVEMENTS OF THOSE SPECIES INDIGENOUS TO THE BODY OF WATER, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA. CULVERTS WILL BE DESIGNED TO PROVIDE SUFFICIENT PASSAGE FOR AQUATIC LIFE AND INSTALLED TO MAINTAIN LOW FLOW. RATE OF FLOW CANNOT BE MADE HIGHER THAN WHAT WAS PRIOR TO THE START OF CONSTRUCTION. EROSION CONTROL MEASURES SHOULD BE UTILIZED AROUND THE PERIMETER OF NEW STRUCTURES TO AVOID SILT BUILD UP. CAUTION SHOULD BE TAKEN TO MINIMIZE HARM IF CONSTRUCTION ACTIVITIES TAKE PLACE WITHIN A STREAM OR RIVER CHANNEL AND CREATE A CONFINED BODY OF WATER, CAUSE ADVERSE EFFECTS TO THE AQUATIC SYSTEM IN ANY WAY, AND/OR RESTRICTING ITS FLOW.

MANAGEMENT OF WATER FLOWS:

CONSTRUCTION ACTIVITIES MAY NOT IMPEDE THE PASSAGE OF NORMAL OR HIGH FLOWS. TO THE GREATEST EXTENT POSSIBLE, THE PRE- CONSTRUCTION COURSE, CONDITIONS, CAPACITY AND LOCATION OF OPEN WATERS MUST BE MAINTAINED. THIS INCLUDES STREAM CHANNELIZATION AND STORM WATER MANAGEMENT.

SUITABLE MATERIAL:

NO ACTIVITY MAY USE UNSUITABLE MATERIAL (E.G., TRASH, DEBRIS, CAR BODIES, ASPHALT, ETC.). MATERIALS USED FOR CONSTRUCTION OR DISCHARGED MUST BE FREE FROM TOXIC POLLUTANTS IN TOXIC AMOUNTS (SEE SECTION 307 OF CLEAN WATER ACT).

PROPER MAINTENANCE

ANY AUTHORIZED STRUCTURE OR FILL SHALL BE PROPERLY MAINTAINED, INCLUDING MAINTENANCE TO ENSURE PUBLIC SAFETY AND COMPLIANCE WITH APPLICABLE NATION WIDE PERMIT GENERAL CONDITIONS, AS WELL AS ANY ACTIVITY- SPECIFIC CONDITIONS ADDED BY THE DISTRICT ENGINEER TO AN NATIONWIDE PERMIT AUTHORIZATION

HAZARDOUS MATERIALS:

HAZARDOUS MATERIALS, CHEMICALS, FUELS, LUBRICATING OILS AND OTHER SUCH SUBSTANCES SHOULD BE STORED AWAY FROM ANY STREAM OR RIVER CHANNEL (SEE SECTION 307 OF CLEAN WATER ACT)

EQUIPMENT:

HEAVY EQUIPMENT WORKING IN WETLANDS OR MUDFLATS MUST BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE; FOR EXAMPLE IF WETLANDS ARE PRESENT WITHIN THE CONSTRUCTION, THE FOOTPRINT WILL BE SHOWN ON THE PLANS. MEASURES SHOULD BE TAKEN TO PREVENT DISCHARGE INTO ANY WATERS OF THE STATE (e.g. CONCRETE WASHOUT).

SOIL EROSION AND SEDIMENT CONTROLS:

APPROPRIATE SOIL EROSION AND SEDIMENT CONTROLS MUST BE USED AND MAINTAINED IN EFFECTIVE OPERATING CONDITION DURING CONSTRUCTION, AND ALL EXPOSED SOILS AND OTHER FILLS, AS WELL AS ANY WORK WITHIN STREAM OR RIVER CHANNELS OR BANKS, MUST BE PERMANENTLY STABILIZED AS SOON AS POSSIBLE.

404 COMPLIANCE:

IN ORDER TO REMAIN COMPLIANT WITH THE 404 PERMIT, THE PROJECT MUST COMPLY WITH ALL FEDERAL ENVIRONMENTAL PROTECTION LAWS ASSOCIATED AND, THE ENVIRONMENTAL COMMITMENTS AS SHOWN ON THE PLANS. THIS INCLUDES BUT IS NOT LIMITED TO COMPLIANCE WITH ALL ENVIRONMENTAL NOTES IN THE PLANS, INCLUDING CULTURAL RESOURCES, HAZARDOUS WASTE, BIOLOGICAL FOR PROTECTED SPECIES, AND DEQ STORM WATER REGULATIONS AS THEY PERTAIN TO THE SWMP SHEET WITHIN THE PLANS. ALL OF THE 404 PERMIT GENERAL AND SPECIFIC CONDITIONS MUST BE ADHERED TO. A COPY OF THESE CONDITIONS CAN BE FOUND IN THE CONTRACT WITH THE 404 PERMIT.

SHEET NUMBERS: _____

PERMIT GENERAL CONDITIONS

FUELING:

ALL FUELING AND SERVICING OF VEHICLES AND EQUIPMENT SHALL BE DONE ABOVE THE ORDINARY HIGH WATER MARK (OHWM).

MATERIAL STORAGE:

STORE MATERIAL AND FUEL OUTSIDE OF THE ORDINARY HIGH WATER MARK OR ANY AREA LIKELY TO FLOOD.

DEBRIS STORAGE:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ANY MATERIALS, DEBRIS, OR REFUSE WHICH HAS FALLEN INTO ANY STREAM OR RIVER CHANNELS RESULTING FROM THE EXECUTION OF THE PROJECT AS SOON AS POSSIBLE

SEE NATIONWIDE PERMIT 14 IN THE CONTRACT

401 CERTIFICATION CONDITIONS

THE CONTRACTOR SHALL BE RESPONSIBLE BUT NOT LIMITED TO THE FOLLOWING HIGHLIGHTS OF THE 401 CERTIFICATION (SEE CONTRACT FOR COMPLETE LIST):

ALL SPILLS OF FUEL OR POLLUTANTS IN EXCESS OF FIVE GALLONS SHALL BE REPORTED TO ODEQ WITHIN 24 HRS AND REPORTED TO POLLUTION PREVENTION HOTLINE (1-800-522-0206)

ALL FUELING AND SERVICING OF VEHICLES AND EQUIPMENT SHALL BE DONE OUTSIDE THE ORDINARY HIGH WATER MARK

THE PERMITTEE SHALL PROVIDE ACCESS TO THE PROPERTY TO ODEQ FOR INSPECTIONS.

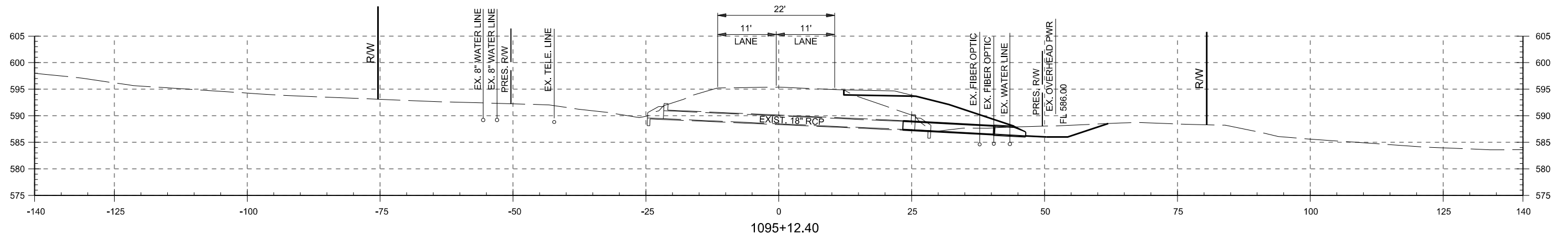
ANY STOCKPILE SHALL BE ABOVE ORDINARY HIGH WATER MARK AND REMOVED FROM LIKELY FLOOD ZONE

BEST MANAGEMENT PRACTICES SHOULD BE USED TO CONTROL SOIL EROSION AND MAINTAIN COMPLIANCE WITH WATER QUALITY STANDARDS.

FOR ANY PROJECT THAT INVOLVES BANK STABILIZATION, THE PERMITTEE SHALL CONSIDER INSTALLING BIOENGINEERING PRACTICES IN PLACE OF STRUCTURAL PRACTICES (RIPRAP) TO MINIMIZE IMPACTS TO AQUATIC RESOURCES

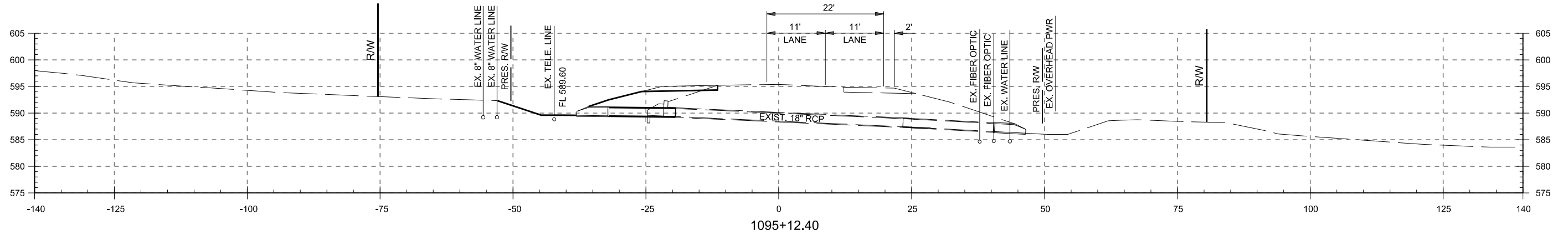
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DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL INC.		
COUNTY	ATOKA		HIGHWAY SH-7 STATE JOB NO. 24066(04) SHEET NO. E001

PRINT DATE: 5/16/2018 C:\Projects\560-SH7 Final Plans\IGN\E001-24066(04)-SECTION 404 PERMIT COMPLIANCE.dgn



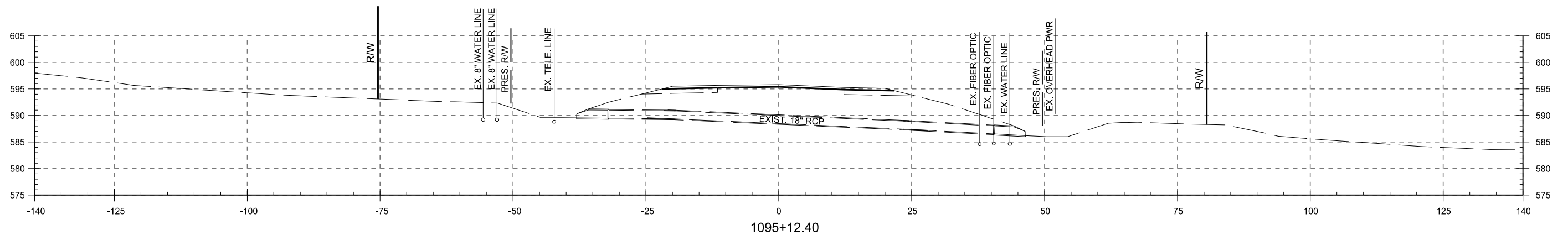
CONSTRUCTION SEQUENCE (ROADWAY) - PHASE 1

SCALE: 1" = 10'



CONSTRUCTION SEQUENCE (ROADWAY) - PHASE 2

SCALE: 1" = 10'



CONSTRUCTION SEQUENCE (ROADWAY) - PHASE 3

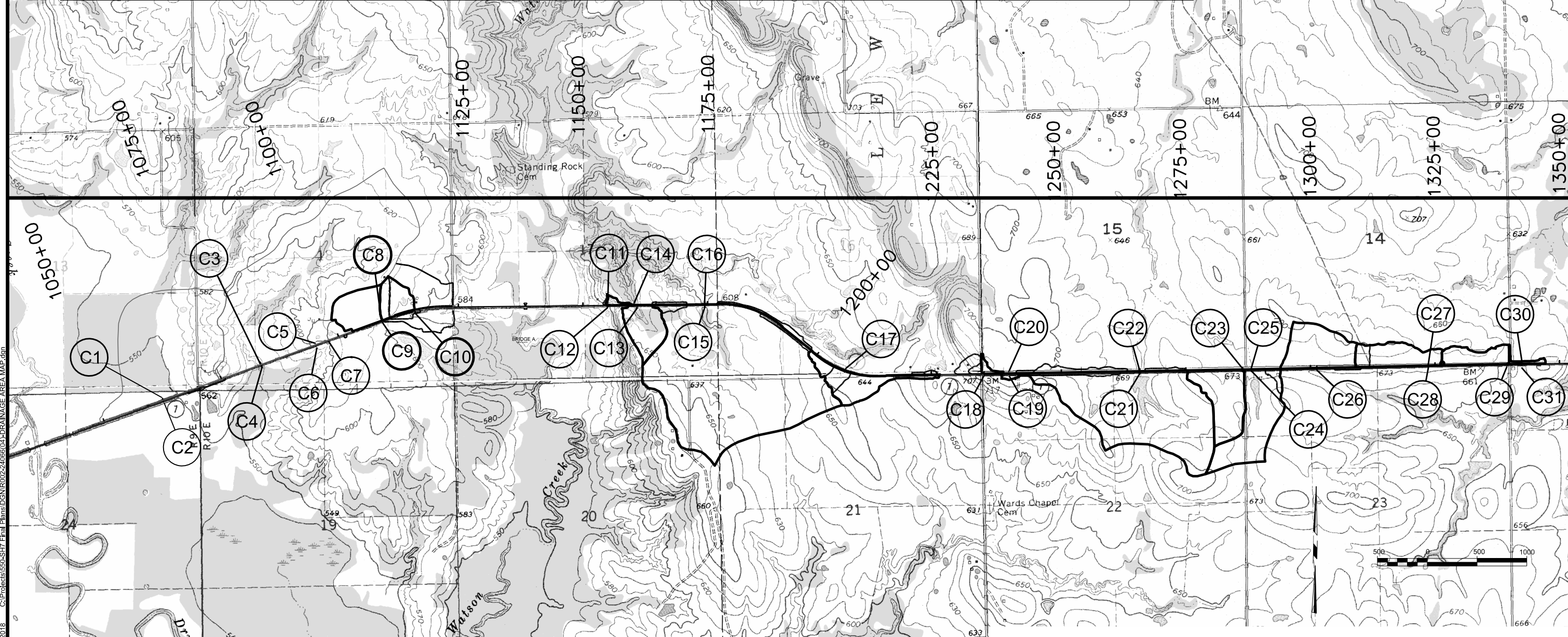
SCALE: 1" = 10'

PRINT DATE: 8/23/2018 C:\Projects\650-SHT Final Plans\OGN\R001-24066(04)\West Const Seq Phasing.dgn

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION ROADWAY CONSTRUCTION SEQUENCE PHASING JOB PIECE NO. 24066 (04) SHEET NO. R001
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

DRAINAGE STRUCTURE DESIGN RECORD

Str. No.	Design Yr.	CRL Station	Structure Size & Type	D.A.		Basin Elevation		Channel Elevation		Length of Drainage Area		Slope of Drainage Area		(C) Runoff Coefficient Weighted	(Tc) Time of Concentration	Intensity of Design Year Rainfall			Design Year Discharge			Plus Upstream Bypass	Bypass Flow		(Tw) Design Tailwater	Flow Line Inlet	Flow Line Outlet	Structure Length	Structure Slope	Maximum Allowable Headwater	Flow Velocity	Controlling Headwater	Type of Hydraulic Control												
				Ac.	Ft.	Ft.	Ft.	Ft.	Ft.	Overland	Channel	Overland	Channel			%	%	Min.	In./Yr.		cfs			From										To	cfs	Q50 Total	Ft.	Elev.	Elev.	Ft.	Ft./Ft.	Elev.	V50	Elev.	
																			10	50	100		Q10																						Q50
																																		q		Q		Ft.		Elev.					
C1 & C2	50	1062+55.70	4' x 3' RCB	101.40	603.00	580.00	580.00	544.32	370	3150	6.22	1.13	0.3640	50.63	2.99	3.79	4.24	110.4	167.9	195.6				1.51	544.32	543.67	66.00	0.0098	549.61	12.20	547.00	INLET													
C3 & C4	50	1083+43.10	10' x 7' RCB	182.46	625.00	610.00	610.00	557.00	1080	5060	1.39	1.05	0.4240	75.70	2.32	2.94	3.29	179.5	272.9	318.2				4.10	556.75	555.80	66.00	0.0144	566.91	13.47	561.08	INLET													
C5 & C6	50	1095+12.40	18" RCP	3.79	614.50	613.00	613.00	589.57	115	685	1.30	3.42	0.4563	21.53	4.73	5.99	6.71	8.2	12.4	14.5				0.54	589.57	586.21	85.00	0.0395	594.82	12.43	592.01	INLET													
C7	50	1098+03.84	18" RCP	2.30	614.50	610.00	610.00	597.85	220	194	2.05	6.26	0.4609	19.04	5.00	6.33	7.09	5.3	3.1	9.4				0.35	597.59	591.73	105.00	0.0558	608.00	12.65	599.24	INLET													
C8 & C9	50	1109+22.85	18" RCP	15.38	618.00	612.00	612.00	605.00	495	590	1.21	1.19	0.4552	34.20	3.75	4.74	5.31	26.3	39.8	46.5				1.89	604.99	603.25	73.00	0.0238	608.00	9.81	608.44	INLET													
C10	50	1116+00.00	2 - 24" RCP	9.57	622.00	618.00	618.00	599.00	300	720	1.33	2.64	0.4599	28.09	4.16	5.26	5.89	18.3	27.8	32.4	51.0	C8	C10	23.2	1.32	598.97	598.04	74.00	0.0126	603.60	9.76	602.26	INLET												
C11 & C12	50	1154+94.56	24" RCP	1.10	626.70	620.00	620.00	599.50	110	440	6.09	4.66	0.4191	12.13	5.97	7.56	8.46	2.8	4.2	4.9				0.18	599.46	595.21	74.00	0.0574	602.54	10.86	600.39	INLET													
C13 & C14	50	1160+57.54	3' x 3' RCB	15.44	660.10	644.00	644.00	616.00	710	980	2.27	2.86	0.4001	30.73	3.97	5.02	5.62	24.5	37.2	43.4				0.59	615.36	612.43	110.00	0.0266	625.00	12.98	617.93	INLET													
C15 & C16	50	1174+44.62	5' x 7' RCB	184.92	662.00	633.00	633.00	592.50	1570	3240	1.85	1.25	0.4150	56.47	2.80	3.54	3.97	214.9	325.0	380.8				3.22	592.66	590.87	94.00	0.0190	607.46	17.63	600.72	INLET													
C17	50	1206+50.00	36" RCP	17.22	654.00	642.50	642.50	634.00	362	990	3.18	0.86	0.4559	26.67	4.11	5.21	5.83	32.3	49.1	57.2				0.70	633.89	631.17	94.00	0.0289	639.43	14.47	637.41	INLET													
C18	50	1234+50.00	18" RCP	1.28	717.65	716.43	716.43	702.00	100	270	1.22	5.34	0.4531	14.62	5.57	7.05	7.90	3.2	4.9	5.7				0.20	701.95	700.06	112.00	0.0169	708.90	7.59	703.17	INLET													
C19 & C20	50	1239+45.67	18" RCP	0.74	717.00	716.39	716.39	714.70	194	155	0.31	1.09	0.4531	23.73	4.52	5.72	6.41	2.6	4.0	4.6				0.38	714.67	713.16	84.00	0.0180	719.96	7.20	715.75	INLET													
C21 & C22	50	1265+67.72	2 - 6' x 3' RCB	102.45	704.00	694.00	694.00	561.00	510	2470	1.96	5.38	0.4540	33.07	3.82	4.83	5.41	177.7	269.6	314.5				3.27	660.95	659.48	66.00	0.0223	666.25	13.99	665.11	INLET													
C23	50	1286+37.00	5' x 2' RCB	24.54	695.00	690.00	690.00	669.90	730	1160	0.68	1.73	0.4600	38.39	3.51	4.45	4.98	39.6	60.3	70.3				1.38	669.87	669.06	78.00	0.0104	673.48	9.95	672.62	INLET													
C24 & C25	50	1288+14.60	6' x 2' RCB	35.26	703.00	690.00	690.00	669.48	600	3820	2.17	0.54	0.4500	59.09	2.72	3.44	3.86	43.6	66.2	77.4				1.33	669.46	667.59	78.00	0.0240	673.15	12.41	671.97	INLET													
C26	50	1301+27.00	4' x 2' RCB	24.30	702.00	693.00	693.00	668.50	500	900	1.80	2.72	0.4549	32.40	3.86	4.88	5.47	42.7	64.7	75.6				1.05	668.47	667.12	66.00	0.0205	671.55	12.38	672.28	INLET													
C27 & C28	50	1323+65.90	4' x 2' RCB	16.23	701.00	686.00	686.00	664.80	240	1610	6.25	1.32	0.4579	33.07	3.82	4.83	5.41	28.4	43.1	50.3				0.72	664.78	662.39	66.00	0.0362	668.08	13.36	667.23	INLET													
C29	50	1339+45.00	30" RCP	10.06	682.00	665.00	665.00	638.50	300	1350	5.67	1.96	0.4599	29.64	4.04	5.12	5.73	18.7	28.4	33.1				0.90	638.39	636.51	80.00	0.0235	642.11	12.03	641.13	INLET													
C30 & C31	50	1342+40.20	24" RCP	1.21	647.00	645.00	645.00	635.00	100	480	2.00	2.08	0.4674	18.55	5.06	6.40	7.17	2.9	4.3	5.1				0.44	634.97	634.04	87.00	0.0107	639.87	6.05	635.95	INLET													



PRINT DATE: 6/21/2018 C:\Projects\50-SHT Final Plans\02-24066\04-DRAINAGE AREA MAP.dwg

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION DRAINAGE STRUCTURE DESIGN RECORD JOB PIECE NO. 24066 (04) SHEET NO. R002
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

STORM WATER MANAGEMENT PLAN

SITE DESCRIPTION

EROSION AND SEDIMENT CONTROLS

PROJECT LIMITS: PROJECT BEGINS 6.0 MILES EAST OF THE JOHNSTON C/L, AND EXTENDS EAST 5.4 MILES.

PROJECT DESCRIPTION: THE PROJECT WILL ADD TWO 8' SHOULDERS AND RE-CONSTRUCT BRIDGE OVER UNNAMED CREEK.

SUGGESTED SEQUENCE OF EROSION CONTROL ACTIVITIES: PRIOR TO INITIATING SOIL DISTURBING ACTIVITIES, THE CONTRACTOR WILL INSTALL ALL PERIMETER TEMPORARY SEDIMENT CONTROLS SPECIFIED. STRIP, STOCKPILE AND STABILIZE STOCKPILE AND STABILIZE TOPSOIL. CLEAR AND GRUB ONLY IN NECESSARY AREAS, PRESERVING AS MUCH NATIVE VEGETATION AS POSSIBLE. INSTALL, MAINTAIN AND/OR MOVE TEMPORARY SEDIMENT WITH CONSTRUCTION OPERATIONS AS PRACTICAL. IF DIRECTED BY THE ENGINEER, PLANT TEMPORARY SEEDING. REPLACE SALVAGED TOPSOIL, AND DEVICES WHEN AN ACCEPTABLE VEGETATIVE COVER (AT LEAST 70%) HAS BEEN ATTAINED. AS SITE CONDITIONS WARRANT, THE CONTRACTOR MAY CHOOSE TO MODIFY THE TYPE OR ARRANGEMENT OF SPECIFIED PRACTICES TO IMPROVE THEIR EFFECTIVENESS AS APPROVED BY THE ENGINEER. THE CONTRACTOR WILL MAINTAIN A LOG OF THE DATES OF MAJOR SOIL DISTURBANCE ACTIVITIES, AND ALSO THE DATES OF INSTALLATION OF EROSION CONTROL MEASURES.

SOIL TYPE: BERNOW, DELA AND ENDSAW SERIES

TOTAL AREA OF THE CONSTRUCTION SITE: 105 ACRES

ESTIMATED AREA TO BE DISTURBED: 49.5 ACRES

OFFSITE AREA TO BE DISTURBED: (FOR CONTRACTOR USE)

TOTAL IMPERVIOUS AREA PRE-CONSTRUCTION: 17.96 ACRES

TOTAL IMPERVIOUS AREA POST-CONSTRUCTION: 27.62 ACRES

POST-CONSTRUCTION RUNOFF COEFFICIENT OF THE SITE: 0.43

LATITUDE & LONGITUDE OF CENTER OF PROJECT: 34°22'29.52"N & 96°15'29.63"W

PROJECT WILL DISCHARGE TO:

NAME OF RECEIVING WATERS: TRIBUTARY TO WATSON CREEK, FRENCH HENRY CREEK

SENSITIVE WATERS OR WATERSHEDS: YES NO

303(d) IMPAIRED WATERS: YES NO

IF YES, LIST IMPAIRMENT:

LOCATED IN A TMDL: YES NO

LAKE THUNDERBIRD TMDL: NO

MS4 ENTITY YES NO

IF YES, LOCATION:

NOTE: THIS SHEET SHOULD BE USED IN CONJUNCTION WITH A DRAINAGE MAP THAT ILLUSTRATES THE DRAINAGE PATTERNS/PATHWAYS AND RECEIVING WATERS FOR THIS PROJECT. THIS SHEET SHOULD ALSO BE USED WITH THE EROSION CONTROL SUMMARIES, PAY ITEMS, & NOTES.

SOIL STABILIZATION PRACTICES:

- TEMPORARY SEEDING
- PERMANENT SODDING, SPRIGGING OR SEEDING
- VEGETATIVE MULCHING
- SOIL RETENTION BLANKET
- PRESERVATION OF EXISTING VEGETATION

NOTE: TEMPORARY EROSION CONTROL METHODS MUST BE USED ON ALL DISTURBED AREAS WHERE CONSTRUCTION ACTIVITIES HAVE CEASED FOR OVER 14 DAYS. METHODS USED WILL BE AS SHOWN ON PLANS, OR AS DIRECTED BY THE ENGINEER.

STRUCTURAL PRACTICES:

- STABILIZED CONSTRUCTION EXIT
- TEMPORARY SILT FENCE
- TEMPORARY SILT DIKES
- TEMPORARY FIBER LOG
- DIVERSION, INTERCEPTOR OR PERIMETER DIKES
- DIVERSION, INTERCEPTOR OR PERIMETER SWALES
- ROCK FILTER DAMS
- TEMPORARY SLOPE DRAIN
- PAVED DITCH W/ DITCH LINER PROTECTION
- TEMPORARY DIVERSION CHANNELS
- TEMPORARY SEDIMENT BASINS
- TEMPORARY SEDIMENT TRAPS
- TEMPORARY SEDIMENT FILTERS
- TEMPORARY SEDIMENT REMOVAL
- RIP RAP
- INLET SEDIMENT FILTER
- TEMPORARY BRUSH SEDIMENT BARRIERS
- SANDBAG BERMS
- TEMPORARY STREAM CROSSINGS

OFFSITE VEHICLE TRACKING:

- HAUL ROADS DAMPENED FOR DUST CONTROL
- LOADED HAUL TRUCKS TO BE COVERED WITH TARPULIN
- EXCESS DIRT ON ROAD REMOVED DAILY

NOTES:

THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR THE FOLLOWING:

MAINTENANCE AND INSPECTION:

ALL EROSION AND SEDIMENT CONTROLS WILL BE MAINTAINED IN GOOD WORKING ORDER FROM THE BEGINNING OF CONSTRUCTION UNTIL AN ACCEPTABLE VEGETATIVE COVER IS ESTABLISHED. INSPECTION BY THE CONTRACTOR AND ANY NECESSARY REPAIRS SHALL BE PERFORMED ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS AFTER ANY STORM EVENT GREATER THAN 0.5 INCH AS RECORDED BY A NON-FREEZING RAIN GAUGE TO BE LOCATED ON SITE. POTENTIALLY ERODIBLE AREAS, DRAINAGEWAYS, MATERIAL STORAGE, STRUCTURAL DEVICES, CONSTRUCTION ENTRANCES AND EXITS ALONG WITH EROSION AND SEDIMENT CONTROL LOCATIONS ARE EXAMPLES OF SITES THAT NEED TO BE INSPECTED.

WASTE MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF CONSTRUCTION WASTE MATERIAL IS REQUIRED BY THE CONTRACTOR. MATERIALS INCLUDE STOCKPILES, SURPLUS, DEBRIS AND ALL OTHER BY-PRODUCTS FROM THE CONSTRUCTION PROCESS. PRACTICES INCLUDE DISPOSAL, PROPER MATERIALS HANDLING, SPILL PREVENTION AND CLEANUP MEASURES. CONTROLS AND PRACTICES SHALL MEET THE REQUIREMENTS OF ALL FEDERAL, STATE AND LOCAL AGENCIES.

HAZARDOUS MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF HAZARDOUS WASTE MATERIALS IS REQUIRED. THE CONTRACTOR IS RESPONSIBLE FOR FOLLOWING MANUFACTURER'S RECOMMENDATIONS, STATE AND FEDERAL REGULATIONS TO ENSURE CORRECT HANDLING, DISPOSAL, SPILL PREVENTION AND CLEANUP MEASURES. EXAMPLES INCLUDE BUT ARE NOT LIMITED TO: PAINTS, ACIDS, CLEANING SOLVENTS, CHEMICAL ADDITIVES, CONCRETE CURING COMPOUNDS AND CONTAMINATED SOILS.

GENERAL NOTES:

A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED TO COMPLY WITH THE OKLAHOMA POLLUTION DISCHARGE ELIMINATION SYSTEM (OPDES) REGULATIONS. THIS PLAN IS INITIATED DURING THE DESIGN PHASE, CONFIRMED IN THE PRE-WORK MEETINGS AND AVAILABLE ON THE JOB SITE ALONG WITH COPIES OF THE NOTICE OF INTENT (NOI) FORM AND PERMIT CERTIFICATE THAT HAVE BEEN FILED WITH THE OKLAHOMA DEPARTMENT OF ENVIRONMENTAL QUALITY (ODEQ). THE PLAN MUST BE KEPT CURRENT WITH UP-TO-DATE AMENDMENTS DURING THE PROGRESSION OF THE PROJECT. ALL CONTRACTOR OFF-SITE OPERATIONS ASSOCIATED WITH THE PROJECT MUST BE DOCUMENTED IN THE SWPPP, I.E., BORROW PITS, WORK ROADS, DISPOSAL SITES, ASPHALT/CONCRETE PLANTS, ETC. THE BASIC GOAL OF STORM WATER MANAGEMENT IS TO IMPROVE WATER QUALITY BY REDUCING POLLUTANTS IN STORM WATER DISCHARGES. RUNOFF FROM CONSTRUCTION SITES HAS A POTENTIAL FOR POLLUTION DUE TO EXPOSED SOILS AND THE PRESENCE OF HAZARDOUS MATERIALS USED IN THE CONSTRUCTION PROCESS. THE PREVENTION OF SOIL EROSION, CONTAINMENT OF HAZARDOUS MATERIALS AND/OR THE INTERCEPTION OF THESE POLLUTANTS BEFORE LEAVING THE CONSTRUCTION SITE ARE THE BEST PRACTICES FOR CONTROLLING STORM WATER POLLUTION.

THE FOLLOWING SECTIONS OF THE 2009 ODOT STANDARD SPECIFICATIONS SHOULD BE NOTED:

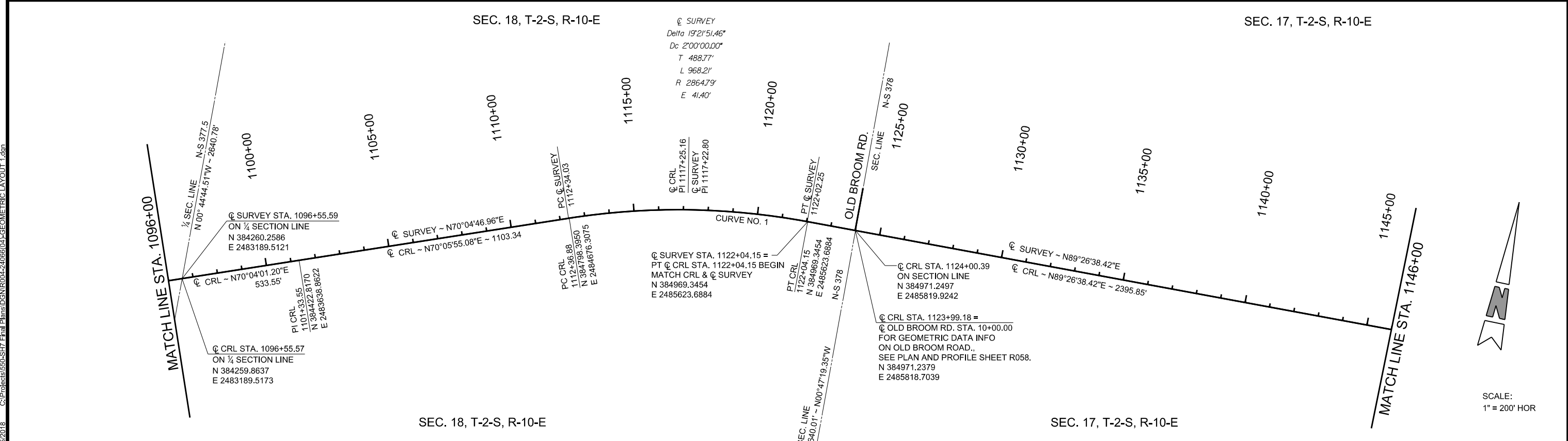
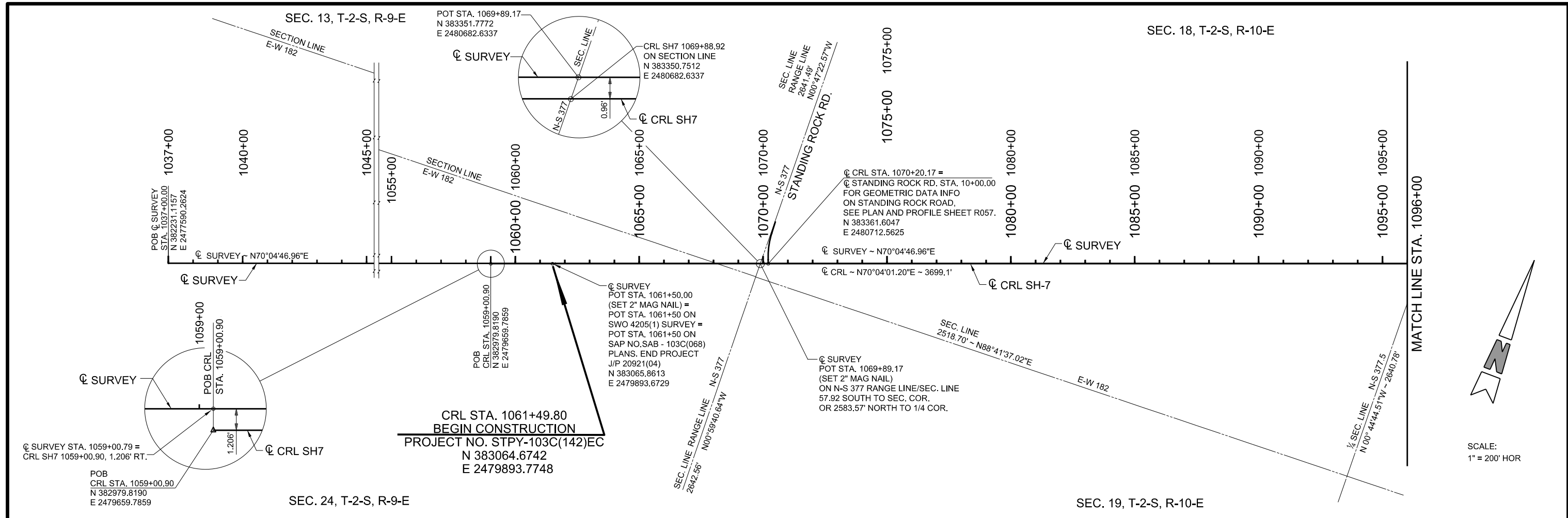
- 103.05 BONDING REQUIREMENTS
- 104.10 FINAL CLEANING UP
- 104.12 CONTRACTOR'S RESPONSIBILITY FOR WORK
- 104.13 ENVIRONMENTAL PROTECTION
- 106.08 STORAGE AND HANDLING OF MATERIAL
- 107.01 LAWS, RULES AND REGULATIONS TO BE OBSERVED
- 107.20 STORM WATER MANAGEMENT
 - 220 MANAGEMENT OF EROSION, SEDIMENTATION AND STORM WATER POLLUTION PREVENTION AND CONTROL
 - 221 TEMPORARY SEDIMENT CONTROL

IN ADDITION:

"ODEQ GENERAL PERMIT (OKR10) FOR STORM WATER DISCHARGES FROM CONSTRUCTION ACTIVITIES WITHIN THE STATE OF OKLAHOMA." ODEQ, WATER QUALITY DIVISION, SEPTEMBER 13, 2017.

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STORM WATER MANAGEMENT PLAN COUNTY: ATOKA HIGHWAY: SH-7 STATE JOB NO.: 24066(04) SHEET NO.: R003
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/25/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\150-SHT Final Plans\150-SHT GEOMETRIC LAYOUT 1.dgn

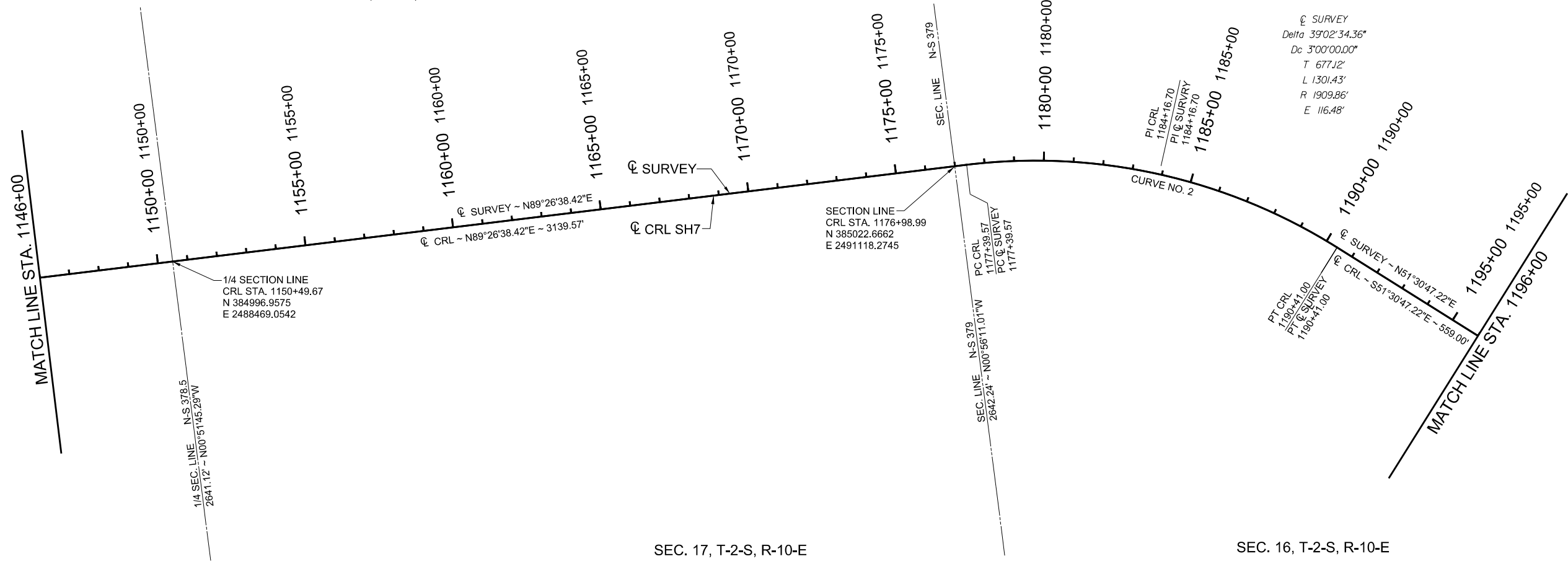


CURVE NO.	P.I. STATION	NORTH COOR.	EAST COOR.	CURVE DATA										V MPH	P.T. STATION	NORTH COOR.	EAST COOR.
				P.C. STATION	NORTH COOR.	EAST COOR.	Δ	D	T (FT.)	R (FT.)	E (FT.)	L (FT.)	S (%)				
CURVE 1	1117+25.16	384964.6072	2485135.4293	1112+36.88	384798.3950	2484676.3075	19°20'43" RT.	02°00'00"	488.28	2864.79	41.31	967.27	4.60%	55	1122+04.15	384969.3454	2485623.6884
CURVE 2	1184+16.70	385029.6307	2491835.9406	1177+39.57	385023.0600	2491158.8512	39°02'34" RT.	03°00'00"	677.12	1909.86	116.48	1301.43	6.00%	55	1190+41.00	384608.2341	2492365.9577
CURVE 3	1206+89.41	383582.3719	2493656.2494	1200+04.99	384008.3093	2493120.5211	39°25'53" LT.	03°00'00"	684.42	1909.86	118.93	1314.38	6.00%	55	1213+19.37	383593.6529	2494340.5740

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION GEOMETRIC DATA (SHEET 1 OF 3) JOB PIECE NO. 24066 (04) SHEET NO. R004
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

SEC. 17, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E



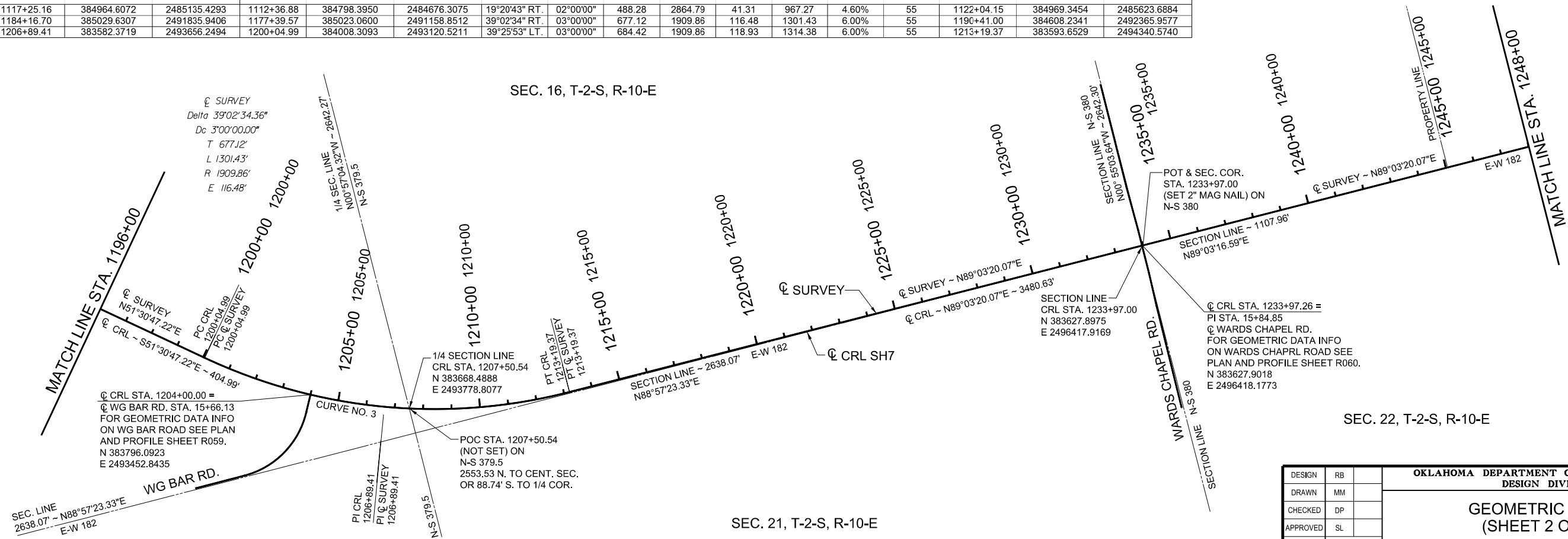
SEC. 17, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E

SCALE:
1" = 200' HOR

CURVE NO.	P.I. STATION	NORTH COOR.	EAST COOR.	P.C. STATION	NORTH COOR.	EAST COOR.	CURVE DATA		T (FT.)	R (FT.)	E (FT.)	L (FT.)	S (%)	V MPH	P.T. STATION	NORTH COOR.	EAST COOR.
							Δ	D									
CURVE 1	1117+25.16	384964.6072	2485135.4293	1112+36.88	384798.3950	2484676.3075	19°20'43" RT.	02°00'00"	488.28	2864.79	41.31	967.27	4.60%	55	1122+04.15	384969.3454	2485623.6884
CURVE 2	1184+16.70	385029.6307	2491835.9406	1177+39.57	385023.0600	2491158.8512	39°02'34" RT.	03°00'00"	677.12	1909.86	116.48	1301.43	6.00%	55	1190+41.00	384608.2341	2492365.9577
CURVE 3	1206+89.41	383582.3719	2493656.2494	1200+04.99	384008.3093	2493120.5211	39°25'53" LT.	03°00'00"	684.42	1909.86	118.93	1314.38	6.00%	55	1213+19.37	383593.6529	2494340.5740

SEC. 15, T-2-S, R-10-E



SEC. 21, T-2-S, R-10-E

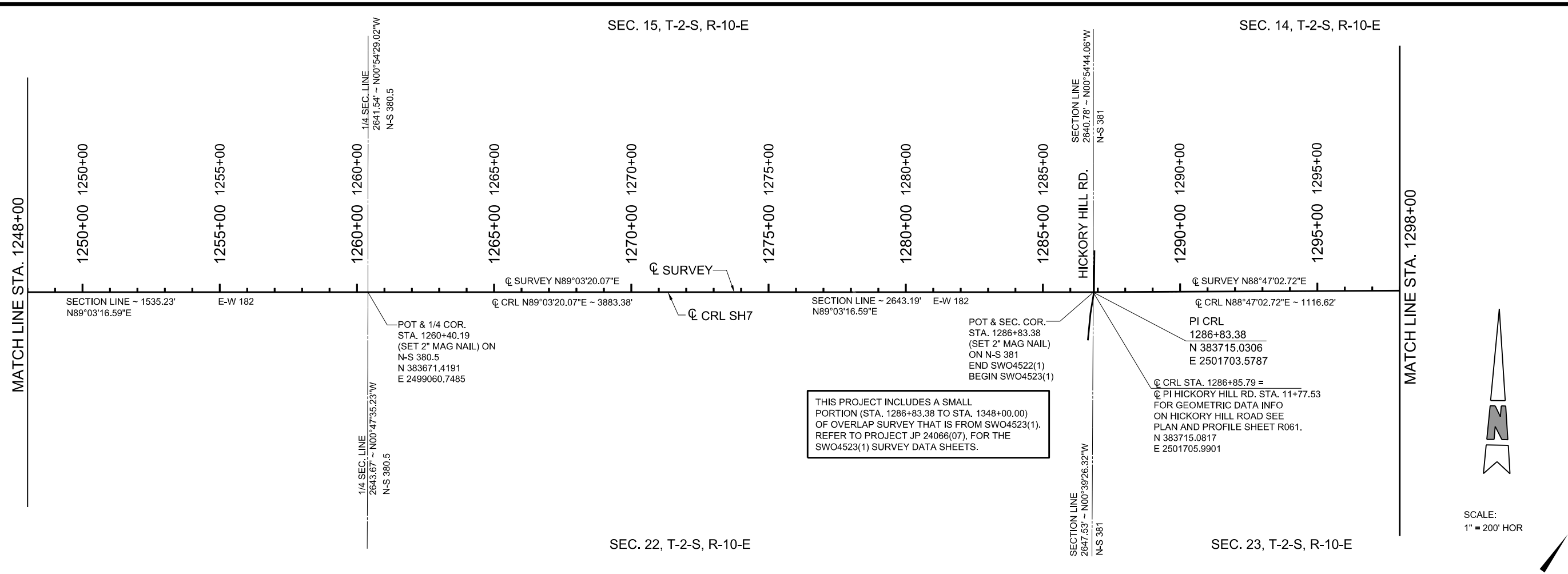
SEC. 22, T-2-S, R-10-E

SCALE:
1" = 200' HOR

PRINT DATE: 5/18/2018 C:\Projects\1550-SHT Final Plans\1550-SHT-24066(04)-GEOMETRIC LAYOUT 2.dgn

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CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

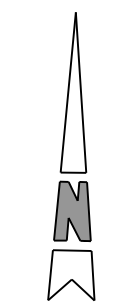
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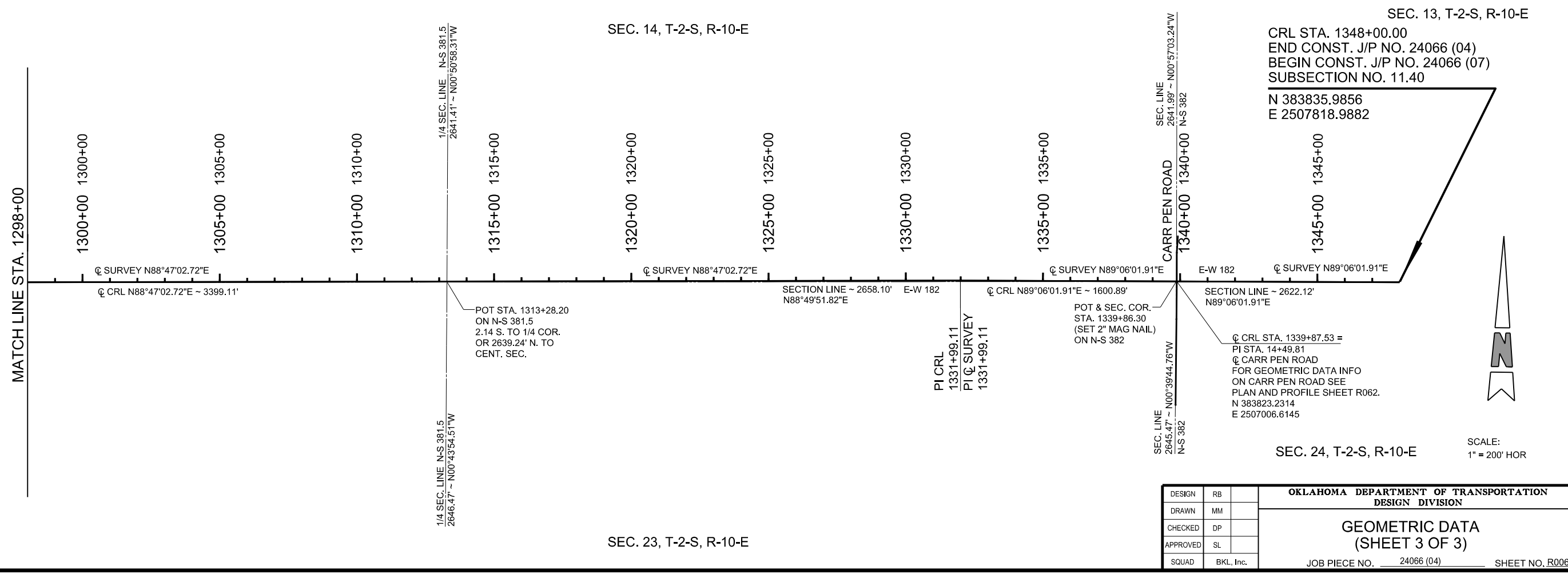
THIS PROJECT INCLUDES A SMALL PORTION (STA. 1286+83.38 TO STA. 1348+00.00) OF OVERLAP SURVEY THAT IS FROM SWO4523(1). REFER TO PROJECT JP 24066(07), FOR THE SWO4523(1) SURVEY DATA SHEETS.

PI CRL
1286+83.38
N 383715.0306
E 2501703.5787

PI CRL
1286+85.79 =
PI HICKORY HILL RD. STA. 11+77.53
FOR GEOMETRIC DATA INFO
ON HICKORY HILL ROAD SEE
PLAN AND PROFILE SHEET R061.
N 383715.0817
E 2501705.9901



SCALE:
1" = 200' HOR



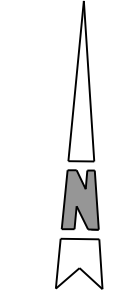
CRL STA. 1348+00.00
END CONST. J/P NO. 24066 (04)
BEGIN CONST. J/P NO. 24066 (07)
SUBSECTION NO. 11.40

N 383835.9856
E 2507818.9882

PI CRL
1331+99.11
PI CARR PEN ROAD
1331+99.11

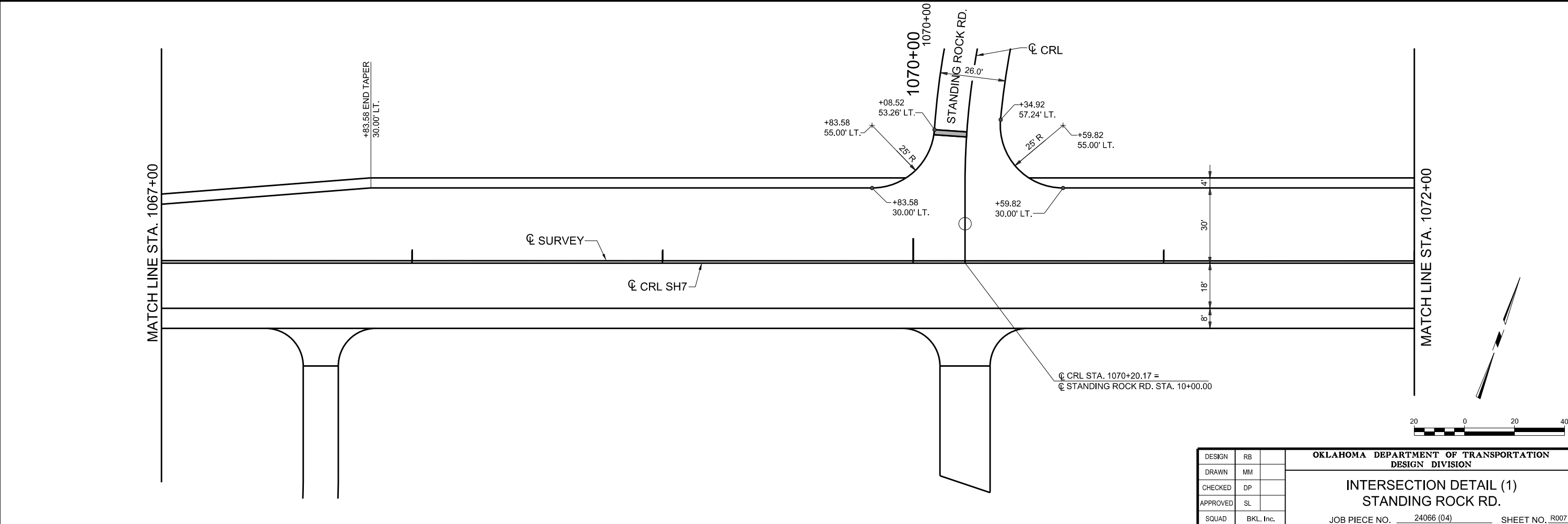
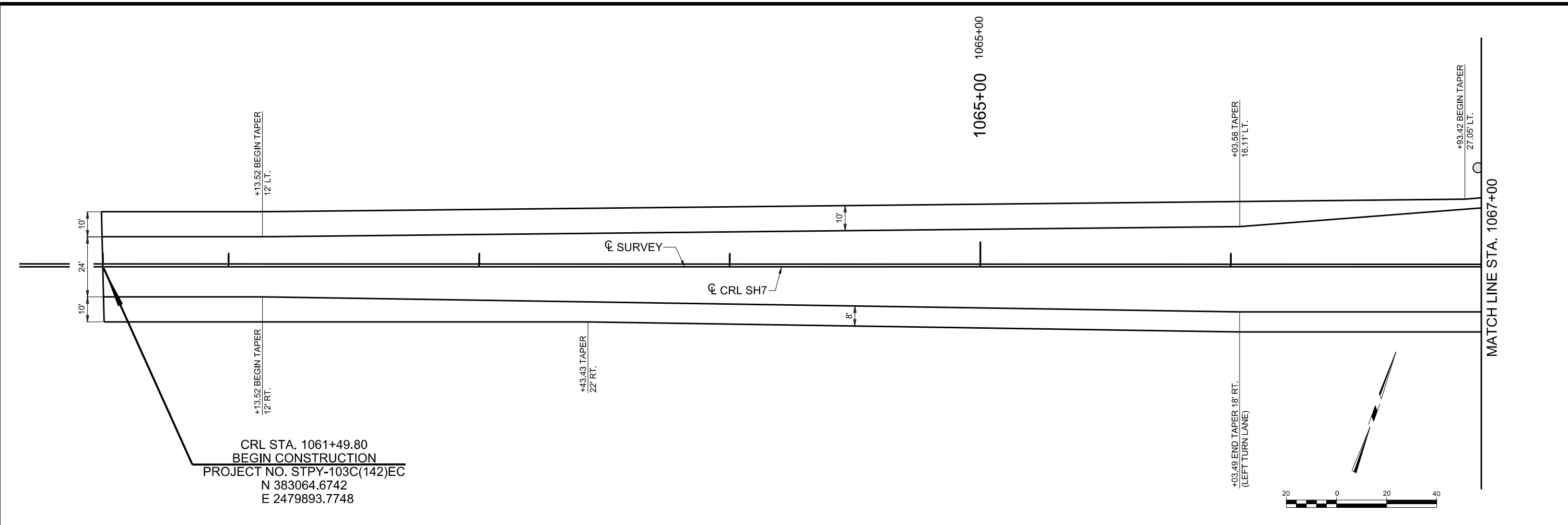
POT & SEC. COR.
STA. 1339+86.30
(SET 2" MAG NAIL)
ON N-S 382

PI CRL
1339+87.53 =
PI STA. 14+49.81
CARR PEN ROAD
FOR GEOMETRIC DATA INFO
ON CARR PEN ROAD SEE
PLAN AND PROFILE SHEET R062.
N 383823.2314
E 2507006.6145



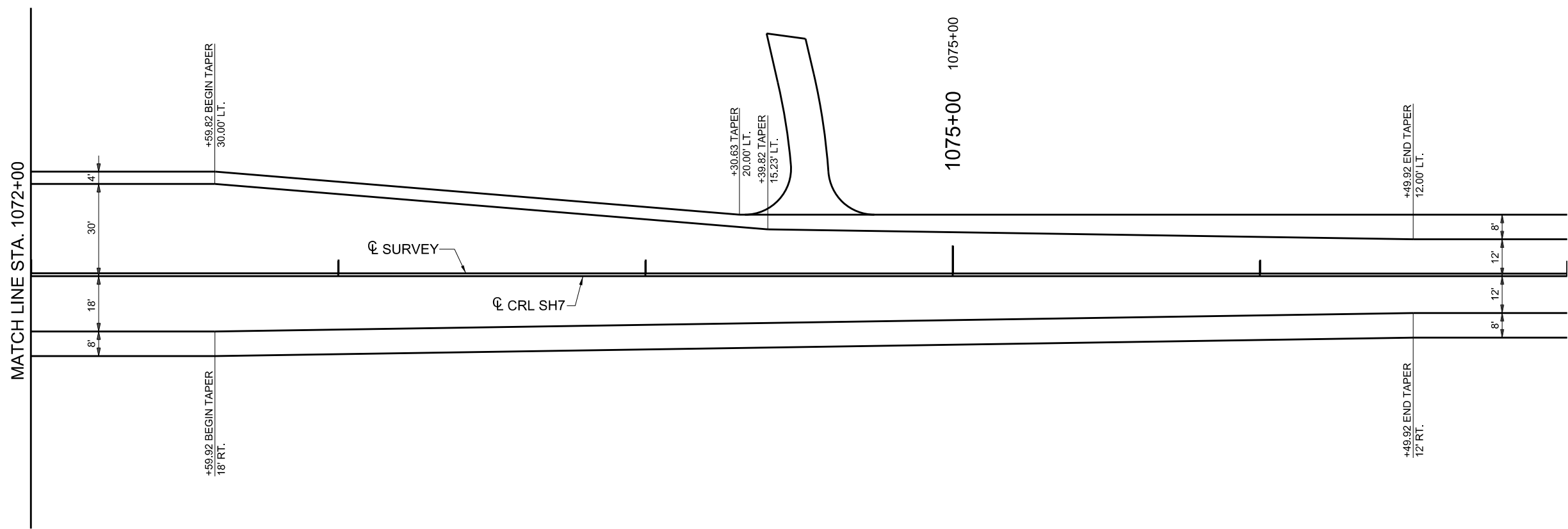
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1" = 200' HOR

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION GEOMETRIC DATA (SHEET 3 OF 3) JOB PIECE NO. 24066 (04) SHEET NO. R006
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CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		



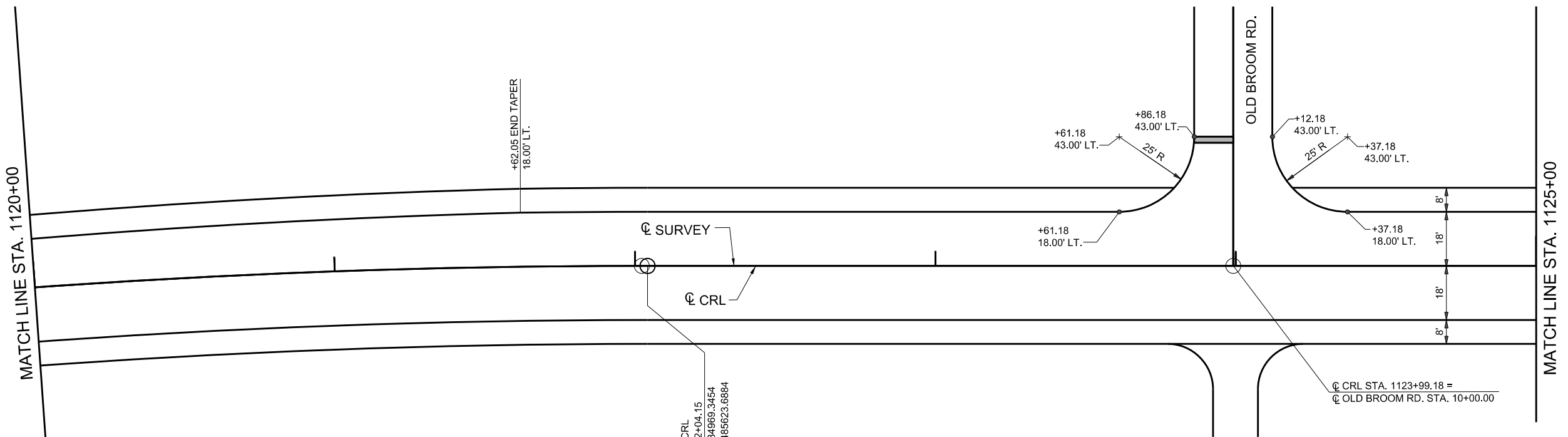
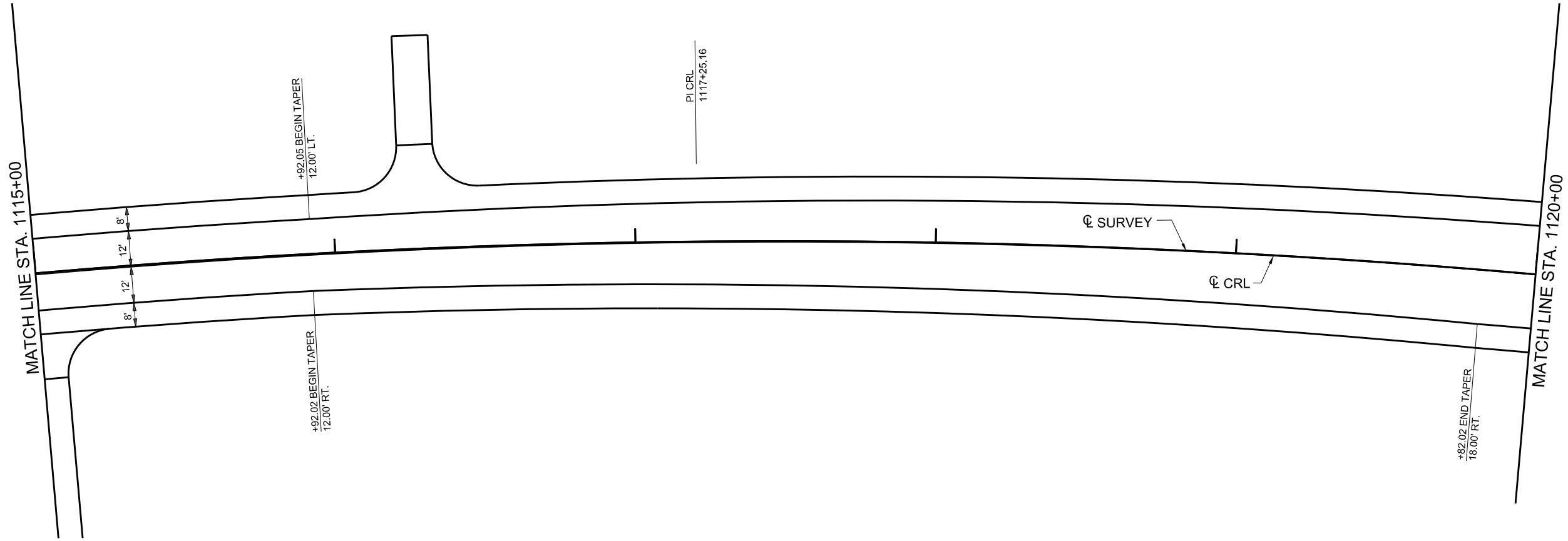
DESIGN	RB
DRAWN	MM
CHECKED	DP
APPROVED	SL
SQUAD	BKL, Inc.

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
 INTERSECTION DETAIL (1)
 STANDING ROCK RD.
 JOB PIECE NO. 24066 (04) SHEET NO. R007



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION INTERSECTION DETAIL (2) STANDING ROCK RD. JOB PIECE NO. 24066 (04) SHEET NO. R008
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

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DESIGN	RB
DRAWN	MM
CHECKED	DP
APPROVED	SL
SQUAD	BKL, Inc.

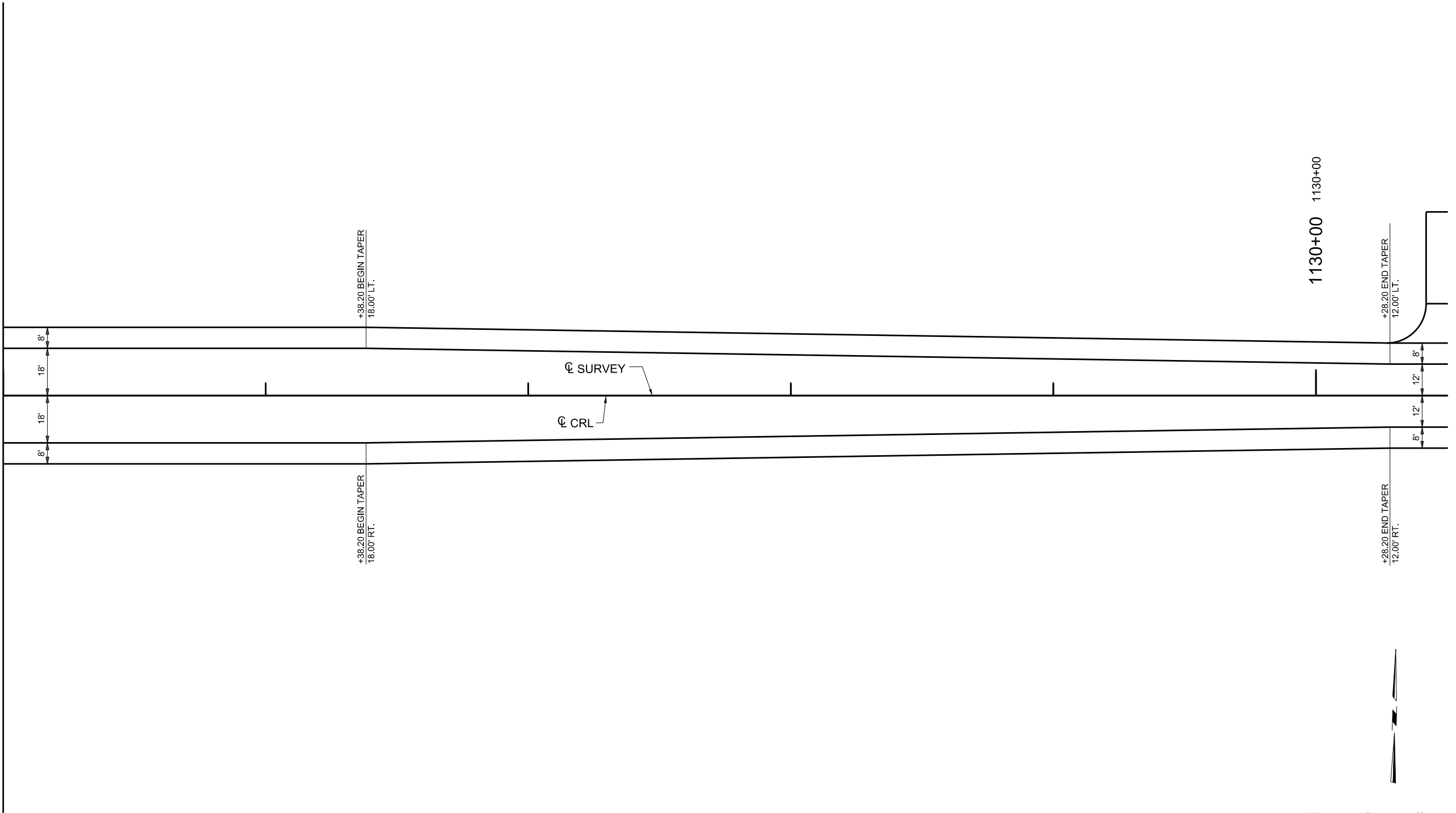
OKLAHOMA DEPARTMENT OF TRANSPORTATION
DESIGN DIVISION

INTERSECTION DETAIL (3)
OLD BROOM RD.

JOB PIECE NO. 24066 (04) SHEET NO. R009

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\650-SHT Final Plans\650-SHT Final Plans\650-SHT STA 1124.2.dgn

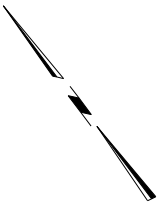
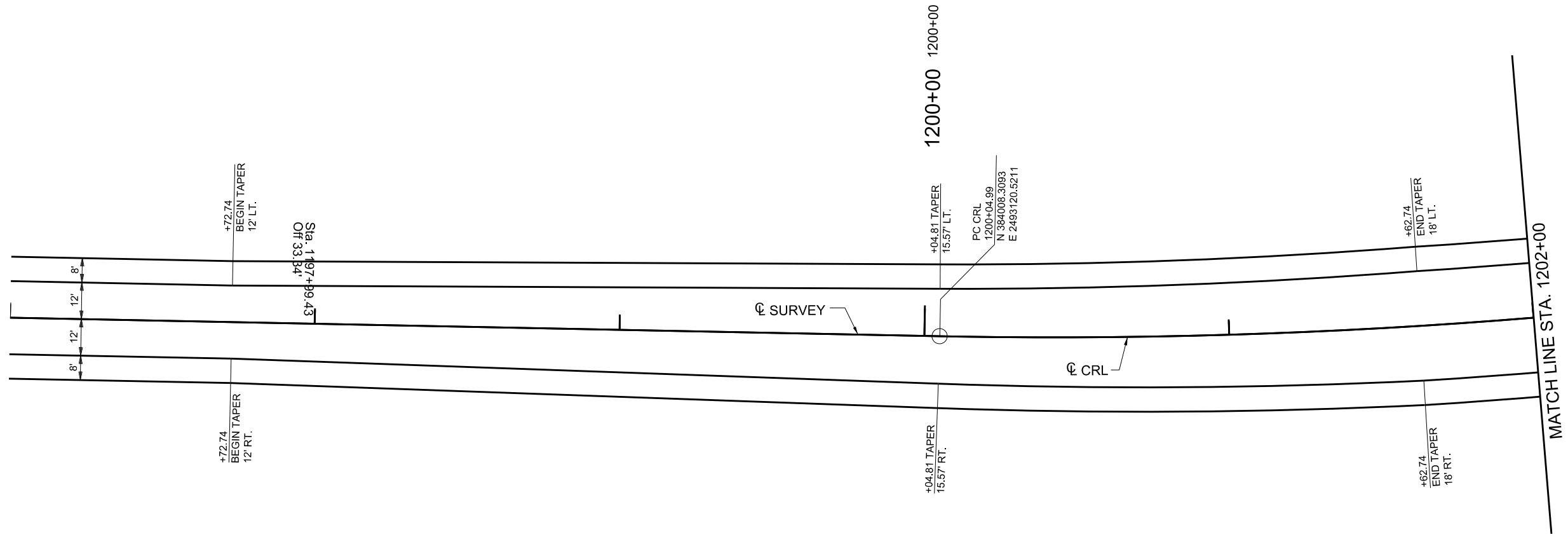
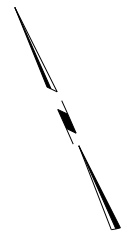
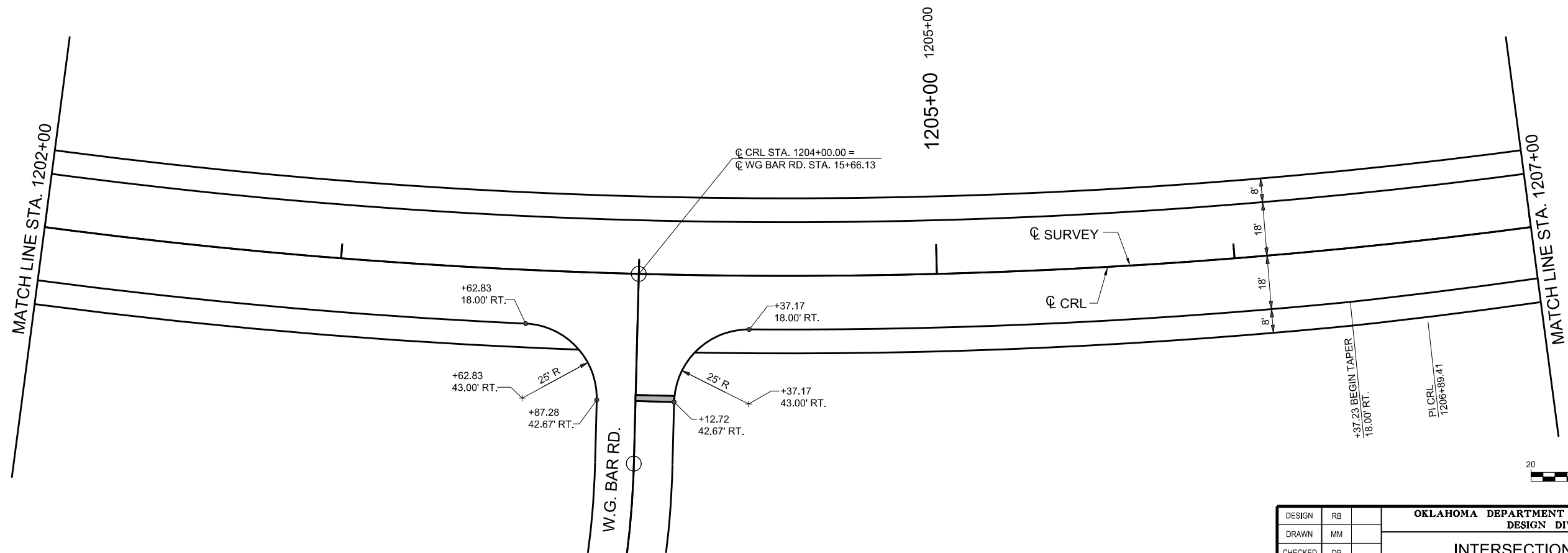
MATCH LINE STA. 1125+00



DESIGN	RB
DRAWN	MM
CHECKED	DP
APPROVED	SL
SQUAD	BKL, Inc.

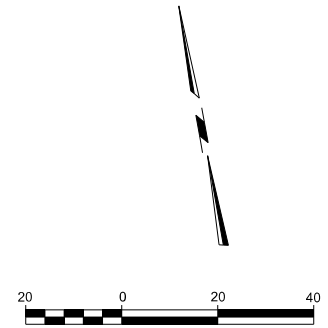
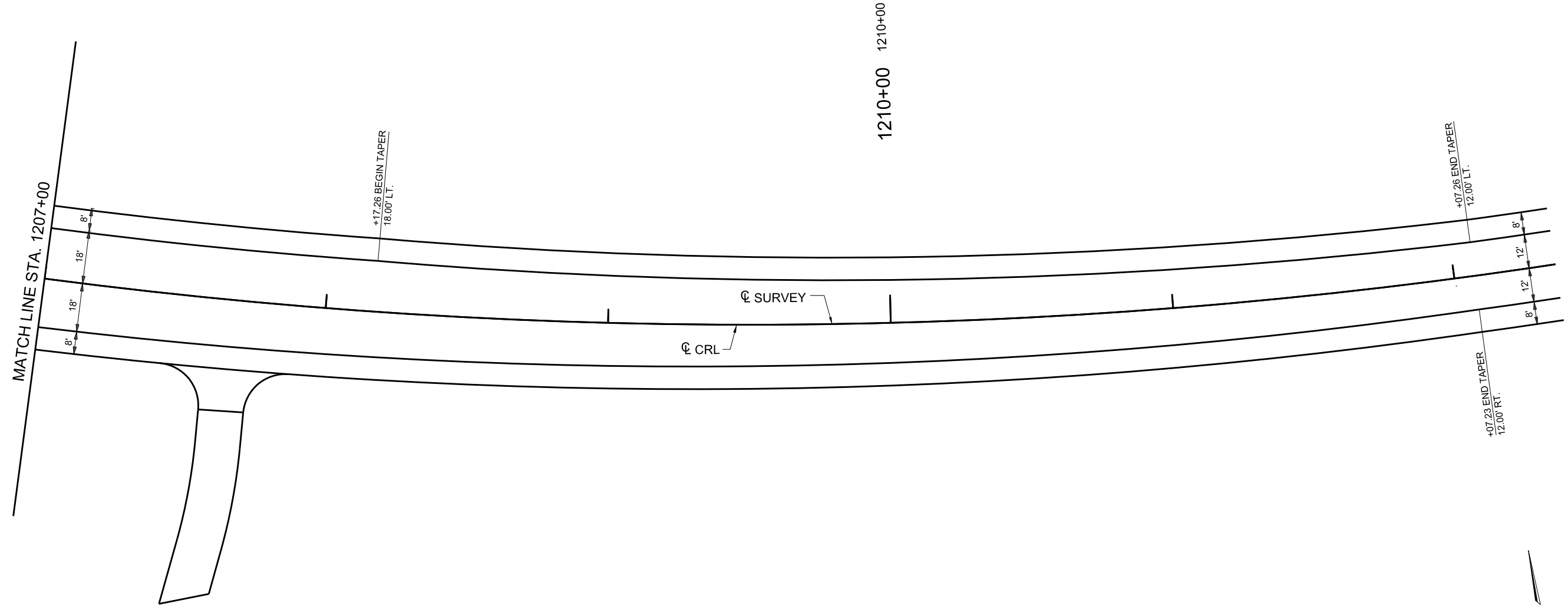
OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
INTERSECTION DETAIL (4)
OLD BROOM RD.
 JOB PIECE NO. 24066 (04) SHEET NO. R010

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\OGN\R011-24066\04-HNT STA 1204-1.dgn



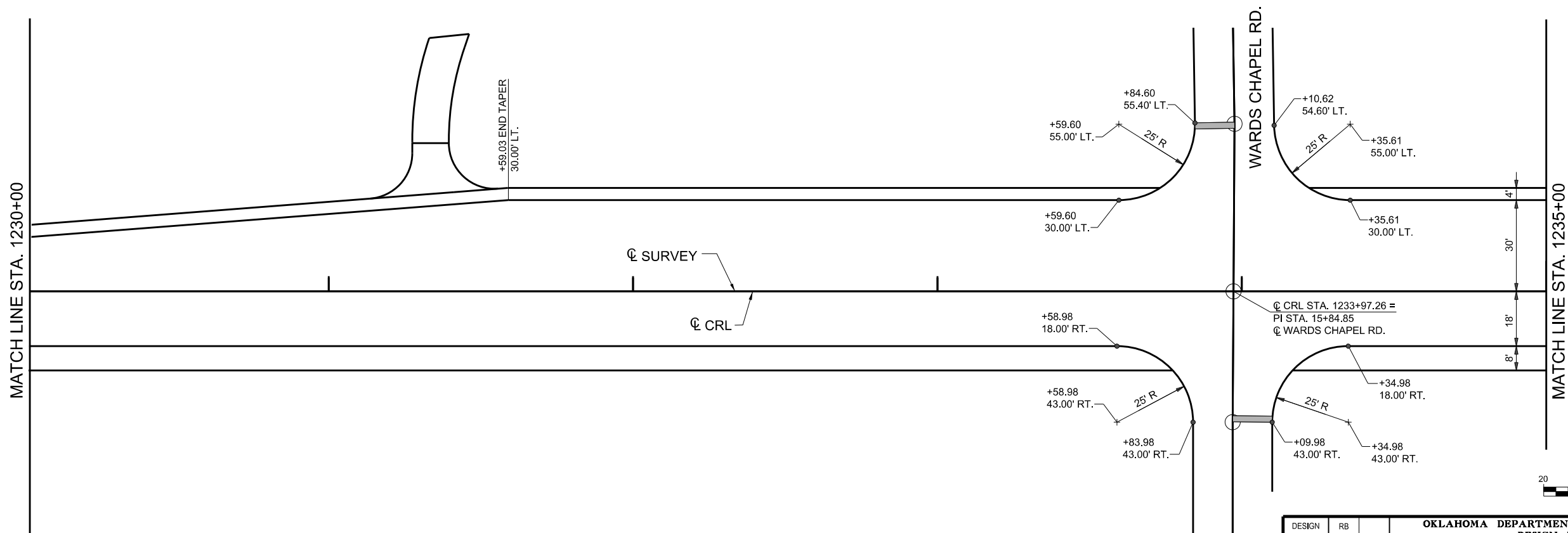
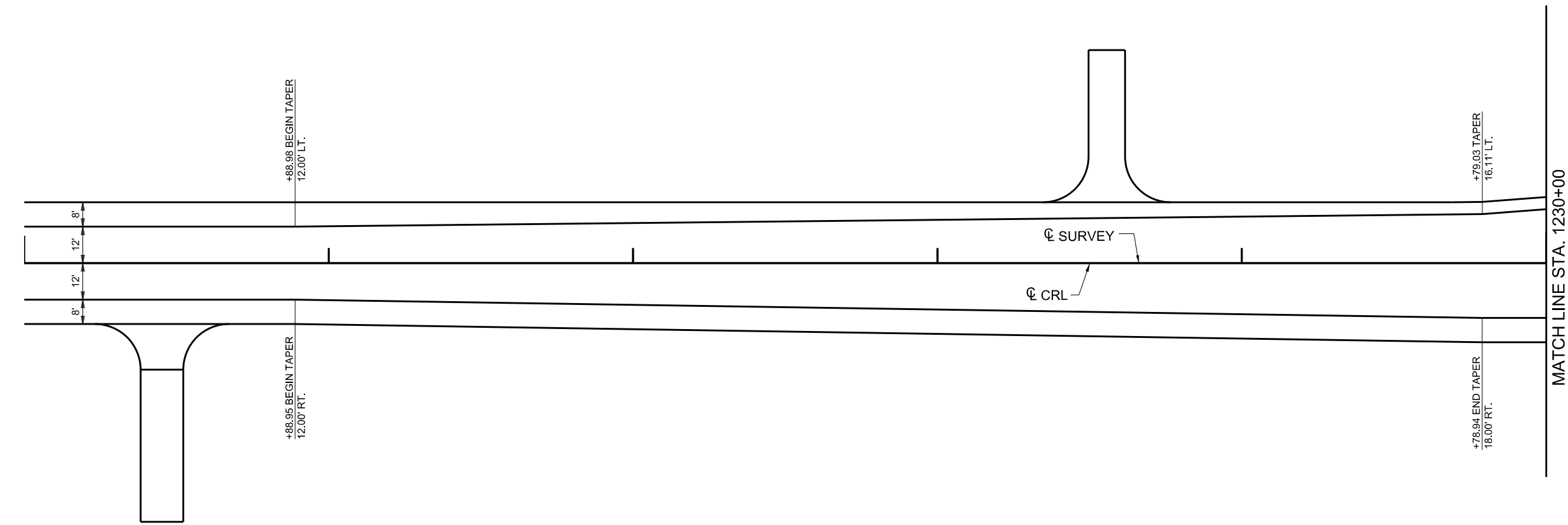
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DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

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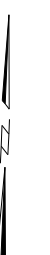
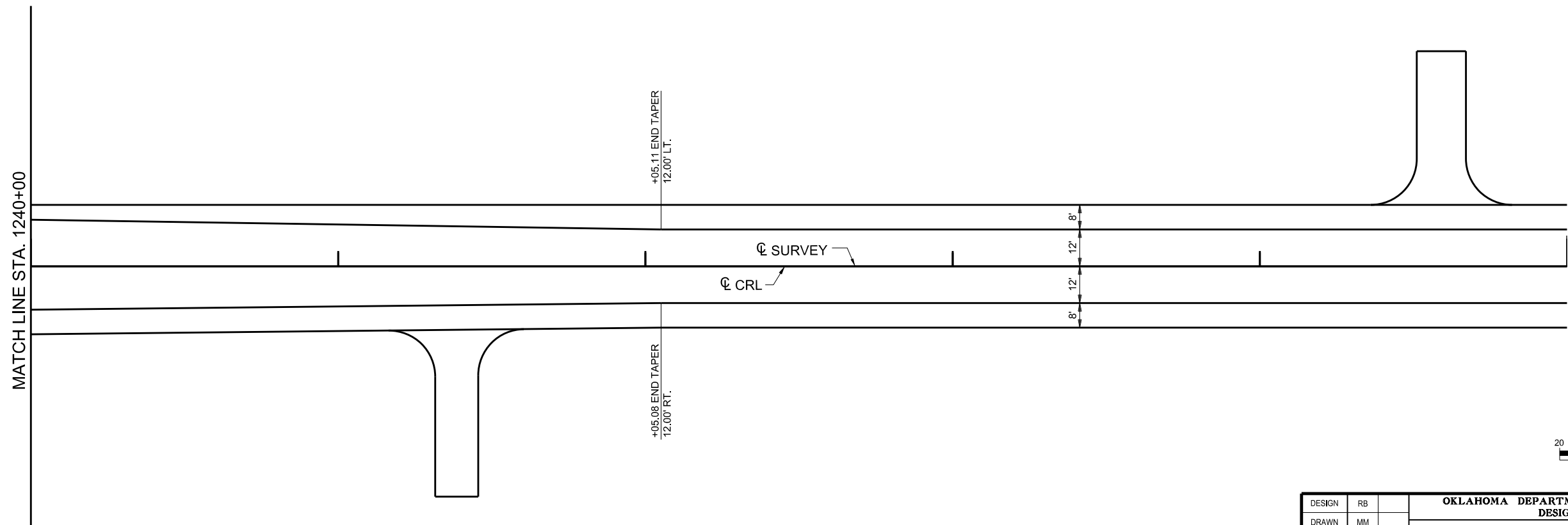
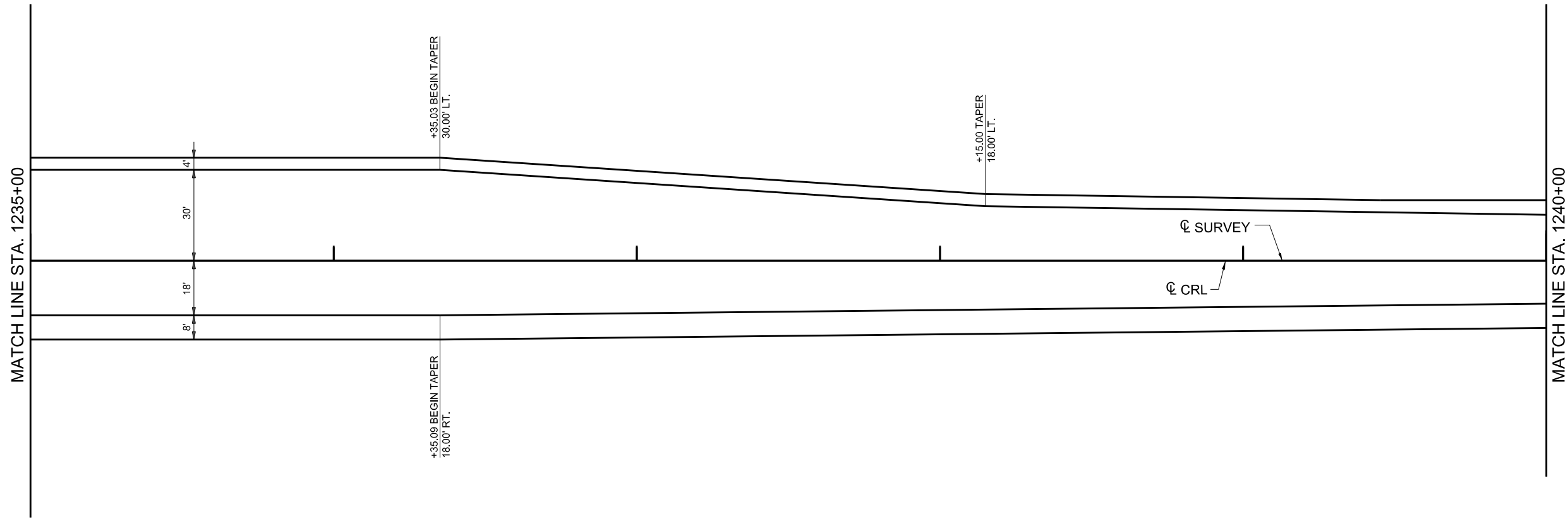
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CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\OGNVR013-24066(04)-INT STA 1234-1.dgn



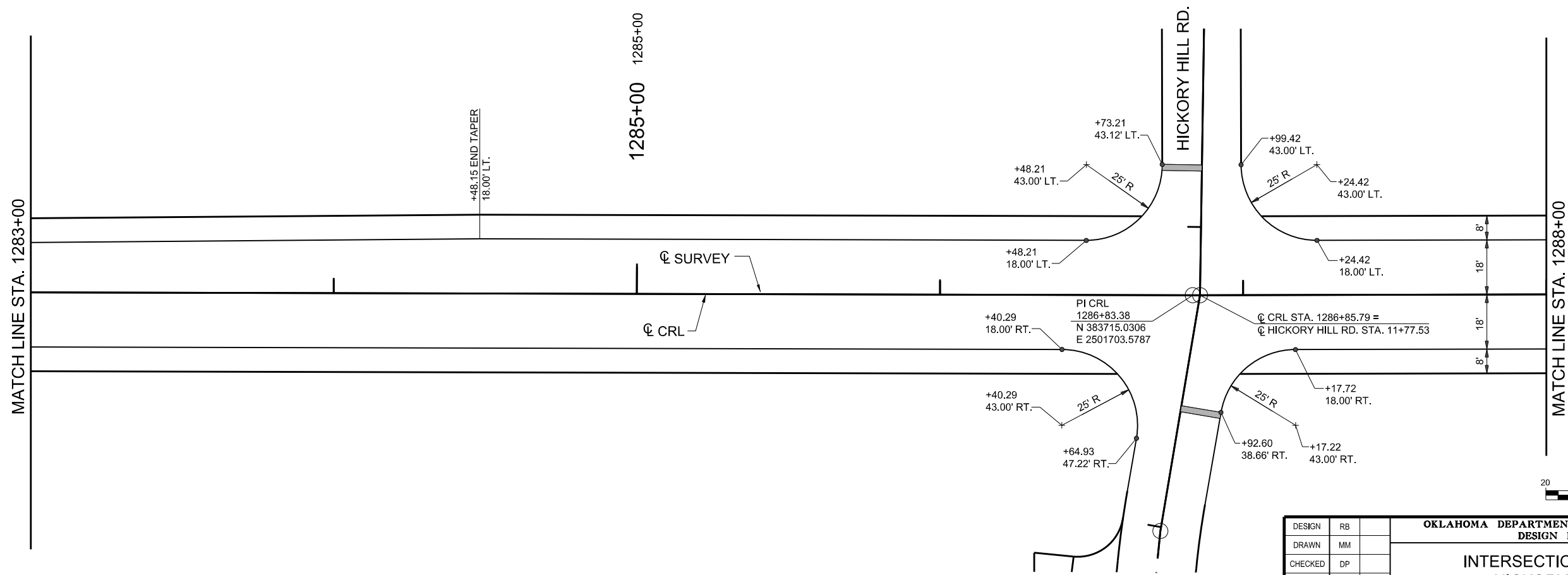
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APPROVED	SL		
SQUAD	BKL, Inc.		

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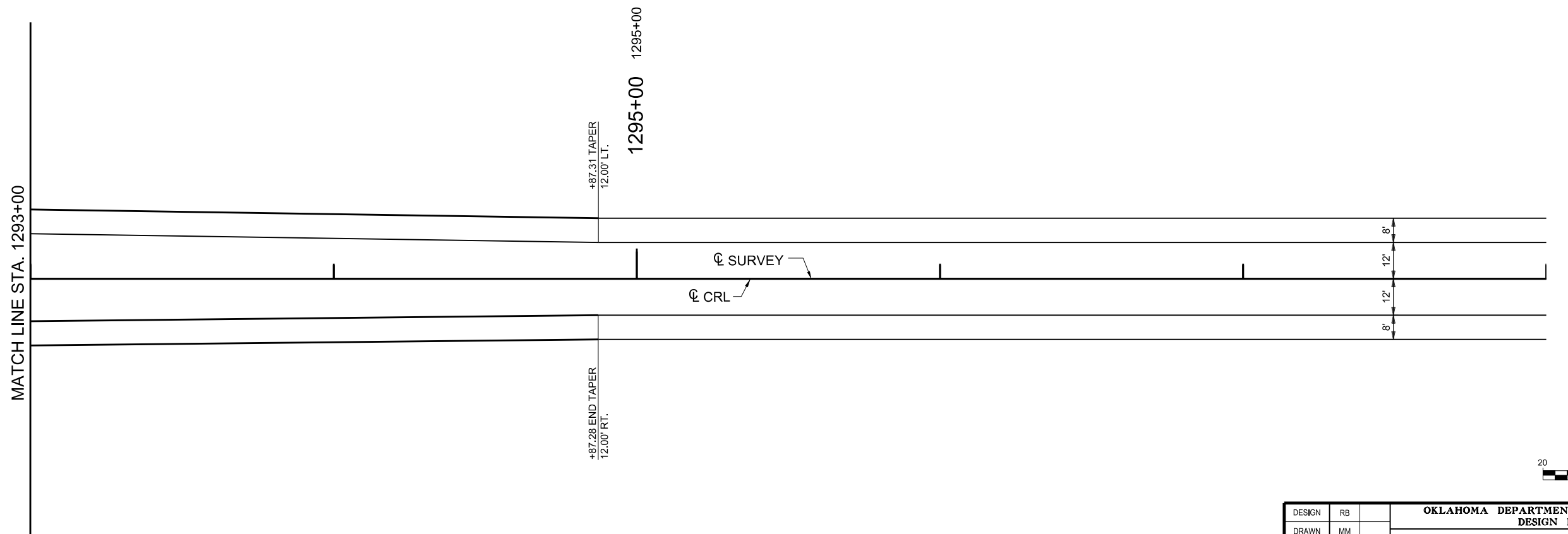
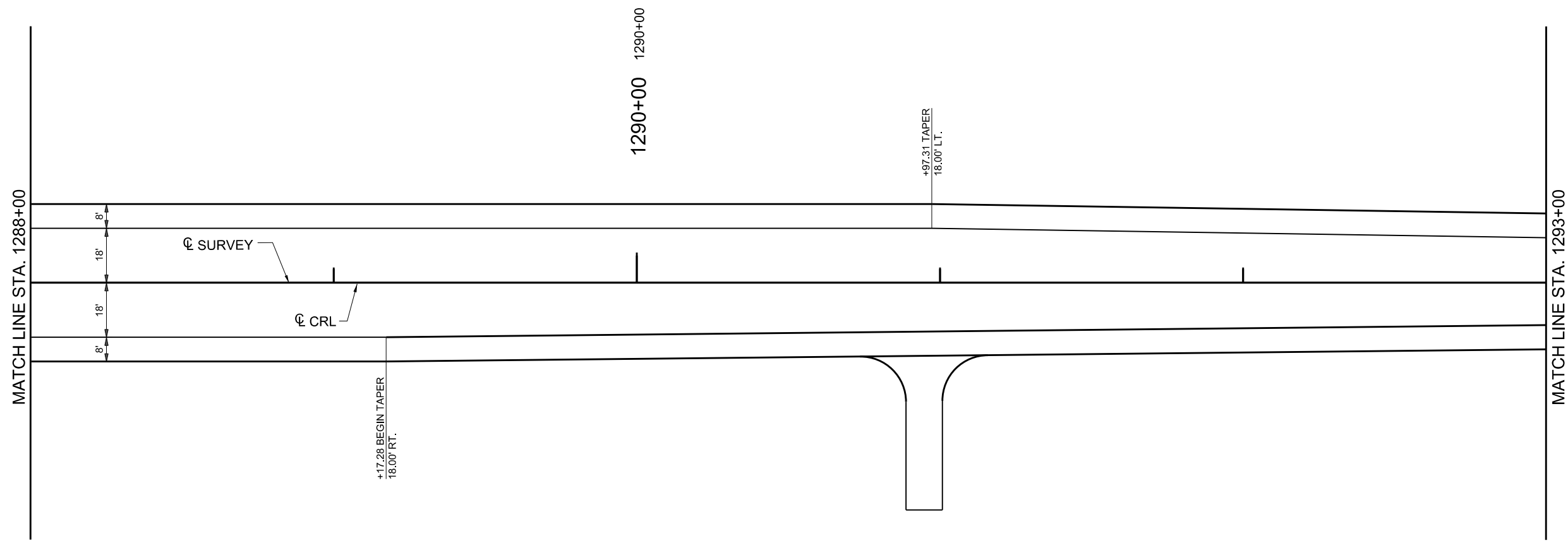
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APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\650-SHT Final Plans\650-SHT STA 1287-1.dwg



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION INTERSECTION DETAIL (9) HICKORY HILL RD. JOB PIECE NO. 24066 (04) SHEET NO. R015
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CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

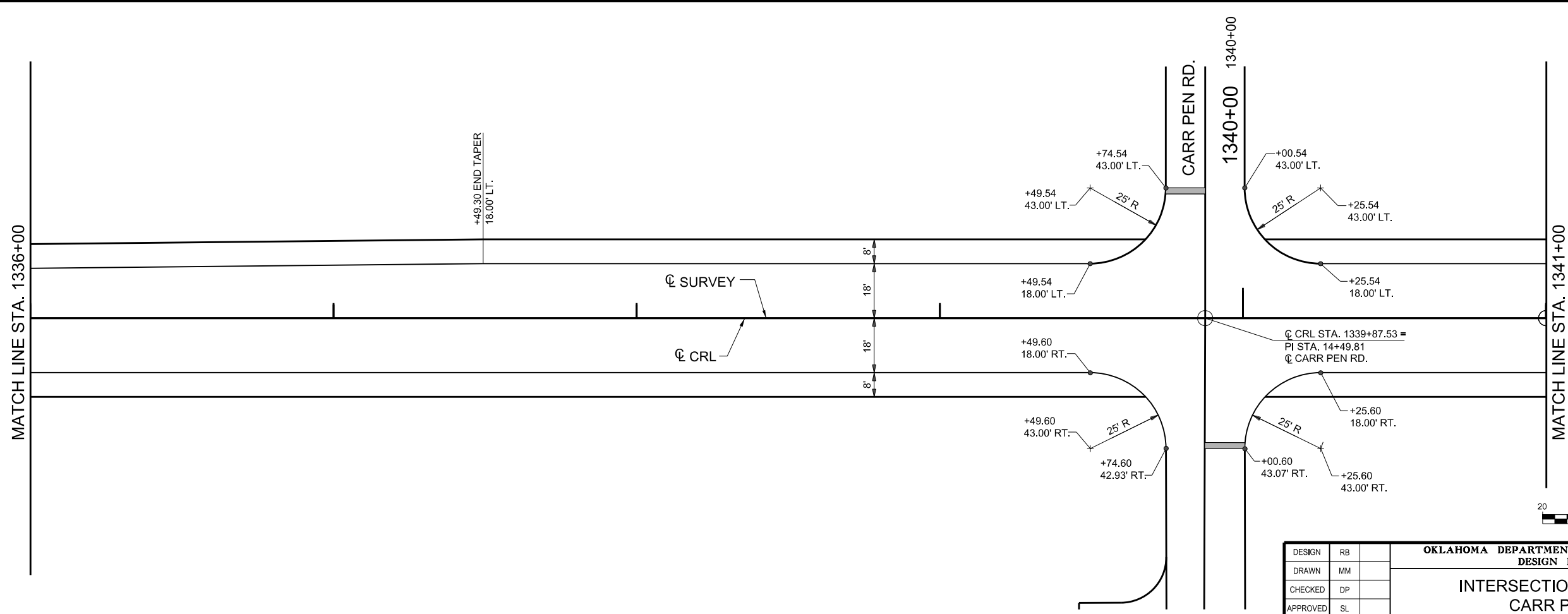
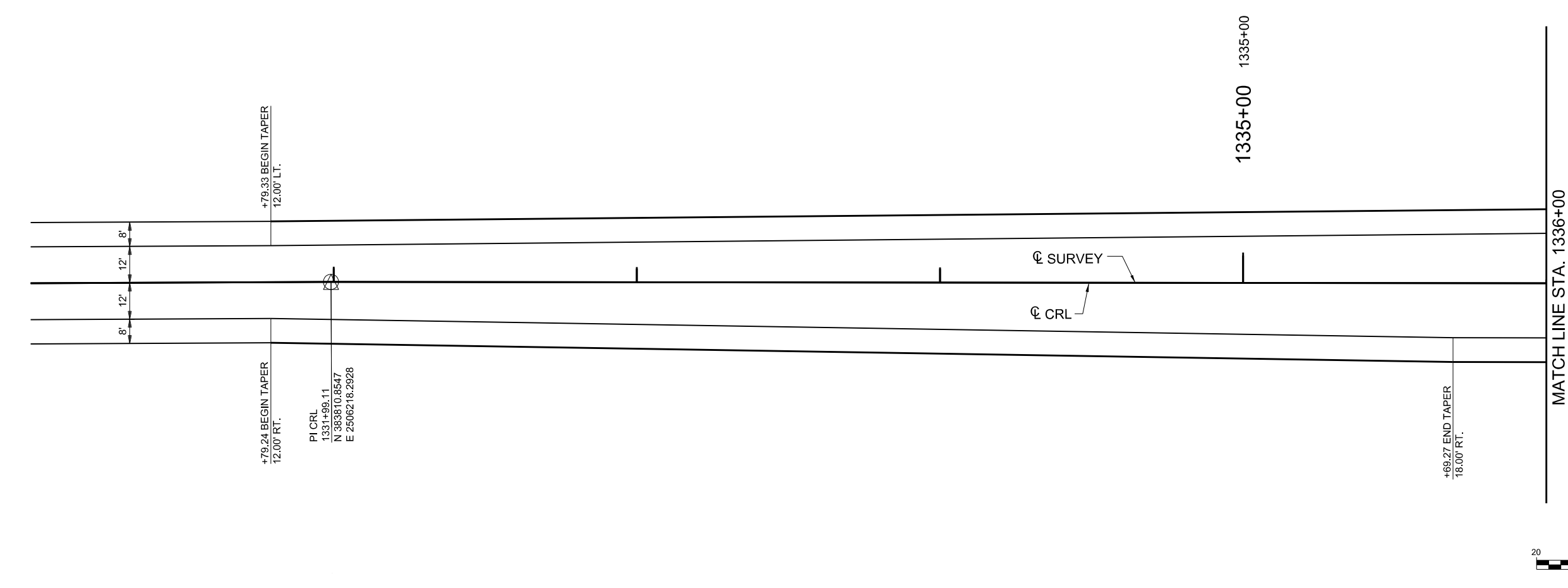
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DESIGN	RB	
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
INTERSECTION DETAIL (10)
HICKORY HILL RD.
 JOB PIECE NO. 24066 (04) SHEET NO. R016

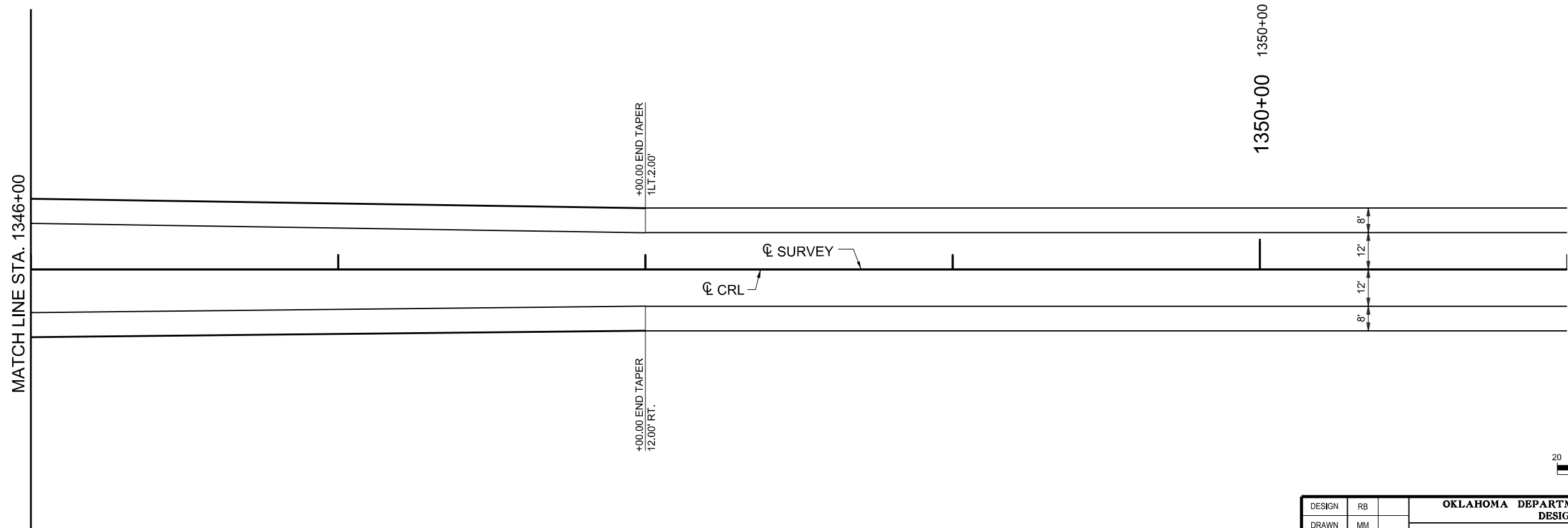
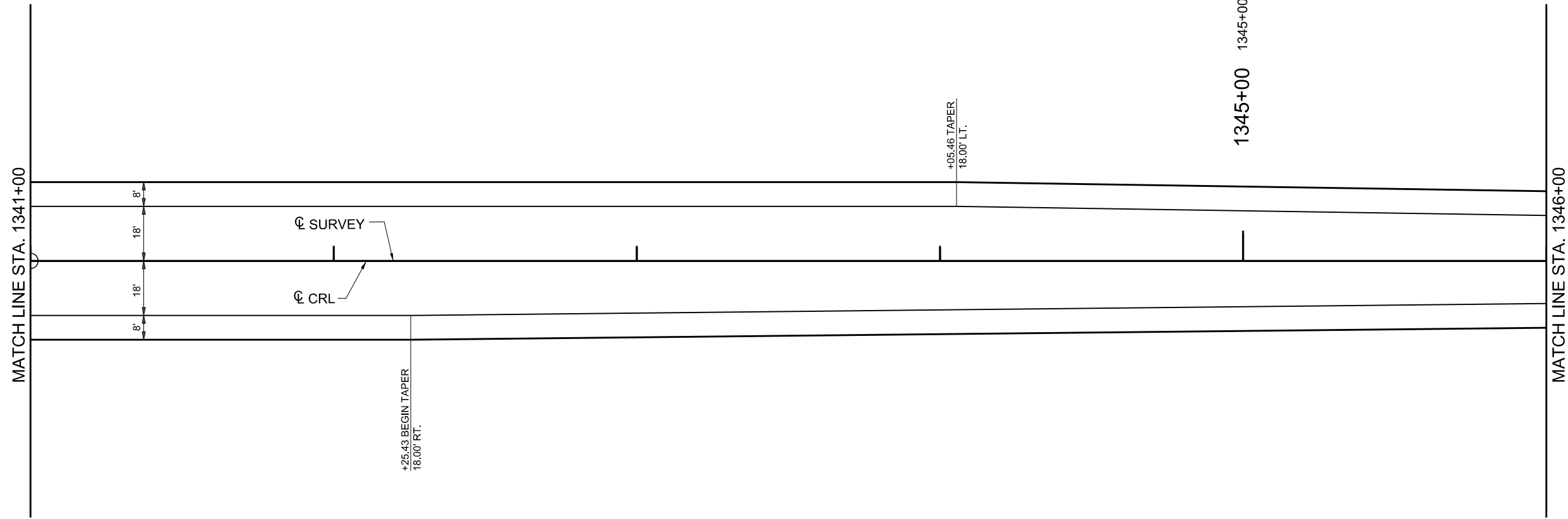
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DESIGN	RB	
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
INTERSECTION DETAIL (11)
CARR PEN RD.
 JOB PIECE NO. 24066 (04) SHEET NO. R017

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\650-SHT Final Plans\650-SHT Final Plans\650-SHT STA 1340.2.dgn



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION INTERSECTION DETAIL (12) CARR PEN RD. JOB PIECE NO. 24066 (04) SHEET NO. R018
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

KEY	
	FLOW DIRECTION ARROW
	TEMPORARY SILT FENCE
	TEMPORARY SILT DIKE
	TEMPORARY SEDIMENT FILTER
	ROCK FILTER DAM

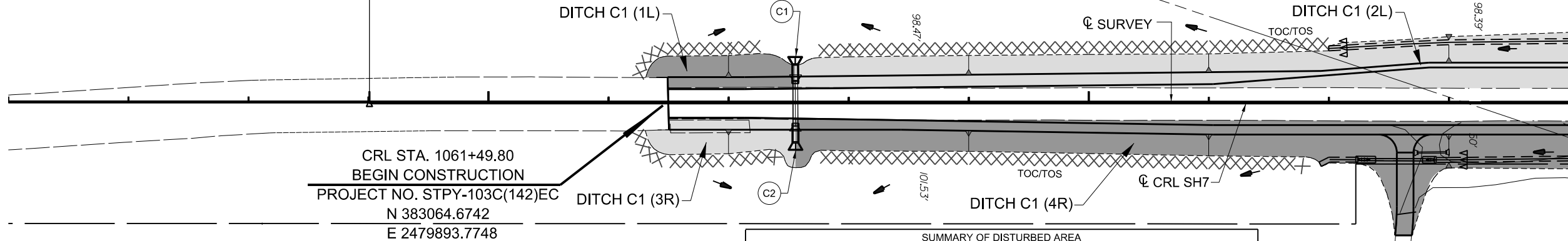
SEC. 13, T-2-S, R-9-E

POB
CRL STA. 1059+00.90
N 382979.8190
E 2479659.7659

1060+00

1065+00

STA. 1069+00



CRL STA. 1061+49.80
BEGIN CONSTRUCTION
PROJECT NO. STPY-103C(142)EC
N 383064.6742
E 2479893.7748

	TYPICAL DITCH
	TYPICAL DITCH (PAVED)
	SPECIAL DITCH
	SPECIAL DITCH (PAVED)

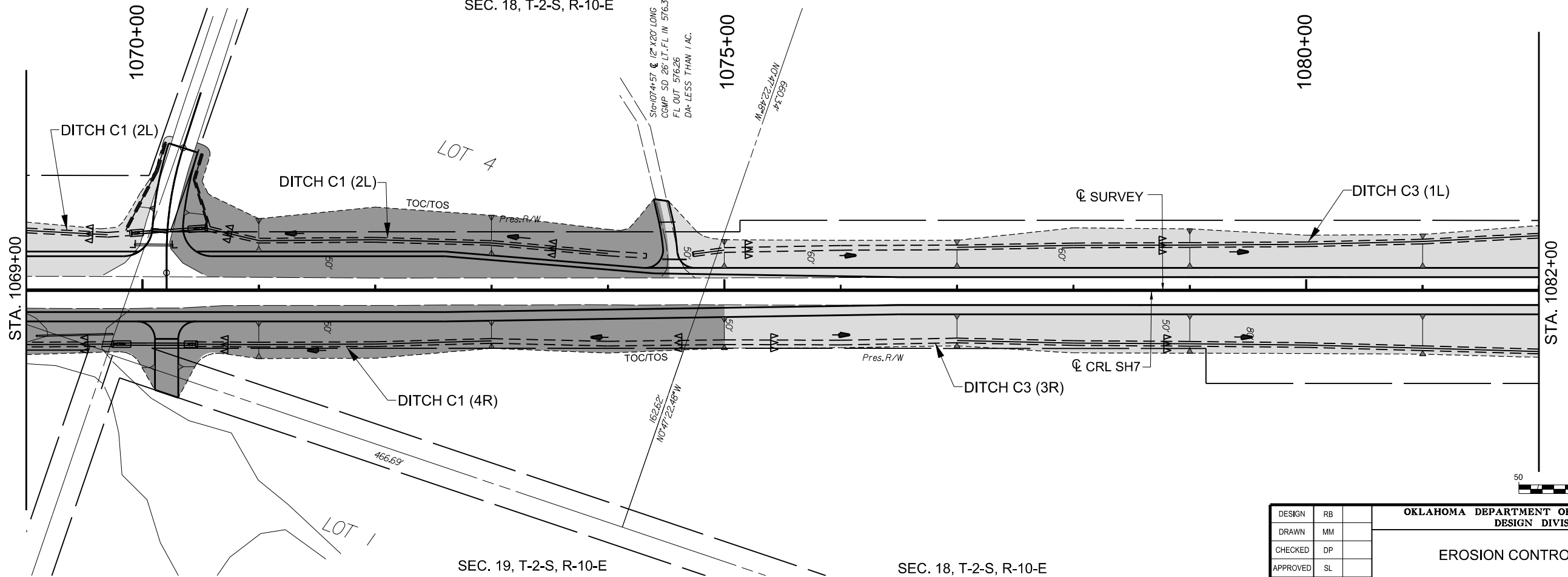
DITCH LEGEND

SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C1 (1L) STA. 1061+30.00 to 1062+55.60	SHEET FLOW	0.07	SILT FENCE
C1 (2L) STA. 1062+55.60 to 1074+54.08	1062+55.60	1.13	SILT DIKE & SILT FENCE
C1 (3R) STA. 1061+30.00 to 1062+55.60	SHEET FLOW	0.08	SILT FENCE
C1 (4R) STA. 1062+55.60 to 1075+00.00	1062+55.60	1.10	SILT DIKE & SILT FENCE
TOTAL =		2.38	

SEC. 24, T-2-S, R-9-E



SEC. 18, T-2-S, R-10-E



STA. 1069+00

1070+00

1075+00

1080+00

STA. 1082+00

SEC. 19, T-2-S, R-10-E

SEC. 18, T-2-S, R-10-E



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION EROSION CONTROL DETAIL (1) JOB PIECE NO. 24066 (04) SHEET NO. R019
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

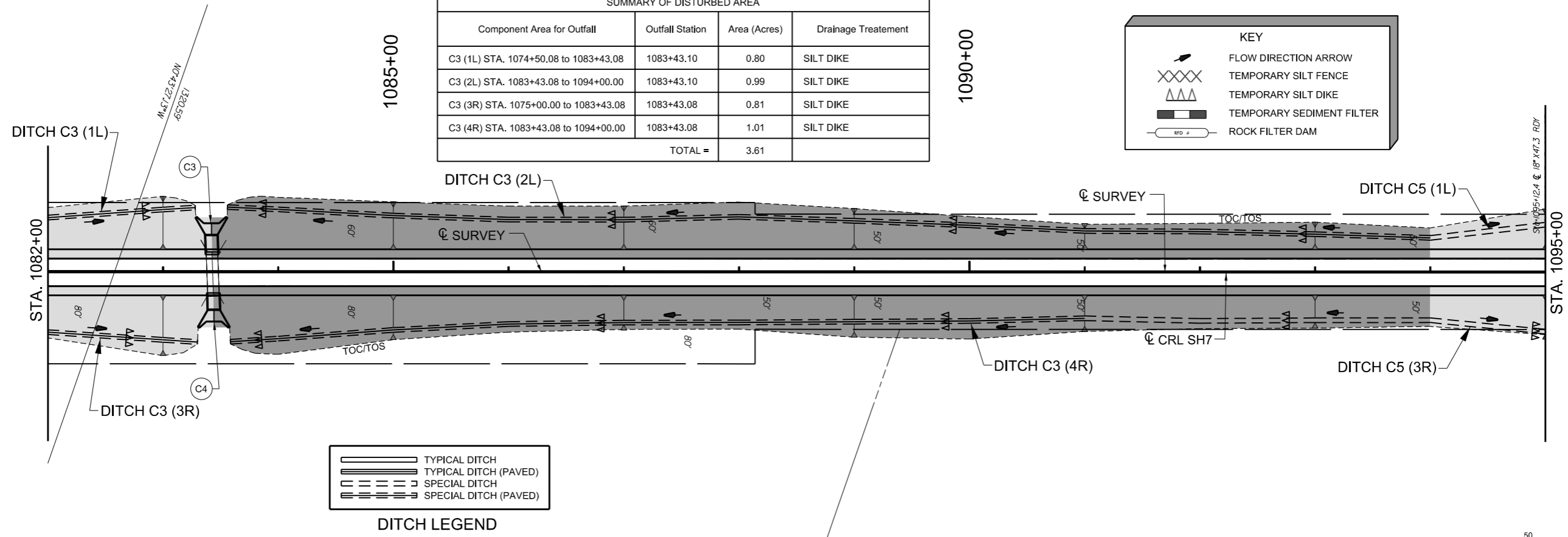
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SEC. 18, T-2-S, R-10-E

SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C3 (1L) STA. 1074+50.08 to 1083+43.08	1083+43.10	0.80	SILT DIKE
C3 (2L) STA. 1083+43.08 to 1094+00.00	1083+43.10	0.99	SILT DIKE
C3 (3R) STA. 1075+00.00 to 1083+43.08	1083+43.08	0.81	SILT DIKE
C3 (4R) STA. 1083+43.08 to 1094+00.00	1083+43.08	1.01	SILT DIKE
TOTAL =		3.61	

KEY

- FLOW DIRECTION ARROW
- TEMPORARY SILT FENCE
- TEMPORARY SILT DIKE
- TEMPORARY SEDIMENT FILTER
- ROCK FILTER DAM



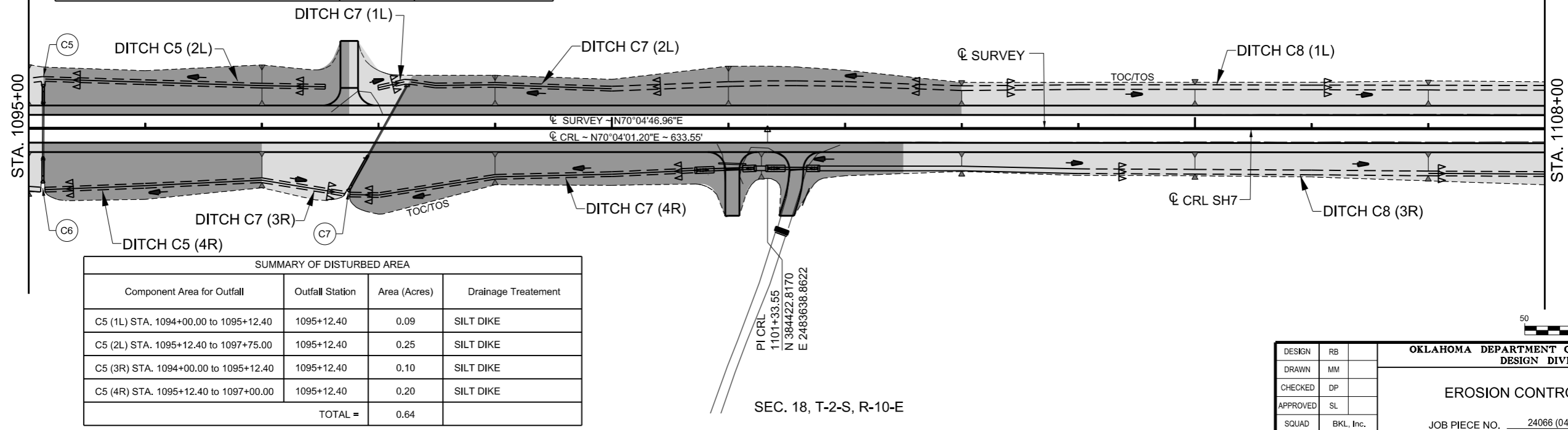
DITCH LEGEND

- TYPICAL DITCH
- TYPICAL DITCH (PAVED)
- SPECIAL DITCH
- SPECIAL DITCH (PAVED)

SEC. 18, T-2-S, R-10-E

SEC. 18, T-2-S, R-10-E

SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C7 (1L) STA. 1097+75.00 to 1098+03.84	1098+03.84	0.05	SILT DIKE
C7 (2L) STA. 1098+03.84 to 1103+00.00	1098+03.84	0.39	SILT DIKE
C7 (3R) STA. 1097+00.00 to 1098+03.84	1098+03.84	0.09	SILT DIKE
C7 (4R) STA. 1098+03.84 to 1102+50.00	1098+03.84	0.44	SILT DIKE
TOTAL =		0.97	



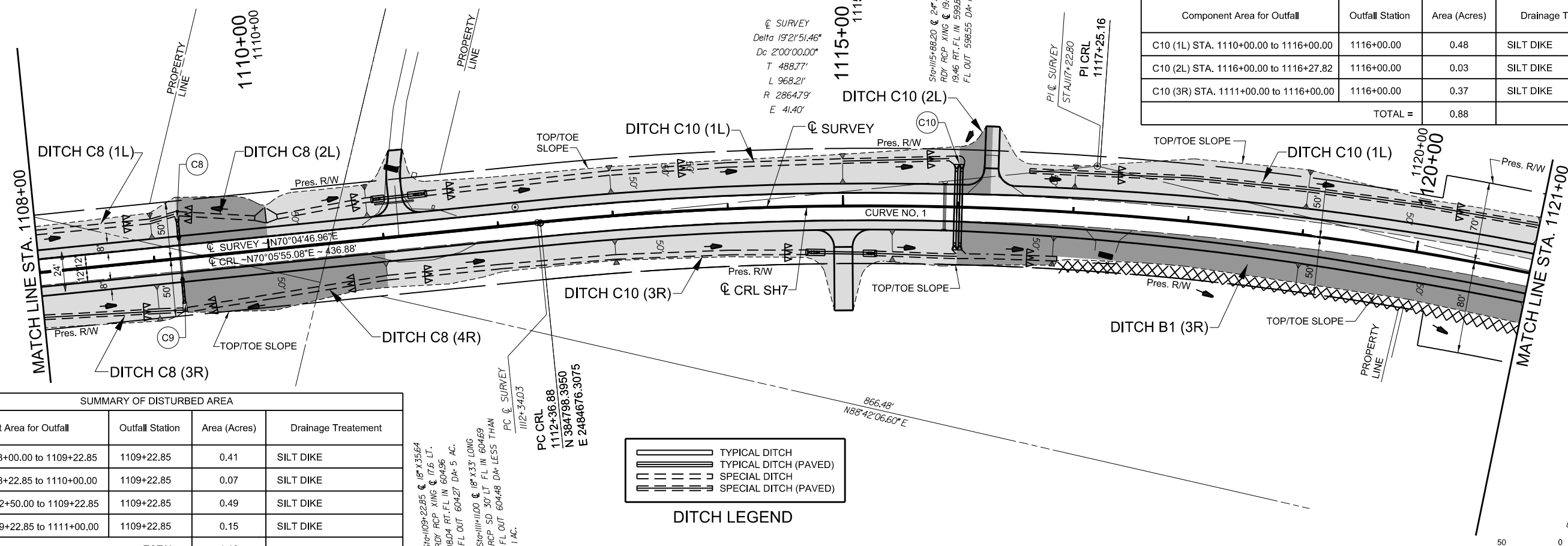
SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C5 (1L) STA. 1094+00.00 to 1095+12.40	1095+12.40	0.09	SILT DIKE
C5 (2L) STA. 1095+12.40 to 1097+75.00	1095+12.40	0.25	SILT DIKE
C5 (3R) STA. 1094+00.00 to 1095+12.40	1095+12.40	0.10	SILT DIKE
C5 (4R) STA. 1095+12.40 to 1097+00.00	1095+12.40	0.20	SILT DIKE
TOTAL =		0.64	

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION EROSION CONTROL DETAIL (2) JOB PIECE NO. 24066 (04) SHEET NO. R020
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

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SEC. 18, T-2-S, R-10-E

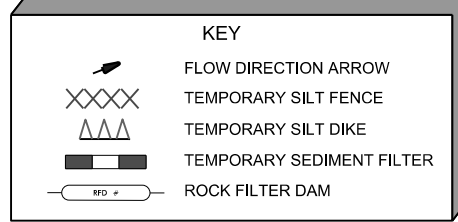
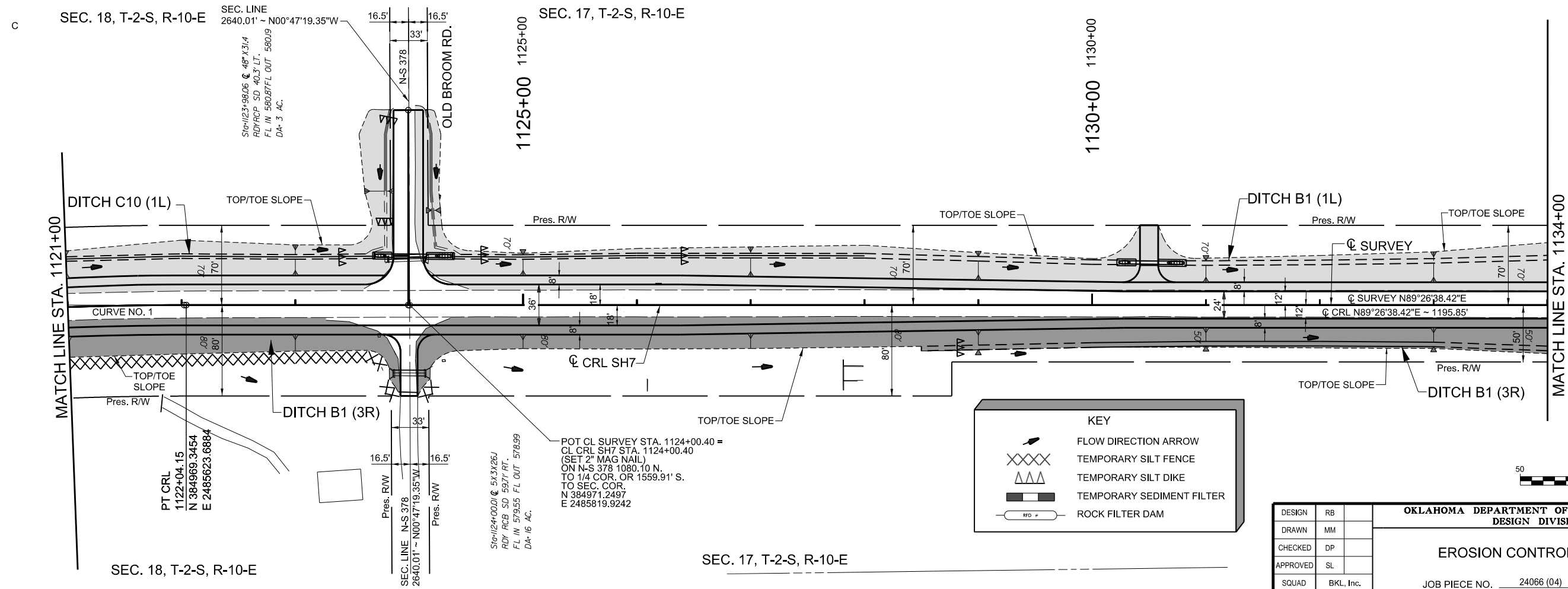
SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C10 (1L) STA. 1110+00.00 to 1116+00.00	1116+00.00	0.48	SILT DIKE
C10 (2L) STA. 1116+00.00 to 1116+27.82	1116+00.00	0.03	SILT DIKE
C10 (3R) STA. 1111+00.00 to 1116+00.00	1116+00.00	0.37	SILT DIKE
TOTAL =		0.88	



SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C8 (1L) STA. 1103+00.00 to 1109+22.85	1109+22.85	0.41	SILT DIKE
C8 (2L) STA. 1098+22.85 to 1110+00.00	1109+22.85	0.07	SILT DIKE
C8 (3R) STA. 1102+50.00 to 1109+22.85	1109+22.85	0.49	SILT DIKE
C8 (4R) STA. 1109+22.85 to 1111+00.00	1109+22.85	0.15	SILT DIKE
TOTAL =		1.12	



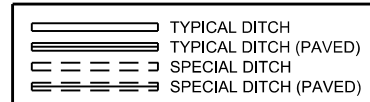
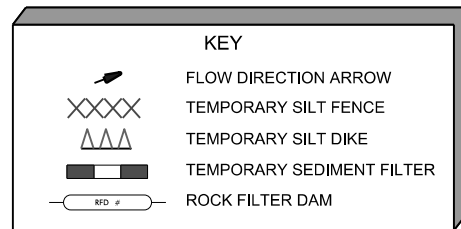
SEC. 18, T-2-S, R-10-E



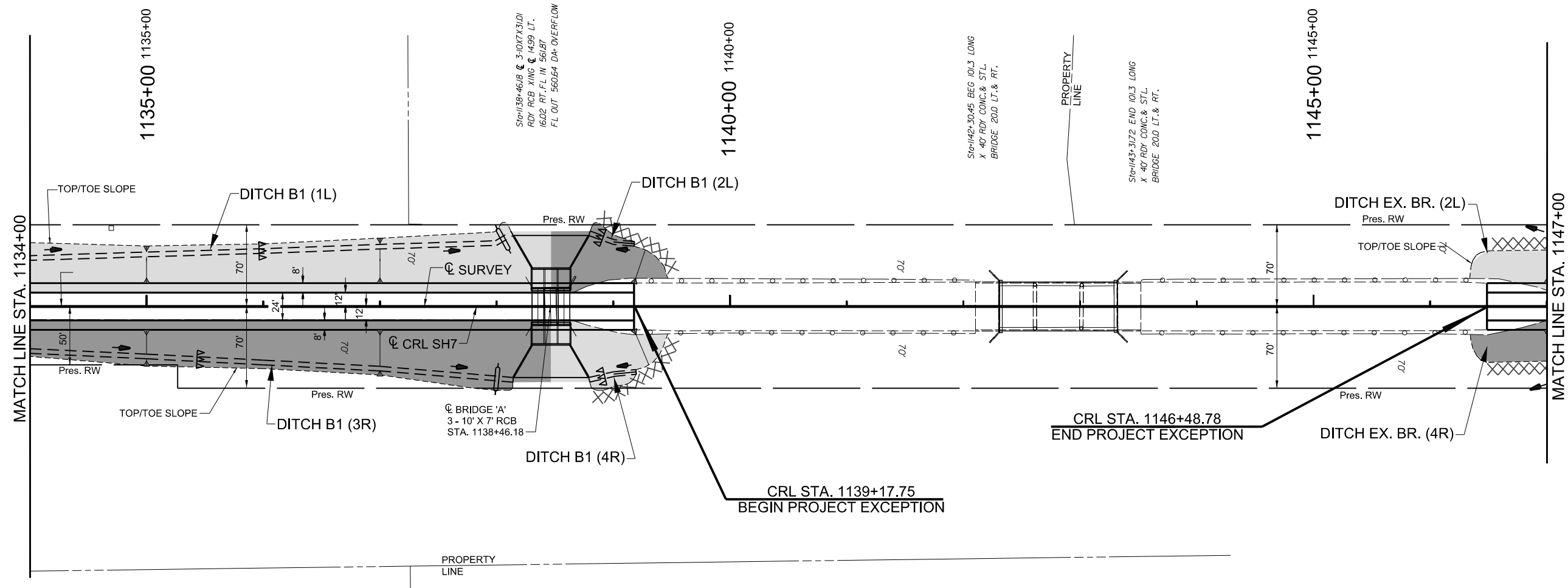
DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION EROSION CONTROL DETAIL (3) JOB PIECE NO. 24066 (04) SHEET NO. R021
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

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SEC. 17, T-2-S, R-10-E



SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
B1 (1L) STA. 1116+27.82 to 1138+46.18	1138+46.18	2.06	SILT DIKE, RFD
B1 (2L) STA. 1138+46.18 to 1139+18.00	1138+46.18	0.09	SILT FENCE, SILT DIKE
B1 (3R) STA. 1116+00.00 to 1138+46.18	1138+46.18	1.65	SILT DIKE, RFD
B1 (4R) STA. 1138+46.18 to 1139+18.00	1138+46.18	0.10	SILT FENCE, SILT DIKE
TOTAL =		3.90	



Sta 1138+46.18 @ 3'-10" X 7'-0" RCB
RDY. RCB. YING @ 1459 LT.
1602 RT. FL IN 56187
FL OUT 56064 DA OVERFLOW

Sta 1142+30.45 BEG 101.3 LONG
X 40 RDY CONC. & STL.
BRIDGE 200 LT. & RT.

Sta 1143+31.72 END 101.3 LONG
X 40 RDY CONC. & STL.
BRIDGE 200 LT. & RT.

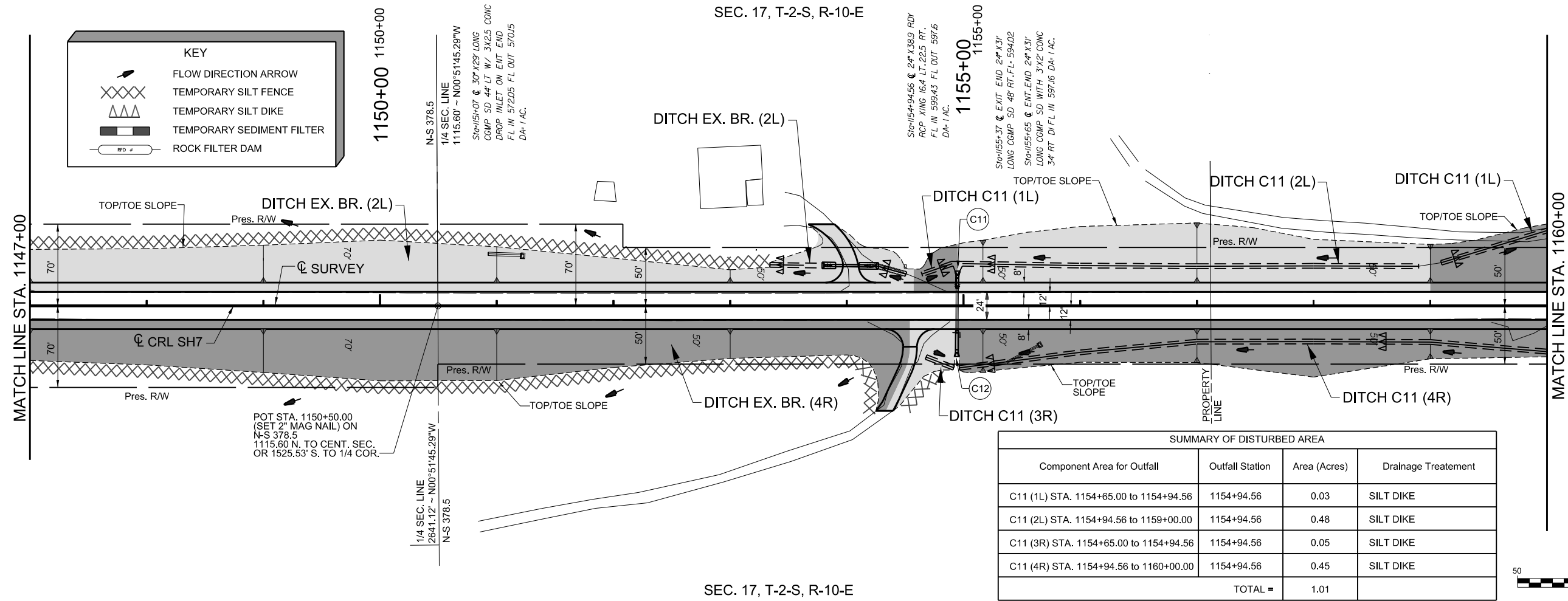
SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
EX. BR. (2L) STA. 1116+27.82 to 1154+65.00	1142+80.00	0.68	SILT FENCE & SILT DIKE
EX. BR. (4R) STA. 1138+46.18 to 1154+65.00	1142+80.00	0.77	SILT FENCE
TOTAL =		1.45	



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DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION EROSION CONTROL DETAIL (4) JOB PIECE NO. 24066 (04) SHEET NO. R022
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

SEC. 17, T-2-S, R-10-E

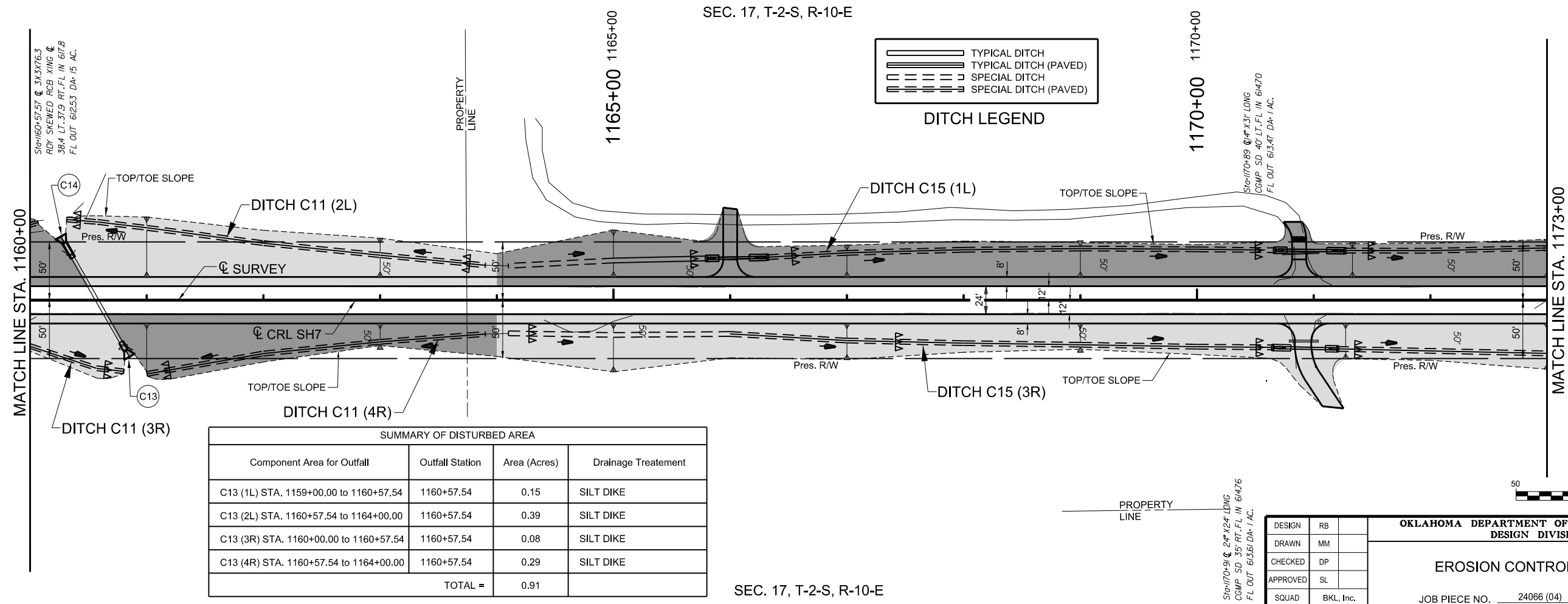


SUMMARY OF DISTURBED AREA				
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment	
C11 (1L) STA. 1154+65.00 to 1154+94.56	1154+94.56	0.03	SILT DIKE	
C11 (2L) STA. 1154+94.56 to 1159+00.00	1154+94.56	0.48	SILT DIKE	
C11 (3R) STA. 1154+65.00 to 1154+94.56	1154+94.56	0.05	SILT DIKE	
C11 (4R) STA. 1154+94.56 to 1160+00.00	1154+94.56	0.45	SILT DIKE	
TOTAL =		1.01		



SEC. 17, T-2-S, R-10-E

SEC. 17, T-2-S, R-10-E



SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C13 (1L) STA. 1159+00.00 to 1160+57.54	1160+57.54	0.15	SILT DIKE
C13 (2L) STA. 1160+57.54 to 1164+00.00	1160+57.54	0.39	SILT DIKE
C13 (3R) STA. 1160+00.00 to 1160+57.54	1160+57.54	0.08	SILT DIKE
C13 (4R) STA. 1160+57.54 to 1164+00.00	1160+57.54	0.29	SILT DIKE
TOTAL =		0.91	



SEC. 17, T-2-S, R-10-E

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Sta=1170-91 @ 24' X 24' LONG
CGMP SD 35' RT. FL IN 6476
FL OUT 61.36 DA-1 AC.

DESIGN	RB	
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

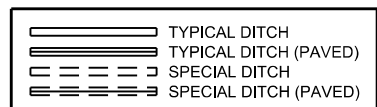
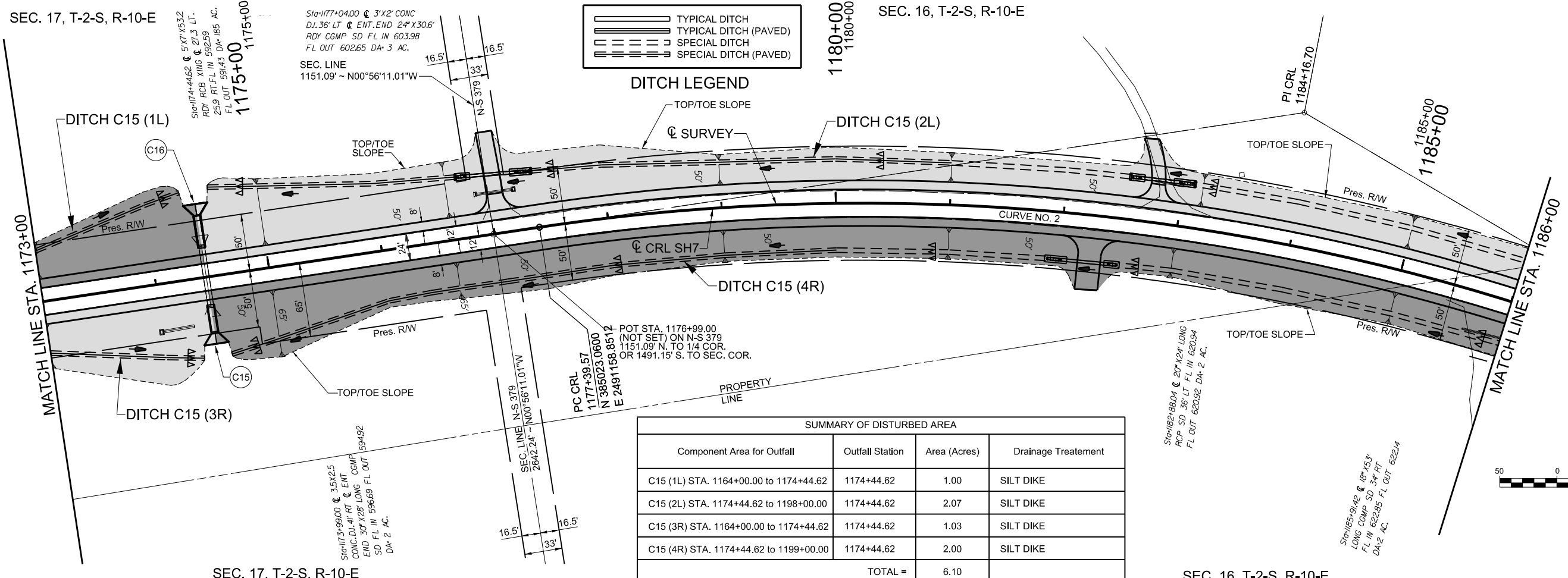
OKLAHOMA DEPARTMENT OF TRANSPORTATION
DESIGN DIVISION

EROSION CONTROL DETAIL (5)

JOB PIECE NO. 24066 (04) SHEET NO. R023

SEC. 17, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E



DITCH LEGEND

TOP/TOE SLOPE
 C SURVEY
 DITCH C15 (2L)
 DITCH C15 (4R)

CURL SH7

DITCH C15 (4R)

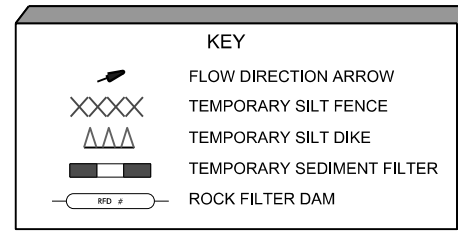
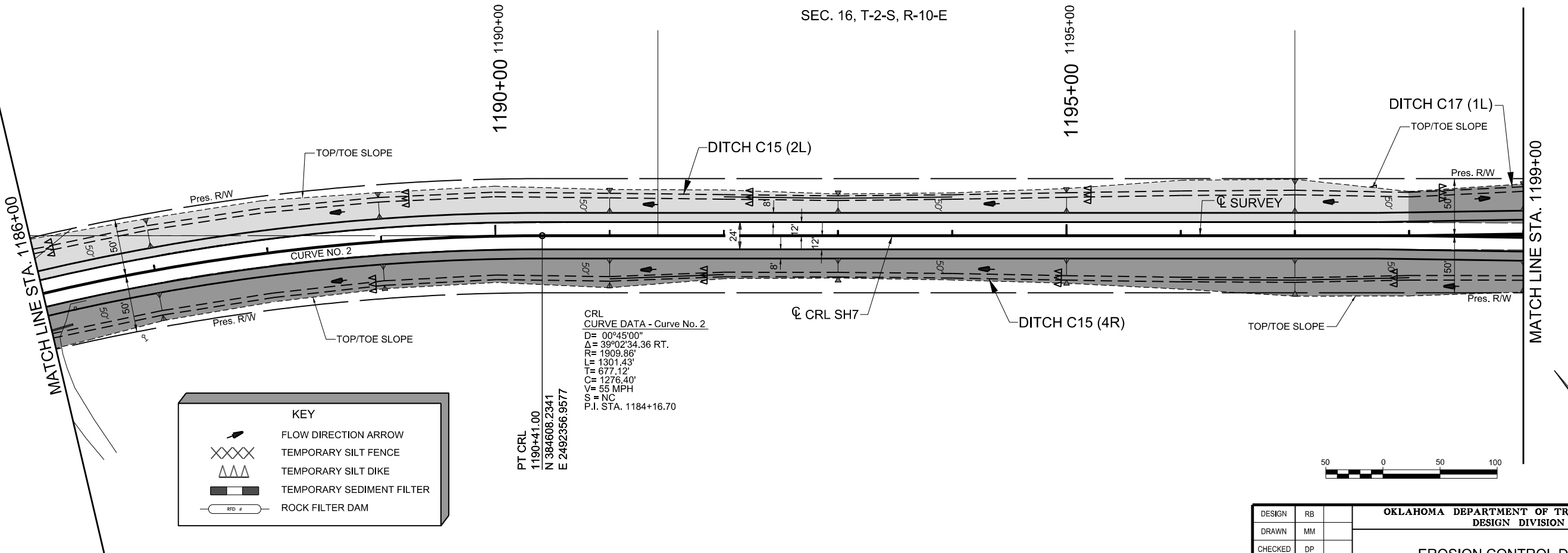
SUMMARY OF DISTURBED AREA

Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C15 (1L) STA. 1164+00.00 to 1174+44.62	1174+44.62	1.00	SILT DIKE
C15 (2L) STA. 1174+44.62 to 1198+00.00	1174+44.62	2.07	SILT DIKE
C15 (3R) STA. 1164+00.00 to 1174+44.62	1174+44.62	1.03	SILT DIKE
C15 (4R) STA. 1174+44.62 to 1199+00.00	1174+44.62	2.00	SILT DIKE
TOTAL =		6.10	

SEC. 17, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E



CURL
 CURVE DATA - Curve No. 2
 D = 00°45'00"
 Δ = 39°02'34.36 RT.
 TR = 1909.86'
 T = 1301.43'
 T = 677.12'
 C = 1276.40'
 V = 55 MPH
 S = NC
 P.I. STA. 1184+16.70

PT CRL
 1190+41.00
 N 384608.2341
 E 2492356.9577

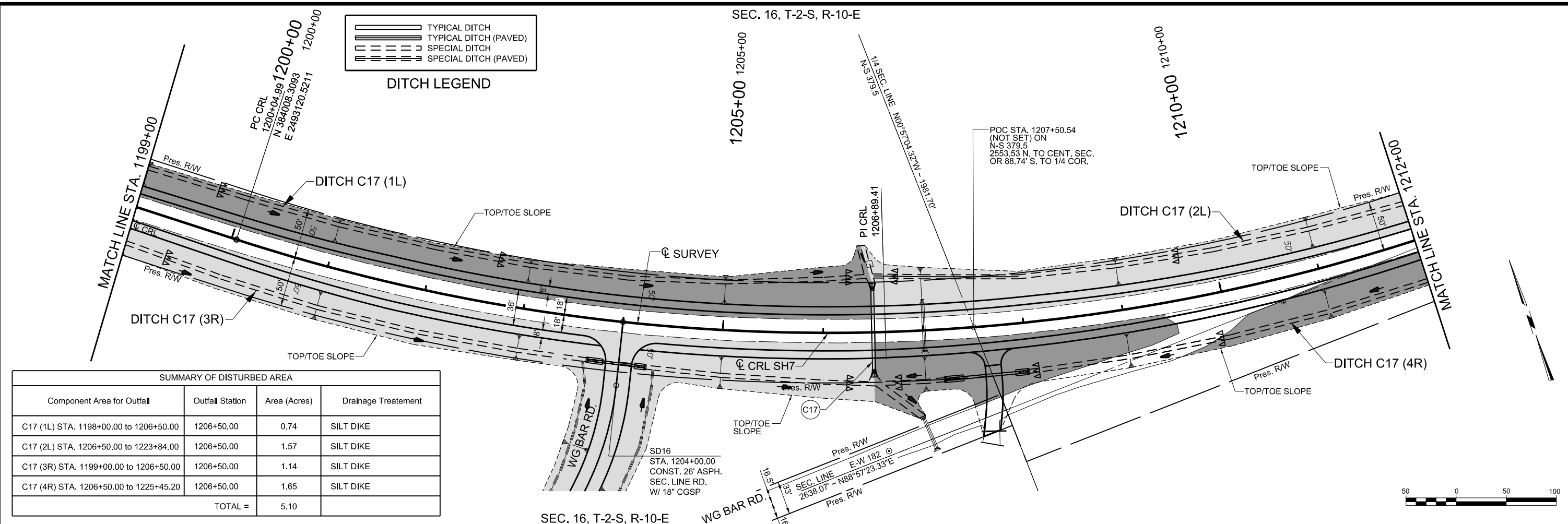


SEC. 16, T-2-S, R-10-E

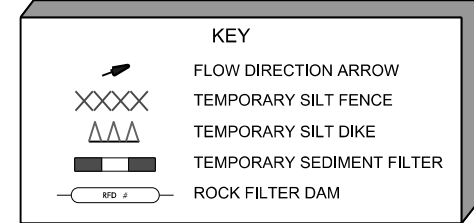
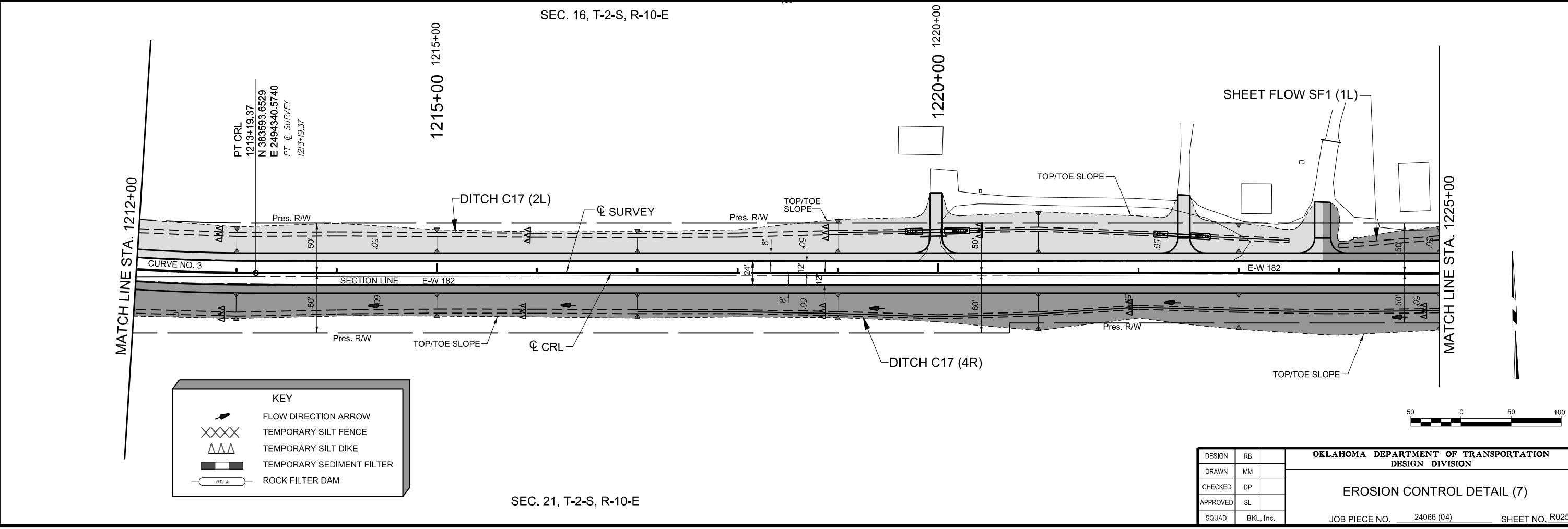
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DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

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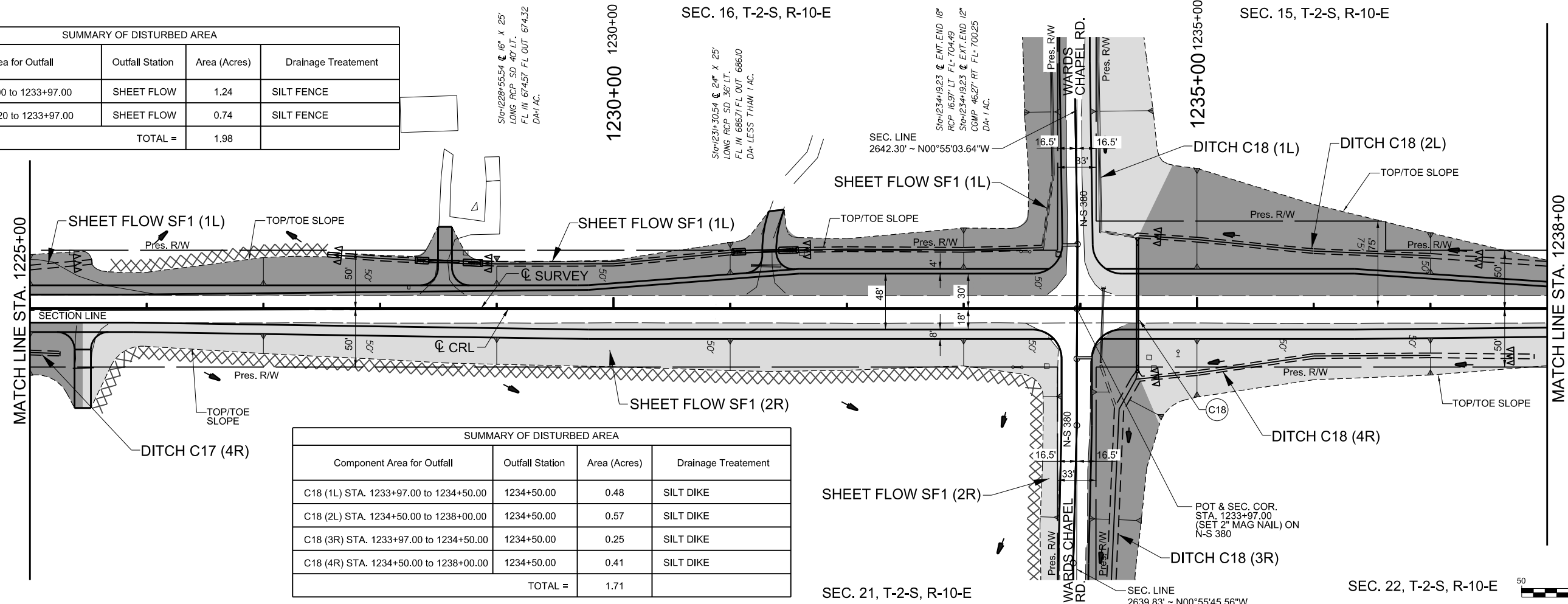


Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C17 (1L) STA. 1198+00.00 to 1206+50.00	1206+50.00	0.74	SILT DIKE
C17 (2L) STA. 1206+50.00 to 1223+84.00	1206+50.00	1.57	SILT DIKE
C17 (3R) STA. 1199+00.00 to 1206+50.00	1206+50.00	1.14	SILT DIKE
C17 (4R) STA. 1206+50.00 to 1225+45.20	1206+50.00	1.65	SILT DIKE
TOTAL =		5.10	



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION EROSION CONTROL DETAIL (7) JOB PIECE NO. 24066 (04) SHEET NO. R025
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

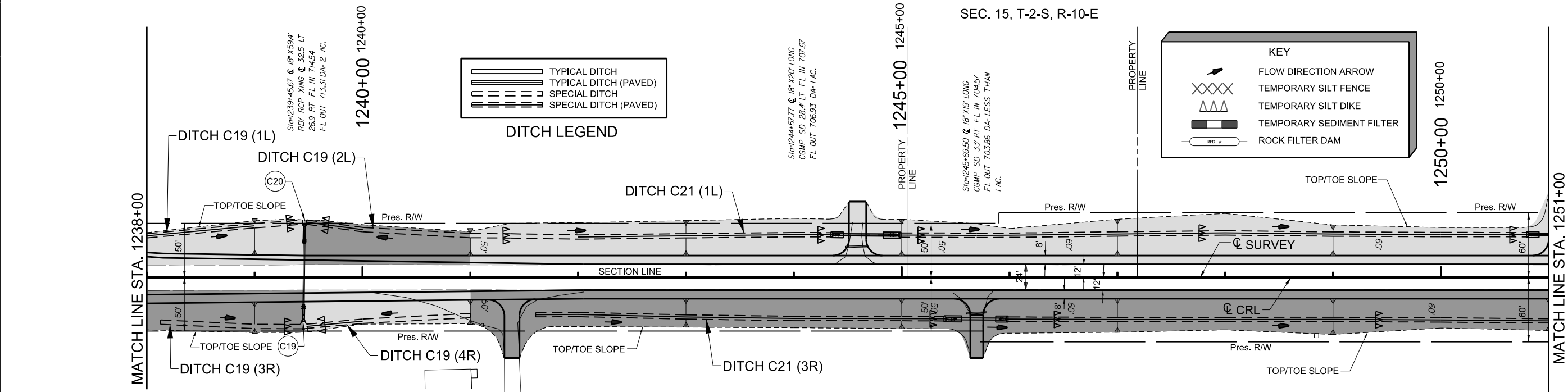
SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
SF1 (1L) STA. 1223+84.00 to 1233+97.00	SHEET FLOW	1.24	SILT FENCE
SF1 (2R) STA. 1225+45.20 to 1233+97.00	SHEET FLOW	0.74	SILT FENCE
TOTAL =		1.98	



SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C18 (1L) STA. 1233+97.00 to 1234+50.00	1234+50.00	0.48	SILT DIKE
C18 (2L) STA. 1234+50.00 to 1238+00.00	1234+50.00	0.57	SILT DIKE
C18 (3R) STA. 1233+97.00 to 1234+50.00	1234+50.00	0.25	SILT DIKE
C18 (4R) STA. 1234+50.00 to 1238+00.00	1234+50.00	0.41	SILT DIKE
TOTAL =		1.71	

DITCH LEGEND	
	TYPICAL DITCH
	TYPICAL DITCH (PAVED)
	SPECIAL DITCH
	SPECIAL DITCH (PAVED)

KEY	
	FLOW DIRECTION ARROW
	TEMPORARY SILT FENCE
	TEMPORARY SILT DIKE
	TEMPORARY SEDIMENT FILTER
	ROCK FILTER DAM

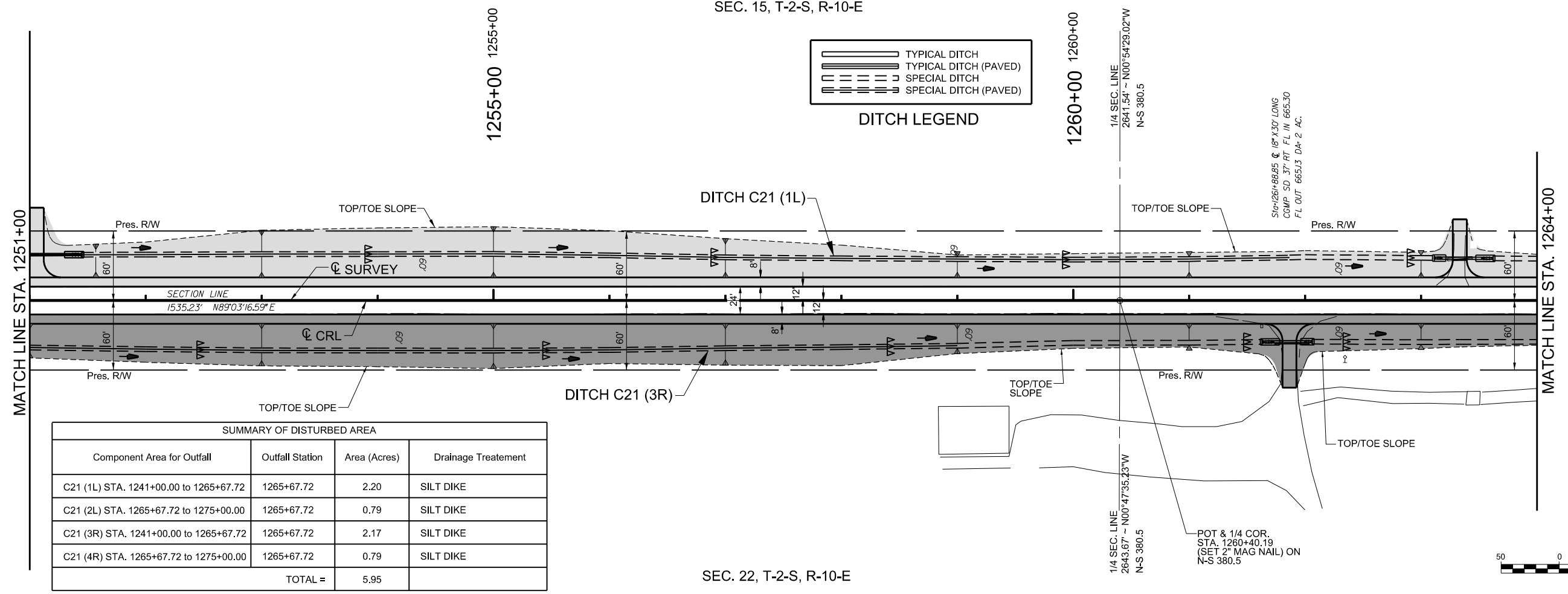


SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C19 (1L) STA. 1238+00.00 to 1239+45.67	1239+45.67	0.13	SILT DIKE
C19 (2L) STA. 1239+45.67 to 1241+00.00	1239+45.67	0.13	SILT DIKE
C19 (3R) STA. 1238+00.00 to 1239+45.67	1239+45.67	0.13	SILT DIKE
C19 (4R) STA. 1239+45.67 to 1241+00.00	1239+45.67	0.12	SILT DIKE
TOTAL =		0.51	

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION EROSION CONTROL DETAIL (8) JOB PIECE NO. 24066 (04) SHEET NO. R026
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

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SEC. 15, T-2-S, R-10-E



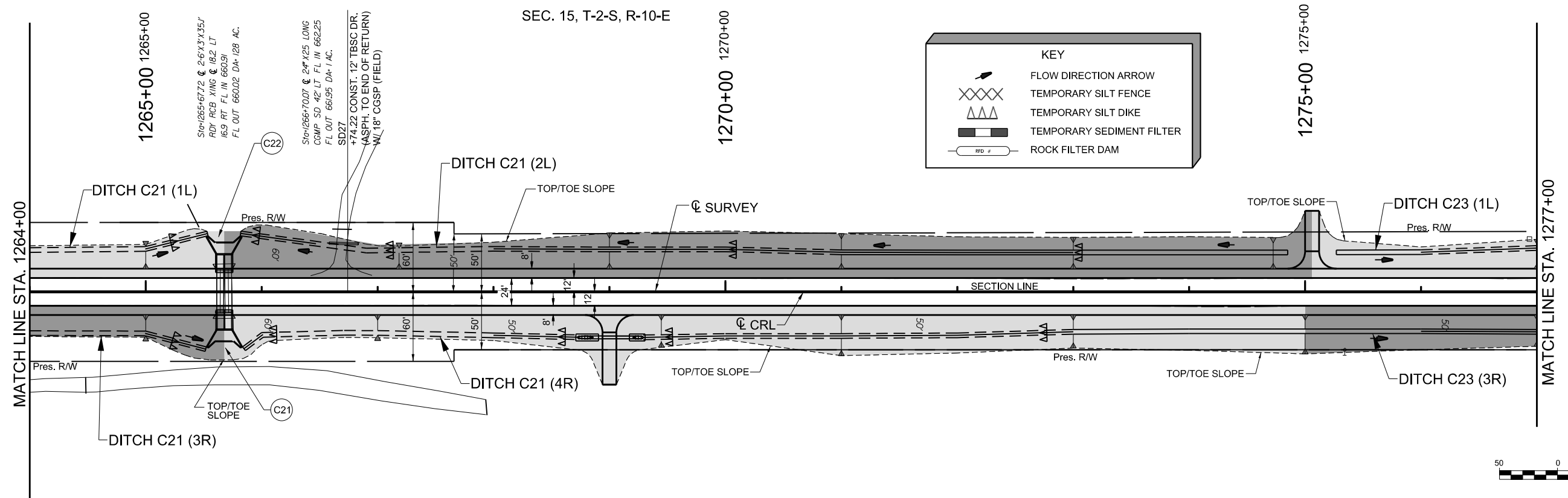
SUMMARY OF DISTURBED AREA

Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C21 (1L) STA. 1241+00.00 to 1265+67.72	1265+67.72	2.20	SILT DIKE
C21 (2L) STA. 1265+67.72 to 1275+00.00	1265+67.72	0.79	SILT DIKE
C21 (3R) STA. 1241+00.00 to 1265+67.72	1265+67.72	2.17	SILT DIKE
C21 (4R) STA. 1265+67.72 to 1275+00.00	1265+67.72	0.79	SILT DIKE
TOTAL =		5.95	

SEC. 22, T-2-S, R-10-E



SEC. 15, T-2-S, R-10-E



KEY

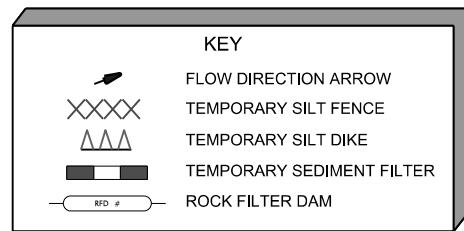
- Flow Direction Arrow
- Temporary Silt Fence
- Temporary Silt DiKE
- Temporary Sediment Filter
- Rock Filter Dam



SEC. 22, T-2-S, R-10-E

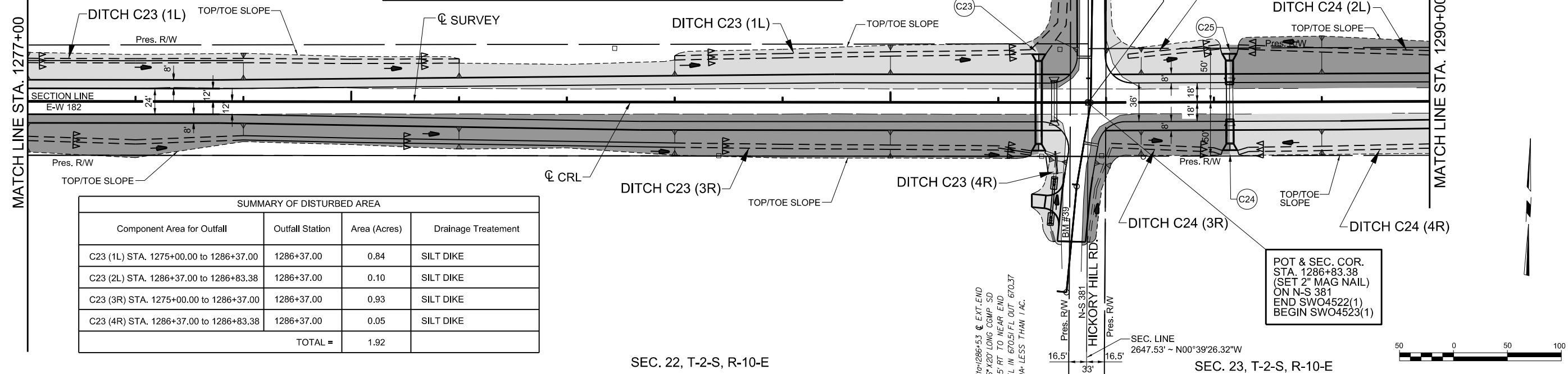
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DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

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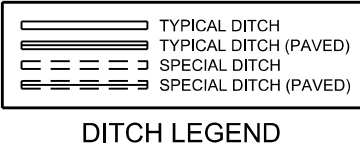
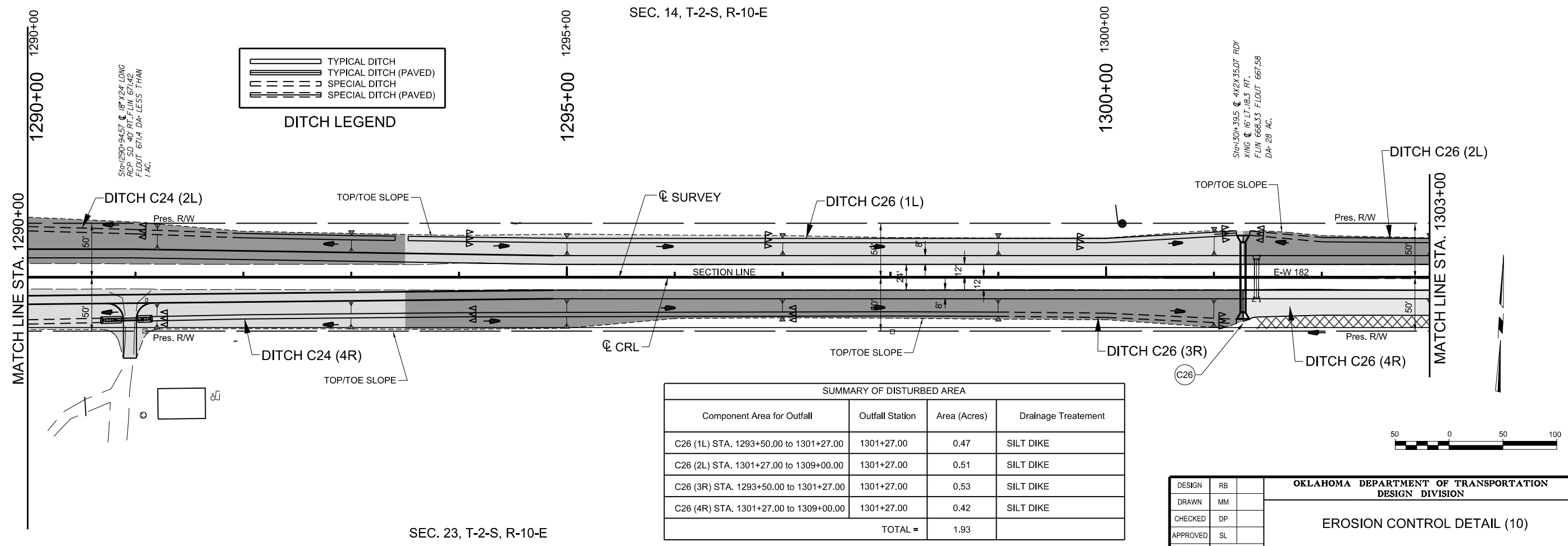
SUMMARY OF DISTURBED AREA

Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C24 (1L) STA. 1286+83.38 to 1288+14.60	1288+14.60	0.11	SILT DIKE
C24 (2L) STA. 1288+14.60 to 1293+50.00	1288+14.60	0.47	SILT DIKE
C24 (3R) STA. 1286+83.38 to 1288+14.60	1288+14.60	0.14	SILT DIKE
C24 (4R) STA. 1288+14.60 to 1293+50.00	1288+14.60	0.47	SILT DIKE
TOTAL =		1.19	



SUMMARY OF DISTURBED AREA

Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C23 (1L) STA. 1275+00.00 to 1286+37.00	1286+37.00	0.84	SILT DIKE
C23 (2L) STA. 1286+37.00 to 1286+83.38	1286+37.00	0.10	SILT DIKE
C23 (3R) STA. 1275+00.00 to 1286+37.00	1286+37.00	0.93	SILT DIKE
C23 (4R) STA. 1286+37.00 to 1286+83.38	1286+37.00	0.05	SILT DIKE
TOTAL =		1.92	



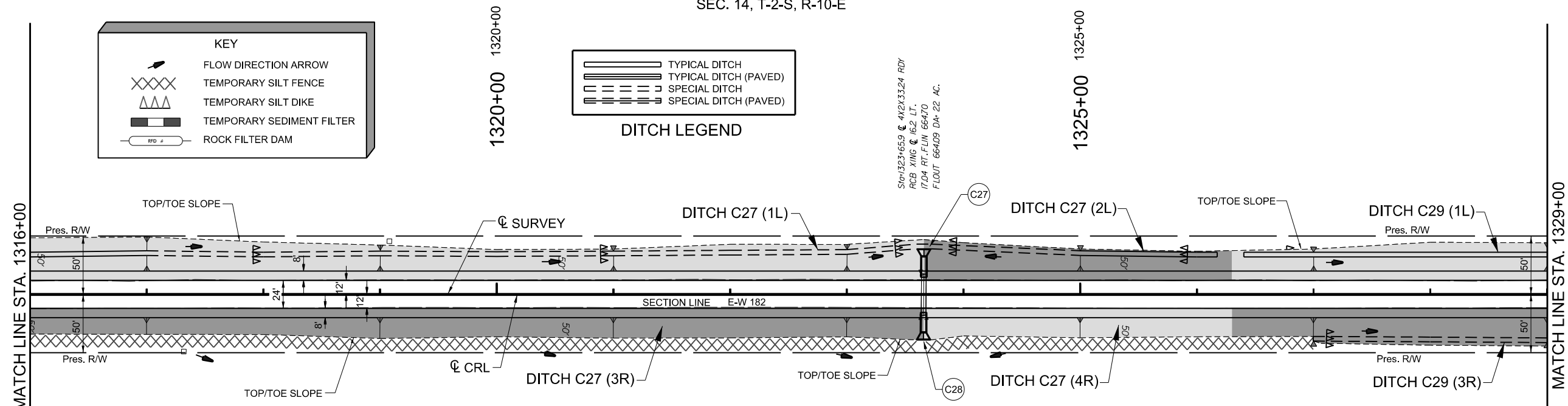
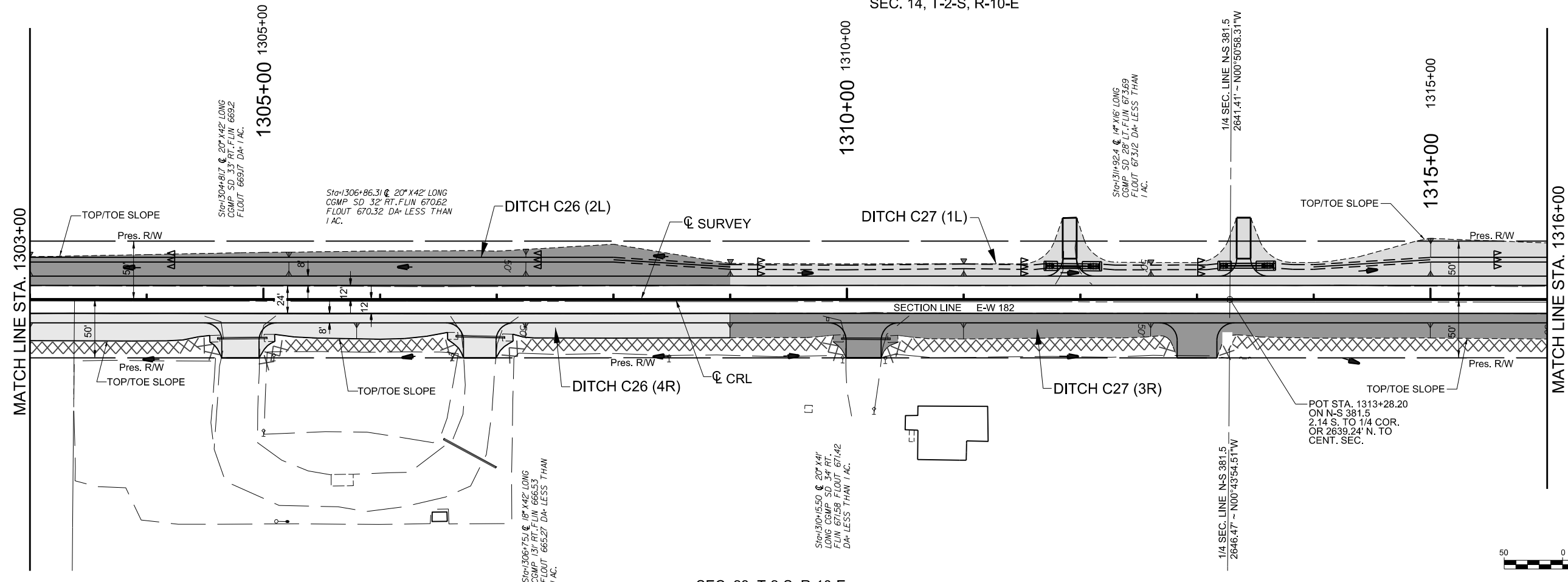
SUMMARY OF DISTURBED AREA

Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C26 (1L) STA. 1293+50.00 to 1301+27.00	1301+27.00	0.47	SILT DIKE
C26 (2L) STA. 1301+27.00 to 1309+00.00	1301+27.00	0.51	SILT DIKE
C26 (3R) STA. 1293+50.00 to 1301+27.00	1301+27.00	0.53	SILT DIKE
C26 (4R) STA. 1301+27.00 to 1309+00.00	1301+27.00	0.42	SILT DIKE
TOTAL =		1.93	



DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION EROSION CONTROL DETAIL (10) JOB PIECE NO. 24066 (04) SHEET NO. R028
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

PRINT DATE: 5/25/2018 C:\Projects\150-SHT Final Plans\04-Erosion Control\10.dwg



KEY

- FLOW DIRECTION ARROW
- TEMPORARY SILT FENCE
- TEMPORARY SILT DIKE
- TEMPORARY SEDIMENT FILTER
- ROCK FILTER DAM

DITCH LEGEND

- TYPICAL DITCH
- TYPICAL DITCH (PAVED)
- SPECIAL DITCH
- SPECIAL DITCH (PAVED)

SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C27 (1L) STA. 1309+00.00 to 1325+65.90	1325+65.90	0.97	SILT DIKE
C27 (2L) STA. 1301+27.00 to 1326+30.00	1325+65.90	0.17	SILT DIKE
C27 (3R) STA. 1309+00.00 to 1325+65.90	1325+65.90	0.81	SILT DIKE
C27 (4R) STA. 1325+65.90 to 1326+30.00	1325+65.90	0.15	SILT DIKE
TOTAL =		2.10	

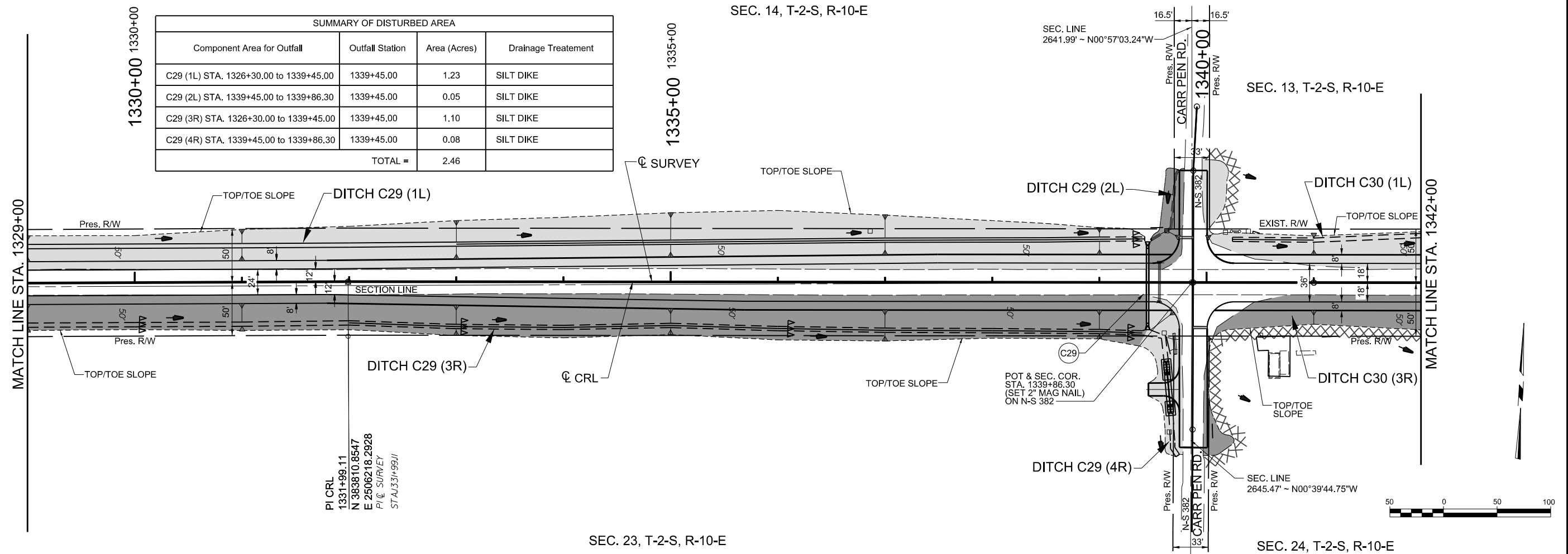


DESIGN	RB	<p align="center">OKLAHOMA DEPARTMENT OF TRANSPORTATION</p> <p align="center">DESIGN DIVISION</p> <p align="center">EROSION CONTROL DETAIL (11)</p> <p align="center">JOB PIECE NO. 24066 (04) SHEET NO. R029</p>
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

SEC. 14, T-2-S, R-10-E

SEC. 13, T-2-S, R-10-E

SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C29 (1L) STA. 1326+30.00 to 1339+45.00	1339+45.00	1.23	SILT DIKE
C29 (2L) STA. 1339+45.00 to 1339+86.30	1339+45.00	0.05	SILT DIKE
C29 (3R) STA. 1326+30.00 to 1339+45.00	1339+45.00	1.10	SILT DIKE
C29 (4R) STA. 1339+45.00 to 1339+86.30	1339+45.00	0.08	SILT DIKE
TOTAL =		2.46	

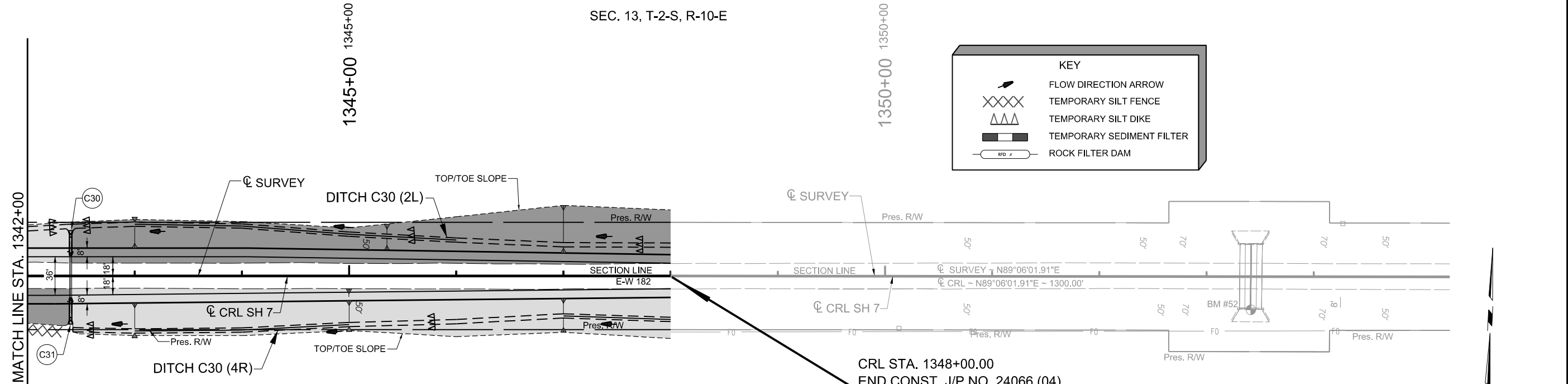


SEC. 23, T-2-S, R-10-E

SEC. 24, T-2-S, R-10-E

SEC. 13, T-2-S, R-10-E

KEY	
	FLOW DIRECTION ARROW
	TEMPORARY SILT FENCE
	TEMPORARY SILT DIKE
	TEMPORARY SEDIMENT FILTER
	ROCK FILTER DAM



SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
C30 (1L) STA. 1339+86.30 to 1342+40.20	1342+40.20	0.19	SILT DIKE
C30 (2L) STA. 1342+40.20 to 1348+00.00	1342+40.20	0.55	SILT DIKE
C30 (3R) STA. 1339+86.30 to 1342+40.20	1342+40.20	0.17	SILT DIKE
C30 (4R) STA. 1342+40.20 to 1348+00.00	1342+40.20	0.50	SILT DIKE
TOTAL =		1.41	

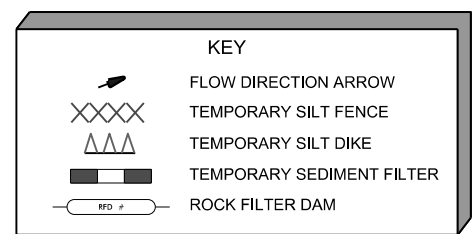
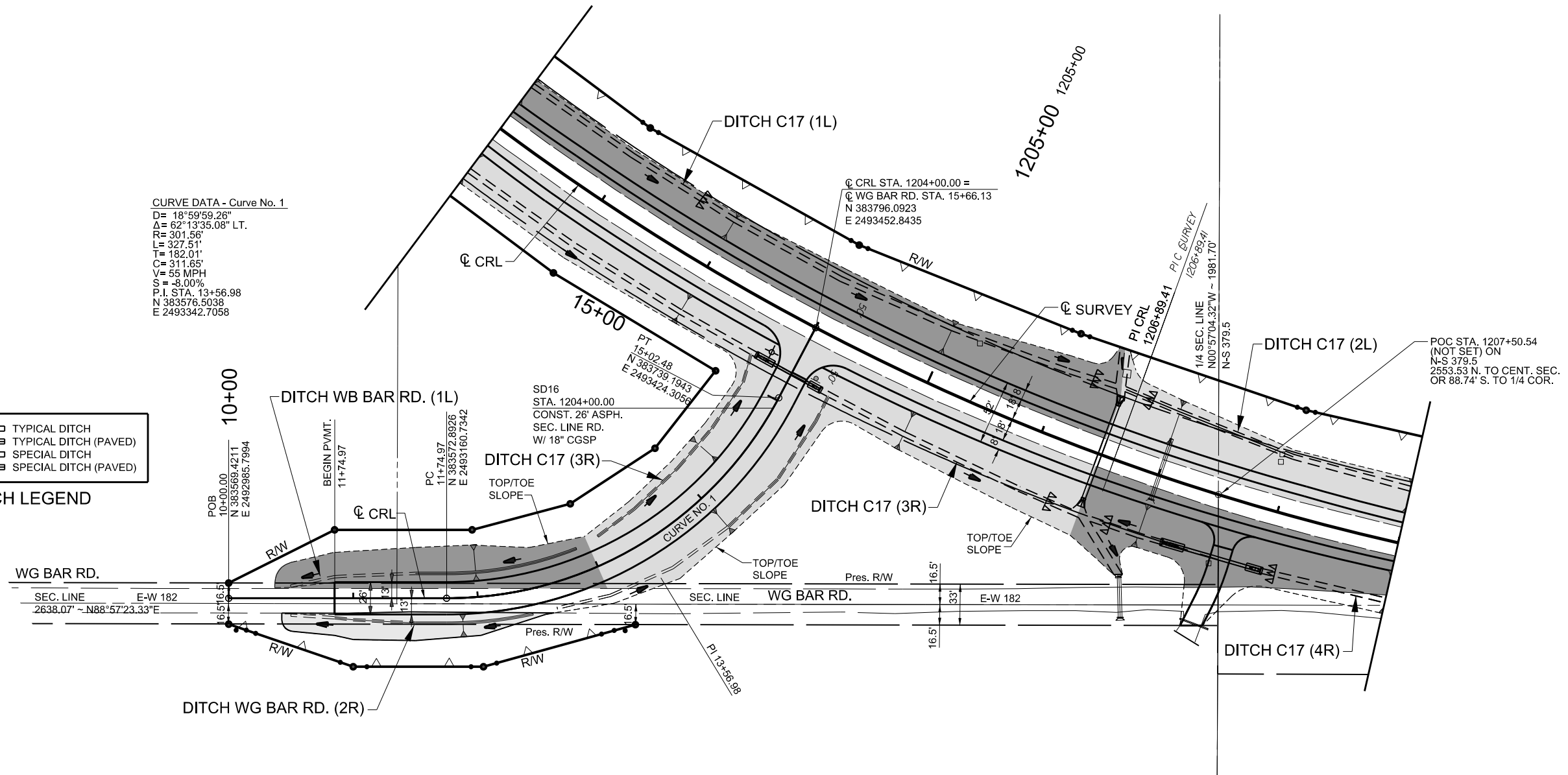
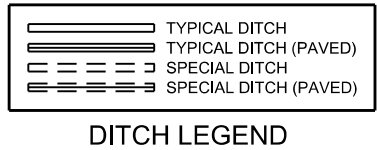
CRL STA. 1348+00.00
 END CONST. J/P NO. 24066 (04)
 BEGIN CONST. J/P NO. 24066 (07)
 SUBSECTION NO. 11.40

SEC. 24, T-2-S, R-10-E

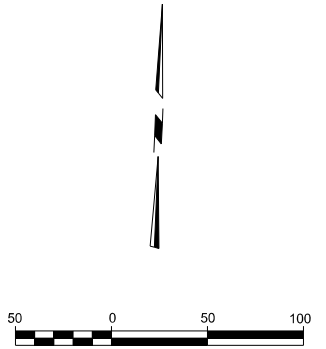
DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION EROSION CONTROL DETAIL (12) JOB PIECE NO. 24066 (04) SHEET NO. R030
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

PRINT DATE: 6/21/2018 C:\Projects\650-SHT Final Plans\OGN\R030-24066(04)-Erosion Control 12.dgn

CURVE DATA - Curve No. 1
 D= 18°59'59.26"
 Δ= 62°13'35.08" LT.
 R= 301.56'
 L= 327.51'
 T= 182.01'
 C= 311.65'
 V= 55 MPH
 S = -8.00%
 P.I. STA. 13+56.98
 N 383576.5038
 E 2493342.7058

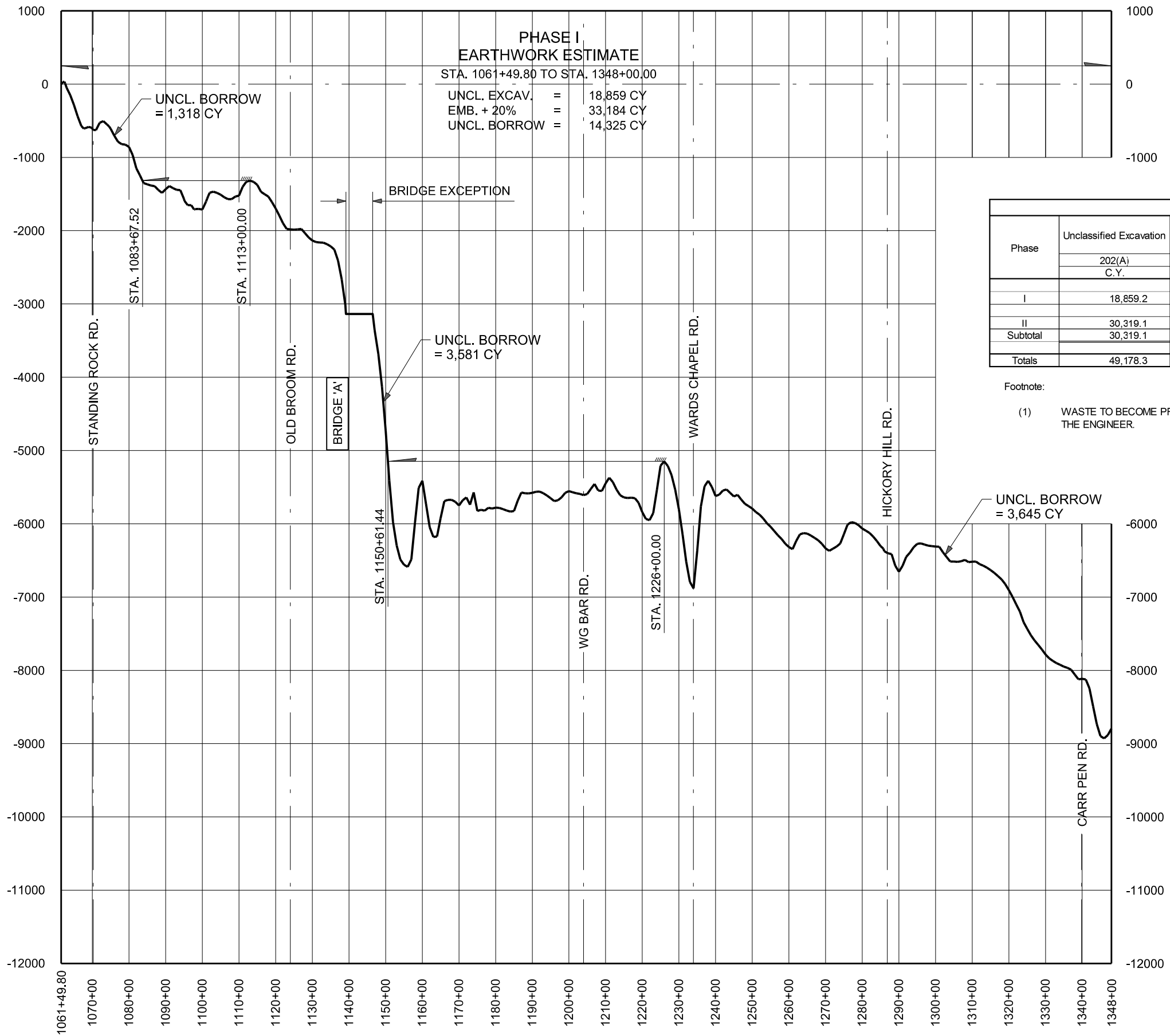


SUMMARY OF DISTURBED AREA			
Component Area for Outfall	Outfall Station	Area (Acres)	Drainage Treatment
WG BAR RD. (1L) STA. 10+40.00 to 13+00.00	10+40.00	0.19	SILT DIKE
WG BAR RD. (2R) STA. 10+40.00 to 13+00.00	10+40.00	0.08	SILT DIKE
TOTAL =		0.27	



DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION EROSION CONTROL DETAIL (13) JOB PIECE NO. 24066 (04) SHEET NO. R031
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

PRINT DATE: 5/25/2018 C:\Projects\650-SHT Final Plans\031-24066(04)-Erosion Control 13.dwg



**PHASE I
EARTHWORK ESTIMATE**
STA. 1061+49.80 TO STA. 1348+00.00

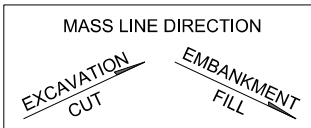
UNCL. EXCAV.	=	18,859 CY
EMB. + 20%	=	33,184 CY
UNCL. BORROW	=	14,325 CY

SUMMARY OF EARTHWORK					
Phase	Unclassified Excavation	Embankment + 20%	Excess Excavation	Unclassified Borrow	Waste
	202(A) C.Y.	202(D) C.Y.			
I	18,859.2	33,183.7	0.0	14,324.5	0.0
II	30,319.1	25,631.6	4,687.5	0.0	4,687.5
Subtotal	30,319.1			0.0	4,687.5 (1)
Totals	49,178.3	58,815.3	4,687.5	14,324.5	4,687.5

Footnote:
(1) WASTE TO BECOME PROPERTY OF THE CONTRACTOR AND DISPOSED OF IN A MANNER APPROVED BY THE ENGINEER.

PRINT DATE: 5/16/2018 C:\Projects\550-SHT Final Plans\OGN\R032-24066(04)-Mass Diagram Phase I.dgn

MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS, WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTION FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



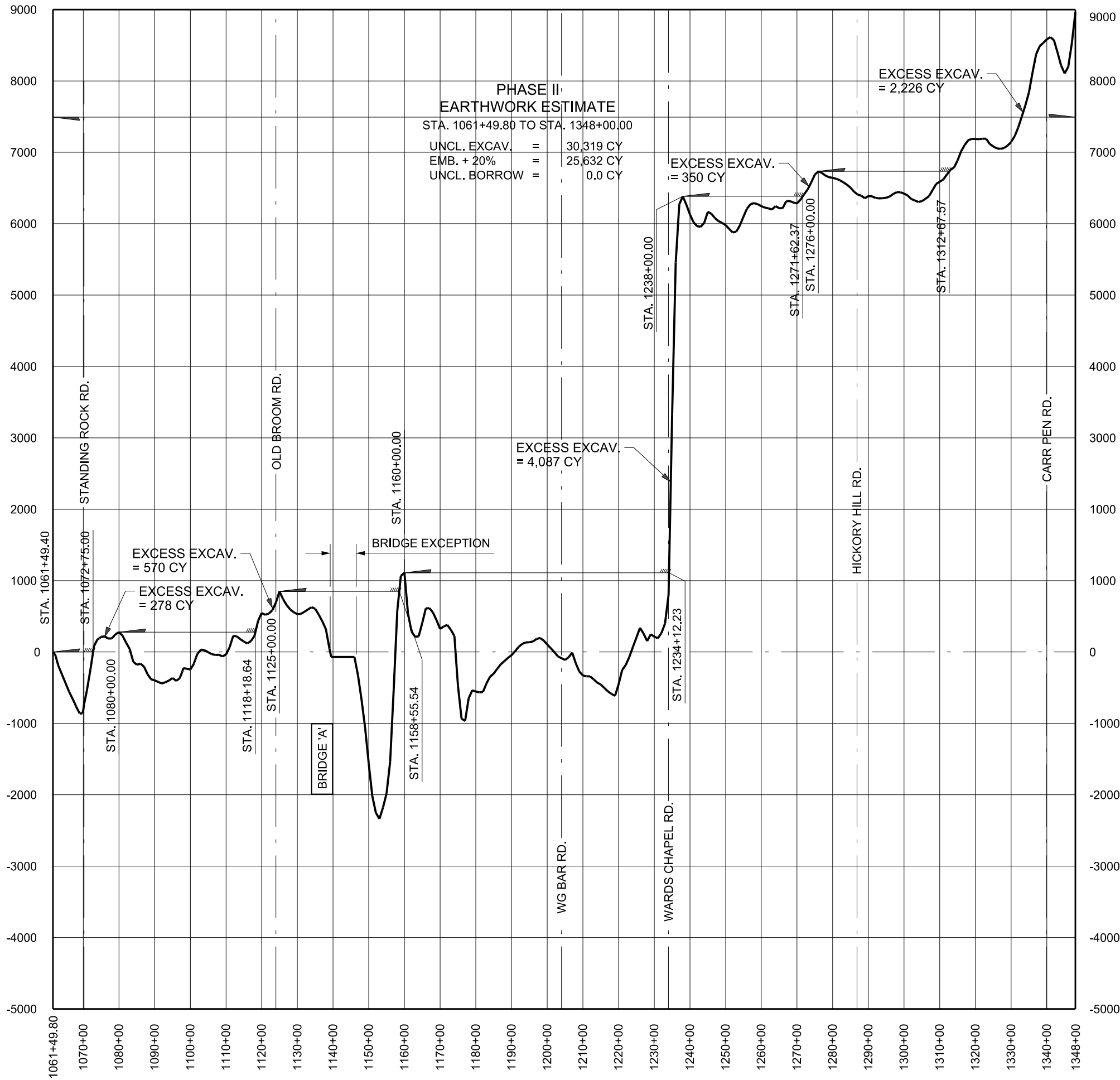
DESIGN	RB
DRAWN	MM
CHECKED	DP
APPROVED	SL
SQUAD	BKL, Inc.

OKLAHOMA DEPARTMENT OF TRANSPORTATION
DESIGN DIVISION

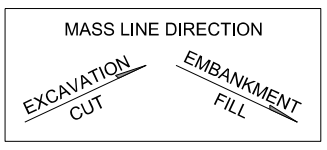
**MASS DIAGRAM (1)
(PHASE I)**

JOB PIECE NO. 24066 (04) SHEET NO. R032

PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\OGN\R033-24066(04) Mass Diagram Phase II.dgn



MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTION FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.

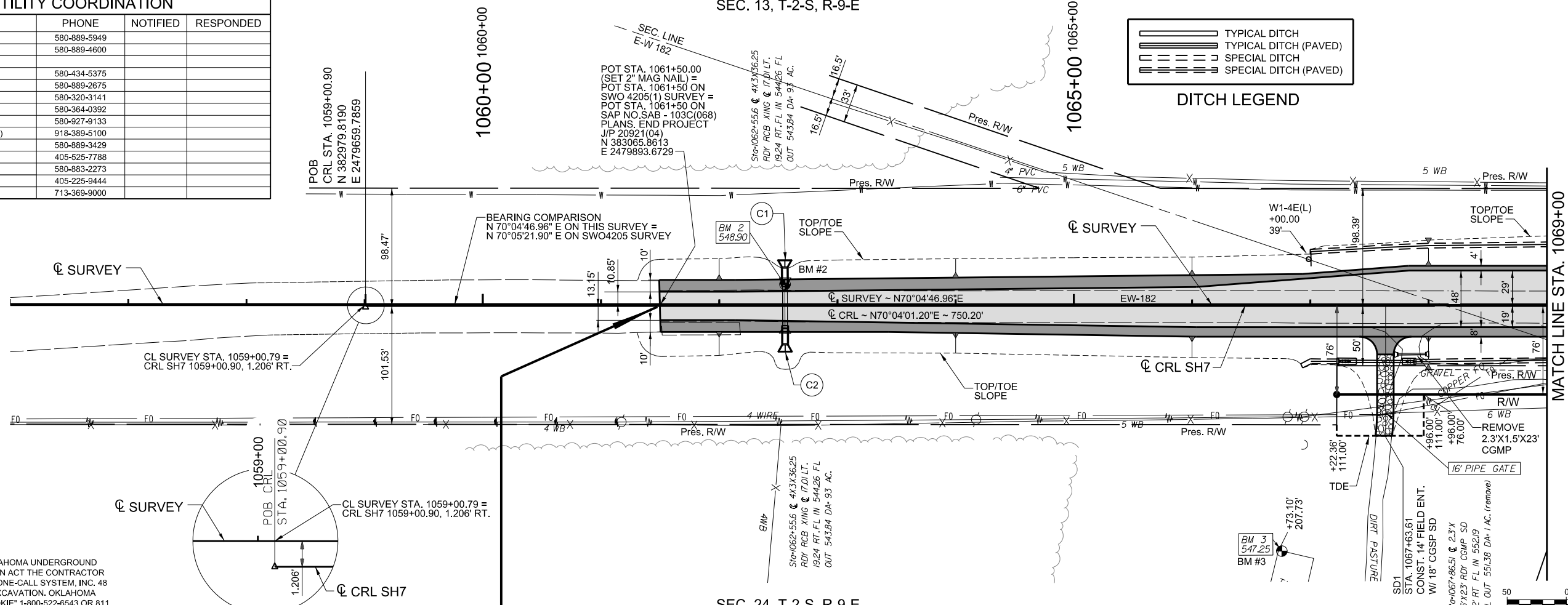
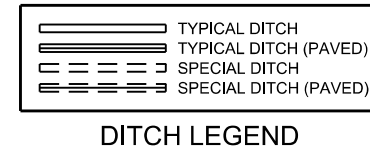


DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MASS DIAGRAM (2) (PHASE II) JOB PIECE NO. 24066 (04) SHEET NO. R033
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

UTILITY COORDINATION

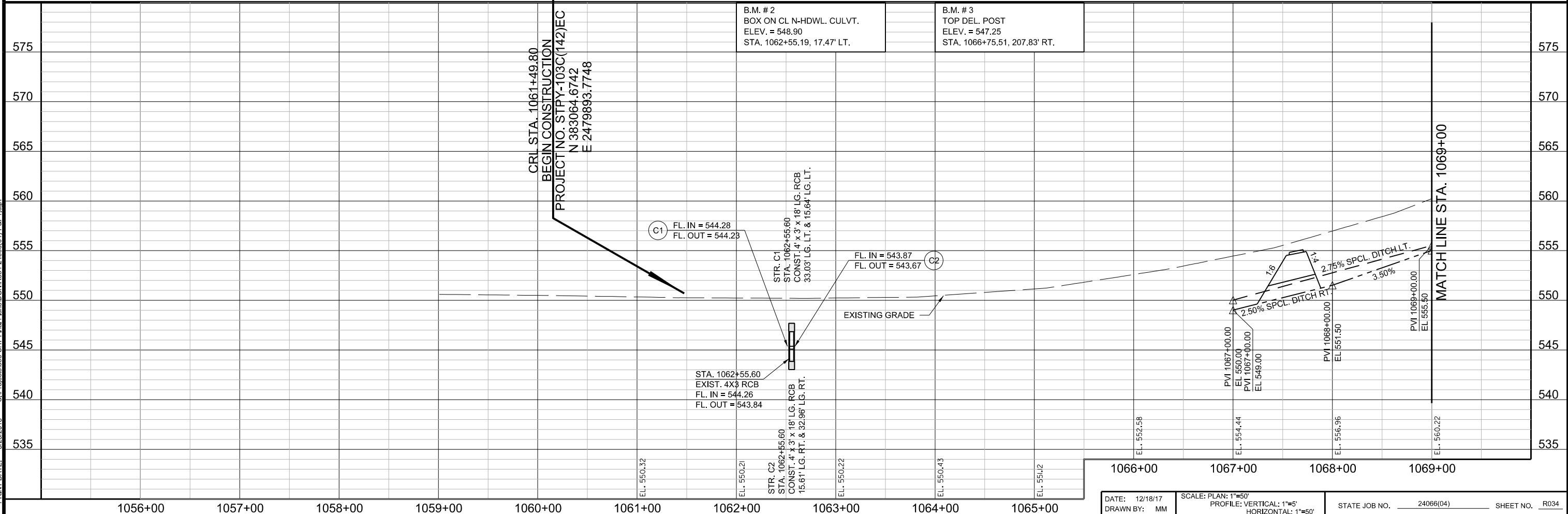
UTILITY	PHONE	NOTIFIED	RESPONDED
ATOKA RURAL WATER DIST. # 2	580-889-5949		
AEP/PSO	580-889-4600		
AT&T			
CHEROKEE COMMUNICATIONS	580-434-5375		
CITY OF ATOKA	580-889-2675		
SCISSOR TAIL	580-320-3141		
ENOGEX	580-364-0392		
ANTERO RESOURCES CORP.	580-927-9133		
MARK WEST, LLC (PRIVATE OWNER)	918-389-5100		
CENTERPOINT ENERGY	580-889-3429		
ENABLE MIDSTREAM PARTNERS	405-525-7788		
ATLAS PIPELINE	580-883-2273		
ONENET	405-225-9444		
KINDER-MORGAN	713-369-9000		

SEC. 13, T-2-S, R-9-E



IN ACCORDANCE WITH THE OKLAHOMA UNDERGROUND FACILITIES DAMAGE PREVENTION ACT THE CONTRACTOR SHALL NOTIFY THE OKLAHOMA ONE-CALL SYSTEM, INC. 48 HOURS PRIOR TO BEGINNING EXCAVATION. OKLAHOMA ONE-CALL SYSTEM, INC. "CALL OKIE" 1-800-522-6543 OR 811.

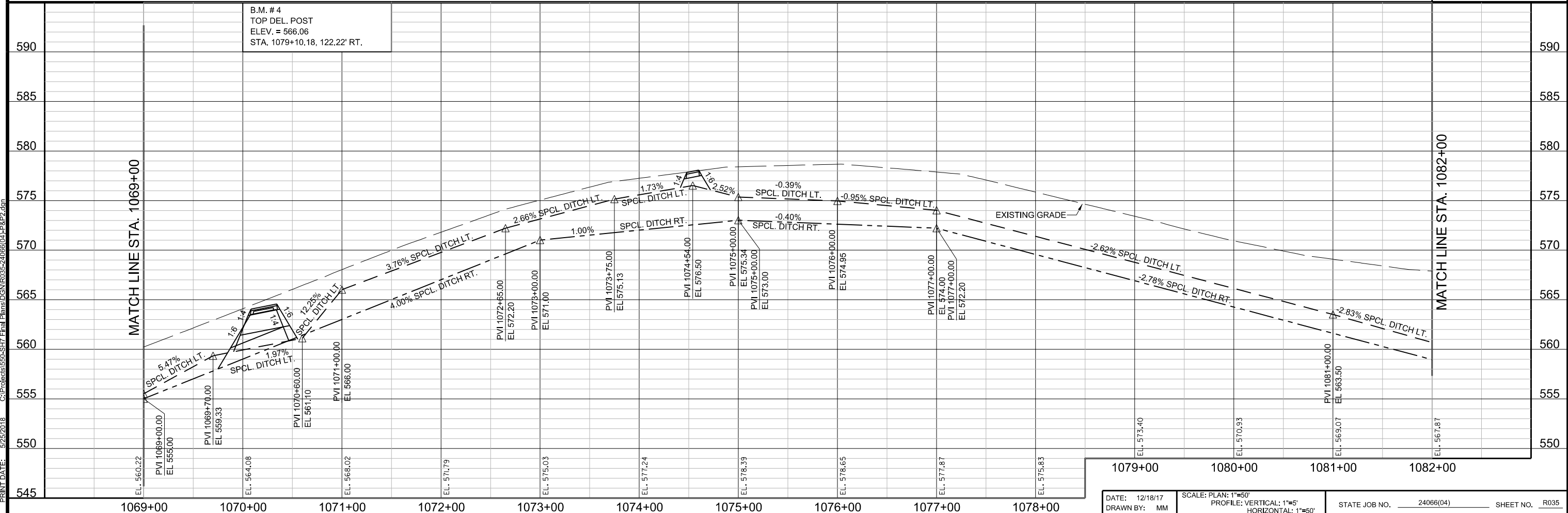
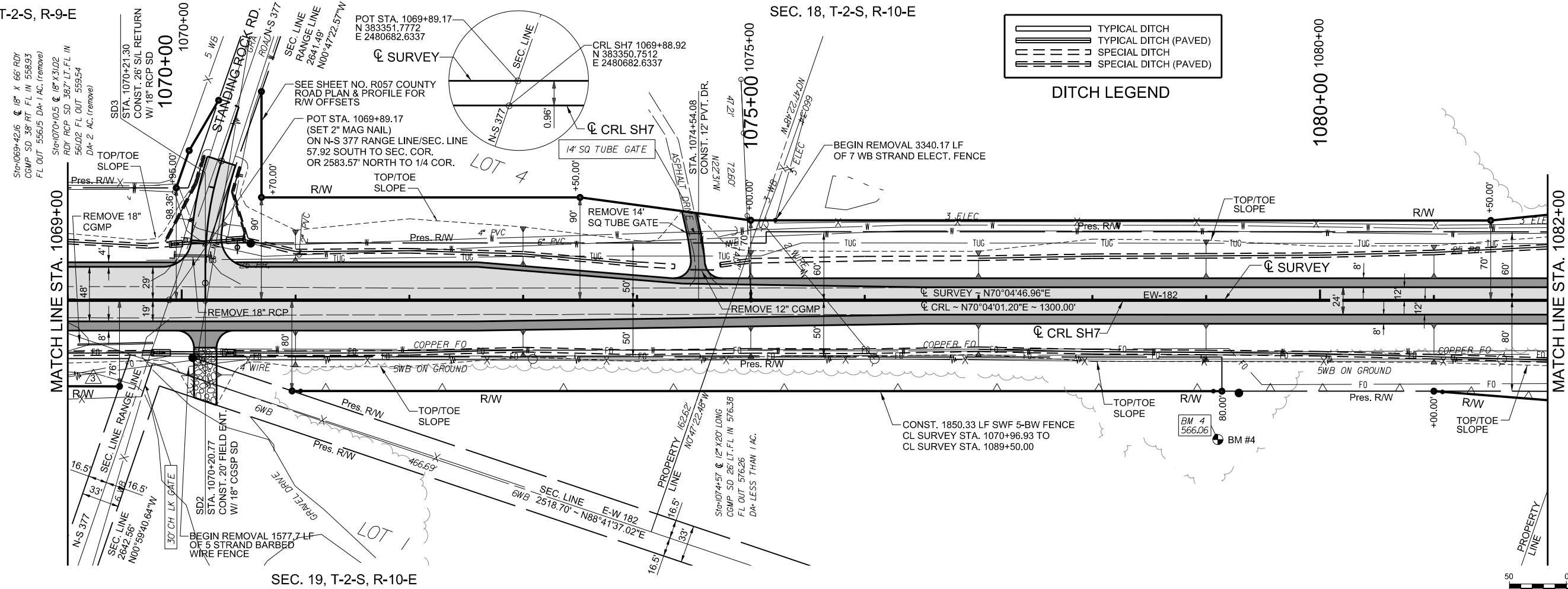
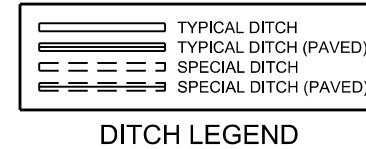
SEC. 24, T-2-S, R-9-E



SEC. 13, T-2-S, R-9-E

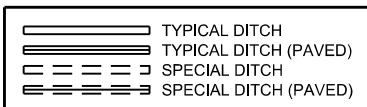
SEC. 18, T-2-S, R-10-E

SEC. 19, T-2-S, R-10-E

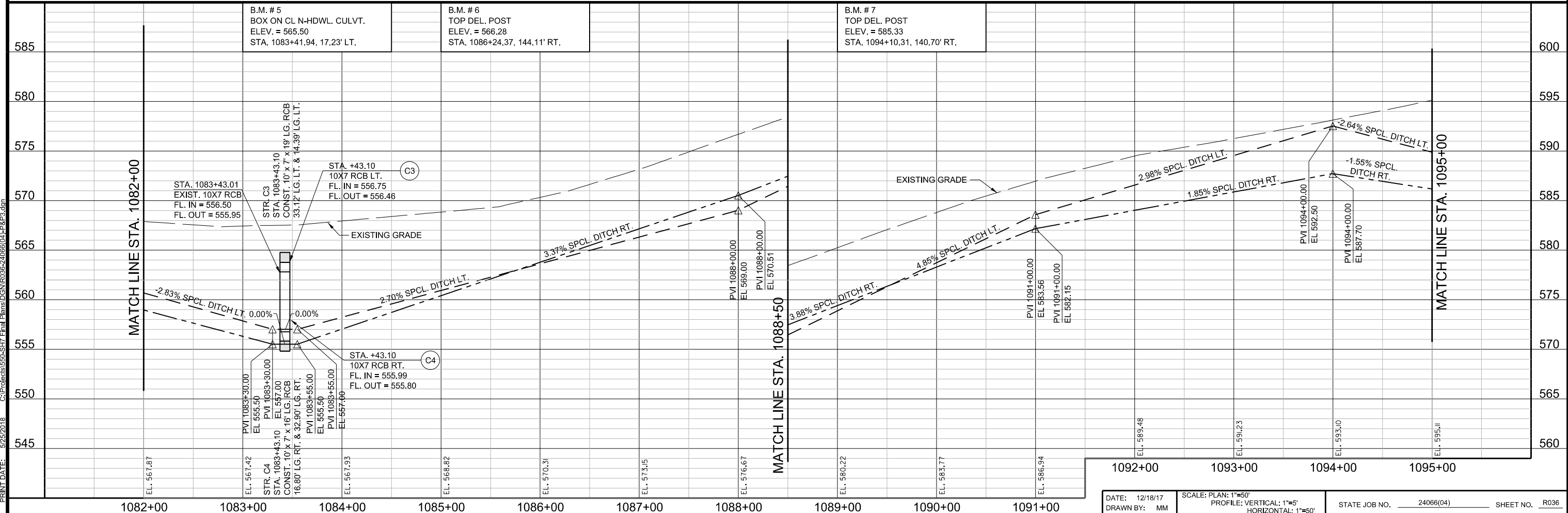
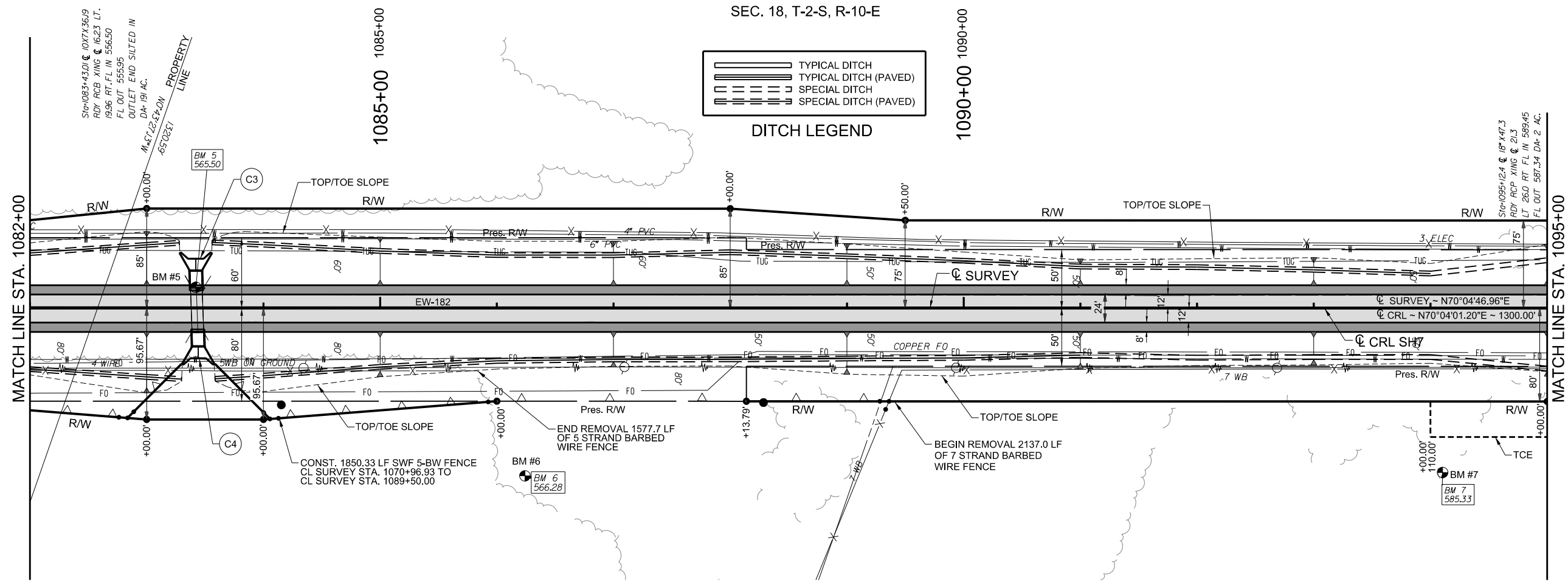


PRINT DATE: 5/25/2018 C:\Projects\1550-SHT Final Plans\1550-SHT-24066(04)1-P&P2.dgn

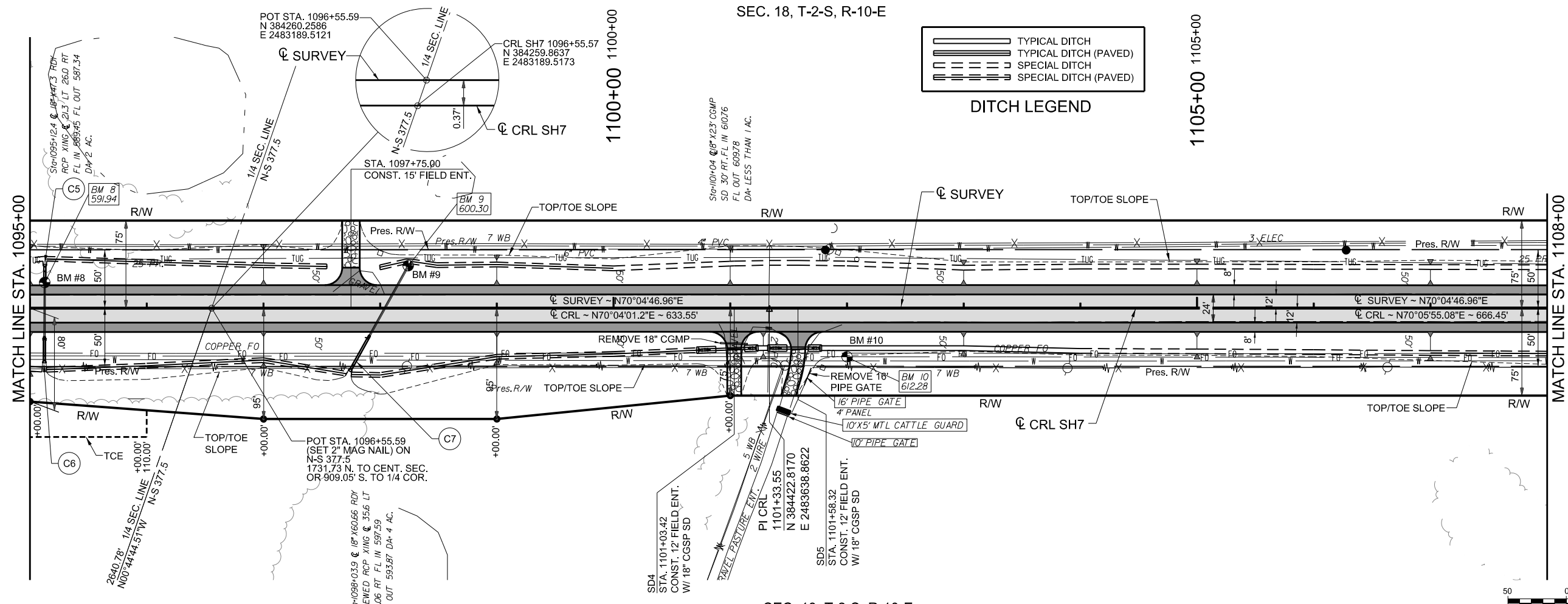
1090+00 1095+00



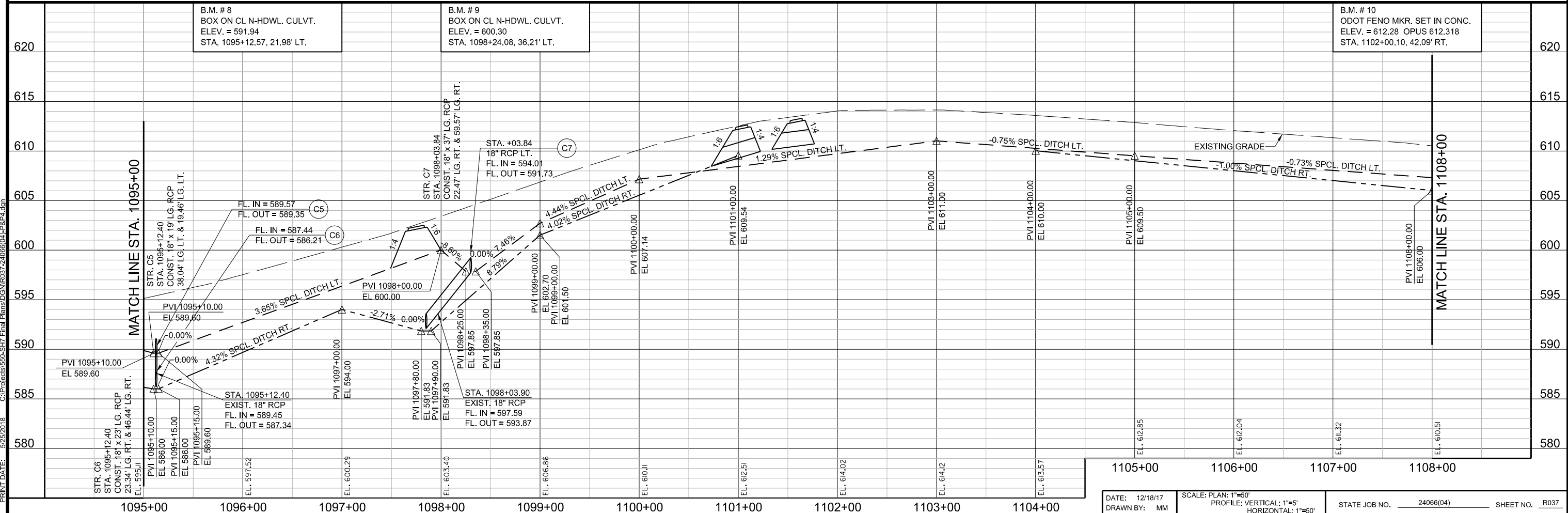
DITCH LEGEND



SEC. 18, T-2-S, R-10-E

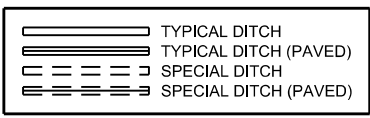
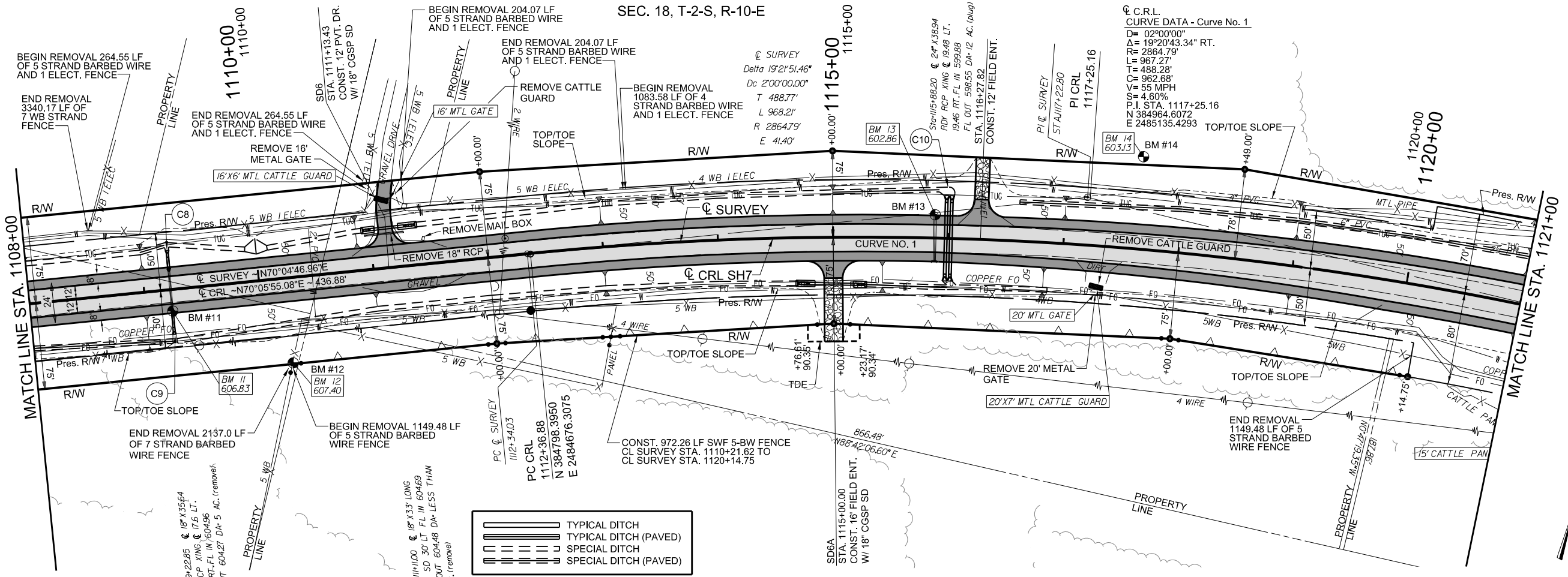


SEC. 18, T-2-S, R-10-E



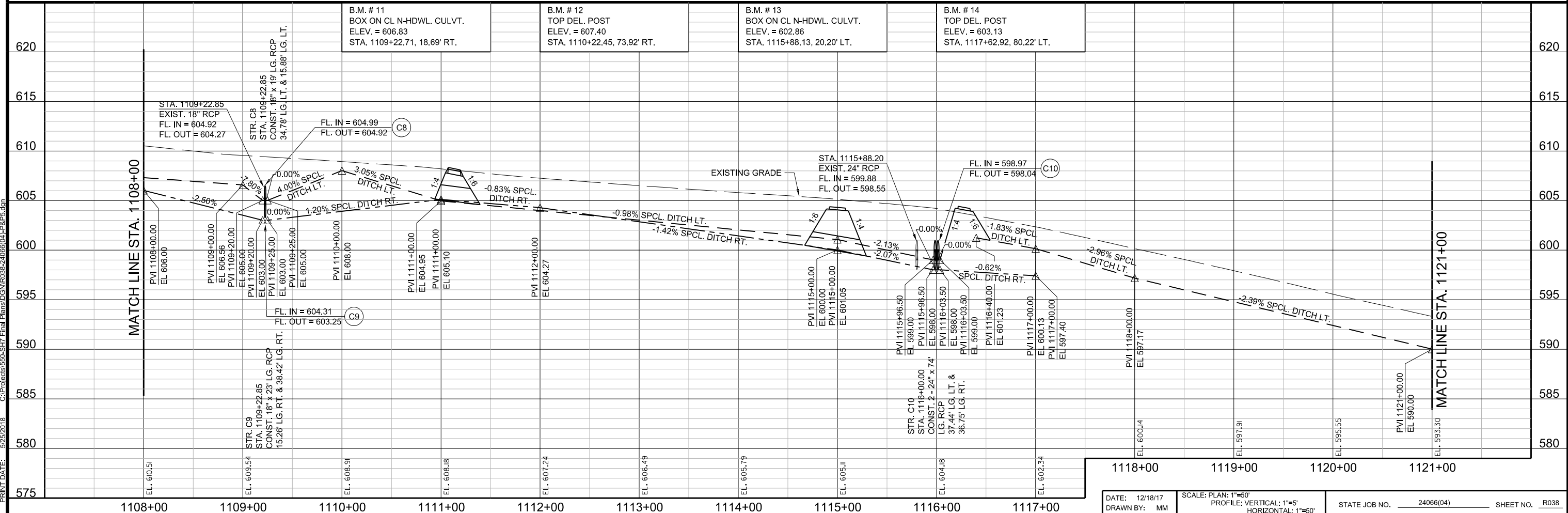
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SEC. 18, T-2-S, R-10-E



DITCH LEGEND

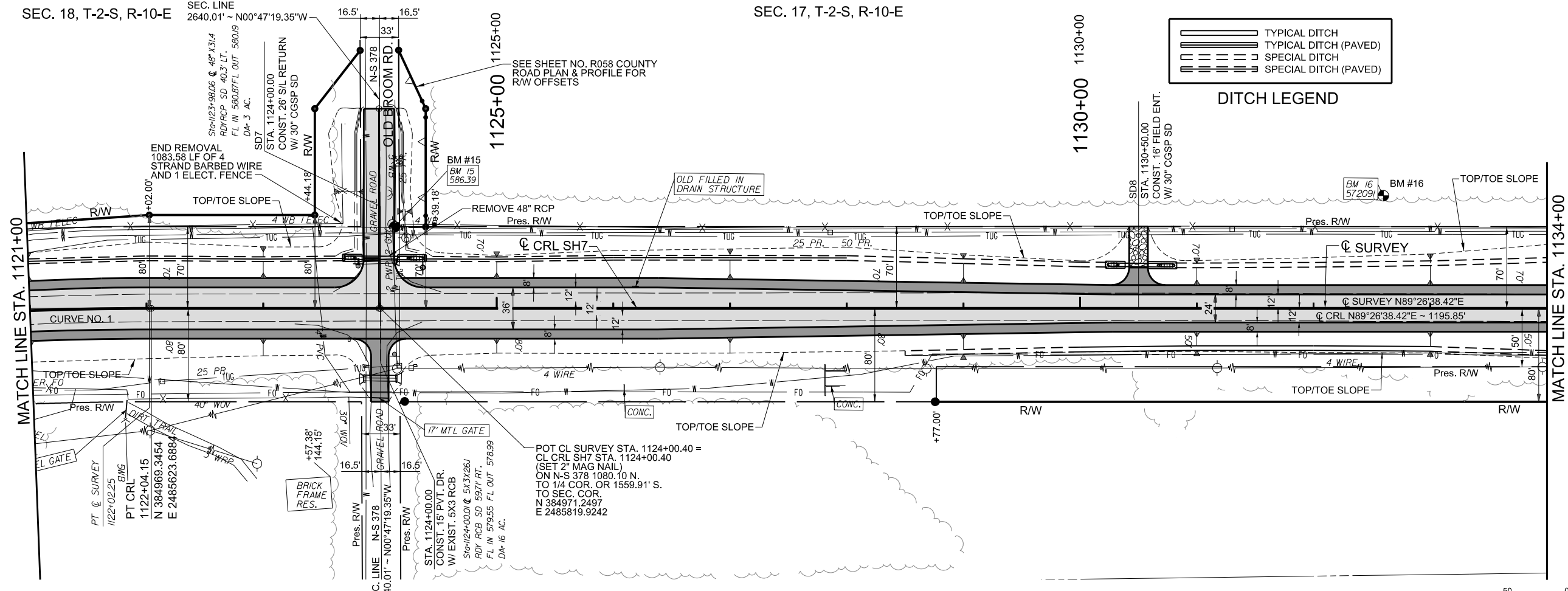
SEC. 18, T-2-S, R-10-E



PRINT DATE: 5/25/2018 C:\Projects\150-SHT Final Plans\038-24066(04)R038.dwg

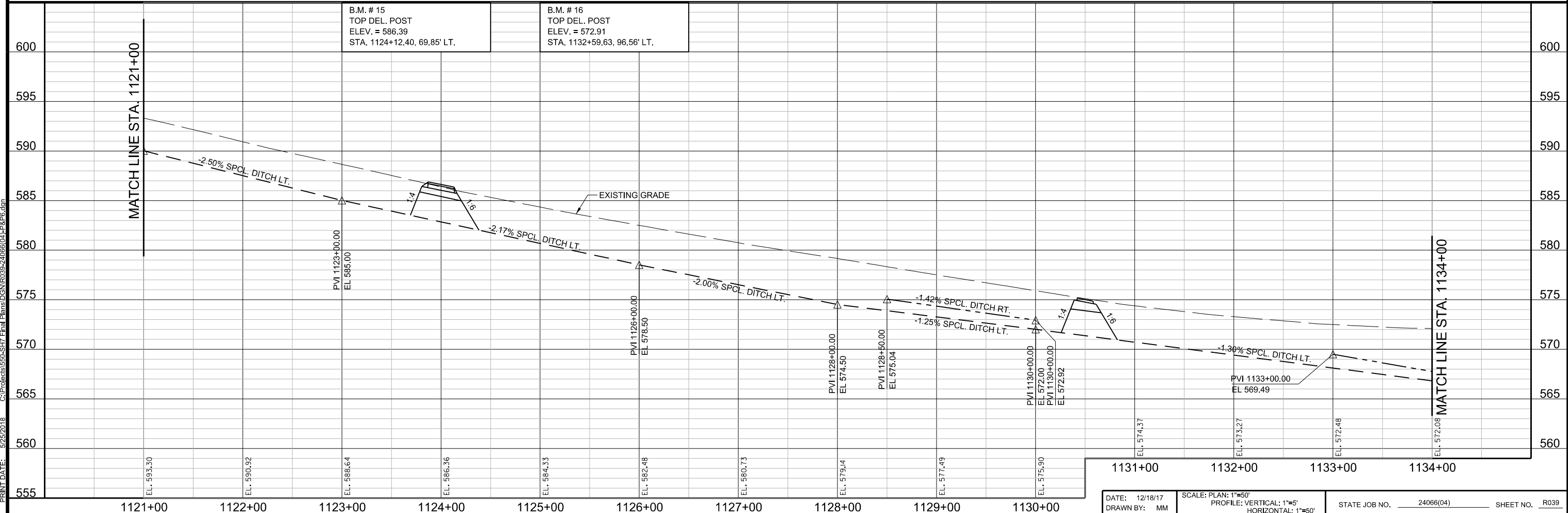
SEC. 18, T-2-S, R-10-E

SEC. 17, T-2-S, R-10-E

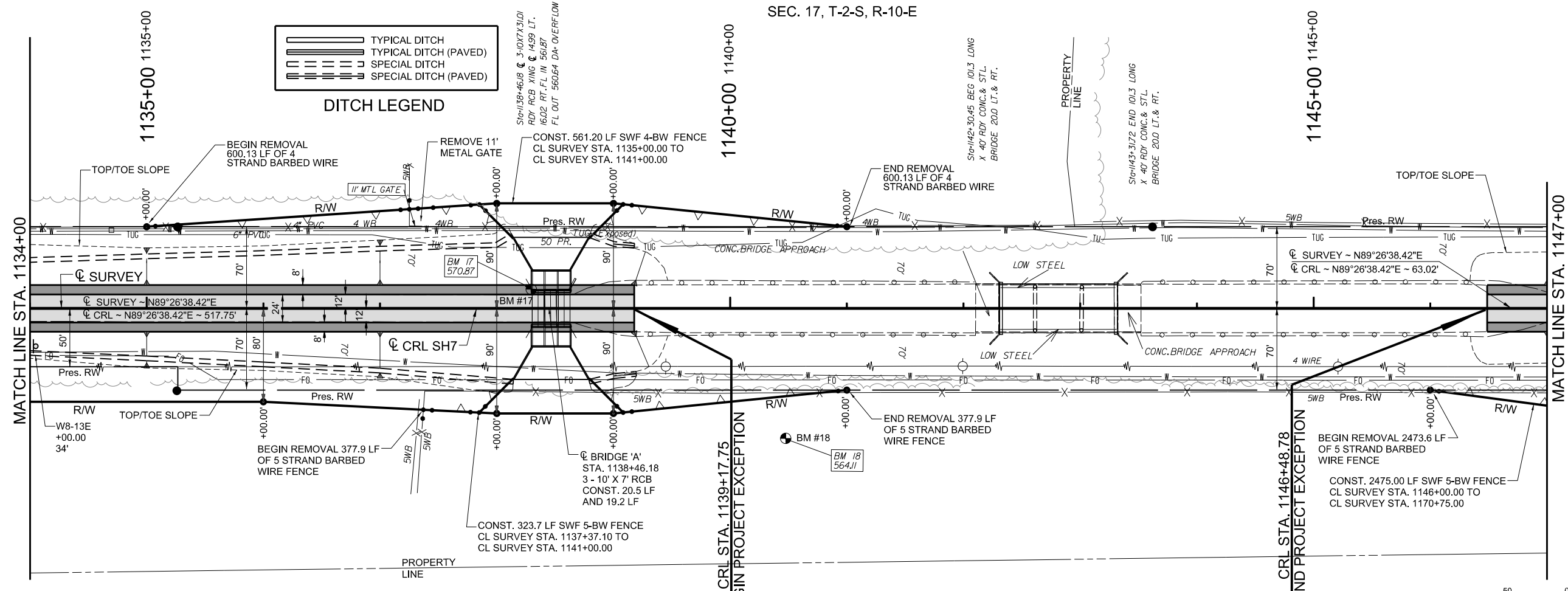


SEC. 18, T-2-S, R-10-E

SEC. 17, T-2-S, R-10-E

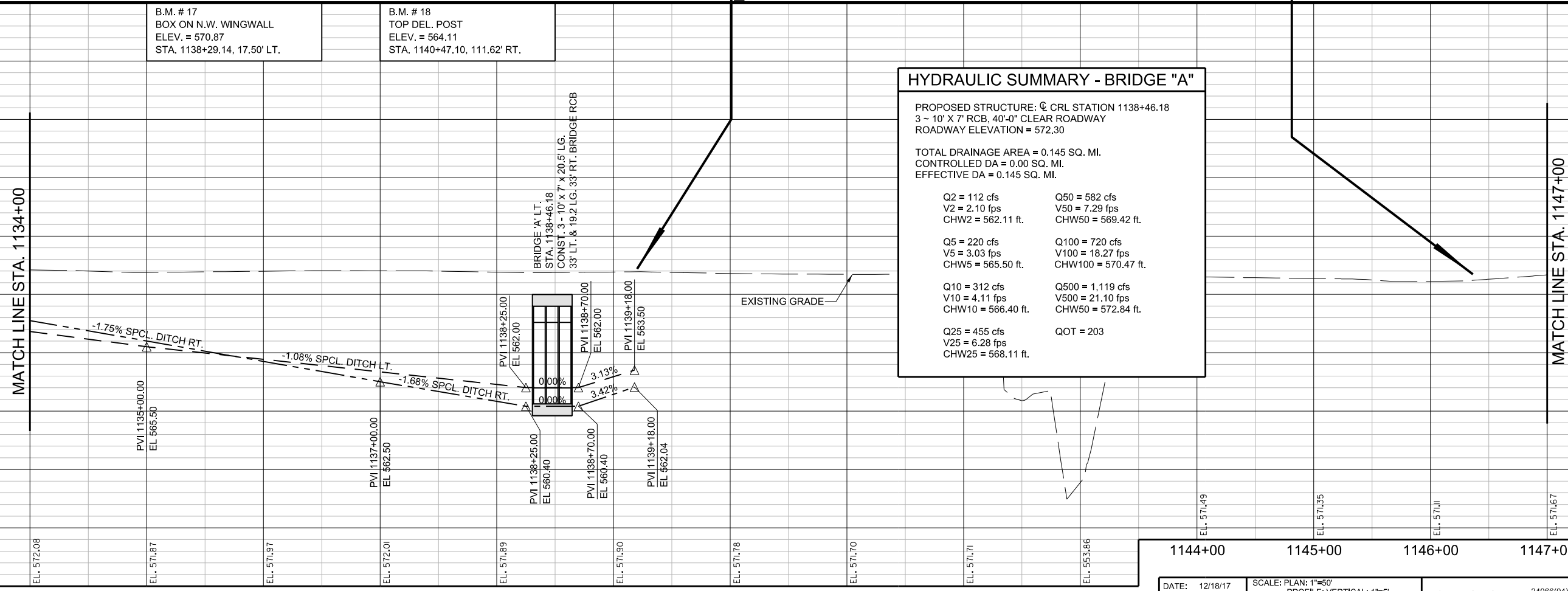


PRINT DATE: 5/25/2018 C:\Projects\150-SHT Final Plans\039-24066(04)1-P&P.dwg



CRL STA. 1139+17.75
BEGIN PROJECT EXCEPTION

CRL STA. 1146+48.78
END PROJECT EXCEPTION



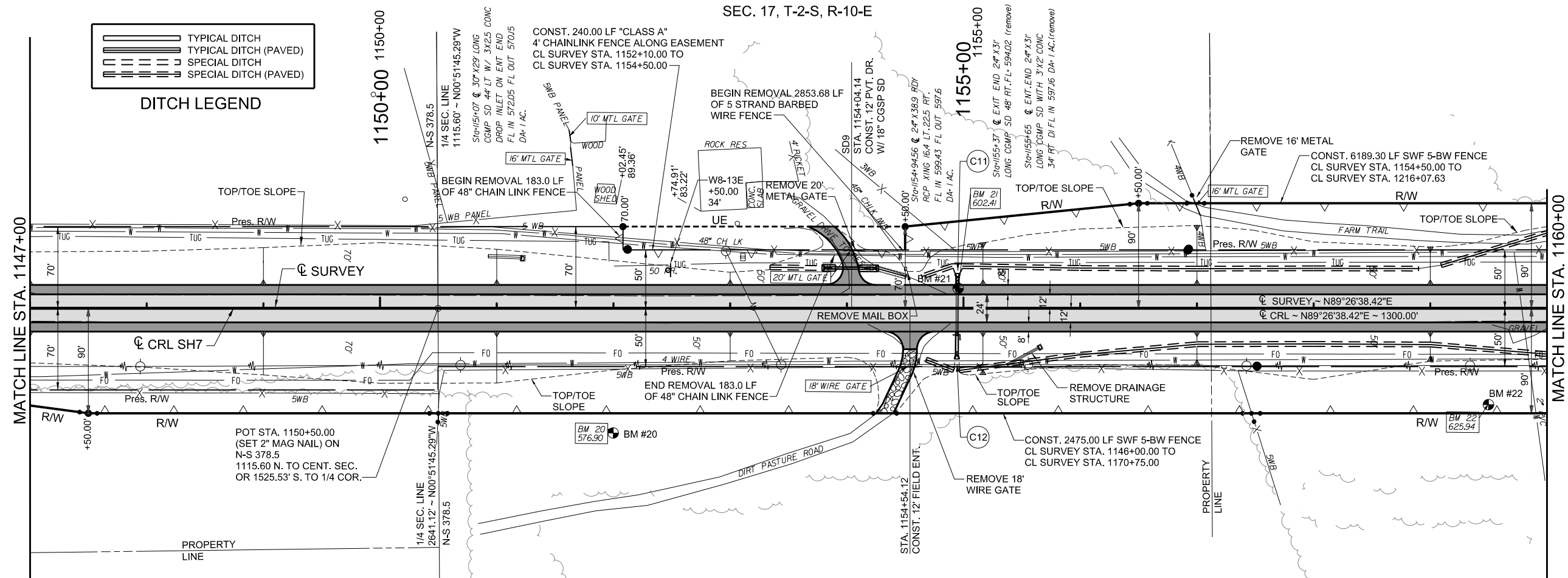
HYDRAULIC SUMMARY - BRIDGE "A"

PROPOSED STRUCTURE: CRL STATION 1138+46.18
3 - 10' X 7' RCB, 40'-0" CLEAR ROADWAY
ROADWAY ELEVATION = 572.30

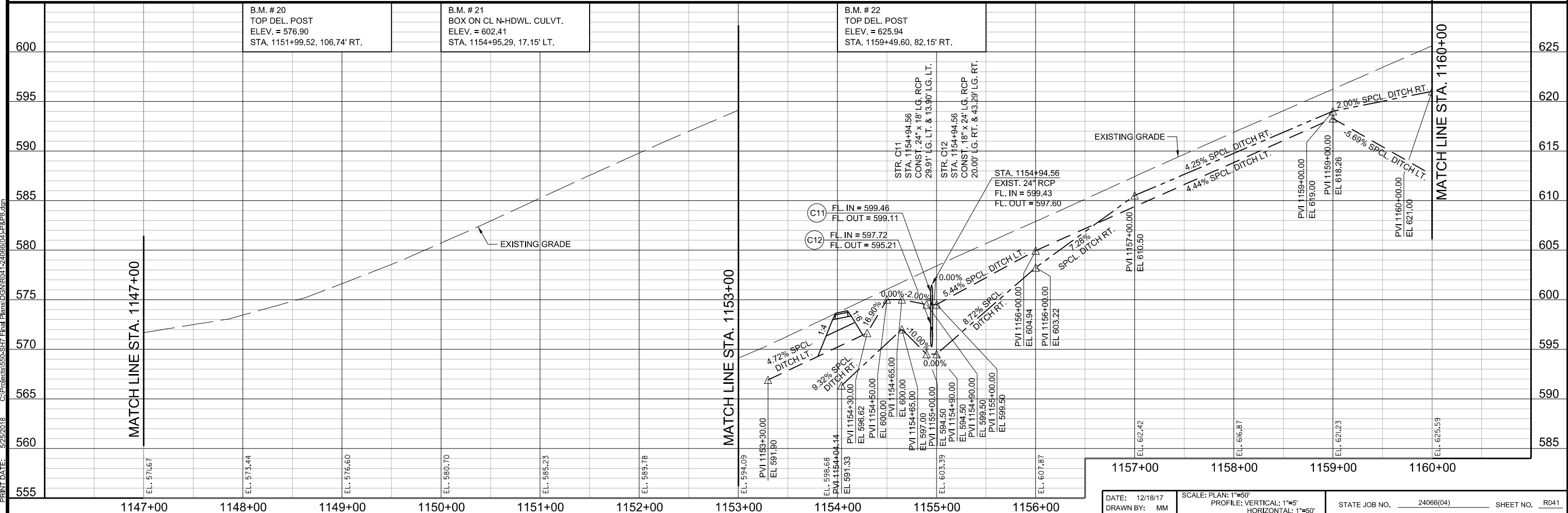
TOTAL DRAINAGE AREA = 0.145 SQ. MI.
CONTROLLED DA = 0.00 SQ. MI.
EFFECTIVE DA = 0.145 SQ. MI.

Q2 = 112 cfs	Q50 = 582 cfs
V2 = 2.10 fps	V50 = 7.29 fps
CHW2 = 562.11 ft.	CHW50 = 569.42 ft.
Q5 = 220 cfs	Q100 = 720 cfs
V5 = 3.03 fps	V100 = 18.27 fps
CHW5 = 565.50 ft.	CHW100 = 570.47 ft.
Q10 = 312 cfs	Q500 = 1,119 cfs
V10 = 4.11 fps	V500 = 21.10 fps
CHW10 = 566.40 ft.	CHW500 = 572.84 ft.
Q25 = 455 cfs	QOT = 203
V25 = 6.28 fps	
CHW25 = 568.11 ft.	

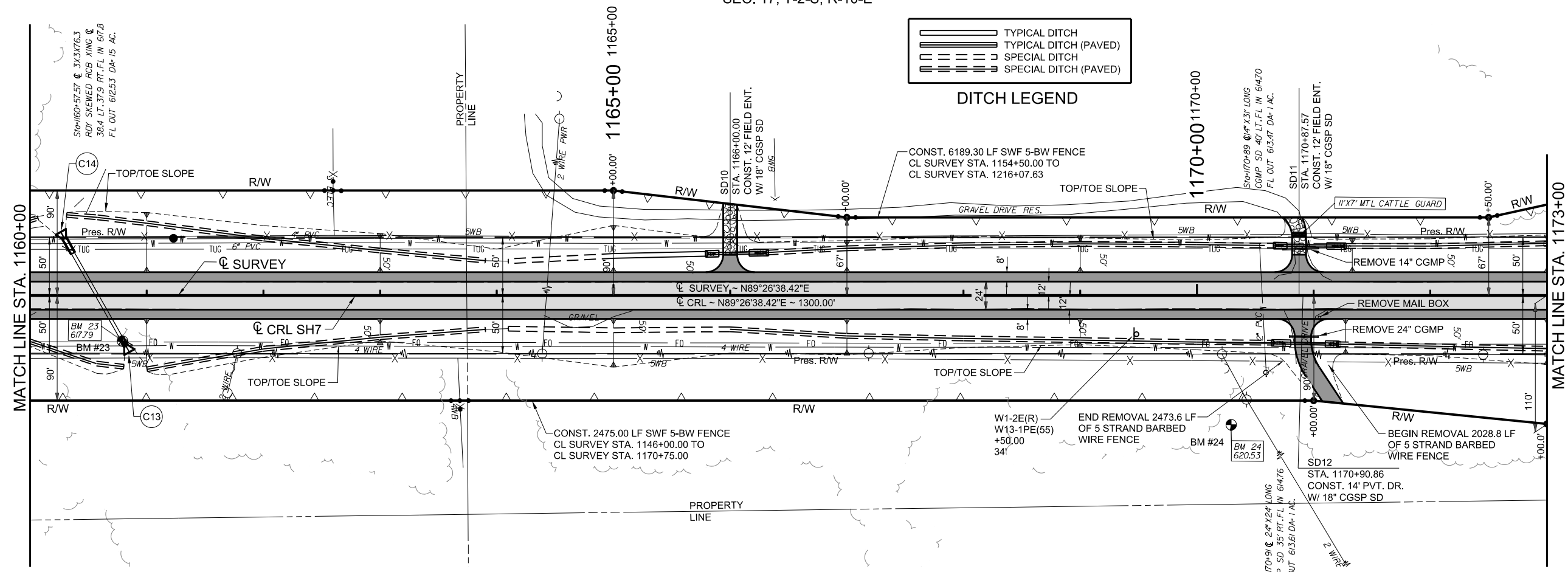
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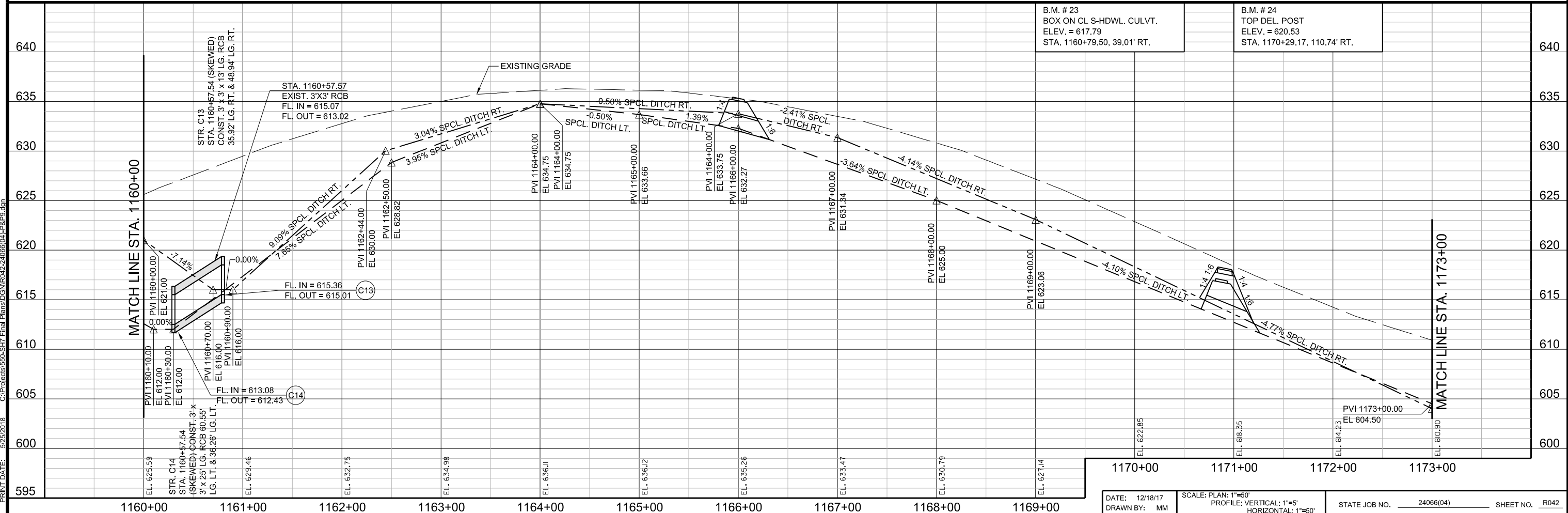
SEC. 17, T-2-S, R-10-E



SEC. 17, T-2-S, R-10-E



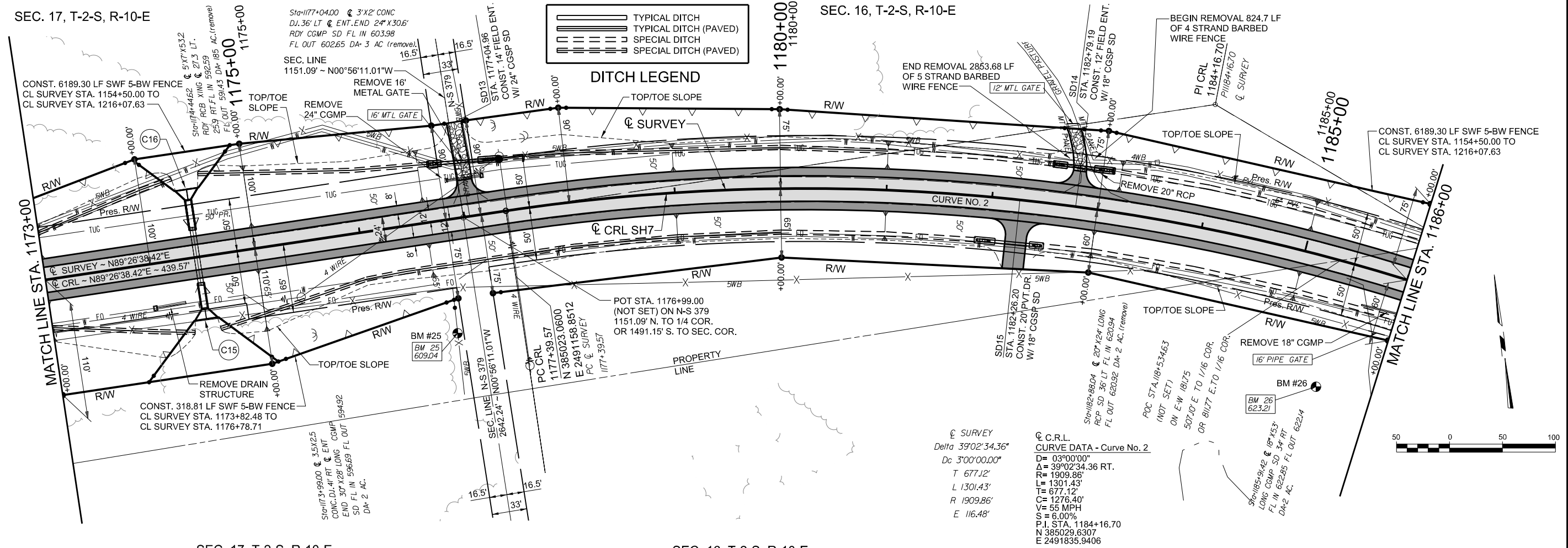
SEC. 17, T-2-S, R-10-E



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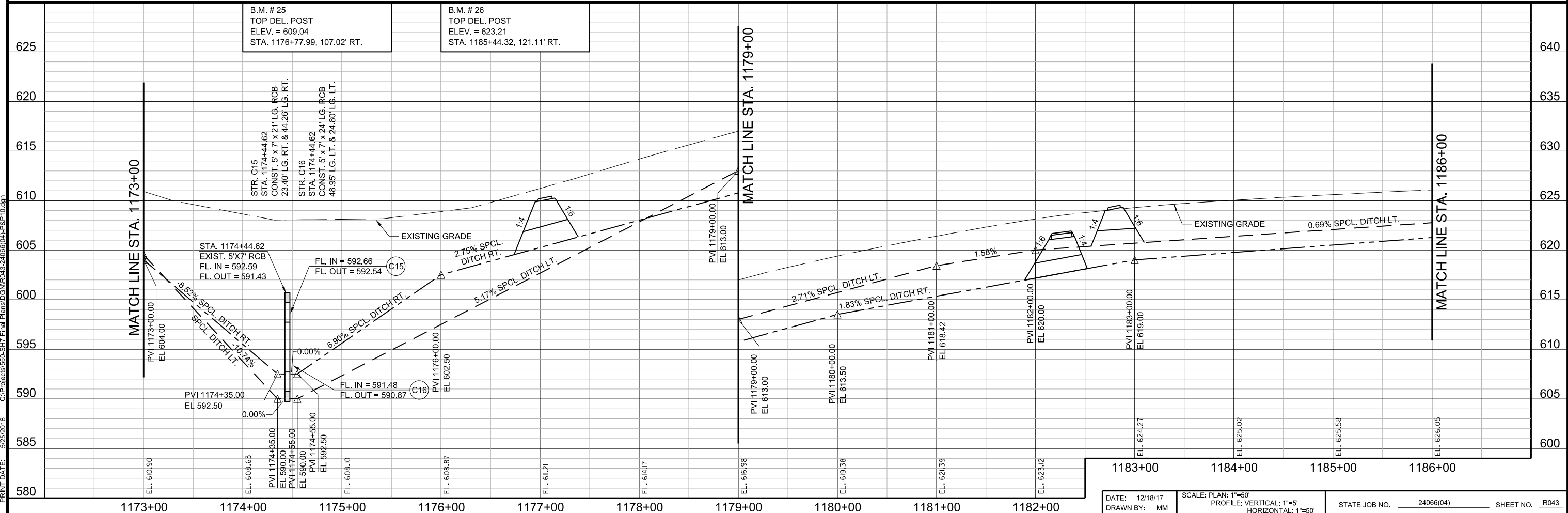
SEC. 17, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E

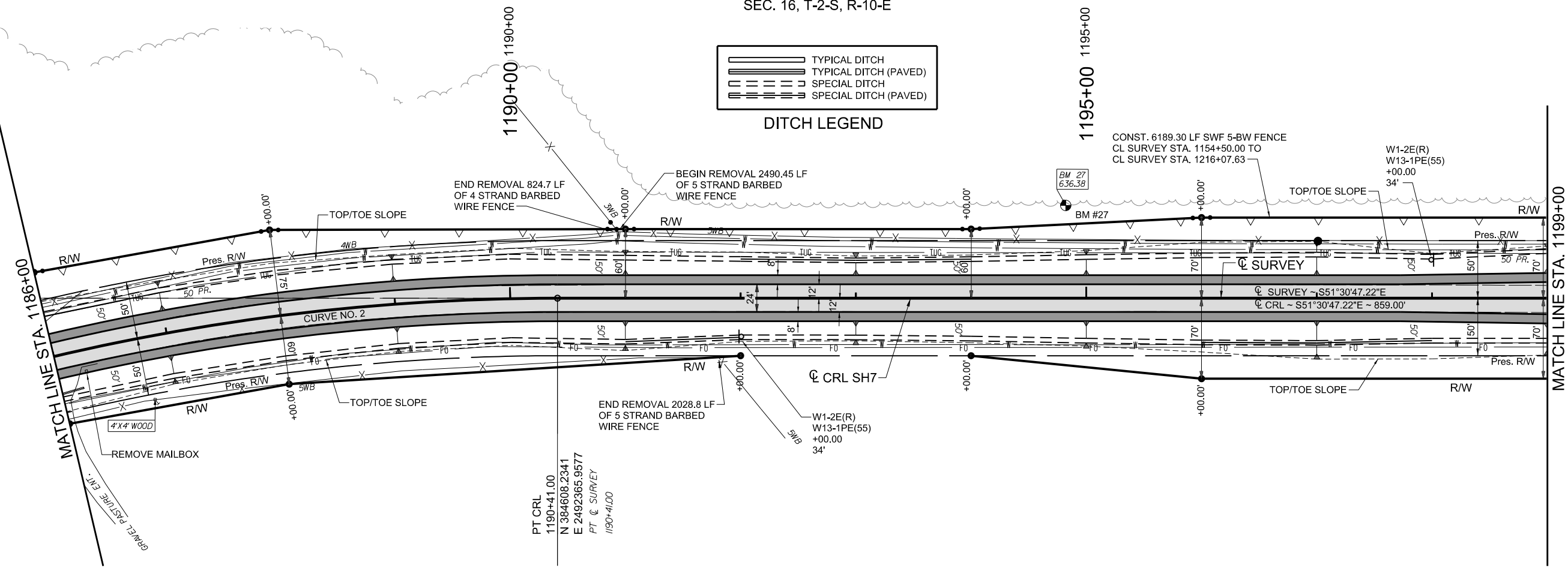


SEC. 17, T-2-S, R-10-E

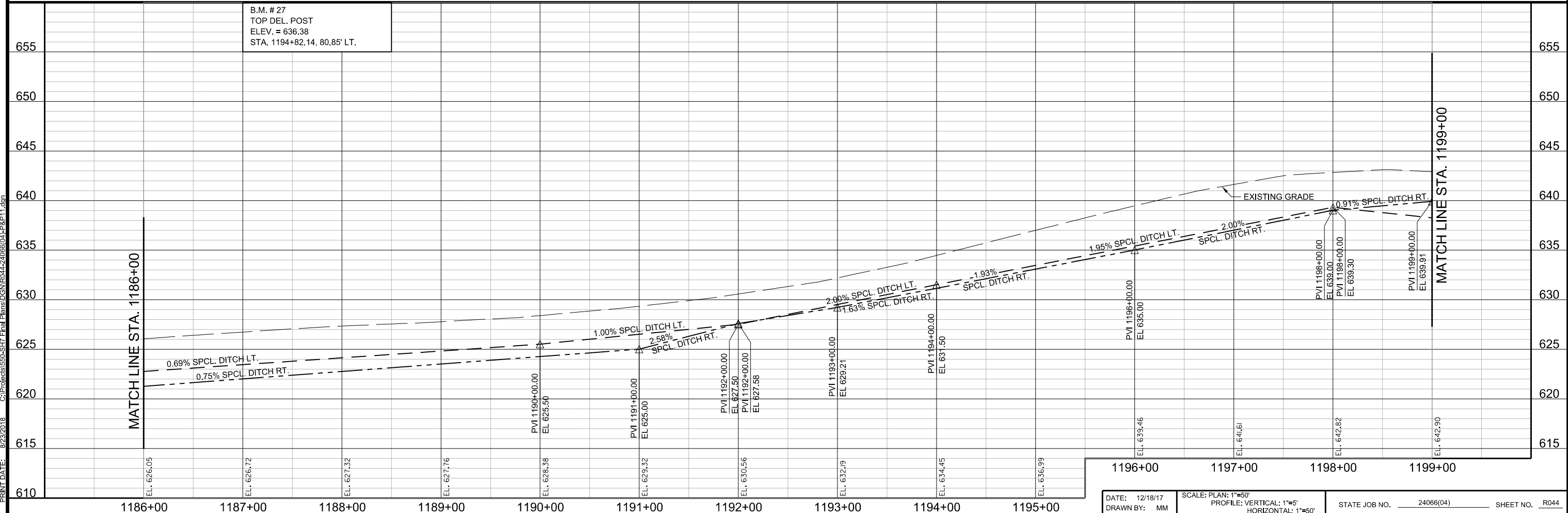
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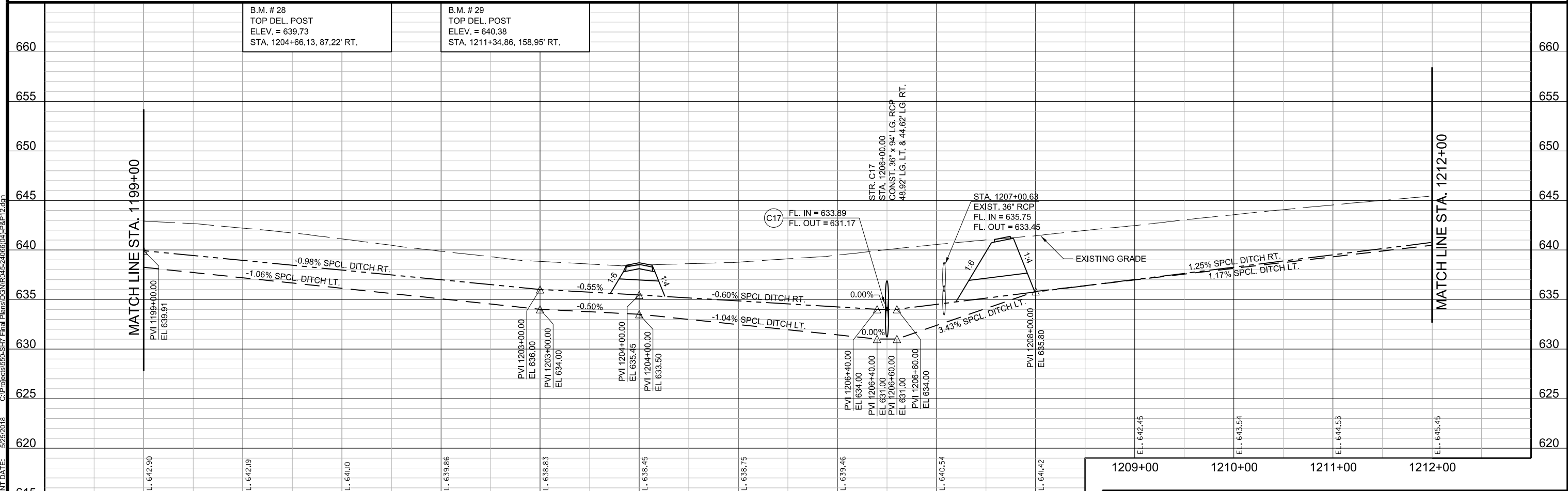
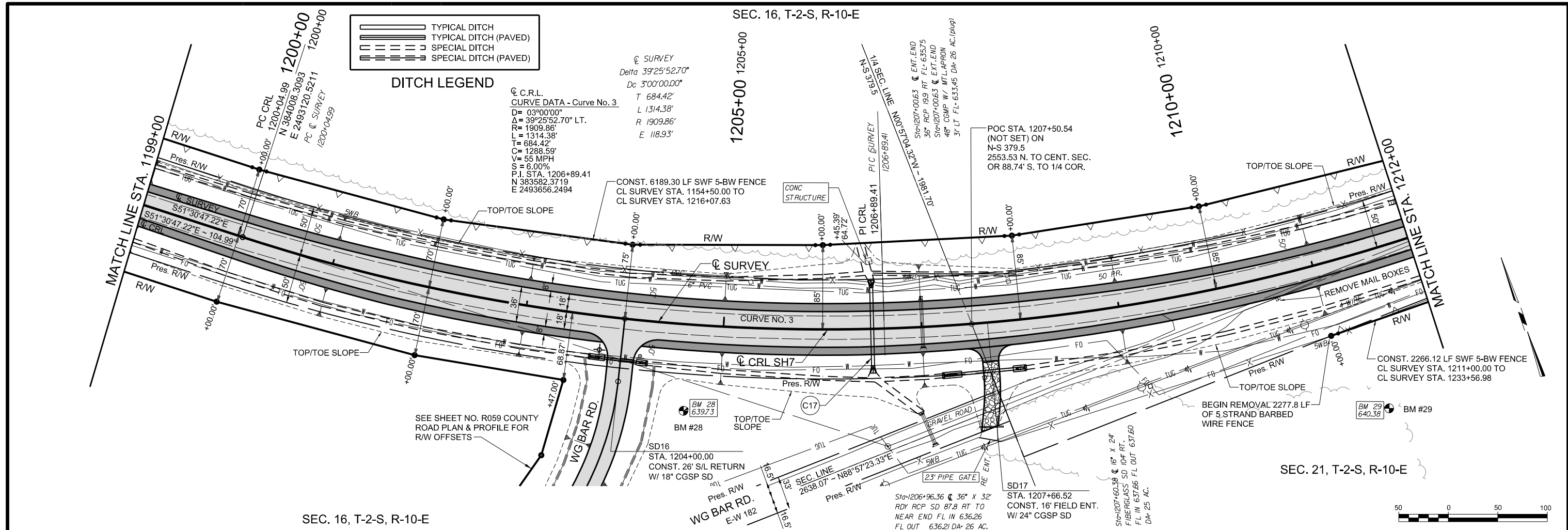
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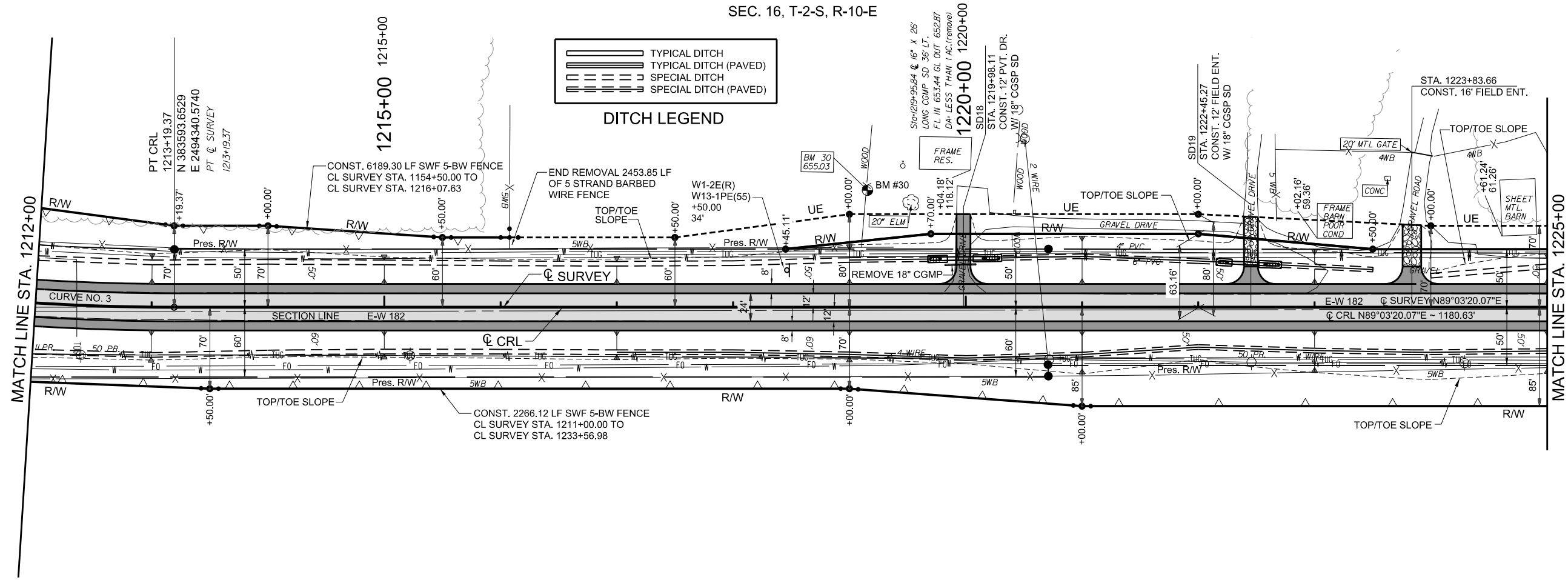
SEC. 16, T-2-S, R-10-E



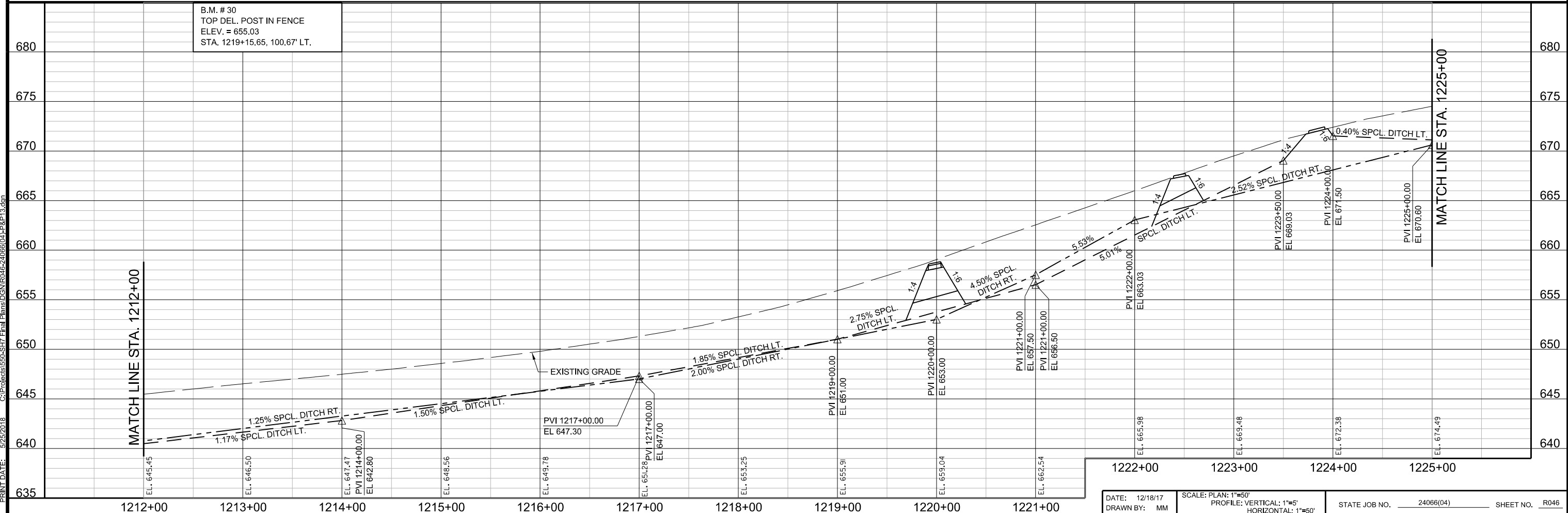
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SEC. 16, T-2-S, R-10-E

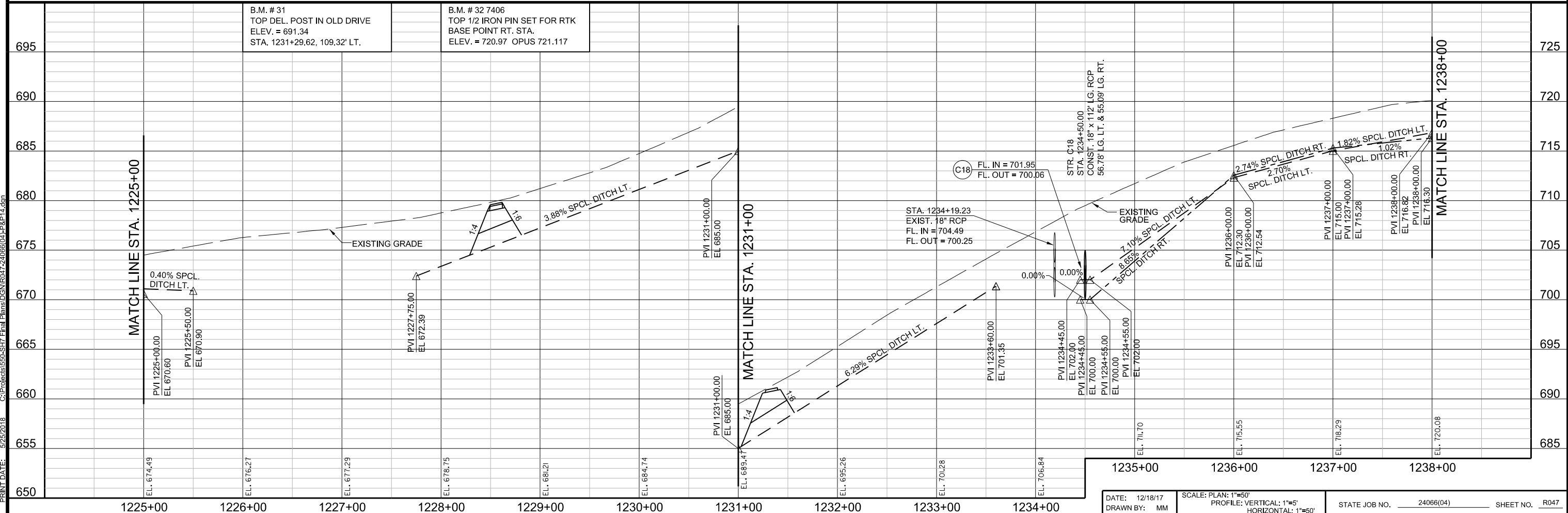
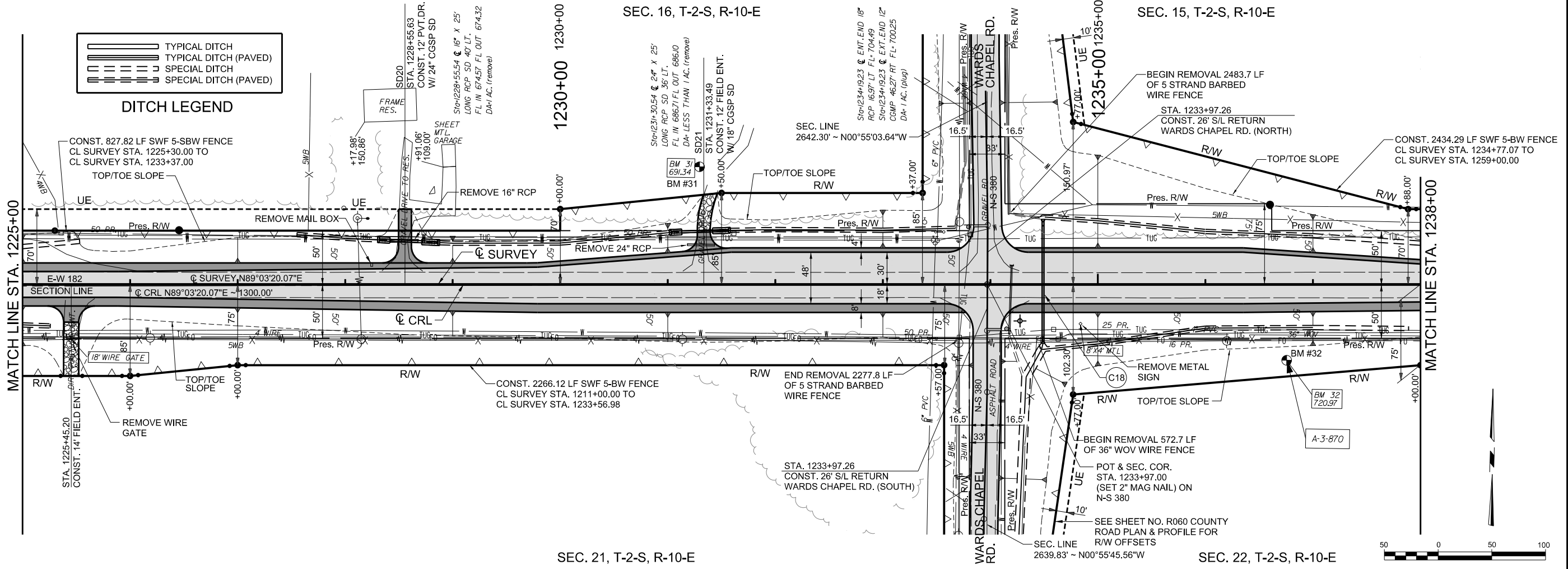


SEC. 21, T-2-S, R-10-E

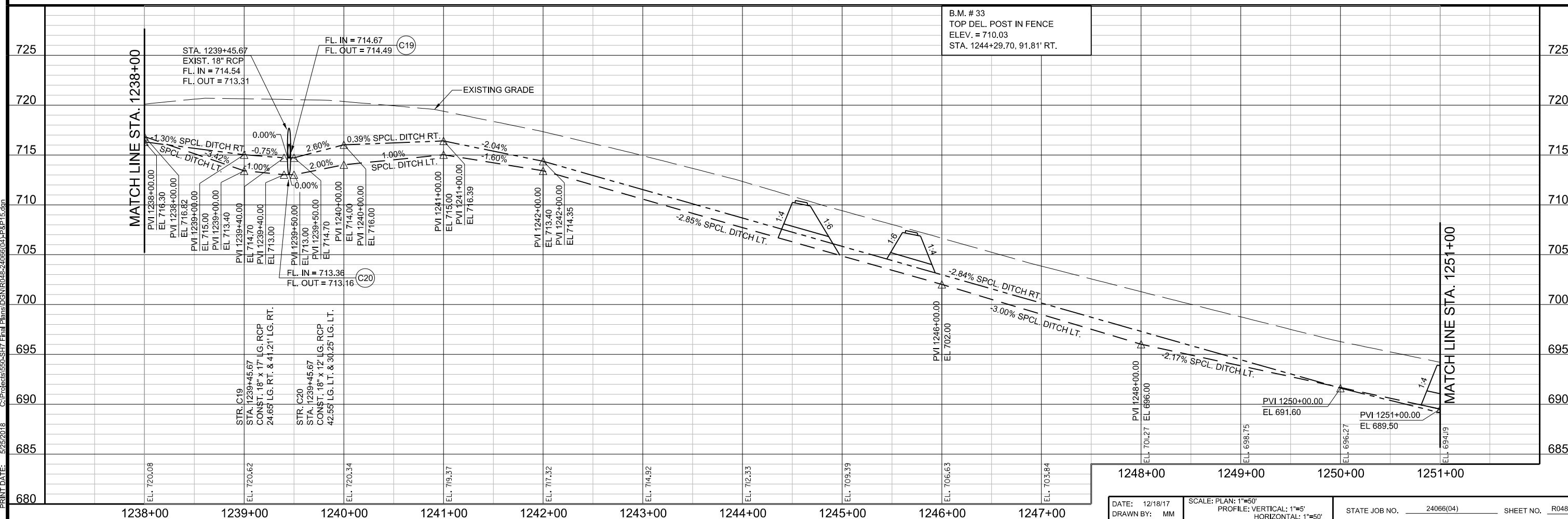
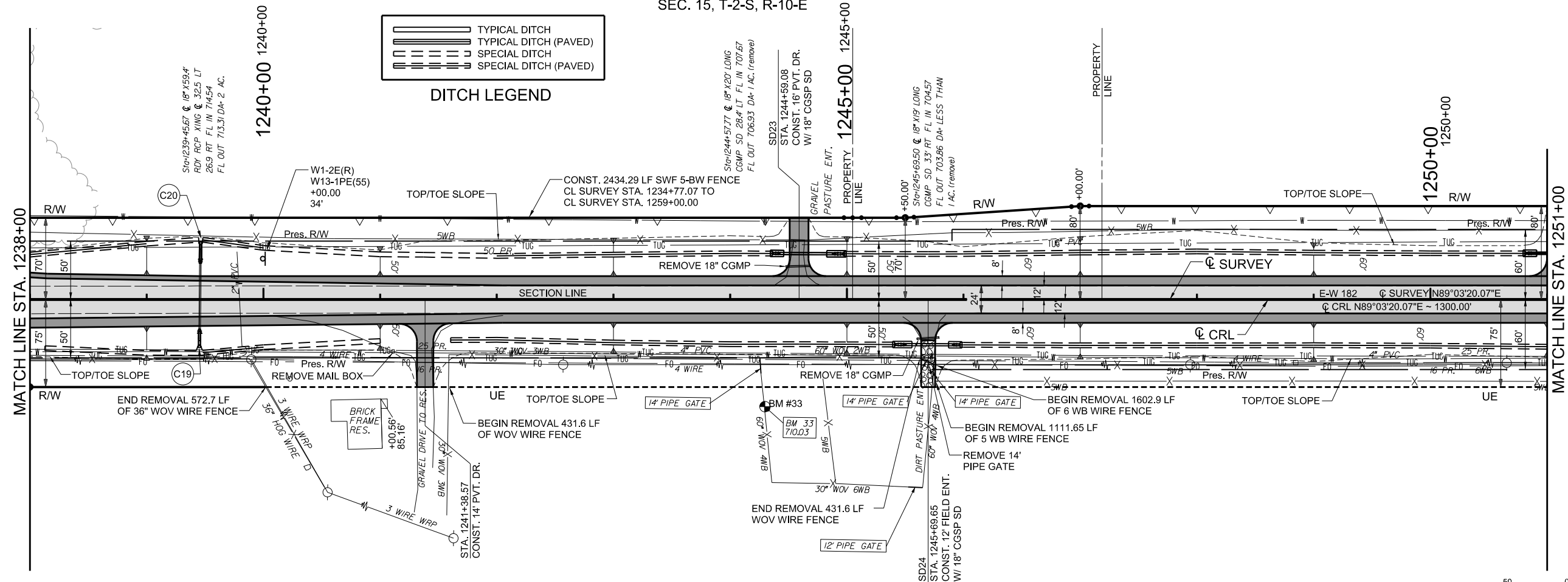


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	TYPICAL DITCH
	TYPICAL DITCH (PAVED)
	SPECIAL DITCH
	SPECIAL DITCH (PAVED)

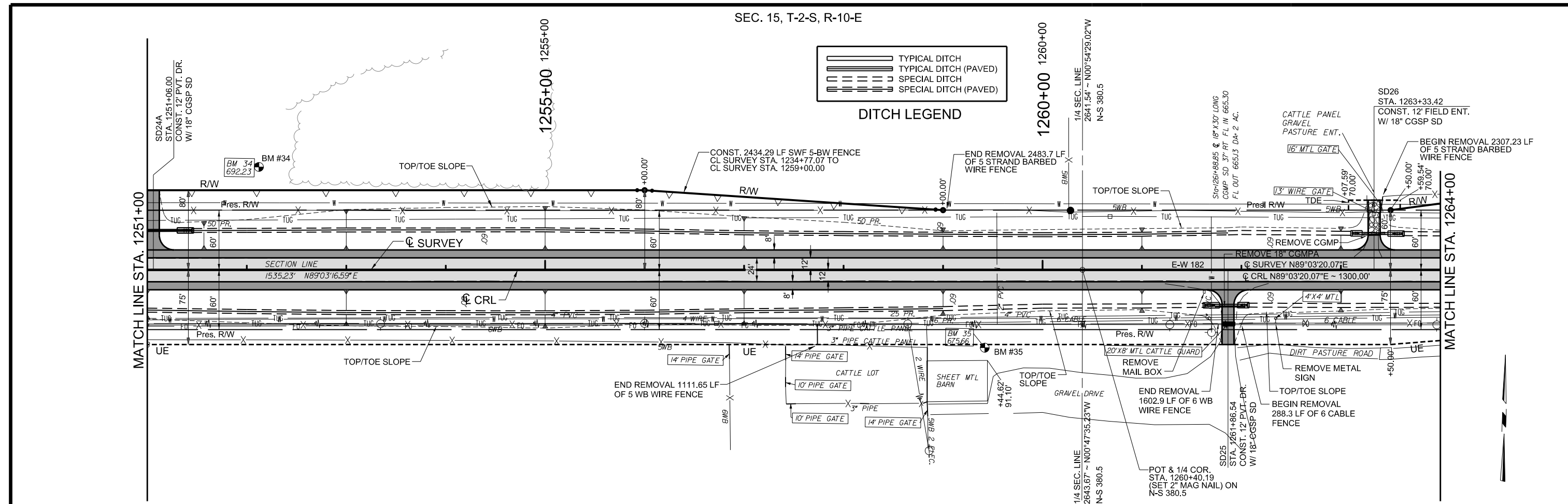


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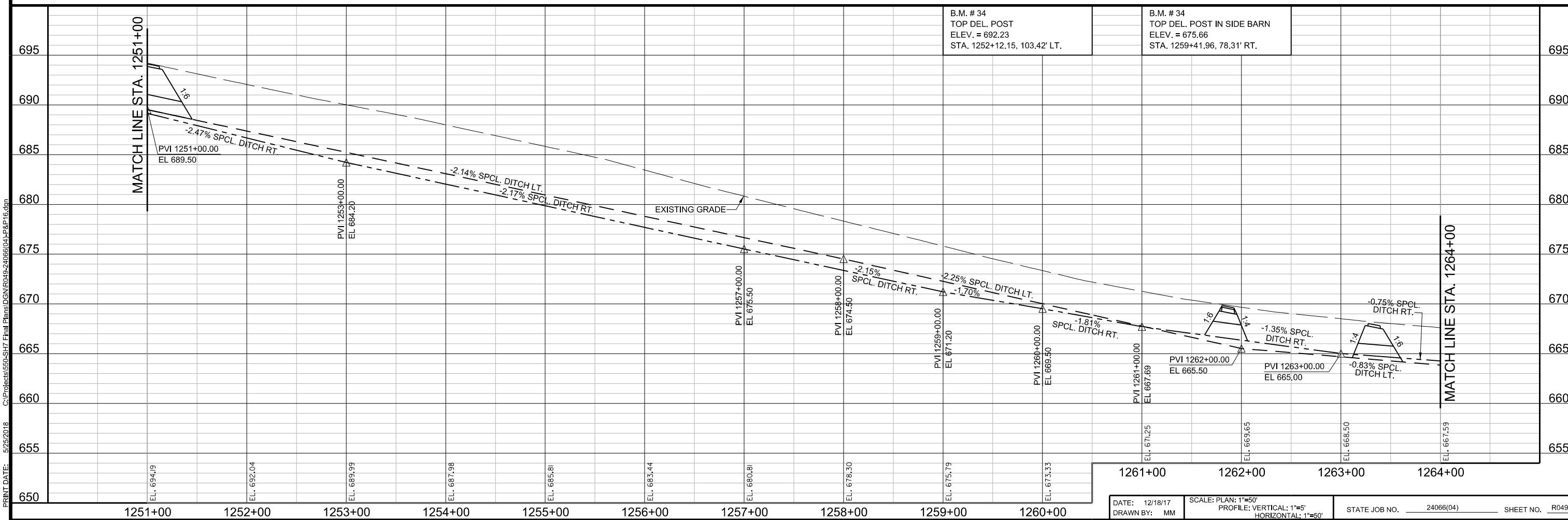


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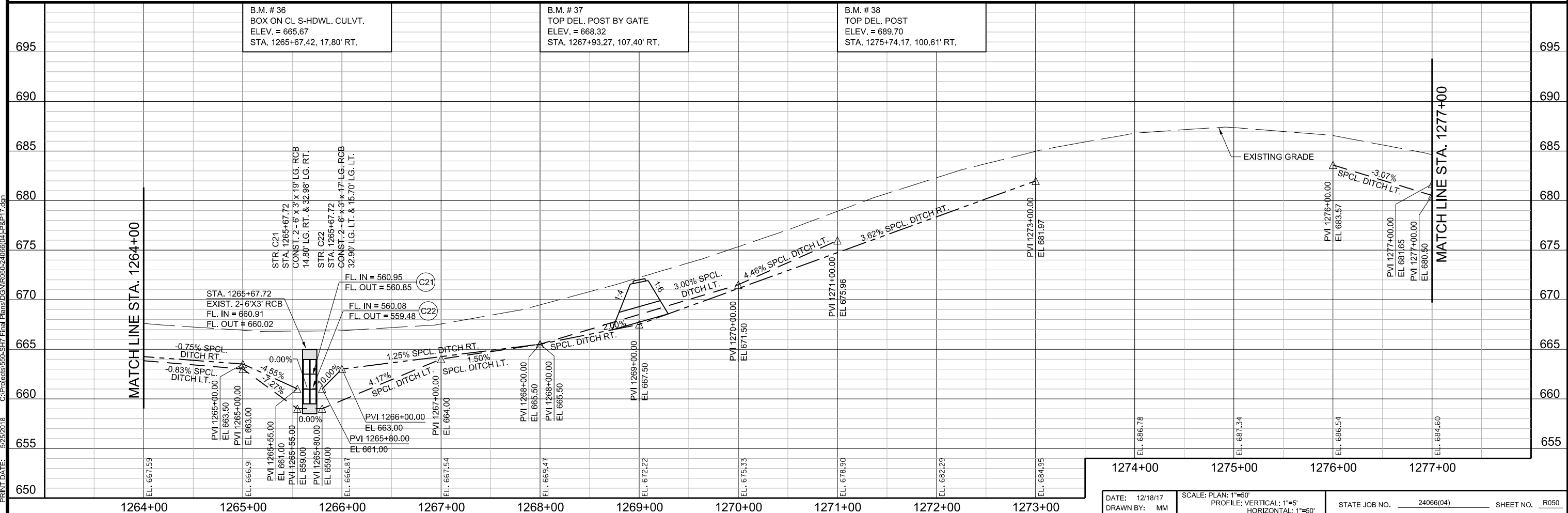
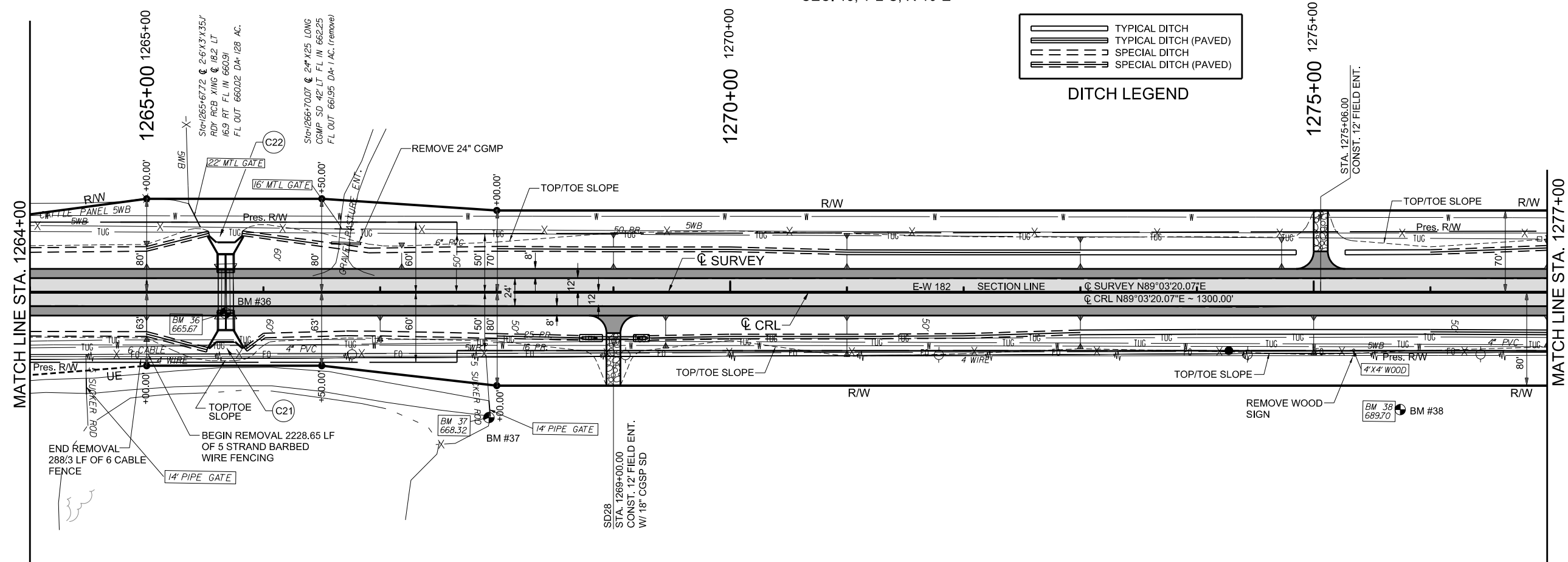
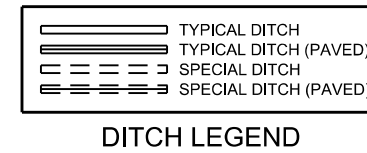
SEC. 15, T-2-S, R-10-E



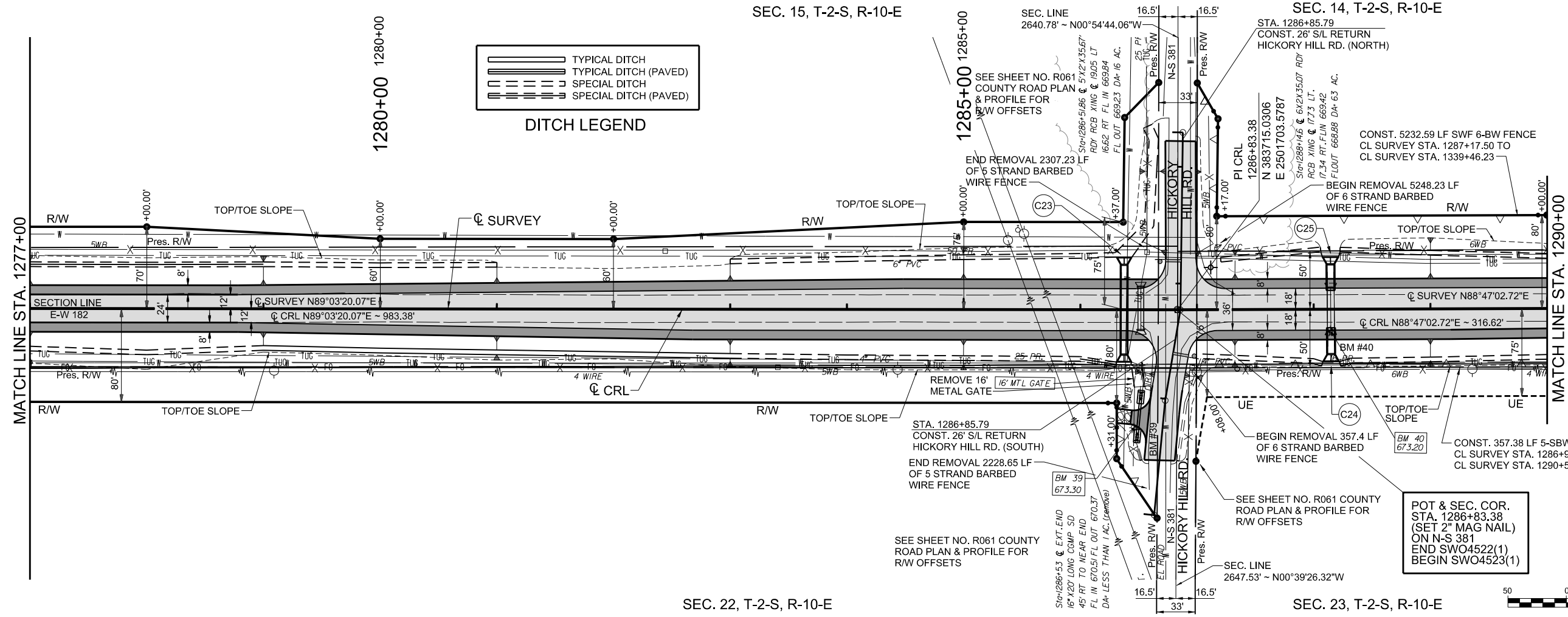
SEC. 22, T-2-S, R-10-E



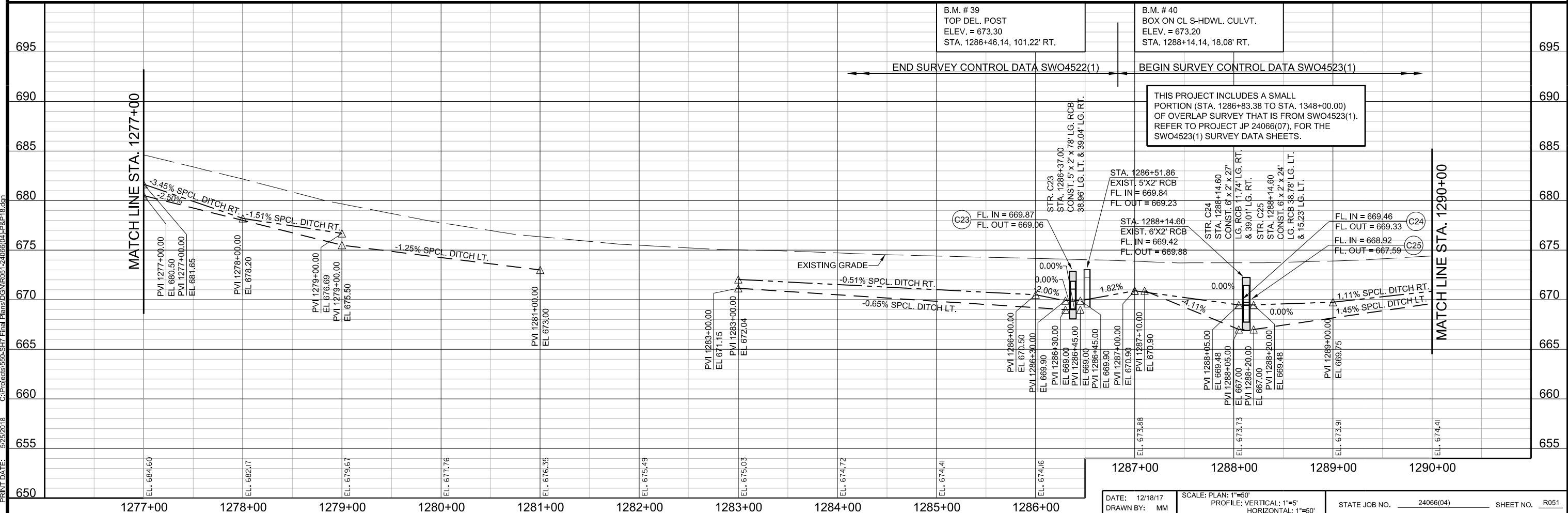
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PRINT DATE: 5/25/2018 C:\Projects\550-SHT Final Plans\550-24066(04)-P&P17.dwg

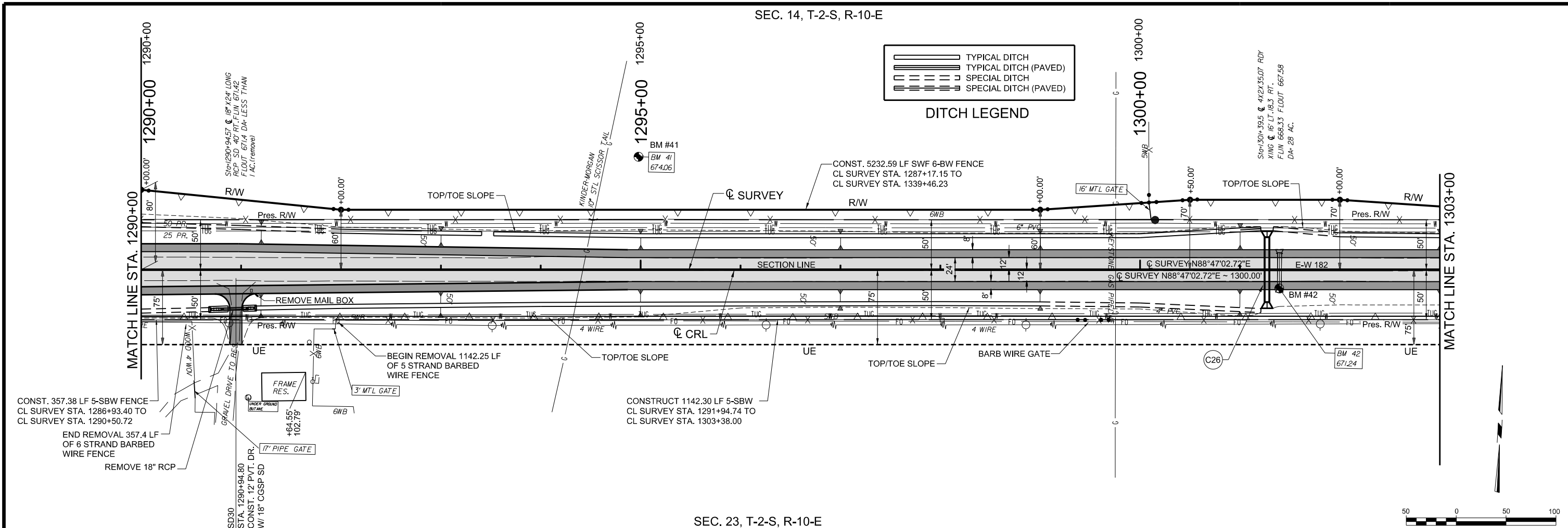


POT & SEC. COR.
 STA. 1286+83.38
 (SET 2" MAG NAIL)
 ON N-S 381
 END SWO4522(1)
 BEGIN SWO4523(1)

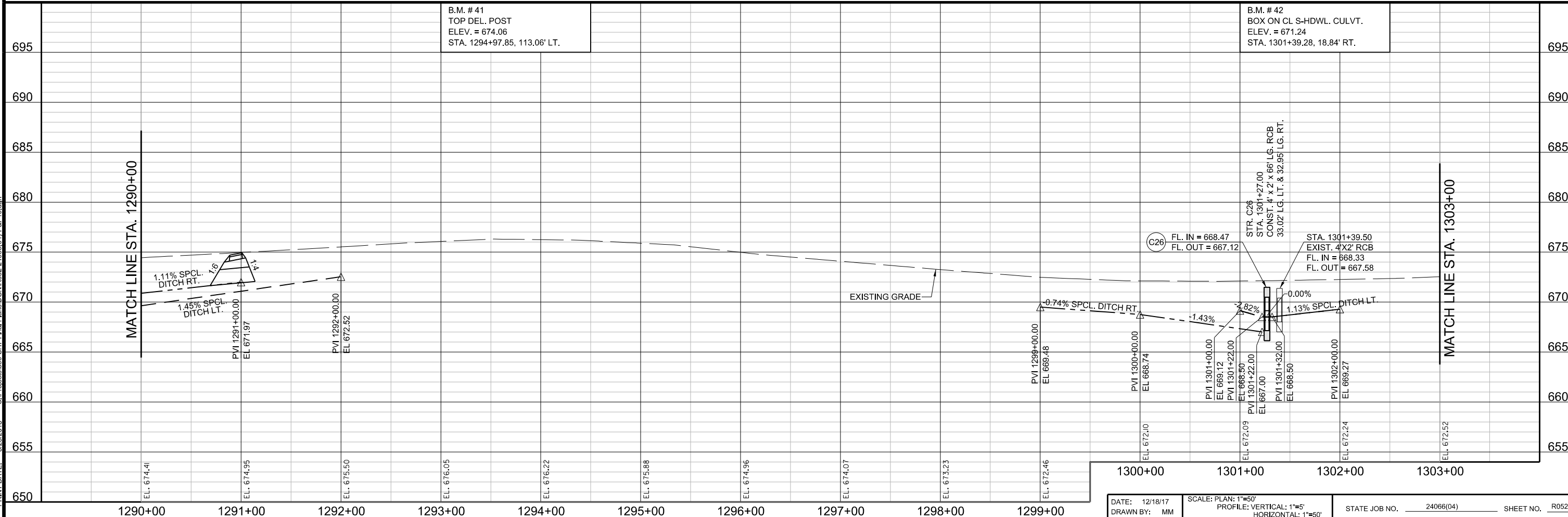


PRINT DATE: 5/25/2018
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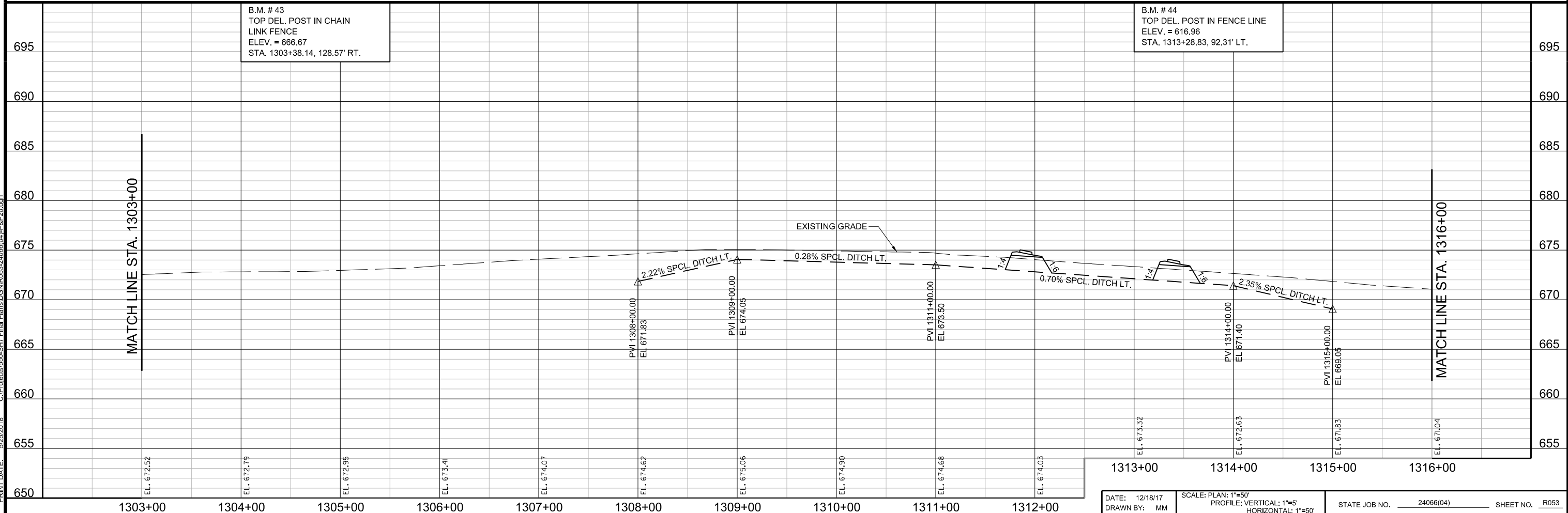
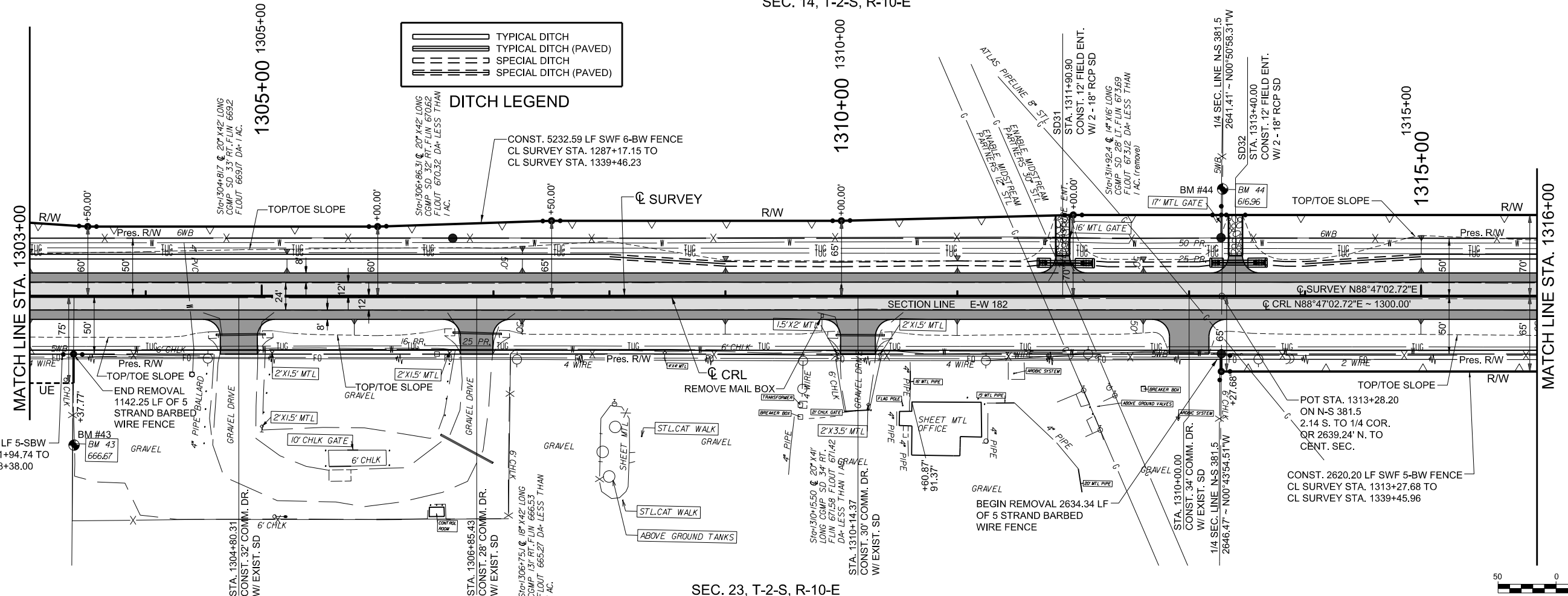
SEC. 14, T-2-S, R-10-E



SEC. 23, T-2-S, R-10-E

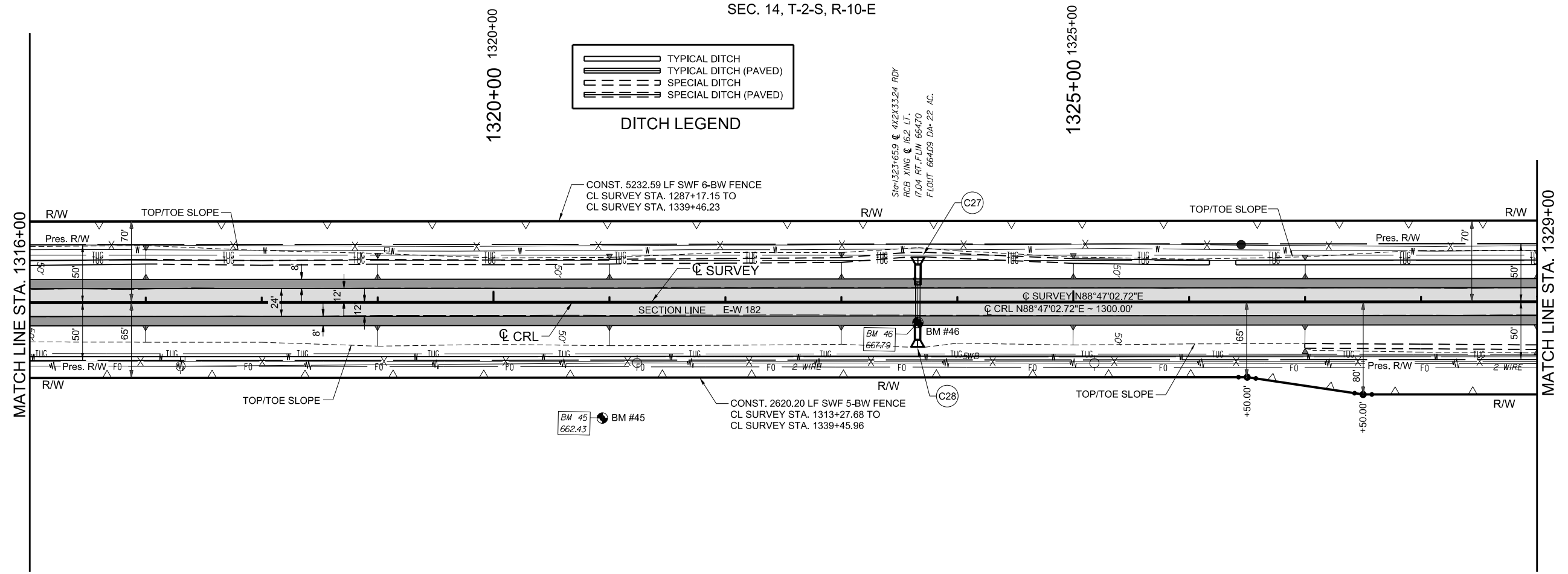


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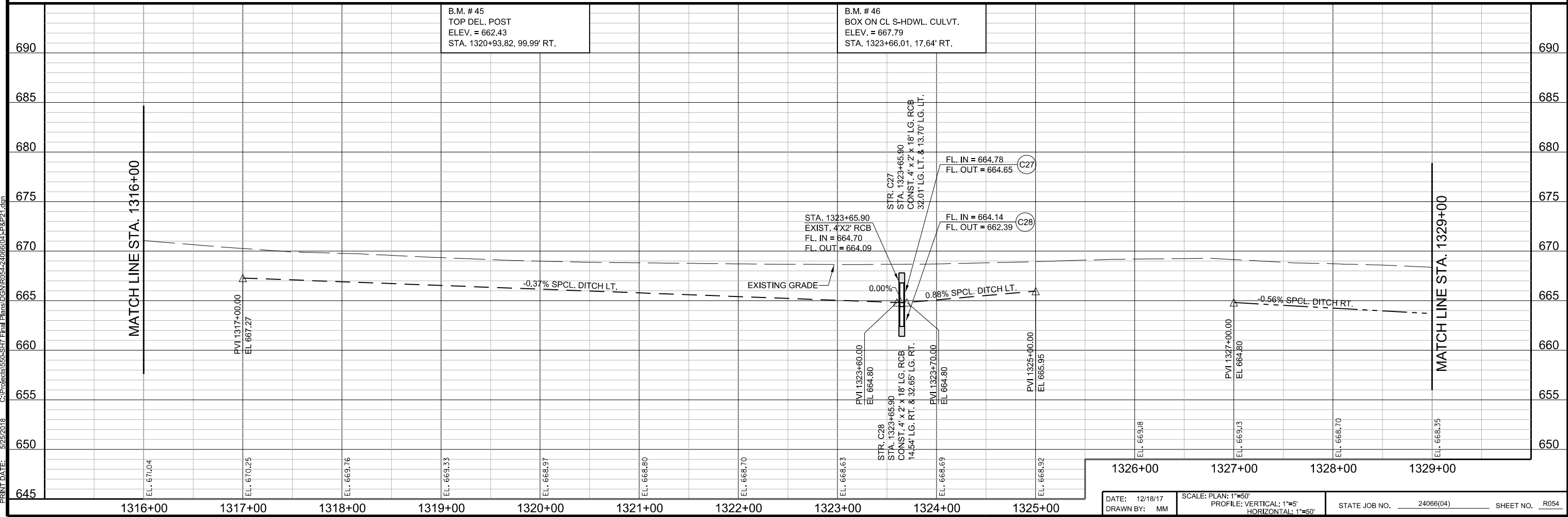


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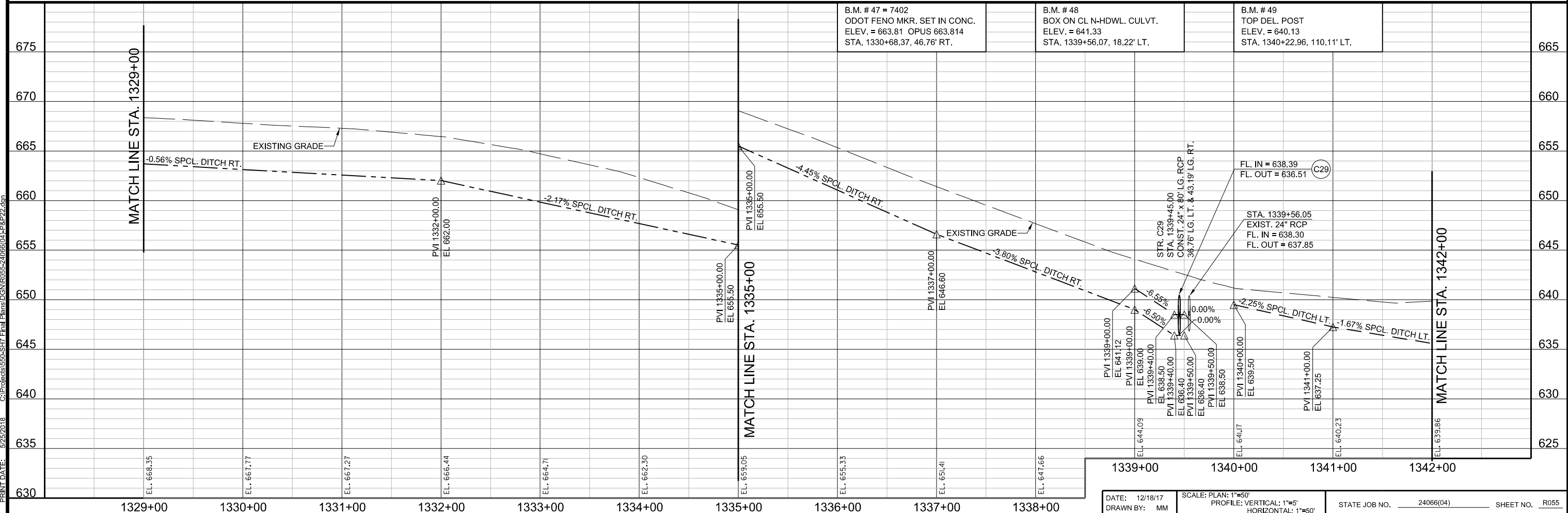
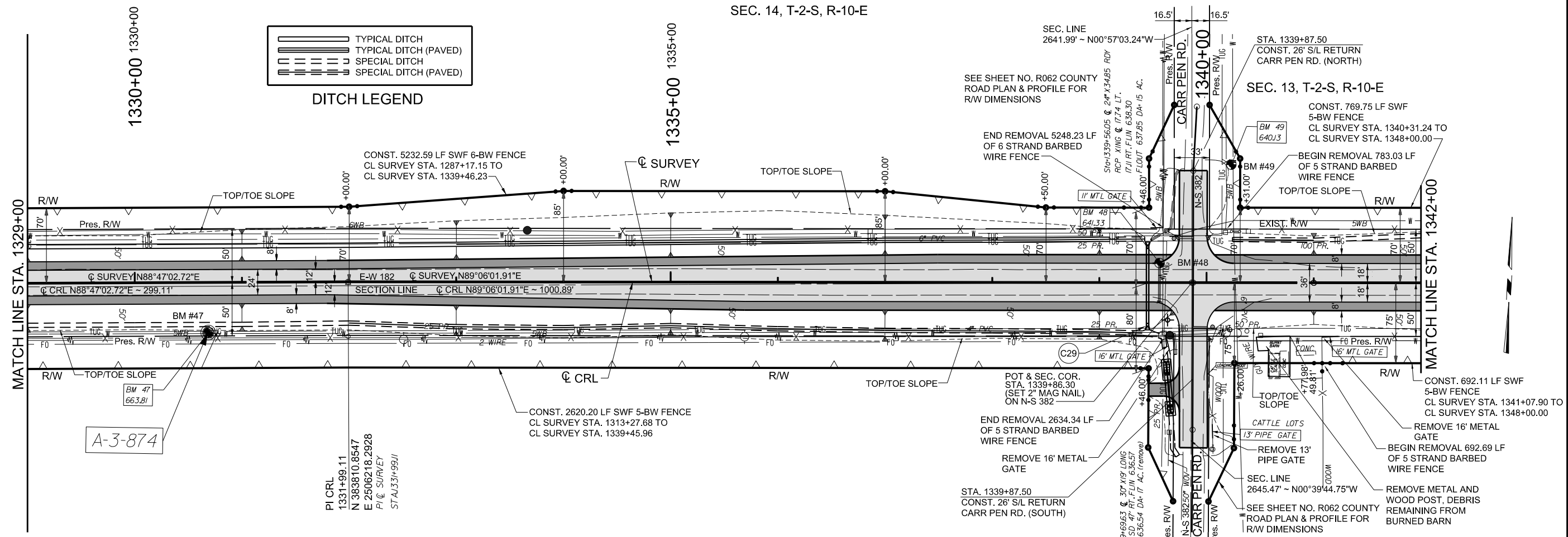
SEC. 14, T-2-S, R-10-E



SEC. 23, T-2-S, R-10-E

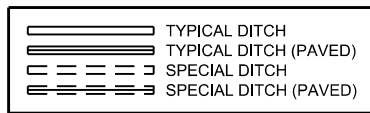


PRINT DATE: 5/25/2018 C:\Projects\150-SHT Final Plans\DNV\150-SHT Final Plans\04-1-P&P21.dgn

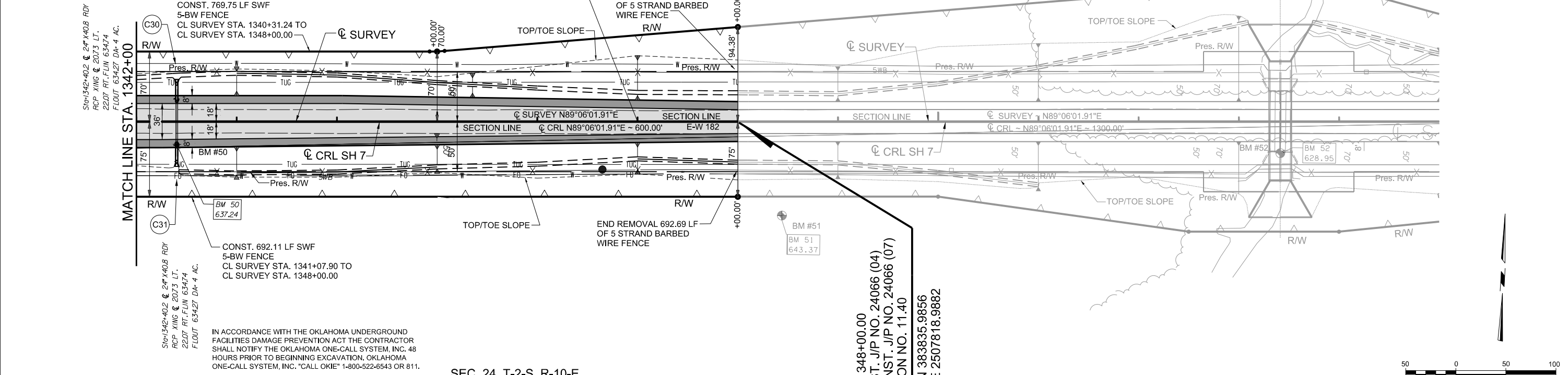


PRINT DATE: 5/25/2018 C:\Projects\1500-SHT Final Plans\DNV\1505-24066(04)1-P&P22.dgn

UTILITY COORDINATION			
UTILITY	PHONE	NOTIFIED	RESPONDED
ATOKA RURAL WATER DIST. #4	580-889-5949		
SOUTHEASTERN ELECTRIC COOP			
AT&T			
WESTERN FARMERS ELECTRIC COOP			
CITY OF ATOKA	580-889-2675		
SCISSOR TAIL	580-320-3141		
ENOGEX	580-364-0392		
ANTERO	580-927-9133		
MARKWEST	918-389-5100		
CENTER POINT ENERGY	580-889-3429		



SEC. 13, T-2-S, R-10-E

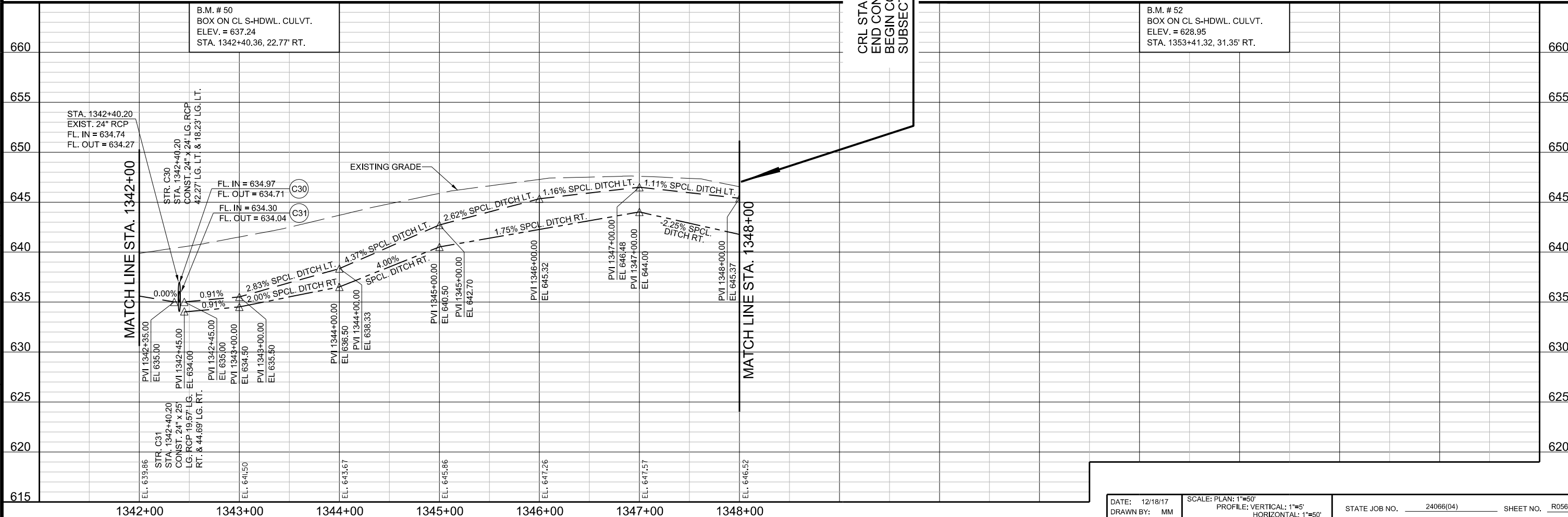


IN ACCORDANCE WITH THE OKLAHOMA UNDERGROUND FACILITIES DAMAGE PREVENTION ACT THE CONTRACTOR SHALL NOTIFY THE OKLAHOMA ONE-CALL SYSTEM, INC. 48 HOURS PRIOR TO BEGINNING EXCAVATION. OKLAHOMA ONE-CALL SYSTEM, INC. "CALL OKIE" 1-800-522-6543 OR 811.

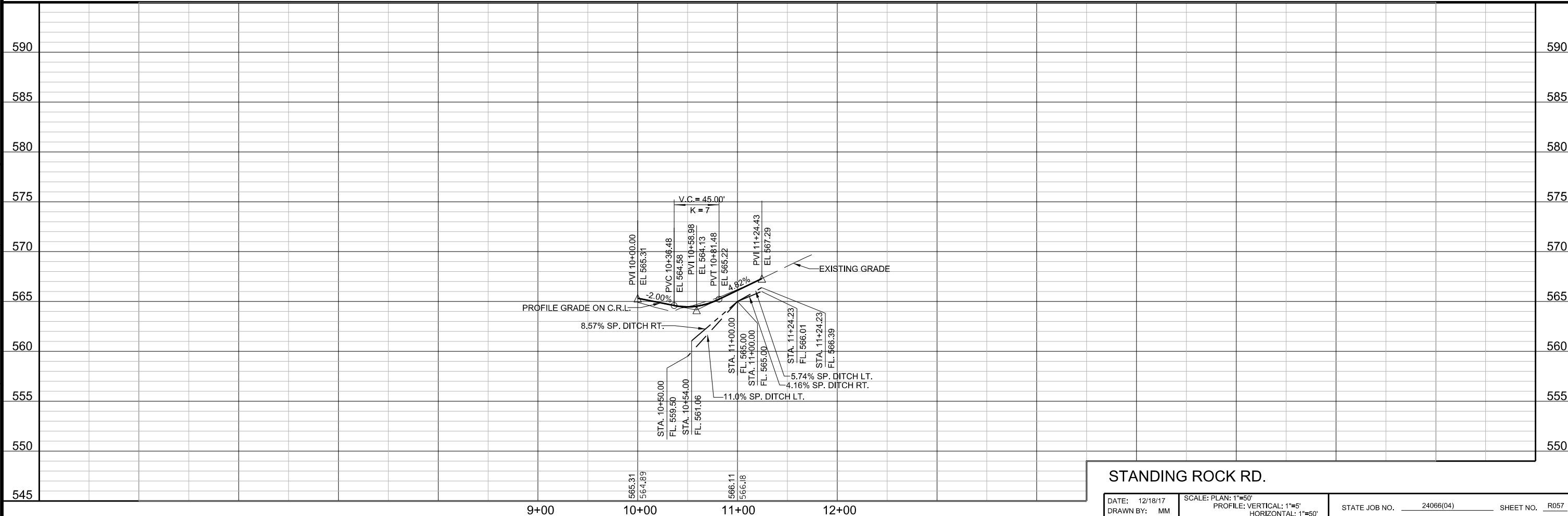
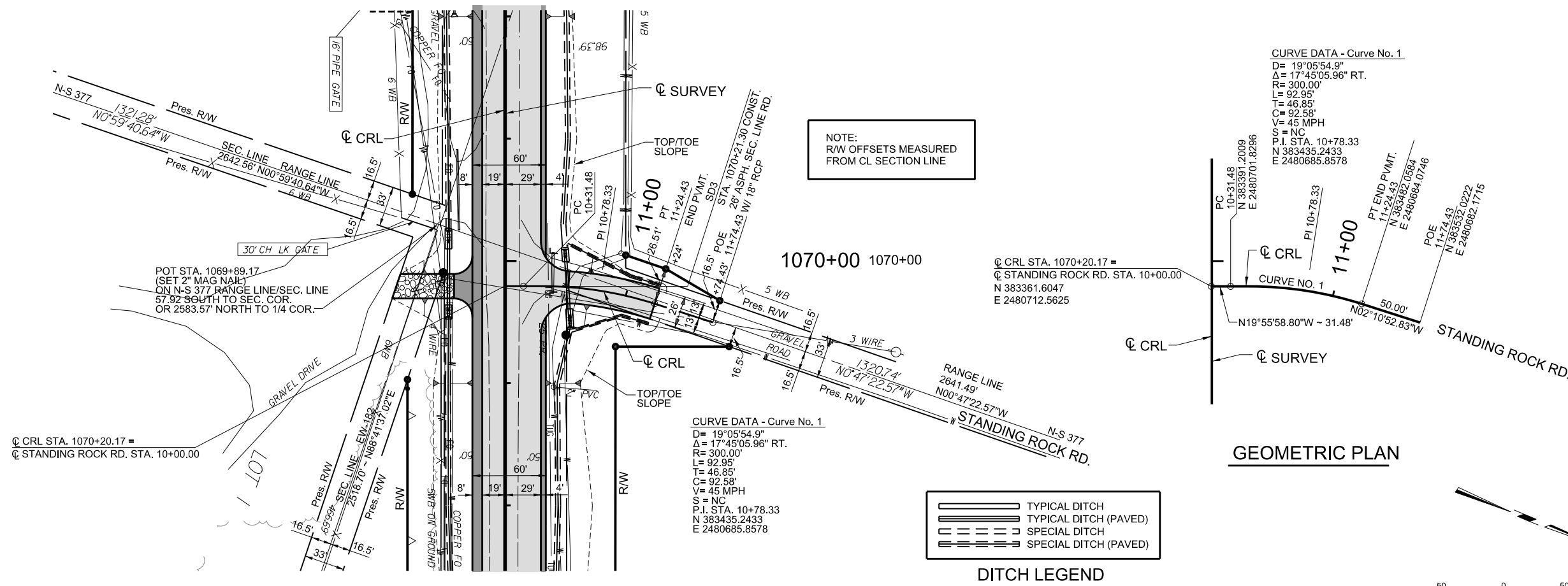
SEC. 24, T-2-S, R-10-E

CRL STA. 1348+00.00
 END CONST. J/P NO. 24066 (04)
 BEGIN CONST. J/P NO. 24066 (07)
 SUBSECTION NO. 11.40

N 383835.9856
 E 2507818.9882

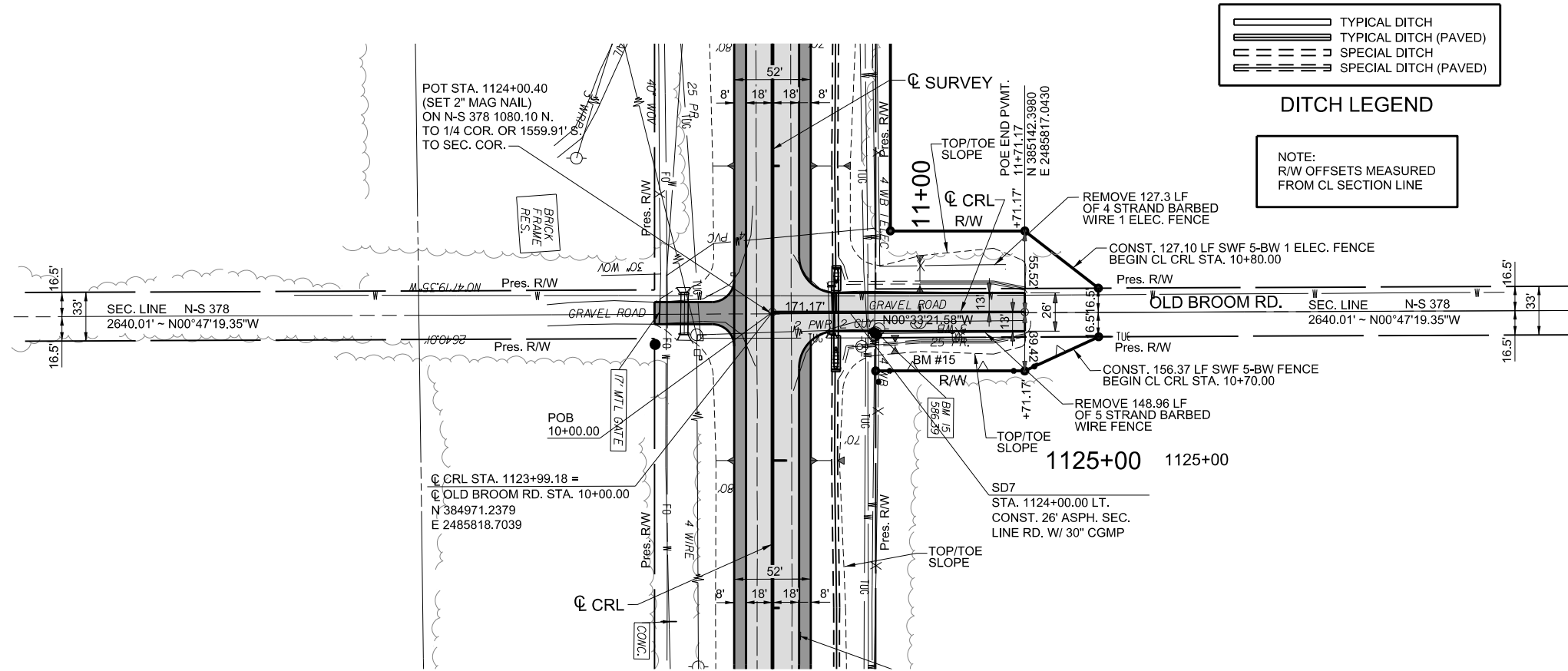


PRINT DATE: 5/25/2018 C:\Projects\1550-SHT Final Plans\DWG\1550-24066(04)-P&P23.dwg

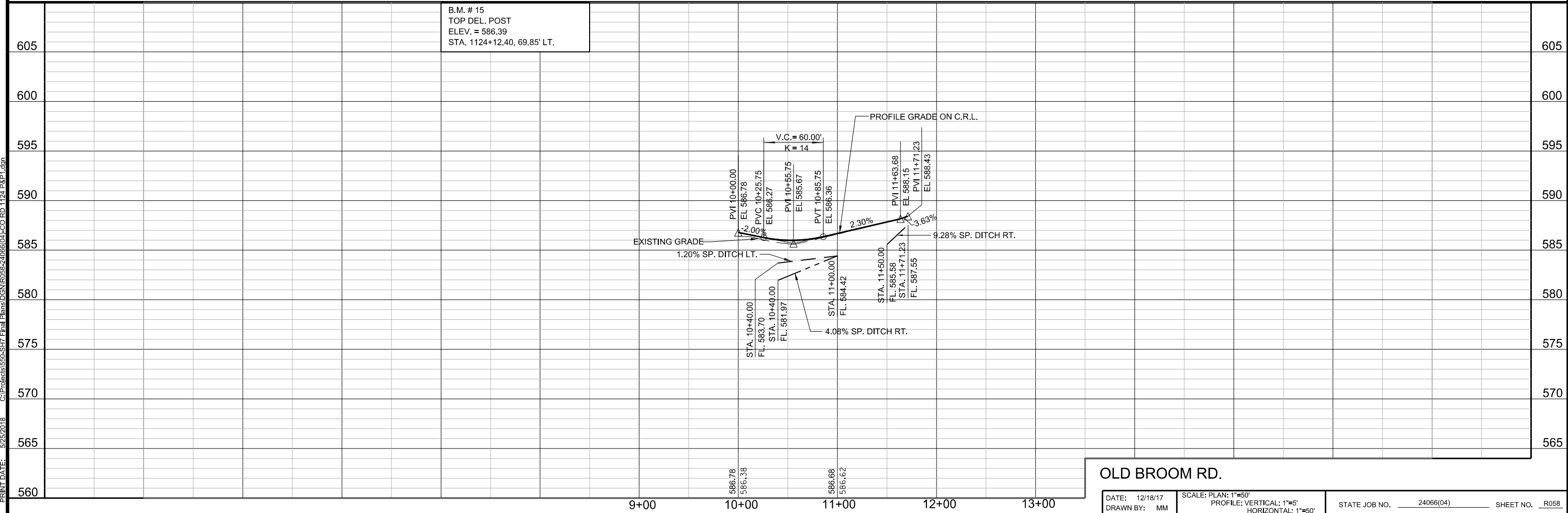


PRINT DATE: 8/24/2018 C:\Projects\150-SHT Final Plans\057-24066(04)-CO RD 1070 P&P1.dgn

SEC. 18, T-2-S, R-10-E



SEC. 17, T-2-S, R-10-E



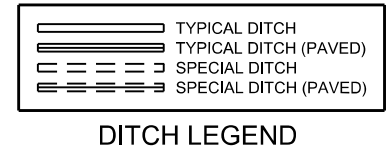
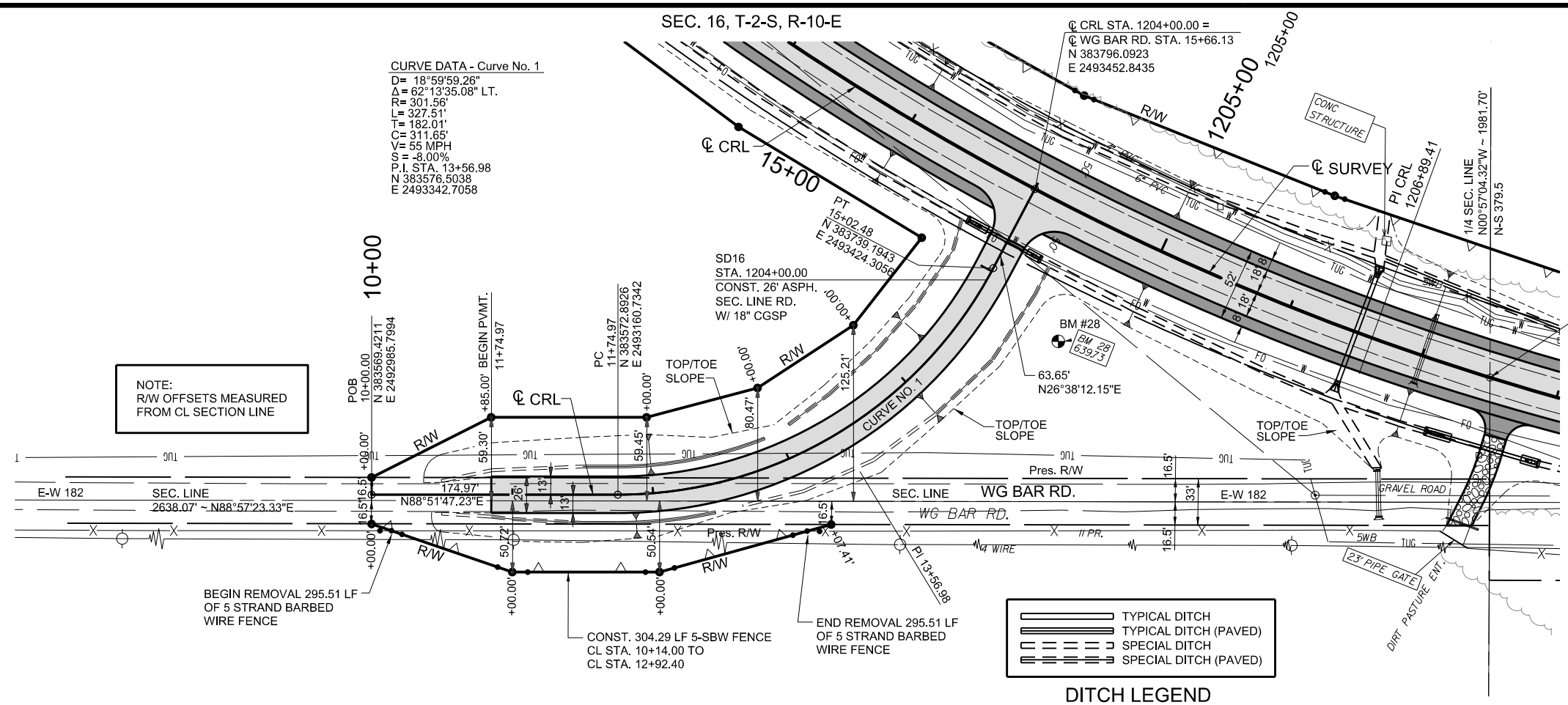
PRINT DATE: 5/25/2018 C:\Projects\550-SHT Final Plans\DWG\18-24066(04)-CO RD 1124 P&P1.dwg

OLD BROOM RD.

SEC. 16, T-2-S, R-10-E

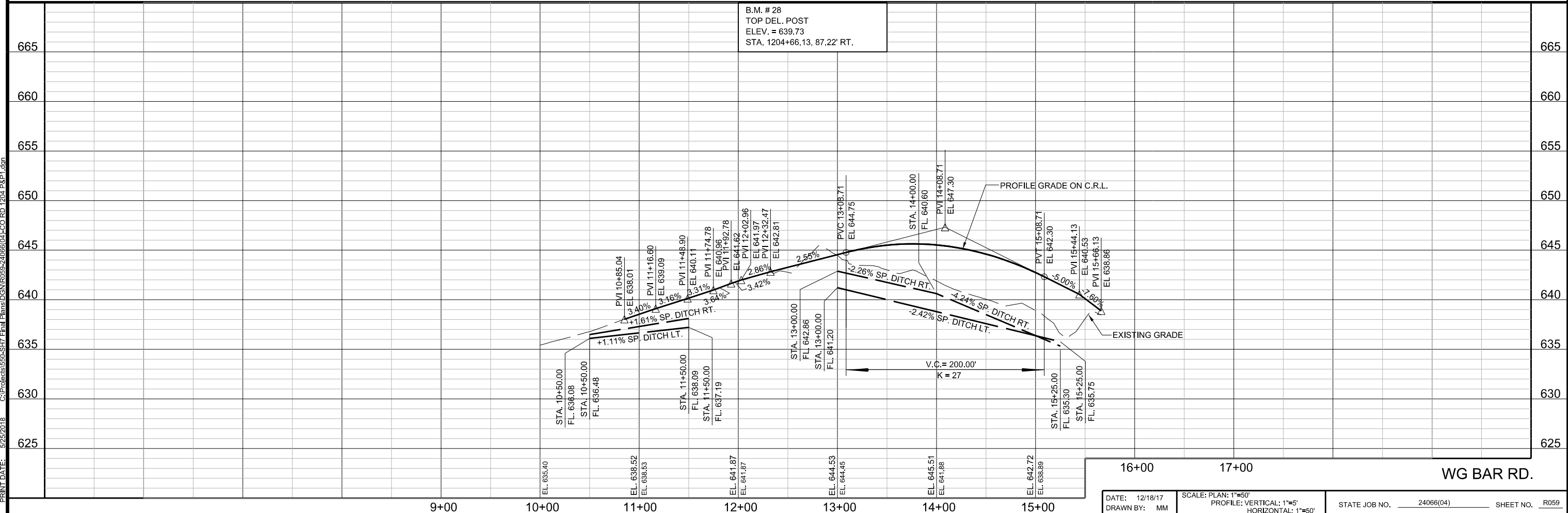
CURVE DATA - Curve No. 1
 D = 18°59'59.26"
 Δ = 62°13'35.08" LT.
 R = 301.56'
 L = 327.51'
 T = 182.01'
 C = 311.65'
 V = 55 MPH
 S = -8.00%
 P.I. STA. 13+56.98
 N = 383376.5038
 E = 2493342.7058

NOTE:
 R/W OFFSETS MEASURED
 FROM CL SECTION LINE

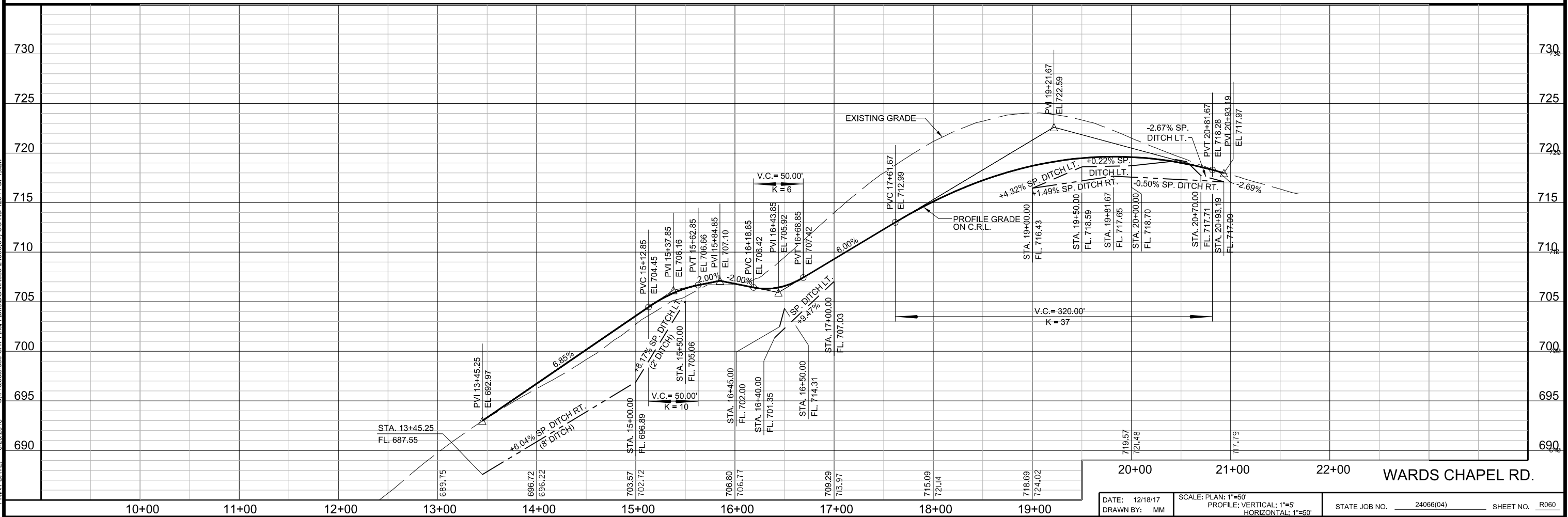
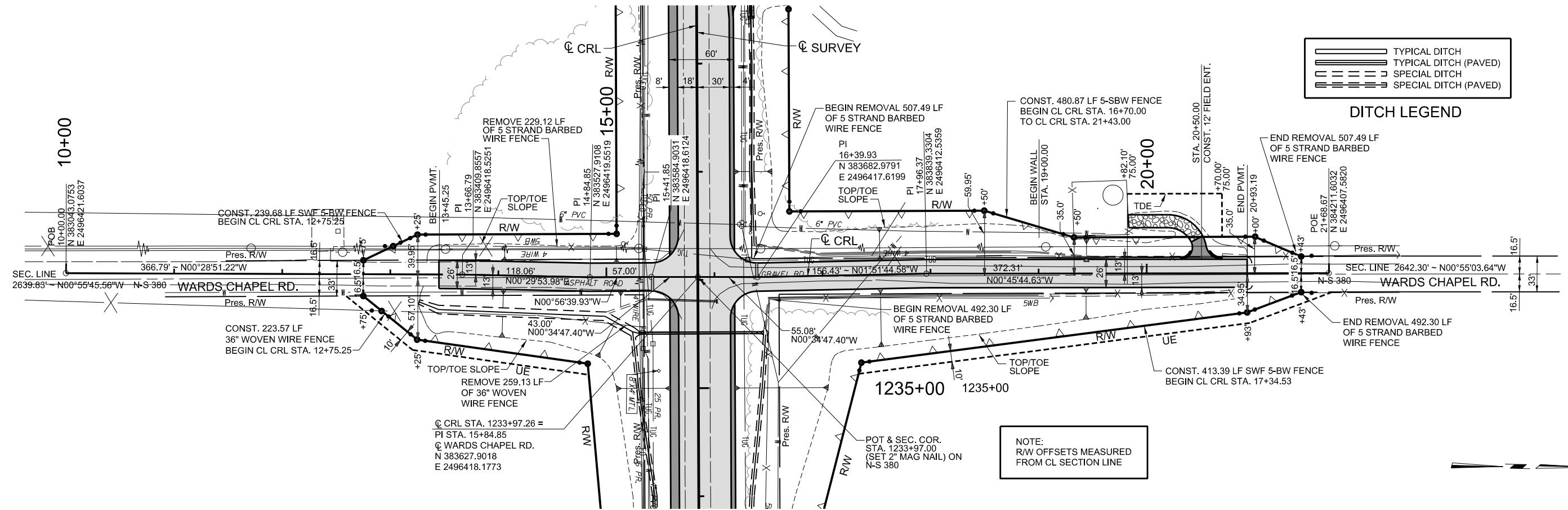


SEC. 21, T-2-S, R-10-E

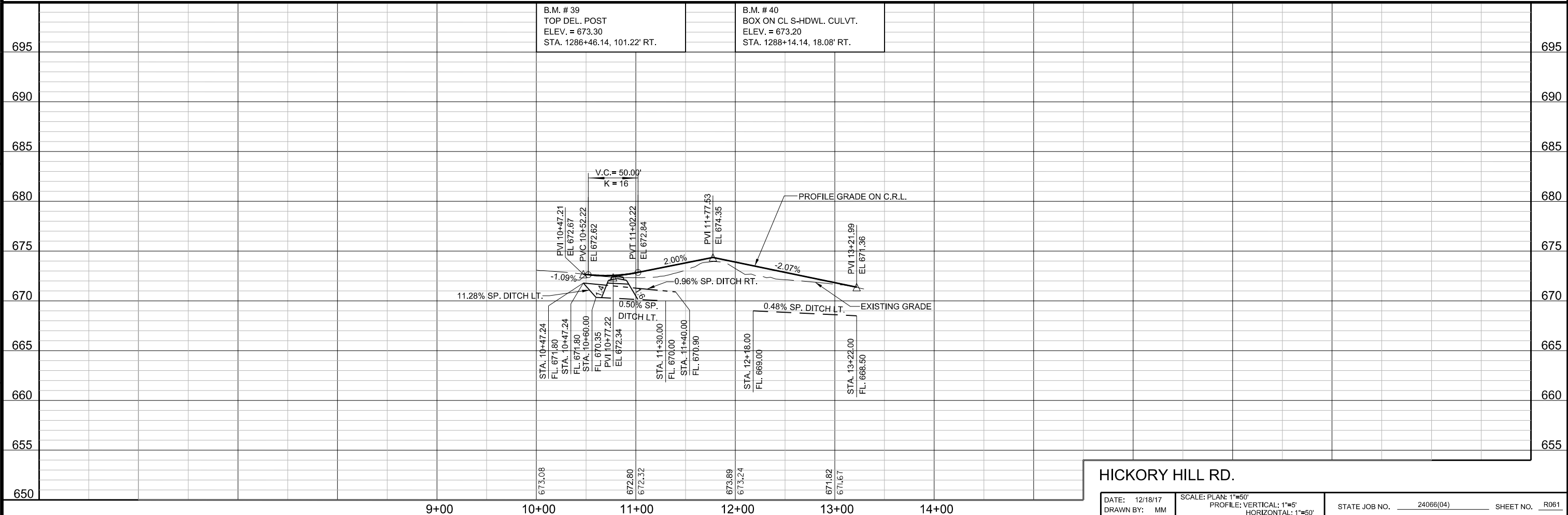
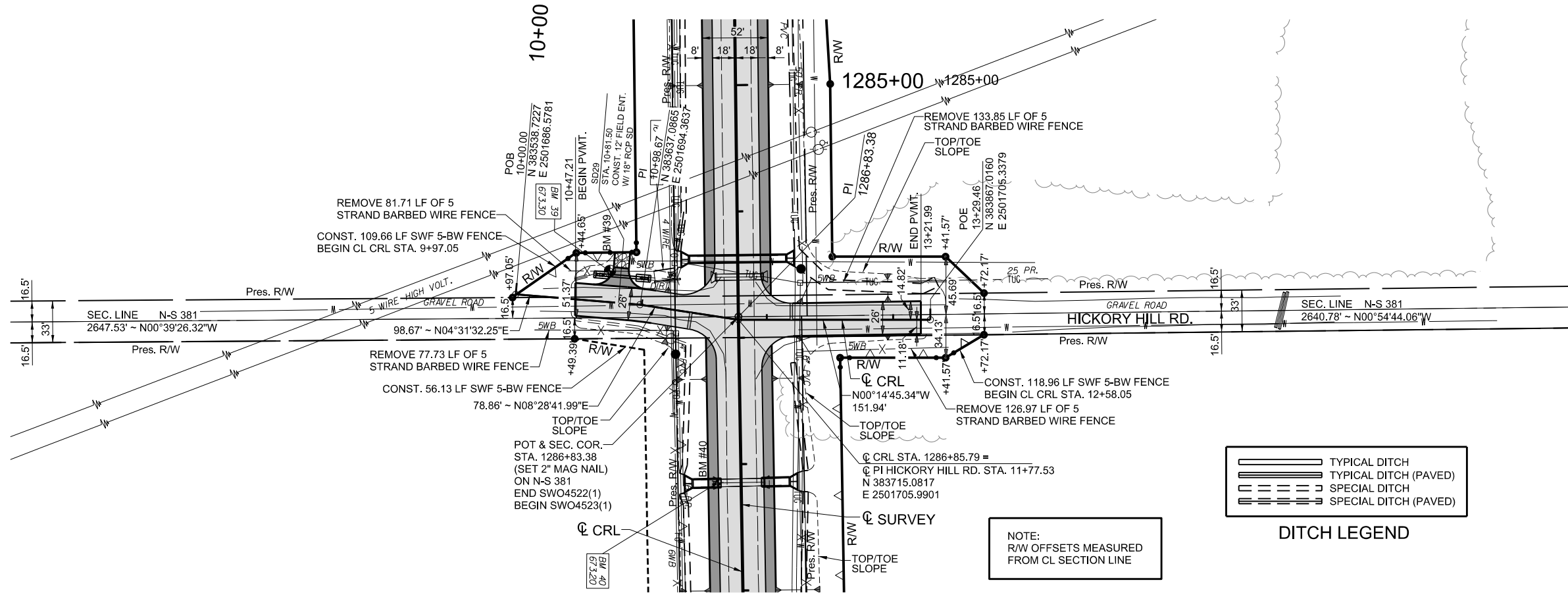
B.M. # 28
 TOP DEL. POST
 ELEV. = 639.73
 STA. 1204+66.13, 87.22' RT.



PRINT DATE: 5/25/2018 C:\Projects\150-SHT Final Plans\DNV\150-SHT Final Plans\04-CO.RD.1204.P&P1.dgn

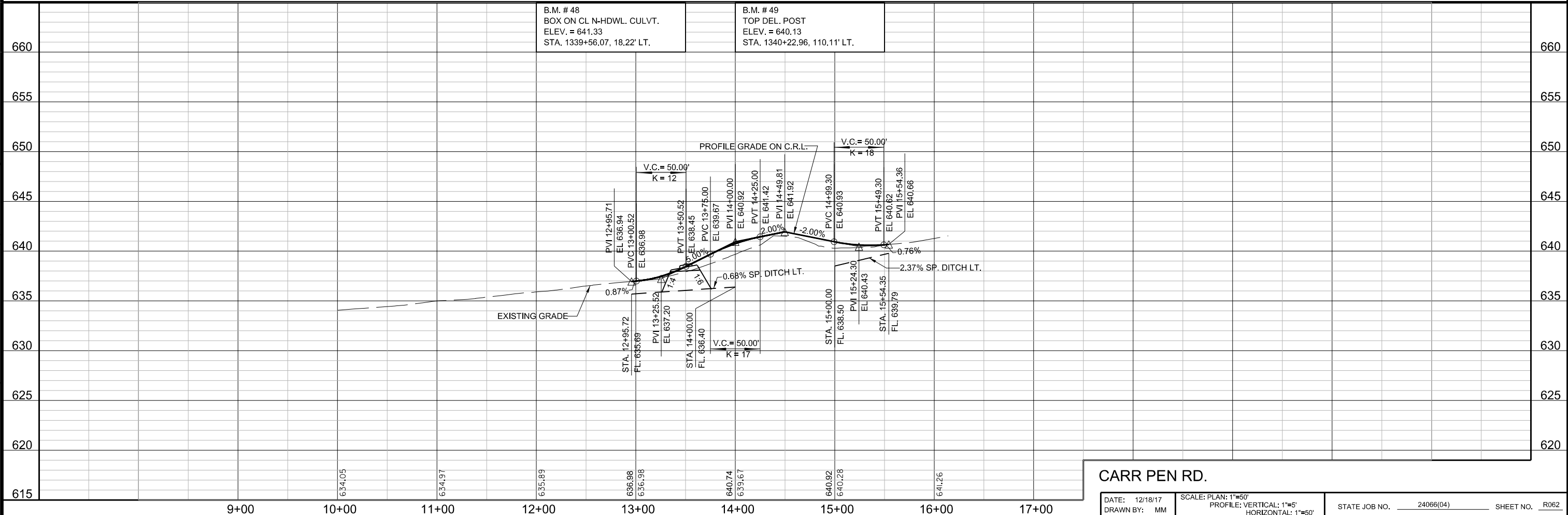
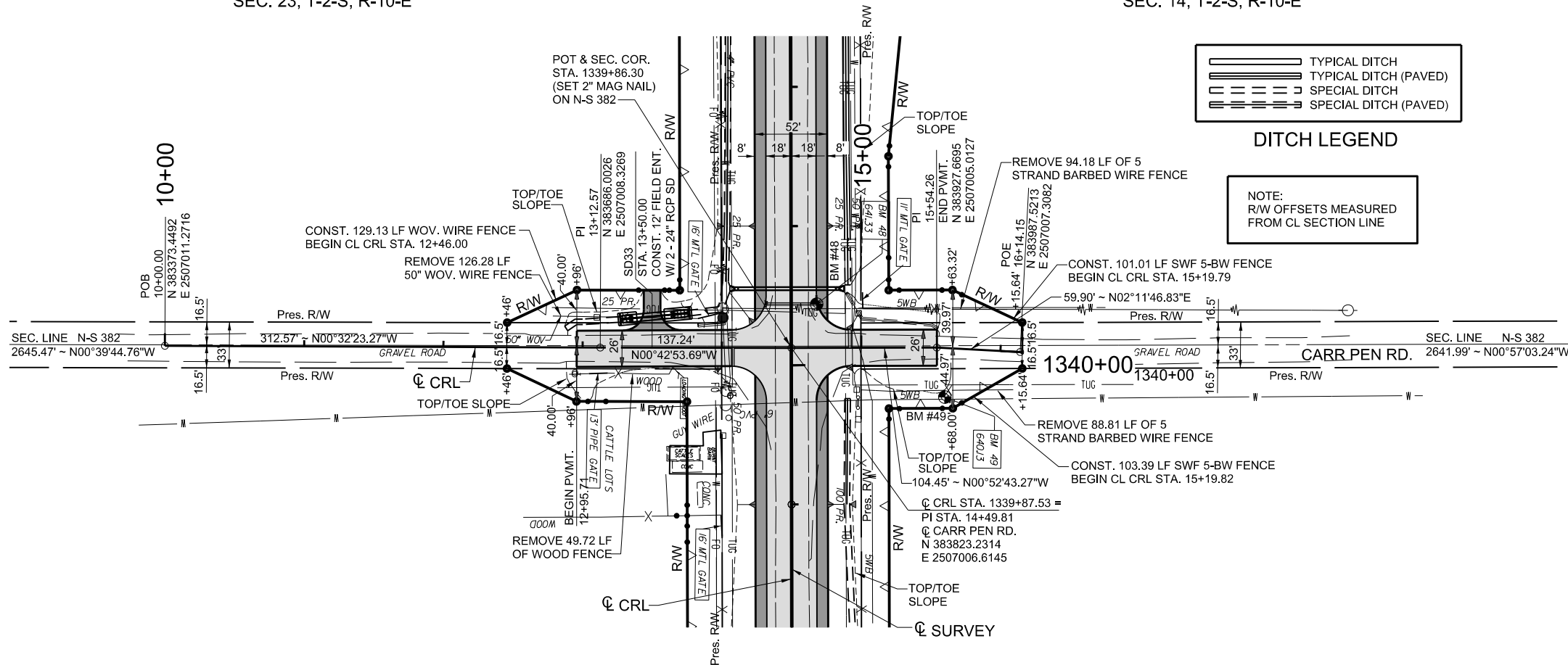


PRINT DATE: 5/25/2018 C:\Projects\150-SHT Final Plans\DWG\150-SHT Final Plans\04-CO.RD.1234.P&P1.dwg



HICKORY HILL RD.

PRINT DATE: 5/25/2018 C:\Projects\150-SHT Final Plans\DG\N\1061-24066(04)-CO RD 1287 P&P1.dgn



PRINT DATE: 5/25/2018 C:\Projects\150-SHT Final Plans\04-CO RD 1340 P&P1.dgn

CARR PEN RD.

OKLAHOMA DEPARTMENT OF TRANSPORTATION						
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS	
6	OKLA.					
DESCRIPTION		REVISIONS		DATE		

SURVEY DATA SHEETS

ATOKA COUNTY

S.H. 7

SWO 4522(1) STATE JOB NO. 24066(05)
From 6.4 miles East of the Johnston County Line, East to N-S 381

SURVEY CONTROL DATA

1. HORIZONTAL CONTROL:

- A. HORIZONTAL CONTROL FOR THIS SURVEY IS THE NGS OKLAHOMA STATE PLANE COORDINATE SYSTEM, NAD83 (1993), LAMBERT PROJECTION (SOUTH ZONE).
- B. ACCURACY - THE PRIMARY CONTROL NETWORK, THE SECONDARY CONTROL NETWORK AND SECTION BOUNDARIES FOR THIS SURVEY ARE IN GENERAL COMPLIANCE WITH THE NGS SECOND ORDER, CLASS II STANDARDS FOR HORIZONTAL CONTROL (1:20,000).

2. BEARINGS:

THE BEARINGS SHOWN HEREIN OR HEREON ARE GRID BEARINGS DERIVED FROM THE NGS OKLAHOMA STATE PLANE COORDINATE SYSTEM AND ARE NOT ASTRONOMICAL. THE ANGLE OF VARIANCE BETWEEN GRID NORTH (GN) AND THE ASTRONOMICAL TRUE NORTH (TN) IS DEPICTED DIAGRAMMATICALLY.

3. VERTICAL CONTROLS:

- A. LEVEL DATUM IS NAVD 88.
- B. ACCURACY - VERTICAL CONTROL FOR THIS SURVEY IS WITHIN THE CLOSURE REQUIREMENT OF NOAA/NGS "CLASSIFICATION, STANDARDS OF ACCURACY, AND GENERAL SPECIFICATIONS OF GEODETIC CONTROL SURVEYS" (FEB. 1974, REPRINTED FEB. 1977) THIRD ORDER STANDARDS AS A MINIMUM.

SURVEY BEGAN: SEPTEMBER 1, 2009 .
SURVEY COMPLETED: JANUARY 28, 2010.

PARTY PERSONNEL
RICKY E. STEELE PLS 1198
DANNY M. GREGG TRANS. SPEC. III
BRYAN L. HINDMAN, TRANS. TECH
MICHAEL BOLING, TRANS. TECH II
CLAYTON MACK, TANS. TECH.

UTILITY INFO:

CALL OKIE LOCATE NUMBERS

WATER LINES ON THIS SURVEY OWNED BY ATOKA CO. RURAL WTR. DIST. #4

ALL WATER LINES ON THIS SURVEY OWNED BY

ALL WATER LINES ON THIS SURVEY OWNED BY

TELEPHONE LINES ON THIS SURVEY ARE OWNED BY AT&T ALL LINES WERE LOCATED OR TAKEN FROM COMPUTER PRINTS PROVIDED BY KEVIN GATHER AN EMPLOYEE OF SMP A CONTRACT LOCATE CO.

GAS LINES ON THIS SURVEY ARE OWNED BY OKLA. NATURAL GAS CO. ALL LINES WERE LOCATED OR TAKEN FROM COMPUTER PRINTS PROVIDED BY KEVIN GATHER AN EMPLOYEE OF SMP A CONTRACT LOCATE CO.

SCALES
SURVEY DATA SHEETS 1" = 100'
GEOMETRIC DATA SHEETS 1" = 500'

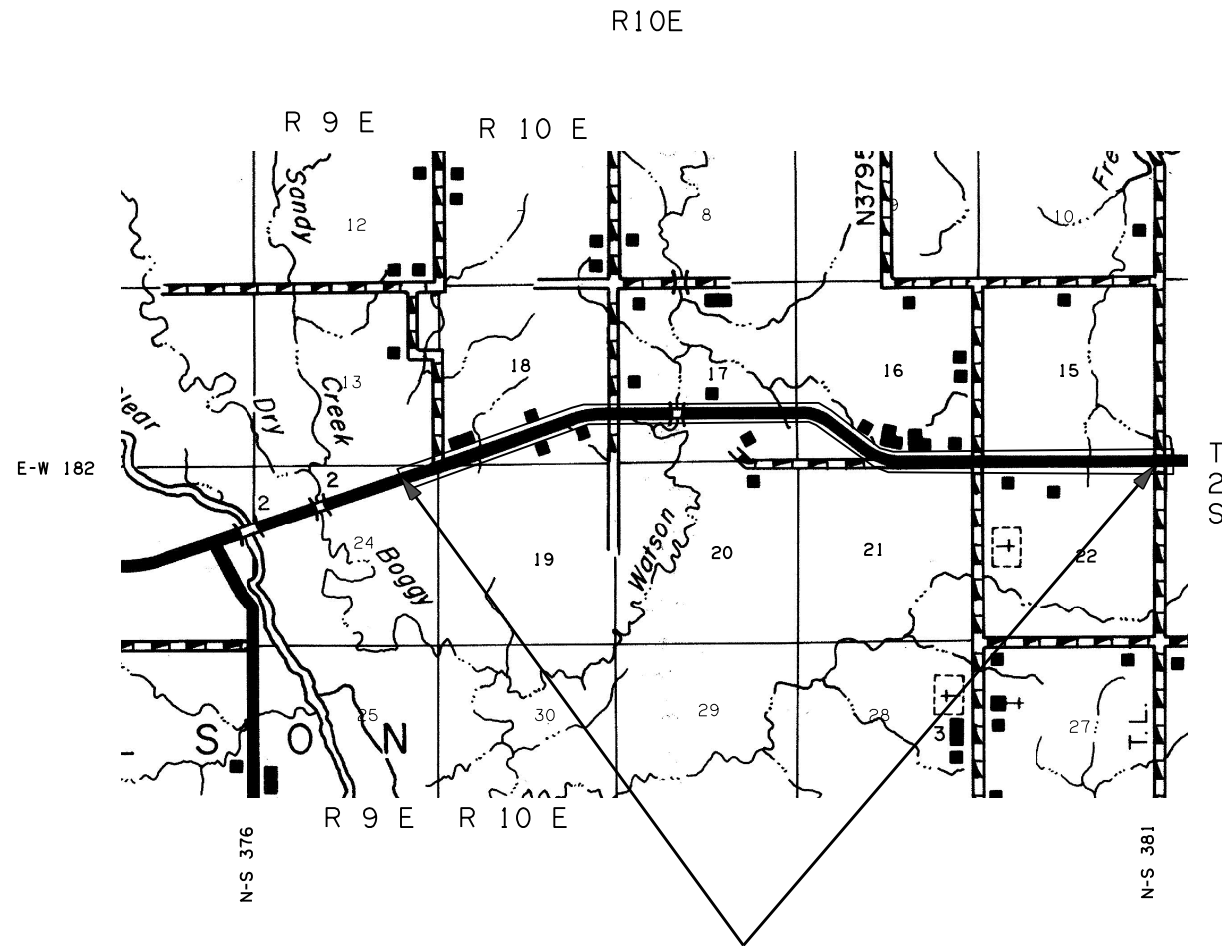
CONVENTIONAL SYMBOLS

	RAILROADS
	RANGE & TOWNSHIP
	SECTION LINES
	QUARTER SECTION LINES
	FENCES
	EXISTING ROADS
	BASE LINE
	TELEPHONE & TELEGRAPH
	POWER LINES
	BUILDINGS
	OILWELL
	DRAINAGE STRUCTURES - IN PLACE

THIS SURVEY MEETS THE OKLAHOMA MINIMUM STANDARDS FOR THE PRACTICE OF LAND SURVEYING AS ADOPTED BY THE OKLAHOMA STATE BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS AND LAND SURVEYORS, JUNE 11, 2001.

SPECIFICATIONS FOR SURVEYS FOR PRIMARY AND SECONDARY HIGHWAYS DATED SEPTEMBER 11, 2001 GOVERN.

SDS _____ OF _____



PROJECT LOCATION

INDEX OF SURVEY SHEETS

1. TITLE SHEET
2. HORIZONTAL CONTROL DIAGRAM & HISTORICAL LETTER & WRITTEN REPORT
3. BENCHMARK LIST
- 4-5 COGO POINTS, ALIGNMENT REPORT,
- 6-18. SURVEY DATA SHEETS
- 19-23 GEOMETRIC DATA SHEETS

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

SWO_4522(1) Job/Piece_24066_(05) Engr.Contract No. _____

LAND SURVEYOR'S CERTIFICATION

I hereby certify that all land and property sub-division distances, angles, corners and monumentation made or used in conjunction with this survey and depicted or recorded herein or hereon were recovered, established or re-established in substantial conformity with:

- applicable instructions contained in the U.S. Government Bureau of Land Management publication "Manual of Surveying Instructions";
- its supplement, "Restoration of Lost or Obliterated Corners and Subdivision of Sections";
- "Oklahoma Minimum Standards for the Practice of Land Surveying" as adopted by the State Board of Registration for Professional Engineers and Land Surveyors; and
- sound land surveying practices;

Including a thorough search, study, analysis and consideration of all existing records and field evidence.

I further certify that all survey monuments depicted exist and that all land survey work was done by me or under my direct supervision and that it is true, accurate and correct to the best of my knowledge and belief.

Dated this 28th day of JANUARY, 2010

(seal)

Land Surveyor _____

Signature

RICKY E. STEELE

Printed Name

Oklahoma Registered Land Surveyor No. 1198

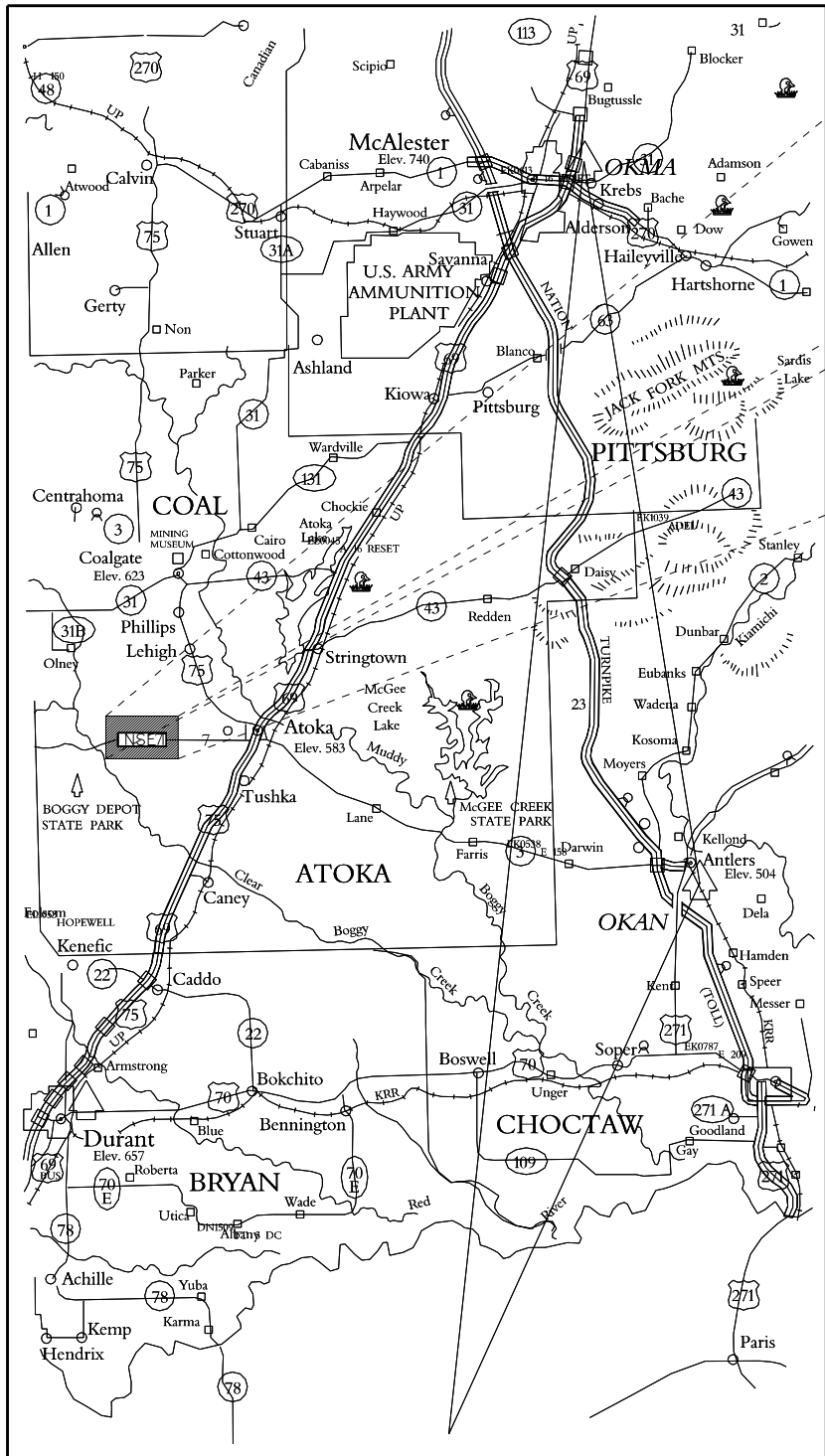


Electronic File Transfer Disclaimer:

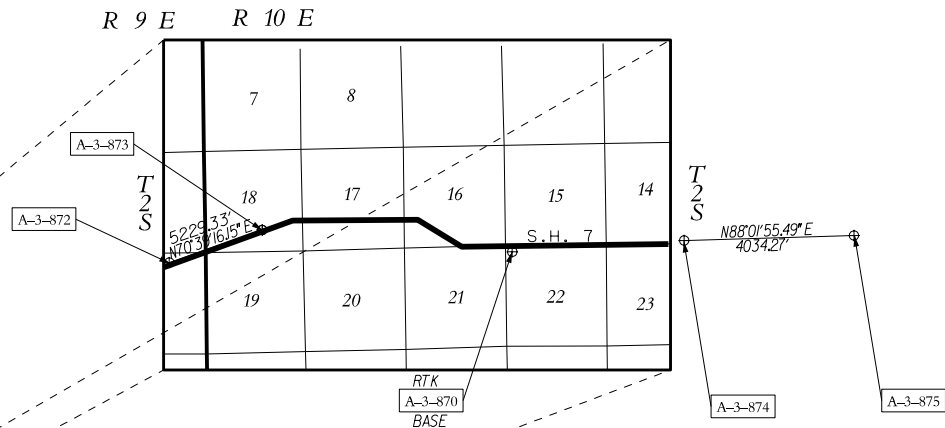
These Files, Drawings and/or Notes are provided for information only. The Oklahoma Department of Transportation (ODOT) and the Owner cannot be held responsible for the content or accuracy of these Files, Drawings and/or Notes due to conversions, software translations, or any other manipulation of said Files, Drawings and/or Notes. ODOT expressly disclaims any responsibility arising from any use of these Files, Drawings and/or Notes. To the full extent permitted by applicable law, the recipient of these Files, Drawings and/or Notes hereby agrees to defend, indemnify, and hold harmless ODOT and the Owner from and against any and all claims, suits, actions, damages, loss, liability or costs of every nature or description (including reasonable attorney's fees) arising from, or in any way attributable to or connected with any of these Files, Drawings and/or Notes.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS	RES	SURVEY DATA SHEET	
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS		
SWO 4522(1) STATE JOB NO. 24066(04) SHEET NO. 5001			

HORIZONTAL CONTROL



INSET



7400 - A-3-872
 FILE:74002440.09o 000105287

NGS OPUS SOLUTION REPORT

USER: rsteel@odot.org DATE: September 02, 2009
 RINEX FILE: 7400244n.09o TIME: 17:35:40 UTC

SOFTWARE: page5 0908J3 master.pl 081023 START: 2009/09/01 13:42:00
 EPHEMERIS: lgu15472.eph [ultra-rapid] STOP: 2009/09/01 17:33:00
 NAV FILE: brdc2440.09n OBS USED: 8180 / 8309 : 98%
 ANT NAME: LEIAT502 NONE * FIXED AMB: 29 / 35 : 83%
 ARP HEIGHT: 1.653 OVERALL RMS: 0.012(m)

REF FRAME: NAD_83(CORS96)EPOCH:2002.0000 ITRF00 (EPOCH:2009.6675)

X: -579121.669(m) 0.041(m) -579122.386(m) 0.041(m)
 Y: -5238202.669(m) 0.028(m) -5238201.259(m) 0.028(m)
 Z: 3580762.939(m) 0.047(m) 3580762.776(m) 0.047(m)

LAT: 34 22 23.23032 0.030(m) 34 22 23.25019 0.030(m)
 E LON: 263 46 28.15037 0.043(m) 263 46 28.11641 0.043(m)
 W LON: 96 18 31.84963 0.043(m) 96 18 31.88359 0.043(m)
 EL HGT: 140.458(m) 0.040(m) 139.274(m) 0.040(m)
 ORTHO HGT: 167.567(m) 0.047(m) [NAVD88 (Computed using GEOID03)]

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Northing (Y) [meters] 3806810.551 116639.245
 Easting (X) [meters] 747463.509 755534.185
 Convergence [degrees] 1.52014269 0.99992658
 Point Scale 1.00035491 0.99994269
 Combined Factor 1.00033285 0.99992064

US NATIONAL GRID DESIGNATOR: 14SQD4746406811NAD 83)

PID DESIGNATION LATITUDE LONGITUDE DISTANCE(m)
 DG9767 OKAR ARMORE CORS ARP N341006.450 W0971009.251 82423.7
 DE8097 OKAD ADA CORS ARP N344801.213 W0964417.707 61630.4
 DE7170 OKMA MCALESTER CORS ARP N345540.833 W0954414.056 80841.5

A-3-873 7401
 FILE: 66722390.000 000103522

NGS OPUS SOLUTION REPORT

USER: rsteel@odot.org DATE: August 31, 2009
 RINEX FILE: 6672239s.09o TIME: 13:21:56 UTC

SOFTWARE: page5 0908J3 master.pl 0810233 START: 2009/08/27 18:46:00
 EPHEMERIS: lgr15464.eph [rapid] STOP: 2009/08/27 21:20:30
 NAV FILE: brdc2390.09n OBS USED: 4674 / 4885 : 96%
 ANT NAME: LEIAT502 NONE * FIXED AMB: 23 / 25 : 92%
 ARP HEIGHT: 1.710 OVERALL RMS: 0.012(m)

REF FRAME: NAD_83(CORS96)EPOCH:2002.0000 ITRF00 (EPOCH:2009.6543)

X: -577588.681(m) 0.018(m) -577589.398(m) 0.018(m)
 Y: -5238102.171(m) 0.083(m) -5238100.761(m) 0.083(m)
 Z: 3581188.528(m) 0.065(m) 3581188.365(m) 0.065(m)

LAT: 34 22 39.54255 0.099(m) 34 22 39.56242 0.099(m)
 E LON: 263 42 27.35673 0.024(m) 263 42 27.32279 0.024(m)
 W LON: 96 17 32.64327 0.024(m) 96 17 32.67721 0.024(m)
 EL HGT: 159.249(m) 0.033(m) 158.265(m) 0.033(m)
 ORTHO HGT: 186.654(m) 0.041(m) [NAVD88 (Computed using GEOID03)]

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Northing (Y) [meters] 3807353.461 117167.248
 Easting (X) [meters] 748962.800 757038.086
 Convergence [degrees] 1.52961887 0.96926172
 Point Scale 1.00036408 0.99994240
 Combined Factor 1.00033904 0.99991738

US NATIONAL GRID DESIGNATOR: 14SQD48963073531NAD 83)

PID DESIGNATION LATITUDE LONGITUDE DISTANCE(m)
 DG9767 OKAR ARMORE CORS ARP N341006.450 W0971009.251 84016.1
 DE8097 OKAD ADA CORS ARP N344801.213 W0964417.707 62225.4
 DE7170 OKMA MCALESTER CORS ARP N345540.833 W0954414.056 79484.9

A-3-874 - 7402
 FILE: 74022440.09o 000105288

NGS OPUS SOLUTION REPORT

USER: rsteel@odot.org DATE: September 02, 2009
 RINEX FILE: 7402244o.09o TIME: 17:36:55 UTC

SOFTWARE: page5 0908J3 master.pl 081023 START: 2009/09/01 14:28:00
 EPHEMERIS: lgu15472.eph [ultra-rapid] STOP: 2009/09/01 17:41:00
 NAV FILE: brdc2440.09n OBS USED: 7404 / 7578 : 98%
 ANT NAME: LEIAT502 NONE * FIXED AMB: 33 / 36 : 92%
 ARP HEIGHT: 1.653 OVERALL RMS: 0.012(m)

REF FRAME: NAD_83(CORS96)EPOCH:2002.0000 ITRF00 (EPOCH:2009.6676)

X: -570834.546(m) 0.090(m) -570835.263(m) 0.090(m)
 Y: -5239033.816(m) 0.097(m) -5239032.405(m) 0.097(m)
 Z: 3580937.676(m) 0.003(m) 3580937.513(m) 0.003(m)

LAT: 34 22 29.33908 0.048(m) 34 22 29.35899 0.048(m)
 E LON: 263 46 54.1172 0.000(m) 263 46 54.07784 0.000(m)
 W LON: 96 13 5.88828 0.000(m) 96 13 5.92216 0.000(m)
 EL HGT: 174.767(m) 0.072(m) 173.582(m) 0.072(m)
 ORTHO HGT: 202.369(m) 0.076(m) [NAVD88 (Computed using GEOID03)]

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Northing (Y) [meters] 3807223.529 116970.688
 Easting (X) [meters] 755786.696 763857.366
 Convergence [degrees] 1.5714107 1.01132133
 Point Scale 1.00040655 0.99994258
 Combined Factor 1.00037910 0.99991515

US NATIONAL GRID DESIGNATOR: 14SQD55787072241NAD 83)

PID DESIGNATION LATITUDE LONGITUDE DISTANCE(m)
 DL3498 TXBN BONHAM CORS ARP N333624.195 W0961031.156 85293.0
 DE7170 OKMA MCALESTER CORS ARP N345540.833 W0954414.056 75572.2
 DE7168 OKAN ANTLERS CORS ARP N341142.708 W0953716.847 58465.7

A-3-875 - 7403
 FILE: 68122390.000 000103526

NGS OPUS SOLUTION REPORT

USER: rsteel@odot.org DATE: August 31, 2009
 RINEX FILE: 6812239s.09o TIME: 13:27:26 UTC

SOFTWARE: page5 0908J3 master.pl 0810233 START: 2009/08/27 18:37:00
 EPHEMERIS: lgr15464.eph [rapid] STOP: 2009/08/27 21:4:00
 NAV FILE: brdc2390.09n OBS USED: 5207 / 5294 : 98%
 ANT NAME: LEIAT502 NONE * FIXED AMB: 28 / 31 : 90%
 ARP HEIGHT: 2.00 OVERALL RMS: 0.013(m)

REF FRAME: NAD_83(CORS96)EPOCH:2002.0000 ITRF00 (EPOCH:2009.6543)

X: -569610.882(m) 0.022(m) -569611.599(m) 0.022(m)
 Y: -5239154.698(m) 0.105(m) -5239153.287(m) 0.105(m)
 Z: 3580954.093(m) 0.042(m) 3580953.930(m) 0.042(m)

LAT: 34 22 30.00288 0.027(m) 34 22 30.02280 0.027(m)
 E LON: 263 47 42.23566 0.033(m) 263 47 42.20179 0.033(m)
 W LON: 96 12 17.76434 0.033(m) 96 12 17.79821 0.033(m)
 EL HGT: 173.943(m) 0.108(m) 172.757(m) 0.108(m)
 ORTHO HGT: 201.622(m) 0.111(m) [NAVD88 (Computed using GEOID03)]

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Northing (Y) [meters] 3807277.796 117012.919
 Easting (X) [meters] 757015.699 765086.266
 Convergence [degrees] 1.57897835 1.01890909
 Point Scale 1.00041432 0.99994257
 Combined Factor 1.00038700 0.99991527

US NATIONAL GRID DESIGNATOR: 14SQD57016072781NAD 83)

PID DESIGNATION LATITUDE LONGITUDE DISTANCE(m)
 DL3498 TXBN BONHAM CORS ARP N333624.195 W0961031.156 85264.8
 DE7170 OKMA MCALESTER CORS ARP N345540.833 W0954414.056 74846.9
 DE7168 OKAN ANTLERS CORS ARP N341142.708 W0953716.847 57317.2

PLS	RES	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	BLH	SURVEY DATA SHEET
CHECKED		
APPROVED		
CREW	ANTLERS SWO 4522.11 PROJECT NO. 240661041 SHEET NO. S002	

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				

DESCRIPTION	REVISIONS	DATE

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 Survey Division (405) 5212621 Fax 4055220364

Date: January 28, 2010

To: Mr. Larry Reser, Chief of Surveys
 From: Ricky E. Steele, Professional Land Surveyor

Subject: SWO SWO4522 (1) - J/P24066 (05) - S.H 7 Atoka County
 From 6.4 miles East of the Johnston Co. Line, East approx. 4 miles to N-S 381.

HISTORICAL LETTER AND WRITTEN REPORT

Method of Survey Conventional Survey Methods
 Units of Measurement U.S. Survey Foot.
 Survey Began September 1, 2009
 Survey Completed January 28, 2010

1. General

Previous Surveys and plans used on this project.

Surveys:
 SWO 1577 (1) -S.H.7 From U.S. 69 at Atoka, west toward Wapanucka.
 SWO4205 (1) Bridge over Clear Boggy Overflow approx. 5.2 mi. East
 Of the Johnston County Line a Contract Survey by SRB.

Plans:
 S.A.P. No. 1043) S.H.7 From U.S. 69 at Atoka, west toward Wapanucka.

2. Survey Assignment:

This project was assigned to the Antlers Survey Crew, under my direct supervision verbally by Mr. Jeff King Transportation Survey Manager.

3. Purpose of Survey:

The purpose of this survey is to obtain adequate information for the design and construction of Upgraded roadway and shoulders.

4. Survey Limits:

This survey begins at POT STA1061+50.00 (SET 2" MAG NAIL) =
 POT STA. 1061+50 ON SWO 4205 (1) SURVEY =
 POT STA. 1061+50 ON SAP NO.SAB-103C (068) PLANS. END PROJECT
 J/P20921 (04)

5. Alignment:

The alignment for this survey is along and identical to Present S.H. 7 as depicted in SWO 1577 (1) Survey. There are 4 tangents on this survey, the following original monumentation was used to establish Centerline of Survey:

- a. PI STA. 973+28.78 ON SWO 4205 (1) CONTRACT SURVEY
 FOUND ORIG. IRON PIN.
- b. PI STA. 1117+22.80 (FOUND 1" SQUARE BAR) = PI STA. 465+48.4 ON
 SAP1043 (1) PLANS = PI STA. 465+48.4 ON SWO1577 (1) SURVEY.
- c. PI STA. 1184+16.70 (FD. 1" SQ. BAR) = PI STA. 398+37.36 ON
 SWO1577 (1) SURVEY= PI STA. 398+37.36 ON SAP 1043 (1) PLANS.
- d. PI STA. 1206+89.41 (FD. 1" SQ. BAR) = PI STA. 376+26.55 ON
 SAP 1043 (1) PLANS = PI STA. 376+26.55 ON SWO 1577 (1) SURVEY.
- e. PI AND SEC. COR. STA. 1286+83.38 (SET FROM ORIG. RP) =
 PI AND SEC. COR.S STA. 295+78.4 ON SAP 1043 (1) PLANS =
 PI AND SEC. COR. STA. 295+78.4 ON SWO 1577 (1) SURVEY

6. Stationing:

Stationing on this survey is taken from POT STA. 1061+50 ON SAP NO.SAB-103C (068) PLANS.
 END PROJECT J/P20921 (04) and carried forward without equation to POT & SEC. COR.
 STA. 1286+83.38 (SET 2" MAG NAIL) ON N-S 381, END SWO4522 (1) BEGIN SWO4523 (1)

7. Horizontal Control:

Horizontal Control for this survey is the Oklahoma State Plane Coordinate System,
 NAD 83 (1993) Lambert Projection South Zone derived submitting Leica GPS rawdata from
 3 hour static sessions to NGS OPUS for processing. Four project control monuments were
 established for this survey; two near the beginning, and end of the project. The project control Monuments
 on this project are ODOT Std. Feno markers set flush with the ground in concrete. Secondary Control for
 this survey is Centerline of Survey POT, POST and Curve Points set and referenced using GPS Real Time
 Kinematic methods, following accepted ODOT Survey Division methods and techniques.
 The primary control network, the secondary control network and the section boundaries for this survey are in
 general compliance with NGS Second Order, Class II Standards for horizontal control (1:20,000). It is assumed
 that the GPS positional accuracies obtained have met or exceeded this standard.

8. Vertical Control:

Level datum for this survey is (NGS) N.A.V.D. 88 taken from Primary Control Monument
 A-3-872 (The most westerly control point) and a double set of spirit levels were run with a Leica NA 2002
 Digital Level through the project benchmarks and tied to ODOT BRASS MON. NO. A-3-672 CL W-HDWL.
 CULVT. STA. 824+70 ON F-219 (6) US 69 PLANS. And ODOT BRASS MON. NO. A-3-673 CL EAST
 END CONC. RET. WALL RT. APPROX. STA. 84+14 US 69 PLANS. These monuments were established
 under SWO 3915 (1) Benchmark densification project. This survey meets the requirements of N.G.S.
 3rd order standards as a minimum.

9. Topography:

Topographic information was obtained by field conventional methods. Surface improvements and underground
 utilities such as; Fences, drives, mail boxes, signs etc. have been labeled and can be found in the project TOPO File.

10. Surface features:

The surface features on this project were obtained by the field conventional method, a Surface Feature File (SFF)
 which is a Microstation V8 Design File containing breaklines and random ground points for contouring and quantity
 computations has been created and archived as per instructions.

11. Land & Property Ties:

Complete land ties were obtained on Sections 15, 16, 17, 18, 19, 21 & 22, T-2-S, R-10-E.
 the following is a complete history of the PLSS Corners established on this project.
 NOTE See the Geometric Data Sheets for Land Corner Information.

12. RightofWay:

The Right-of-Way on this survey is taken from SAP1043 (1) Plans and verified using R/W deeds provided by Right-of-Way Division.

13. Utilities:

The underground utilities on this project were flagged or taken from digital records by the owning companies or their
 representatives with the help of crew personnel. Utilities were located by conventional methods and placed in the project design file at zero elevation
 and labeled with owner name, size and depth, when provided. ODOT Form SD7, "Public and Privately Owned Utilities List." have
 been submitted with this project. Contact information for utility owners can be found on the 1st Survey Data Sheet.

14. Hazardous Materials or Waste Sites:

There were no hazardous waste sites found during this survey.
 There were no abandoned underground storage tanks found on this project.

15. Drainage Information:

- a. Drainage areas for all drains crossing the Survey Centerline were taken from digital USGS Quad. maps provided by the Aerial Survey Branch ,
 the divide lines between these drainage areas have been placed in a Microstation V8 Design File and archived as per instructions.
- b. Highwater information was obtained by interviewing longtime residents in the area, this information can be found placed in the
 Microstation V8 Drainage Design File.
- c. No Ravine sections for drains crossing the Survey Centerline were required on this survey.
- d. Flowline profiles have been obtained on drains crossing Centerline of Survey 500' Upstream and 1000' downstream .

16. Submission of Survey Data:

All digital survey data has been placed in the appropriate project folder on the ODOT Intranet
 storage system as per archiving instruction dated May 20, 2003

For a complete listing of computer files created and archived on this project see;
http://intranet/enggrp/survey/fsvarch/SWO4522_1/index.txt

In addition to the computer files submitted, hard copies of the following have been submitted
 to the Central Survey Office:

- A. Historical Letter & Written Report
- B. Form SD-1, Transmittal Letter w/FSVARCH.INDEX attached.
- Note: The FSVARCH.INDEX is a listing of all computer files archived during the course of the survey,
 as well as a written description of what is contained in each of the files, and the date the files were archived.
- C. Form SD-7, Public and Privately Owned Utilities List w/ vicinity map on back.
- D. Form SD-9, Final Cost Report of Survey
- E. (4) Form SD-11, Position and Description of Survey Monuments (GPS control monuments,
 Brass/Aluminum Caps for benchmarks, etc.) (if applicable)
- F. Form SD-20, Survey Control Data Statement.
- H. Form SD-41, Surveyor's Certification.
- I. Cogo Data (coordinate list with alignments)
- J. Benchmarks & Check Levels list, including the SWO and description of the project.
- K. (46) Original and reduced copies (8" x 11") of Certified Land Corner Forms.

Personnel:

Ricky E. Steele Prof. Land Surveyor II
 Danny M. Gregg Transportation Specialist III
 Bryan L. Hindman Transportation Tech. I
 Michael W. Boling Transportation Tech. II
 Clayton R. Mack Transportation Tech. I

Respectfully;

Ricky E. Steele, Prof. Land Surveyor

PLS				OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. 5003
DRAWN				
CHECKED				
APPROVED				
CREW				

SW04522(1) COGOLIST

2	384073.1054	2522784.2461	0.00	SURVEY MONUMENT
3	384069.7597	2522784.4669	0.00	SEC COR
4	384092.5135	2520173.8162	0.00	REFERENCE POINT
5	383958.2427	2519251.4323	0.00	REFERENCE POINT
6	384064.0050	2518981.8034	0.00	REFERENCE POINT
10	384285.1429	2525329.6469	0.00	REFERENCE POINT
11	384227.3356	2525362.7272	0.00	REFERENCE POINT
12	384116.8537	2525426.1208	0.00	SURVEY MONUMENT
13	384078.2228	2519249.3158	0.00	REFERENCE POINT
14	384116.2052	2525426.3217	0.00	
15	391739.4222	2506880.5349	0.00	FILLER CAP
24	383562.0249	2496697.5919	0.00	
25	383065.8613	2479893.6729	0.00	
28	384406.2128	2483715.7872	0.00	
34	382673.9230	2478781.7124	0.00	
35	380060.3615	2471600.2482	0.00	CENTERLINE PI
38	383525.8232	2481162.8991	0.00	ORIG STONE
39	383951.7138	2482338.1085	0.00	POT
40	384445.7469	2483701.3514	0.00	POT
41	384991.1293	2487869.4780	0.00	POT
42	383846.9011	2513408.8578	0.00	
43	383810.8547	2506218.2928	0.00	PI
45	383905.5333	2512248.7776	0.00	
47	372960.9346	2491310.4660	0.00	SURVEY MONUMENT
48	383842.9670	2513116.2596	0.00	
49	385009.0907	2483716.3452	0.00	POT
50	383715.0306	2501703.5781	0.00	
51	384084.9453	2523551.3195	0.00	
52	383922.4636	2518146.8242	0.00	
53	384198.1172	2492881.7876	0.00	POT
54	383611.4644	2495421.0569	0.00	POT
55	383636.1883	2496920.8531	0.00	POT
56	383659.2639	2498320.6629	0.00	POT
57	383695.5255	2500520.3640	0.00	POT
58	383732.3594	2502520.0194	0.00	POT
59	373069.9786	2496587.5251	0.00	SURVEY MONUMENT
60	383762.0675	2503919.7042	0.00	POT
61	383807.6907	2506069.2200	0.00	POT
62	383834.4158	2507719.0015	0.00	POT
63	383857.9629	2509218.8167	0.00	POT
64	383872.0912	2510118.7058	0.00	POT
65	383872.5142	2515016.6425	0.00	POT
66	372960.7946	2491310.4660	0.00	SURVEY MONUMENT
67	383890.0650	2516116.5025	0.00	POT
68	384036.2020	2520411.8507	0.00	POT
69	384054.0547	2521561.7121	0.00	POT
70	384084.3267	2523511.4771	0.00	POT
71	384099.3095	2524411.3524	0.00	POT
72	384140.2235	2526861.0107	0.00	POT
73	372852.4411	2486027.4276	0.00	SURVEY MONUMENT
102	383351.7772	2480682.6337	0.00	POT
301	390370.5977	2506907.2426	0.00	FENCE
302	31750.5247	2506879.7120	0.00	FILLER CAP
303	384018.2321	2519250.3740	0.00	PC
304	386538.4304	2519250.3740	0.00	SURVEY MONUMENT
305	384014.9129	2518982.8553	0.00	PI
306	389178.3075	2512165.6977	0.00	SURVEY MONUMENT
310	378618.9450	2512349.4692	0.00	SURVEY MONUMENT
311	367963.4969	2507218.4863	0.00	SURVEY MONUMENT
312	367950.6181	2507228.9747	0.00	SURVEY MONUMENT
313	370603.2229	2507187.1443	0.00	SURVEY MONUMENT
314	373245.0578	2507149.8563	0.00	cl drd
315	375221.9880	2507117.2036	0.00	cl fe
316	376552.8349	2507096.4564	0.00	cl fe
321	368054.5600	2512510.1300	0.00	orig townshp cor.
400	383293.8586	2480683.3017	0.00	SURVEY MONUMENT
401	385935.0965	2480646.9001	0.00	SURVEY MONUMENT
402	388576.6958	2480610.5043	0.00	SURVEY MONUMENT
403	393964.0987	2485673.4703	0.00	SURVEY MONUMENT
404	391328.3595	2485721.7532	0.00	SURVEY MONUMENT
405	388691.0045	2485768.7164	0.00	SURVEY MONUMENT
409	378348.8348	2496503.5494	0.00	SURVEY MONUMENT
413	373125.9685	2501853.6594	0.00	SURVEY MONUMENT
414	375767.5752	2501809.8424	0.00	SURVEY MONUMENT
415	386355.4792	2501661.5351	0.00	SURVEY MONUMENT
416	388994.6432	2501621.3485	0.00	SURVEY MONUMENT
417	388952.5657	2498977.0424	0.00	SURVEY MONUMENT
418	388911.7222	2496333.2813	0.00	SURVEY MONUMENT
419	388861.3260	2493692.5905	0.00	SURVEY MONUMENT
422	380651.6924	2480729.1729	0.00	SURVEY MONUMENT
423	378008.9014	2480733.6518	0.00	SURVEY MONUMENT
426	378132.1050	2485922.1764	0.00	SURVEY MONUMENT
429	378447.7086	2481211.5057	0.00	SURVEY MONUMENT
439	367449.5600	2480902.0500	0.00	80d SPK
450	378198.3009	2488563.0400	0.00	IP LSI103
500	383845.8689	2516479.6717	0.00	REFERENCE POINT
501	383777.9344	2513342.0979	0.00	REFERENCE POINT
504	380060.3615	2471600.2482	0.00	PLANS1/4 COR
510	383582.3719	2493656.2494	0.00	PI
511	385029.6307	2491835.9406	0.00	PI
512	384964.5840	2485133.0390	0.00	
521	384160.3775	2528067.7007	0.00	
600	393864.5800	2480525.5700	0.00	cl rd.
618	389147.9105	2509541.9238	0.00	SURVEY MONUMENT
619	389226.4422	2514804.0175	0.00	SURVEY MONUMENT
622	388815.4945	2491056.2823	0.00	SURVEY MONUMENT
7400	382673.9230	2478781.7124	549.64	gps control
7401	384406.2128	2483715.7872	612.28	gps control
7402	383761.3486	2506088.6361	663.81	gps control
7403	383899.8851	2510120.5244	661.43	gps control
7404	383962.5195	2519116.2118	613.44	gps control
7405	384181.9744	2528005.2983	610.54	gps control
7406	383562.0249	2496697.5919	720.97	rtk base
7600	384019.8264	2522577.9242	0.00	
7601	383999.8288	2522578.2347	0.00	
7602	383298.8125	2480683.2335	0.00	
7603	383974.9467	2520975.6278	0.00	
7604	383984.9455	2520975.4726	0.00	
7605	383404.6479	2480681.7748	0.00	
7606	383577.3804	2481158.4156	0.00	
7607	383586.7821	2481155.0085	0.00	
7608	383586.3773	2517766.8222	0.00	
7609	383836.3798	2517766.1411	0.00	
7610	383556.0486	2495092.9324	0.00	
7611	383799.5889	2513580.3815	0.00	

SOS _____ OF _____

7612	383779.5914	2513580.7006	0.00	
7613	383829.2520	2517319.7980	0.00	
7614	383559.2482	2517318.4894	0.00	
7615	383797.8189	2513174.9762	0.00	
7617	383776.8384	2513408.9411	0.00	
7618	383795.6423	2508434.4566	0.00	
7619	383775.6448	2508434.7706	0.00	
7620	383773.2901	2508284.7891	0.00	
7621	383793.2876	2508284.4751	0.00	
7622	383760.8601	2506219.0777	0.00	
7623	383665.0373	2501704.4028	0.00	
7624	383851.7702	2512009.4821	0.00	
7625	383839.3183	2512484.9309	0.00	
7626	383633.4008	2499787.2007	0.00	
7627	383623.4022	2499787.3657	0.00	
7628	383588.7176	2497079.4071	0.00	
7629	383702.8879	2496416.7157	0.00	
7630	383546.0499	2495093.0973	0.00	
7631	384957.6735	2491119.3367	0.00	
7632	385073.0577	2491158.3663	0.00	
7633	384973.0624	2491159.3366	0.00	
7634	384972.6686	2491118.7597	0.00	
7635	384953.3870	249131.8360	0.00	
7636	385053.3823	2489130.8656	0.00	
7637	384895.8755	2486296.9927	0.00	
7638	384925.8741	2486296.7016	0.00	
7639	384569.0967	2492334.8407	0.00	
7640	384946.9599	2488469.5394	0.00	
7641	384177.9647	2523202.3932	0.00	
7642	384129.5455	2523202.1449	0.00	
7643	384029.5576	2523202.6973	0.00	
7644	383991.1385	2523202.4931	0.00	
7645	384185.2249	2523661.5367	0.00	
7646	384202.7231	2523726.8702	0.00	
7647	384203.8191	2523792.4777	0.00	
7648	384190.0870	2523858.3329	0.00	
7649	384205.4088	2524775.7031	0.00	
7650	384187.4321	2524776.0034	0.00	
7651	384196.7652	2525334.8023	0.00	
7652	384164.7488	2525335.3370	0.00	
7653	383990.5939	2523796.0390	0.00	
7654	383964.0228	2523796.4828	0.00	
7655	383977.0996	2524579.4363	0.00	
7656	384020.0726	2524578.7186	0.00	
7657	384030.5548	2525206.3267	0.00	
7658	384015.7966	2525305.0119	0.00	
7659	384016.3445	2525337.8156	0.00	
7660	384064.7627	2525337.0070	0.00	
7661	383895.6300	2508432.8868	0.00	
7662	383915.6275	2508432.5728	0.00	
7663	383893.2753	2508282.9053	0.00	
7664	383913.2728	2508282.5913	0.00	
7665	383587.2684	2497597.6639	0.00	
7666	383597.2670	2497597.4989	0.00	
7667	383677.8464	2496417.1168	0.00	
7668	383743.4177	2499785.3853	0.00	
7669	383733.4190	2499785.5503	0.00	
7670	383951.7579	2512007.9123	0.00	
7672	384020.4579	2482381.0508	0.00	
7673	384926.9608	2488469.7335	0.00	
7674	384845.0628	2488469.4797	0.00	
7675	384647.3119	2492386.0743	0.00	
7676	384911.5169	2485438.2444	0.00	
7677	383619.9999	2481568.9713	0.00	
7678	383591.5949	2481579.1926	0.00	
7679	383898.2361	2482425.3434	0.00	
7680	383926.4411	2482415.1220	0.00	
7681	384881.6014	2485440.4969	0.00	
7682	384889.3307	2485622.5609	0.00	
7683	385039.3236	2485621.1054	0.00	
7684	384751.0460	2484690.5502	0.00	
7685	384891.0965	2485804.5261	0.00	
7686	384891.4167	2485837.5248	0.00	
7687	385041.4038	2485835.4600	0.00	
7688	385041.0835	2485802.4613	0.00	
7689	385011.2344	2485430.7362	0.00	
7690	385031.1780	2485429.2346	0.00	
7691	384932.1816	2486946.6710	0.00	
7692	384912.1825	2486946.8651	0.00	
7693	385068.4923	2488626.8674	0.00	
7694	385048.4932	2488627.0614	0.00	
7695	384047.4466	2493151.6380	0.00	
7696	384970.5872	2493094.2746	0.00	
7697	384955.5879	2493094.4202	0.00	
7698	383643.6461	2493089.4045	0.00	

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				

DESCRIPTION	REVISIONS	DATE

Project Name: SW04522*1*V2
Description: SH7
Horizontal Alignment Name: A001
Description: SW04522 CENTERLINE SURVEY
Style: Centerline

	STATION	EASTING	NORTHING
Element: Linear			
POB (7)	1037+00.00	2477590.2624	382231.1157
PC (8)	1112+34.03	2484673.5146	384798.0544
Tangent Direction:		N 70°04'46.96" E	
Tangent Length:		7534.03	
Element: Circular			
PC (8)	1112+34.03	2484673.5146	384798.0544
PI ()	1117+22.80	2485133.0390	384964.5840
CC (16)	2485649.5840	382104.6728	
PT (17)	1122+02.25	2485621.7846	384969.3269
Radius:		2864.73	
Delta:		19°21'51.46" Right	
Degree of Curvature(Arc):		2°00'00.00"	
Length:		968.21	
Tangent:		488.77	
Chord:		963.61	
Middle Ordinate:		40.81	
External:		41.40	
Tangent Direction:		N 70°04'46.96" E	
Radial Direction:		S 19°55'13.04" E	
Chord Direction:		N 79°45'42.69" E	
Radial Direction:		S 0°33'21.58" E	
Tangent Direction:		N 89°26'38.42" E	

Element: Linear			
PT (17)	1122+02.25	2485621.7846	384969.3269
PC (18)	1177+39.57	2491158.8514	385023.0600
Tangent Direction:		N 89°26'38.42" E	
Tangent Length:		5537.33	

Element: Circular			
PC (18)	1177+39.57	2491158.8514	385023.0600
PI ()	1184+16.70	2491835.9406	385029.6307
CC (19)	2491177.3843	383113.2906	
PT (20)	1190+41.00	2492365.9575	384608.2342
Radius:		1909.86	
Delta:		39°02'34.36" Right	
Degree of Curvature(Arc):		3°00'00.00"	
Length:		1301.43	
Tangent:		677.12	
Chord:		1276.40	
Middle Ordinate:		109.79	
External:		116.48	
Tangent Direction:		N 89°26'38.42" E	
Radial Direction:		S 0°33'21.58" E	
Chord Direction:		S 71°02'04.40" E	
Radial Direction:		S 38°29'12.78" W	
Tangent Direction:		S 51°30'47.22" E	

Element: Linear			
PT (20)	1190+41.00	2492365.9575	384608.2342
PC (21)	1200+04.99	2493120.5213	384008.3091
Tangent Direction:		S 51°30'47.22" E	
Tangent Length:		963.99	

Element: Circular			
PC (21)	1200+04.99	2493120.5213	384008.3091
PI ()	1206+89.41	2493656.2494	383582.3719
CC (22)	2494309.0944	385503.2527	
PT (23)	1213+19.37	2494340.5738	383593.6529
Radius:		1909.86	
Delta:		39°25'52.70" Left	
Degree of Curvature(Arc):		3°00'00.00"	
Length:		1314.38	
Tangent:		684.42	
Chord:		1288.59	
Middle Ordinate:		111.96	
External:		118.93	
Tangent Direction:		S 51°30'47.22" E	
Radial Direction:		S 38°29'12.78" W	
Chord Direction:		S 71°13'43.57" E	
Radial Direction:		S 0°56'39.93" E	
Tangent Direction:		N 89°03'20.07" E	

Element: Linear			
PT (23)	1213+19.37	2494340.5738	383593.6529
POE (50)	1286+83.38	2501703.5787	383715.0306
Tangent Direction:		N 89°03'20.07" E	
Tangent Length:		7364.01	

Project Name: SW04522*1*V1
Description: SH7
Horizontal Alignment Name: R001
Description: S-R/W S.H.7
Style: Present R/W

	STATION	EASTING	NORTHING
Element: Linear			
POB (7602)	0+00.00	2480683.2335	383298.8125
PI (7677)	9+42.11	2481568.9713	383619.7999
Tangent Direction:		N 70°04'46.96" E	
Tangent Length:		942.11	
Element: Linear			
PI (7677)	9+42.11	2481568.9713	383619.7999
PI (7678)	9+72.11	2481579.1926	383591.5948
Tangent Direction:		S 19°55'13.04" E	
Tangent Length:		30.00	

Element: Linear			
PI (7678)	9+72.11	2481579.1926	383591.5948
PI (7679)	18+72.11	2482425.3434	383898.2361
Tangent Direction:		N 70°04'46.96" E	
Tangent Length:		900.00	

Element: Linear			
PI (7679)	18+72.11	2482425.3434	383898.2361
PI (7680)	19+02.11	2482415.1220	383926.4411
Tangent Direction:		N 19°55'13.04" W	
Tangent Length:		30.00	

Element: Linear			
PI (7680)	19+02.11	2482415.1220	383926.4411
PI (7684)	43+22.34	2484690.5502	384751.0460
Tangent Direction:		N 70°04'46.96" E	
Tangent Length:		2420.24	

Element: Circular			
PC (7684)	43+22.34	2484690.5502	384751.0460
PI ()	47+08.28	2485053.3962	384882.5398
CC (16)	2485649.5840	382104.6728	
PT (7676)	50+89.44	2485438.2444	384911.5167
Radius:		2814.79	
Delta:		15°36'51.71" Right	
Degree of Curvature(Arc):		2°02'07.90"	
Length:		767.09	
Tangent:		385.94	
Chord:		764.72	
Middle Ordinate:		26.09	
External:		26.33	
Tangent Direction:		N 70°04'46.96" E	
Radial Direction:		S 19°55'13.04" E	
Chord Direction:		N 77°53'12.82" E	
Radial Direction:		S 4°18'21.33" E	
Tangent Direction:		N 85°41'38.67" E	

Non-collinear

Element: Linear			
PT (7676)	50+89.44	2485438.2444	384911.5167
PC (7681)	51+19.44	2485440.4969	384881.6014
Tangent Direction:		S 4°18'21.33" E	
Tangent Length:		30.00	

Non-collinear

Element: Circular			
PC (7681)	51+19.44	2485440.4969	384881.6014
PI ()	52+10.60	2485531.4024	384888.4461
CC (16)	2485649.5840	382104.6728	
PT (7682)	53+01.70	2485622.5609	384889.3307
Radius:		2784.79	
Delta:		3°44'59.75" Right	
Degree of Curvature(Arc):		2°03'26.84"	
Length:		182.26	
Tangent:		91.16	
Chord:		182.23	
Middle Ordinate:		1.49	
External:		1.49	
Tangent Direction:		N 85°41'38.67" E	
Radial Direction:		S 4°18'21.33" E	
Chord Direction:		N 87°34'08.54" E	
Radial Direction:		S 0°33'21.58" E	
Tangent Direction:		N 89°26'38.42" E	

Element: Linear			
PT (7682)	53+01.70	2485622.5609	384889.3307
PI (7637)	59+76.16	2486296.9927	384895.8755
Tangent Direction:		N 89°26'38.42" E	
Tangent Length:		674.46	

Element: Linear			
PI (7637)	59+76.16	2486296.9927	384895.8755
PI (7638)	60+06.16	2486296.7016	384925.8741
Tangent Direction:		N 0°33'21.58" W	
Tangent Length:		30.00	

Element: Linear			
PI (7638)	60+06.16	2486296.7016	384925.8741
PI (7691)	66+56.16	2486946.6710	384932.1816
Tangent Direction:		N 89°26'38.42" E	
Tangent Length:		650.00	

Element: Linear			
PI (7691)	66+56.16	2486946.6710	384932.1816
PI (7692)	66+76.16	2486946.8651	384912.1825
Tangent Direction:		S 0°33'21.58" E	
Tangent Length:		20.00	

Element: Linear			
PI (7692)	66+76.16	2486946.8651	384912.1825
PI (7673)	81+99.10	2488469.7335	384926.9608
Tangent Direction:		N 89°26'38.42" E	
Tangent Length:		1522.94	

Element: Linear			
PI (7673)	81+99.10	2488469.7335	384926.9608
PI (7640)	82+19.10	2488469.5394	384946.9599
Tangent Direction:		N 0°33'21.58" W	
Tangent Length:		20.00	

Element: Linear			
PI (7640)	82+19.10	2488469.5394	384946.9599
PI (7696)	106+53.95	2490904.2746	384970.5872
Tangent Direction:		N 89°26'38.42" E	
Tangent Length:		2434.85	

Element: Linear			
PI (7696)	106+53.95	2490904.2746	384970.5872
PI (7697)	106+68.95	2490904.4202	384955.5879
Tangent Direction:		S 0°33'21.58" E	
Tangent Length:		15.00	

Element: Linear			
PI (7697)	106+68.95	2490904.4202	384955.5879
PI (7631)	108+83.88	2491119.3367	384957.6735
Tangent Direction:		N 89°26'38.42" E	
Tangent Length:		214.93	

Element: Linear			
PI (7631)	108+83.88	2491119.3367	384957.6735
PI (7634)	108+98.88	2491118.7596	384972.6686
Tangent Direction:		N 2°12'14.22" W	
Tangent Length:		15.01	

Element: Linear			
PI (7634)	108+98.88	2491118.7596	384972.6686
PC (7633)	109+39.46	2491159.3366	384973.0624
Tangent Direction:		N 89°26'38.42" E	
Tangent Length:		40.58	

Element: Circular			
PC (7633)	109+39.46	2491159.3366	384973.0624
PI ()	115+98.86	2491818.6996	384979.4610
CC (19)	2491177.3843	383113.2906	
PT (7639)	122+06.82	2492334.8407	384569.0967
Radius:		1859.86	
Delta:		39°02'34.36" Right	
Degree of Curvature(Arc):		3°04'50.34"	
Length:		1267.36	
Tangent:		659.39	
Chord:		1242.98	
Middle Ordinate:		106.91	
External:		113.43	
Tangent Direction:		N 89°26'38.42" E	
Radial Direction:		S 0°33'21.58" E	
Chord Direction:		S 71°02'04.40" E	
Radial Direction:		S 38°29'12.78" W	
Tangent Direction:		S 51°30'47.22" E	

Element: Linear			
PT (7639)	122+06.82	2492334.8407	384569.0967
PC (7698)	131+70.81	2493089.4045	383969.1716
Tangent Direction:		S 51°30'47.22" E	
Tangent Length:		963.99	

Element: Circular			
PC (7698)	131+70.81	2493089.4045	383969.1716
PI ()	135+66.25	2493398.9357	383723.0750
CC (22)	2494309.0944	385503.2527	
PT (7701)	139+51.21	2493779.6750	383616.2542
Radius:		1959.86	
Delta:		22°48'53.28" Left	
Degree of Curvature(Arc):		2°55'24.47"	
Length:		780.40	
Tangent:		395.44	
Chord:		775.26	
Middle Ordinate:		38.72	
External:		39.50	
Tangent Direction:		S 51°30'47.22" E	
Radial Direction:		S 38°29'12.78" W	
Chord Direction:		S 62°55'13.86" E	
Radial Direction:		S 15°40'19.50" W	
Tangent Direction:		S 74°19'40.50" E	

Non-collinear

Element: Linear			
PT (7701)	139+51.21	2493779.6750	383616.2542
PI (7702)	140+43.06	2493781.0903	383524.4217
Tangent Direction:		S 0°52'58.61" E	
Tangent Length:		91.84	

Element: Linear			
PI (7702)	140+43.06	2493781.0903	383524.4217
PI (7630)	153+55.24	2495093.0973	383546.0499
Tangent Direction:		N 89°03'20.07" E	
Tangent Length:		1312.19	

Element: Linear			
PI (7630)	153+55.24	2495093.0973	383546.0499
PI (7610)	153+65.24	2495092.9324	383556.0486
Tangent Direction:		N 0°56'39.93" W	
Tangent Length:		10.00	

Element: Linear			
PI (7610)	153+65.24	2495092.9324	383556.0486
PI (7666)	178+70.15	2497597.4989	383597.2670
Tangent Direction:		N 89°03'25.74" E	
Tangent Length:		2504.91	

Element: Linear			
PI (7666)	178+70.15	2497597.4989	383597.2670
PI (7665)	178+80.15	2497597.6639	383587.2684
Tangent Direction:		S 0°56'43.41" E	

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				

DESCRIPTION	REVISIONS	DATE

Project Name: SW04522*1*V1
 Description: SH7
 Horizontal Alignment Name: R002
 Description: N-R/W S.H.7
 Style: Present R/W

Element:	Linear	STATION	EASTING	NORTHING
	POB (7605)	0+00.00	2480681.7748	383404.6479
	PI (7606)	5+06.97	2481158.4156	383577.3804
	Tangent Direction:		N 70°04'46.96" E	
	Tangent Length:		506.97	
Element:	Linear			
	PI (7606)	5+06.97	2481158.4156	383577.3804
	PI (7607)	5+16.97	2481155.0085	383586.7821
	Tangent Direction:		N 19°55'13.04" W	
	Tangent Length:		10.00	
Element:	Linear			
	PI (7607)	5+16.97	2481155.0085	383586.7821
	PI (7740)	18+17.41	2482377.6390	384029.8579
	Tangent Direction:		N 70°04'46.96" E	
	Tangent Length:		1300.44	
Element:	Linear			
	PI (7740)	18+17.41	2482377.6390	384029.8579
	PI (7672)	18+27.41	2482381.0508	384020.4579
	Tangent Direction:		S 19°56'54.65" E	
	Tangent Length:		10.00	
Element:	Linear			
	PI (7672)	18+27.41	2482381.0508	384020.4579
	PC (7674)	42+47.65	2484656.4790	384845.0628
	Tangent Direction:		N 70°04'46.96" E	
	Tangent Length:		2420.24	
Element:	Circular			
	PC (7674)	42+47.65	2484656.4790	384845.0628
	PI ()	46+47.30	2485032.2156	384981.2281
	CC (16)	2485649.5840	382104.6728	
	PT (7689)	50+41.99	2485430.7362	385011.2344
	Radius:		2914.79	
	Delta:		15°36'51.71" Right	
	Degree of Curvature(Arc):		1°57'56.49"	
	Length:		794.34	
	Tangent:		399.65	
	Chord:		791.89	
	Middle Ordinate:		27.02	
	External:		27.27	
	Tangent Direction:		N 70°04'46.96" E	
	Radial Direction:		S 19°55'13.04" E	
	Chord Direction:		N 77°53'12.82" E	
	Radial Direction:		S 4°18'21.33" E	
	Tangent Direction:		N 85°41'38.67" E	
Non-collinear				
Element:	Linear			
	PT (7689)	50+41.99	2485430.7362	385011.2344
	PC (7690)	50+61.99	2485429.2346	385031.1780
	Tangent Direction:		N 4°18'21.33" W	
	Tangent Length:		20.00	
Non-collinear				
Element:	Circular			
	PC (7690)	50+61.99	2485429.2346	385031.1780
	PI ()	51+58.07	2485525.0366	385038.3913
	CC (16)	2485649.5840	382104.6728	
	PT (7683)	52+54.07	2485621.1054	385039.3236
	Radius:		2934.79	
	Delta:		3°44'59.75" Right	
	Degree of Curvature(Arc):		1°57'08.27"	
	Length:		192.08	
	Tangent:		96.07	
	Chord:		192.04	
	Middle Ordinate:		1.57	
	External:		1.57	
	Tangent Direction:		N 85°41'38.67" E	
	Radial Direction:		S 4°18'21.33" E	
	Chord Direction:		N 87°34'08.54" E	
	Radial Direction:		S 0°33'21.58" E	
	Tangent Direction:		N 89°26'38.42" E	
Element:	Linear			
	PT (7683)	52+54.07	2485621.1054	385039.3236
	PI (7693)	82+59.98	2488626.8674	385068.4923
	Tangent Direction:		N 89°26'38.42" E	
	Tangent Length:		3005.90	
Element:	Linear			
	PI (7693)	82+59.98	2488626.8674	385068.4923
	PI (7694)	82+79.98	2488627.0614	385048.4932
	Tangent Direction:		S 0°33'21.58" E	
	Tangent Length:		20.00	
Element:	Linear			
	PI (7694)	82+79.98	2488627.0614	385048.4932
	PC (7632)	108+11.40	2491158.3662	385073.0577
	Tangent Direction:		N 89°26'38.42" E	
	Tangent Length:		2531.42	

Element:	Circular				
	PC (7632)	108+11.40	2491158.3662	385073.0577	
	PI ()	115+06.25	2491853.1815	385079.8003	
	CC (19)	2491177.3843	383113.2906		
	PT (7675)	121+46.90	2492397.0743	384647.3718	
	Radius:		1959.86		
	Delta:		39°02'34.36" Right		
	Degree of Curvature(Arc):		2°55'24.47"		
	Length:		1335.50		
	Tangent:		694.85		
	Chord:		1309.81		
	Middle Ordinate:		112.66		
	External:		119.53		
	Tangent Direction:		N 89°26'38.42" E		
	Radial Direction:		S 0°33'21.58" E		
	Chord Direction:		S 71°02'04.40" E		
	Radial Direction:		S 38°29'12.78" W		
	Tangent Direction:		S 51°30'47.22" E		
Element:	Linear				
	PT (7675)	121+46.90	2492397.0743	384647.3718	
	PC (7695)	131+10.89	2493151.6380	384047.4466	
	Tangent Direction:		S 51°30'47.22" E		
	Tangent Length:		963.99		
Element:	Circular				
	PC (7695)	131+10.89	2493151.6380	384047.4466	
	PI ()	137+77.39	2493673.3408	383632.6605	
	CC (22)	2494309.0944	385503.2527		
	PT (7699)	143+90.86	2494339.7497	383643.6461	
	Radius:		1859.86		
	Delta:		39°25'52.70" Left		
	Degree of Curvature(Arc):		3°04'50.34"		
	Length:		1279.97		
	Tangent:		666.50		
	Chord:		1254.86		
	Middle Ordinate:		109.03		
	External:		115.82		
	Tangent Direction:		S 51°30'47.22" E		
	Radial Direction:		S 38°29'12.78" W		
	Chord Direction:		S 71°13'43.57" E		
	Radial Direction:		S 0°56'39.93" E		
	Tangent Direction:		N 89°03'20.07" E		
Non-collinear					
Element:	Linear				
	PT (7699)	143+90.86	2494339.7497	383643.6461	
	PI (7667)	164+68.51	2496417.1168	383677.8464	
	Tangent Direction:		N 89°03'24.50" E		
	Tangent Length:		2077.65		
Element:	Linear				
	PI (7667)	164+68.51	2496417.1168	383677.8464	
	PI (7629)	164+93.55	2496416.7157	383702.8879	
	Tangent Direction:		N 0°55'03.64" W		
	Tangent Length:		25.04		
Element:	Linear				
	PI (7629)	164+93.55	2496416.7157	383702.8879	
	PI (7705)	167+58.15	2496681.2797	383707.2536	
	Tangent Direction:		N 89°03'16.59" E		
	Tangent Length:		264.60		
Element:	Linear				
	PI (7705)	167+58.15	2496681.2797	383707.2536	
	PI (7706)	167+83.15	2496681.6922	383682.2570	
	Tangent Direction:		S 0°56'43.41" E		
	Tangent Length:		25.00		
Element:	Linear				
	PI (7706)	167+83.15	2496681.6922	383682.2570	
	PI (7703)	177+11.64	2497610.0593	383697.5320	
	Tangent Direction:		N 89°03'26.51" E		
	Tangent Length:		928.49		
Element:	Linear				
	PI (7703)	177+11.64	2497610.0593	383697.5320	
	PI (7700)	177+21.64	2497609.8943	383707.5306	
	Tangent Direction:		N 0°56'43.41" W		
	Tangent Length:		10.00		
Element:	Linear				
	PI (7700)	177+21.64	2497609.8943	383707.5306	
	PI (7668)	177+97.43	2499785.3853	383743.4177	
	Tangent Direction:		N 89°03'17.75" E		
	Tangent Length:		2175.79		
Element:	Linear				
	PI (7668)	198+97.43	2499785.3853	383743.4177	
	PI (7669)	199+07.43	2499785.5503	383733.4190	
	Tangent Direction:		S 0°56'43.41" E		
	Tangent Length:		10.00		
Element:	Linear				
	PI (7669)	199+07.43	2499785.5503	383733.4190	
	POE (7709)	218+24.93	2501702.7116	383769.4856	
	Tangent Direction:		N 88°55'20.11" E		
	Tangent Length:		1917.50		

RTKBASE A-3-870

FILE:SW04522_5382_0819_090822.m00 000097556

NGS OPUS SOLUTION REPORT

All computed coordinate accuracies are listed as peak-to-peak values.
 For additional information: www.ngs.noaa.gov/OPUS/Using_OPUS.html*accuracy

USER: rstele@odot.org DATE: August 20, 2009
 RINEX FILE: sw04231o.090 TIME: 13:19:38 UTC

SOFTWARE: page5 090813 master28.pl 081023 START: 2009/08/19 14:09:00
 EPHEMERIS: igu5453.eph [ultra-rapid] STOP: 2009/08/19 17:28:00
 NAV FILE: brdc2310.09n OBS USED: 8152 / 8455 : 96%
 ANT NAME: LEIAXI202 NONE * FIXED AMB: 47 / 49 : 96%
 ARP HEIGHT: 2 OVERALL RMS: 0.012(m)

REF FRAME: NAD_83(CORS96) EPOCH: 2002.0000 ITRF00 (EPOCH: 2009.6320)

X:	-573683.139(m) 0.020(m)	-573683.855(m) 0.020(m)
Y:	-5238743.329(m) 0.022(m)	-5238741.918(m) 0.022(m)
Z:	3580938.815(m) 0.030(m)	3580938.652(m) 0.030(m)

LAT:	34 22 28.99304 0.018(m)	34 22 29.01295 0.018(m)
E LON:	263 45 2.04517 0.017(m)	263 45 2.01130 0.017(m)
W LON:	96 14 57.95483 0.017(m)	96 14 57.98870 0.017(m)
EL HGT:	192.372(m) 0.028(m)	191.87(m) 0.028(m)

ORTHO HGT: 219.800(m) 0.038(m) [NAVD88 (Computed using GEOID03)]

UTM COORDINATES	STATE PLANE COORDINATES
UTM (Zone 14)	SPC (3502 OK S)
Northing (Y) [meters]	3807134.757 116909.936
Easting (X) [meters]	752923.704 769994.945
Convergence [degrees]	1.55380304 0.99365165
Point Scale	1.00038859 0.99994259
Combined Factor	1.00035838 0.99991239

US NATIONAL GRID DESIGNATOR: 14SQD5292407135(NAD 83)

PID	DESIGNATION	BASE STATIONS USED	LATITUDE	LONGITUDE	DISTANCE(m)
DL3498	TXBN BONHAM CORS ARP		N333624.195	W096103.156	854647
DET170	OKMA MCALESTER CORS ARP		N345540.833	W095441.056	77280.9
DET168	OKAN ANTLERS CORS ARP		N341142.708	W0953716.847	61165.0

PLS	RES		OKLAHOMA DEPARTMENT OF TRANSPORTATION
DRAWN	BLH		SURVEY DIVISION
CHECKED			SURVEY DATA SHEET
APPROVED			
CREW	ANTLERS	SWO 4522 (1) PROJECT NO. 24066104 SHEET NO. S006	

PAGE 1 OF 9 CHECK LEVELS SW04522(1) BENCH MARK LIST NAVD 88 DATUM

BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM 7400	-0.74	-0.74	-0.74	-0.74	549.64	OPUS 549.638	ODOT FENO MKR. SET IN CONC. 10.30 LT. STA. 1049+71.02
BM2	-5.10	-5.10	-5.10	-5.10	548.90		BOX ON CL N-HDWL. CULVT. 17.47' LT. STA. 1062+55.19
BM24	3.45	3.45	3.45	3.45	543.80	544.36	SMITH ROBERTS BM NO. 24 67.43' RT. STA. 1067+93.64
BM3	36.89	36.89	36.89	36.89	547.25		TOP DEL. POST 207.83' RT. STA. 1066+75.51
BM26	-18.08	-18.08	-18.08	-18.08	584.14	584.65	SMITH ROBERTS BM NO. 26 17.26' RT. STA. 1074+83.83
BM4	-0.56	-0.56	-0.56	-0.56	566.06		TOP DEL. POST 122.22' RT. STA. 1079+10.18
BM5	0.77	0.77	0.77	0.77	565.50		BOX ON CL N-HDWL. CULVT. 17.23' LT. STA. 1083+41.94
BM6	19.05	19.06	19.06	19.05	566.28		TOP DEL. POST 144.11' RT. STA. 1086+24.37
BM7	6.61	6.61	6.61	6.61	585.33		TOP DEL. POST 140.70' RT. STA. 1094+10.31
BM8	8.37	8.36	8.37	8.36	591.94		BOX ON CL N-HDWL. CULVT. 21.98' LT. STA. 1095+12.57
BM9	11.98	11.98	11.98	11.98	600.30		BOX ON CL N-HDWL. CULVT. 36.21' LT. STA. 1098+24.08
BM10=7401					612.28	OPUS 612.318	ODOT FENO MKR. SET IN CONC. 42.09' RT. STA. 1102+00.10

PAGE 3 OF 9 CHECK LEVELS SW04522(1) BENCH MARK LIST NAVD 88 DATUM

BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM22	-8.15	-8.15	-8.15	-8.16	625.94		TOP DEL. POST 82.15' RT. STA. 1159+49.60
BM23	2.75	2.74	2.75	2.74	617.79		BOX ON CL S-HDWL. CULVT. 39.01' RT. STA. 1160+79.50
BM24	-11.49	-11.49	-11.49	-11.49	620.53		TOP DEL. POST 110.74' RT. STA. 1170+29.17
BM25	14.17	14.18	14.18	14.17	609.04		TOP DEL. POST 107.02' RT. STA. 1176+77.99
BM26	13.17	13.17	13.17	13.17	623.21		TOP DEL. POST 121.11' RT. STA. 1185+44.32
BM27	3.35	3.36	3.35	3.35	636.38		TOP DEL. POST 80.85' LT. STA. 1194+82.14
BM28	0.67	0.65	0.66	0.65	639.73		TOP DEL. POST 87.22' RT. STA. 1204+66.13
BM29	14.64	14.65	14.64	14.64	640.38		TOP DEL. POST 158.95' RT. STA. 1211+34.86
BM30	36.32	36.31	36.32	36.31	655.03		TOP DEL. POST IN FENCE 100.67' LT. STA. 1219+15.65
BM31	29.63	29.64	29.64	29.63	691.34		TOP DEL. POST IN OLD DRIVE 109.32' LT. STA. 1231+29.62
BM32 7406	-10.94	-10.94	-10.94	-10.94	720.97	OPUS 721.117	TOP 1/2" IRON PIN SET FOR RTKBASE POINT RT. STA.
BM33					710.03		TOP DEL. POST IN FENCE 91.81' RT. STA. 1244+29.70

PAGE 2 OF 9 CHECK LEVELS SW04522(1) BENCH MARK LIST NAVD 88 DATUM

BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM11	-5.45	-5.45	-5.45	-5.45	606.83		BOX ON CL N-HDWL. CULVT. 18.69' RT. STA. 1109+22.71
BM 12	0.57	0.57	0.57	0.57	607.40		TOP DEL. POST 73.92' RT. STA. 1110+22.45
BM13	-4.53	-4.53	-4.53	-4.53	602.86		BOX ON CL N-HDWL. CULVT. 20.20' LT. STA. 1115+88.13
BM14	0.27	0.27	0.27	0.27	603.13		TOP DEL. POST 80.22' LT. STA. 1117+62.92
BM15	-16.74	-16.74	-16.74	-16.74	586.39		TOP DEL. POST 69.85' LT. STA. 1124+12.40
BM16	-13.48	-13.48	-13.48	-13.48	572.91		TOP DEL. POST 96.56' LT. STA. 1132+59.63
BM17	-2.04	-2.04	-2.04	-2.04	570.87		BOX ON N.W. WINGWALL 17.50' LT. STA. 1138+29.14
BM18	-6.76	-6.76	-6.76	-6.76	564.11		TOP DEL. POST 111.62' RT. STA. 1140+47.10
BM19	8.29	8.29	8.29	8.29	572.40		X ON S.W. WINGWALL 15.73' RT. STA. 1142+28.86
BM20	4.52	4.51	4.51	4.51	576.90		TOP DEL. POST 106.74' RT. STA. 1151+99.52
BM21	25.51	25.52	25.51	25.51	602.41		BOX ON CL N-HDWL. CULVT. 17.15' LT. STA. 1154+95.29
	23.53	23.53	23.53	23.53			

PAGE 4 OF 9 CHECK LEVELS SW04522(1) BENCH MARK LIST NAVD 88 DATUM

BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM34	-17.80	-17.78	-17.79	-17.80	692.23		TOP DEL. POST 103.42' LT. STA. 1252+12.15
BM35	-16.58	-16.57	-16.58	-16.58	675.66		TOP DEL. POST N-SIDE BARN 78.31' RT. STA. 1259+41.96
BM36	-9.99	-9.99	-9.99	-9.99	665.67		BOX ON CL S-HDWL. CULVT. 17.80' RT. STA. 1265+67.42
BM37	2.66	2.66	2.66	2.66	668.32		TOP DEL. POST BY GATE 107.40' RT. STA. 1267+93.27
BM38	21.37	21.38	21.38	21.38	689.70		TOP DEL. POST 100.61' RT. STA. 1275+74.17
BM39	-16.41	-16.40	-16.40	-16.40	673.30		TOP DEL. POST 101.22' RT. STA. 1286+46.14
BM40	-0.10	-0.10	-0.10	-0.10	673.20		BOX ON CL S-HDWL. CULVT. 18.08' RT. STA. 1288+14.14
BM41	0.87	0.87	0.87	0.87	674.06		TOP DEL. POST 113.06' LT. STA. 1294+97.85
BM42	-2.82	-2.82	-2.82	-2.82	671.24		BOX ON CL S-HDWL. CULVT. 18.84' RT. STA. 1301+39.28
BM43	-4.57	-4.58	-4.58	-4.58	666.67		TOP DEL. POST IN CHAIN LINK FEN. 128.57' RT. STA. 1303+38.14
	10.29	10.29	10.29	10.29			

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				

DESCRIPTION	REVISIONS	DATE

PAGE 5 OF 9 CHECK LEVELS SW04522(1) BENCH MARK LIST NAVD 88 DATUM							
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED:	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM44	-14.53	-14.53	-14.53	-14.53	676.96		TOP DEL. POST IN FENCE LINE 92.31' LT. STA. 1313+28.83
BM45	5.36	5.36	5.36	5.36	662.43		TOP DEL. POST 99.99' RT. STA. 1320+93.82
BM46	-3.98	-3.98	-3.98	-3.98	667.79		BOX ON CL S-HDWL. CULVT. 17.64' RT. STA. 1323+66.01
BM47=7402	-22.48	-22.48	-22.48	-22.48	663.81	OPUS 663.814	ODOT FENO MKR. SET IN CONC. 46.76' RT. STA. 1330+68.37
BM48	-1.20	-1.20	-1.20	-1.20	641.33		BOX ON CL N-HDWL. CULVT. 18.22' LT. STA. 1339+56.07
BM49	-2.89	-2.89	-2.89	-2.89	640.13		TOP DEL. POST 110.11' LT. STA. 1340+22.96
BM50	6.13	6.13	6.13	6.13	637.24		BOX ON CL S-HDWL. CULVT. 22.77' RT. STA. 1342+40.36
BM51	-14.42	-14.41	-14.42	-14.42	643.37		TOP DEL. POST 93.74' RT. STA. 1348+43.98
BM52	33.50	33.50	33.50	33.50	628.95		BOX ON CL S-HDWL. CULVT. 31.35' RT. STA. 1353+41.32
BM53	-13.79	-13.79	-13.79	-13.79	662.45		TOP DEL. POST 113.00' LT. STA. 1360+47.55'
BM54	12.78	12.78	12.78	12.78	648.65		BOX ON CL S-HDWL. CULVT. 30.24' RT. STA. 1365+83.82

PAGE 7 OF 9 CHECK LEVELS SW04522(1) BENCH MARK LIST NAVD 88 DATUM							
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED:	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM66	-11.10	-11.11	-11.11	-11.10	619.21		TOP DEL. POST 69.82' RT. STA. 1431+75.34
BM67	2.40	2.41	2.41	2.41	608.11		TOP DEL. POST 111.08' RT. STA. 1440+91.08
BM68	7.38	7.37	7.37	7.38	610.51		BOX ON N.W. WINGWALL 17.88' LT. STA. 1444+27.72
BM69	-4.46	-4.45	-4.45	-4.45	617.89		TOP DEL. POST 92.66' RT. STA. 1449+58.68
BM70=7404	-9.22	-9.22	-9.22	-9.22	613.44	OPUS 613.50	ODOT FENO MKR. SET IN CONC. 51.82' RT. STA. 1461+02
BM71	-12.10	-12.10	-12.10	-12.10	604.22		TOP DEL. POST 78.95' RT. STA. 1469+95.67
BM72	-3.64	-3.64	-3.64	-3.64	592.12		TOP DEL. POST 114' RT. STA. 1480+26
BM73	0.95	0.95	0.95	0.95	588.48		BOX ON CL N-HDWL. CULVT. 19.97' LT. STA. 1484+54.82
BM74	-4.92	-4.92	-4.92	-4.92	589.42		TOP DEL. POST IN CH.LK.FE.90.30' RT. STA. 1489+77.60
BM75	-1.63	-1.63	-1.63	-1.63	584.51		BOX ON CL S-HDWL. CULVT. 20.97' RT. STA. 1492+89.73
BM76	0.38	0.39	0.39	0.39	582.88		BOX ON CL N-HDWL. CULVT. 21.69' LT. STA. 1497+57.98
BM77					583.27		BOX ON CL N-HDWL. CULVT. 16.52' LT. STA. 1497+93.83

PAGE 6 OF 9 CHECK LEVELS SW04522(1) BENCH MARK LIST NAVD 88 DATUM							
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED:	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM55=7403	-10.96	-10.96	-10.96	-10.96	661.43	OPUS 661.488	ODOT FENO MKR. SET IN CONC. 27.65' LT. STA. 1371+02.27
BM56	-3.20	-3.20	-3.20	-3.20	650.47		BOX ON CL S-HDWL. CULVT. 17.11' RT. STA. 1378+30.10
BM57	5.89	5.88	5.88	5.88	647.28		TOP DEL. POST 107.90' LT. STA. 1381+25.38
BM58	5.71	5.72	5.72	5.72	653.16		TOP DEL. POST 93.80' LT. STA. 1389+83.96
BM59	-5.94	-5.94	-5.94	-5.94	658.88		BOX ON CL N-HDWL. CULVT. 18.36' LT. STA. 1391+86.06
BM60	-9.85	-9.84	-9.84	-9.84	652.94		TOP DEL. POST 102.80' LT. STA. 1398+39.90
BM61	-0.75	-0.76	-0.76	-0.76	643.09		BOX ON CL S-HDWL. CULVT. 25.22' RT. STA. 1405+17.10
BM62	-12.84	-12.84	-12.84	-12.84	642.34		TOP DEL. POST 99.34' RT. STA. 1407+26.28
BM63	-3.62	-3.62	-3.62	-3.62	629.49		TOP DEL. POST 112.37' RT. STA. 1417+22.98
BM64	-3.35	-3.35	-3.35	-3.35	625.87		BOX ON CL S-HDWL. CULVT. 24.33' RT. STA. 1418+43.41
BM65	-3.30	-3.31	-3.31	-3.31	622.52		TOP DEL. POST 84.28' RT. STA. 1423+41.21

PAGE 8 OF 9 CHECK LEVELS SW04522(1) BENCH MARK LIST NAVD 88 DATUM							
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED:	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM78	-4.12	-4.12	-4.12	-4.12	579.15		TOP DEL. POST 109.32' RT. STA. 1501+91.19
BM79	-11.33	-11.34	-11.34	-11.34	567.81		TOP DEL. POST 123.46' RT. STA. 1510+72.33
BM80	3.91	3.92	3.91	3.91	571.72		BOX ON CL S-HDWL. CULVT. 34.00' RT. STA. 1514+63.45
BM81	-3.52	-3.52	-3.52	-3.52	568.20		TOP DEL. POST 81.39' RT. STA. 1518+38.35
BM82	8.00	8.01	8.00	8.00	576.20		TOP DEL. POST 71.91' RT. STA. 1529+39.43
BM83	0.92	0.91	0.91	0.91	577.12		BOX ON CL S-HDWL. CULVT. 13.76' RT. STA. 1534+40.87
BM84	6.39	6.39	6.39	6.39	583.51		TOP DEL. POST 55.70' RT. STA. 1539+98.10
BM85=7405	27.02	27.03	27.03	27.03	610.54	OPUS 610.54	ODOT FENO MKR. SET IN CONC. 22.64' LT. STA. 1549+94.76
	-23.56	-23.55	-23.56	-23.56			

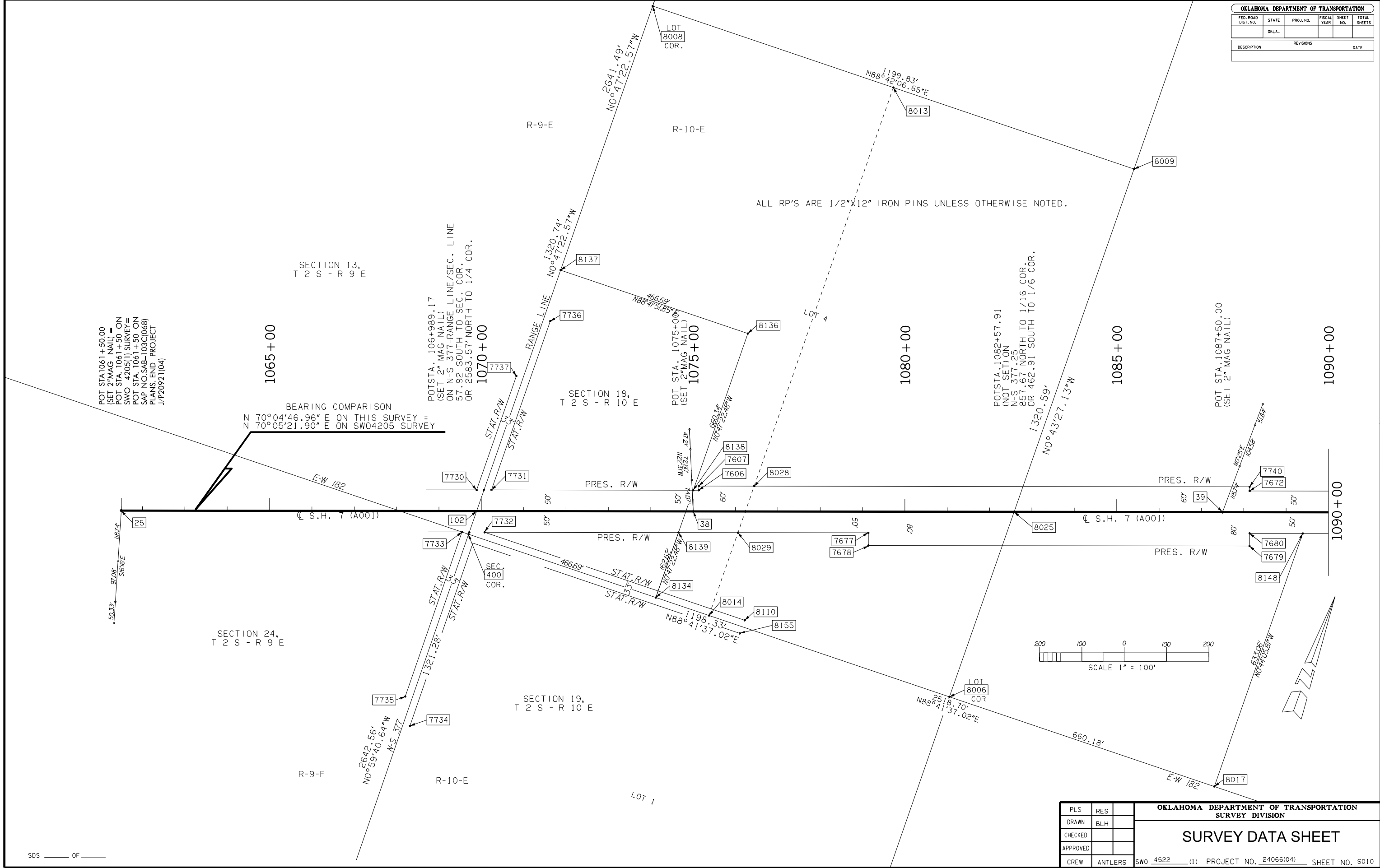
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DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS		

SURVEY DATA SHEET

SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. S008

PAGE 9 OF 9		CHECK LEVELS SW04522(1)				BENCH MARK LIST		NAVD 88 DATUM	
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION		
TBM	-19.61	-19.60	-19.60	-19.60	586.98	356.39	BOX ON CONC. S.W. AT S.E. COR. DOLLAR STORE		
TBM	0.03	0.03	0.03	0.03	567.38		BOX ON CL W-HDWL		
BM	-2.53	-2.53	-2.53	-2.53	567.41		ODOT BRASS MON. NO. A-3-672 CL W-HDWL. CULVT. STA. 824+70 ON F-219 (6) US 69 PLANS.		
BM					564.88		ODOT BRASS MON. NO. A-3-673 CL EAST END CONC. RET. WALL RT. APPROX. STA. 84+14 US 69 PLANS.		

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION		REVISIONS		DATE	



POT STA 1061+50.00
(SET 2" MAG NAIL) =
POT STA. 1061+50 ON
SWO 4205(1) SURVEY =
POT STA. 1061+50 ON
SAP NO. SAB-103C(068)
PLANS END PROJECT
J/P20921(04)

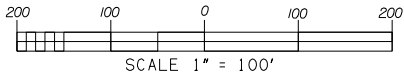
BEARING COMPARISON
N 70°04'46.96" E ON THIS SURVEY =
N 70°05'21.90" E ON SW04205 SURVEY

POT STA. 106+989.17
(SET 2" MAG NAIL)
ON N-S 377-RANGE LINE/SEC. LINE
57.92 SOUTH TO SEC. COR.
OR 2583.57' NORTH TO 1/4 COR.

POT STA. 1075+00
(SET 2" MAG NAIL)

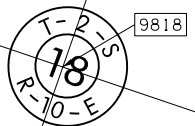
POT STA. 1082+57.91
(NOT SET) ON
N-S 377.25
857.67 NORTH TO 1/16 COR.
OR 462.91 SOUTH TO 1/6 COR.

POT STA. 1087+50.00
(SET 2" MAG NAIL)



OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SWO 4522 (1)	PROJECT NO. 24066(04) SHEET NO. S010

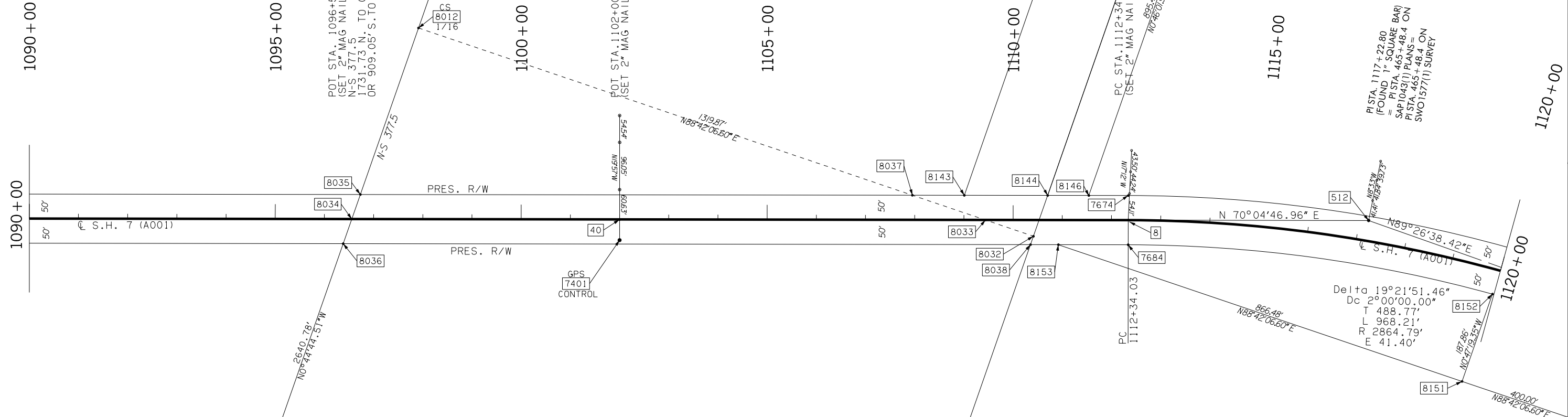
OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS	DATE	



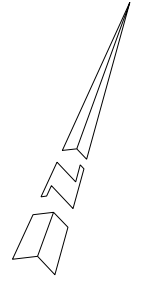
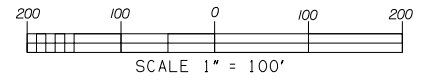
ALL RP'S ARE 1/2"X12" IRON PINS UNLESS OTHERWISE NOTED.

SECTION 18,
T 2 S - R 10 E

PI STA. 1117+22.80
= FOUND 1" SQUARE BAR
= PI STA. 465+48.4 ON
SAP1043(1) PLANS =
PI STA. 465+48.4 ON
SWO1577(1) SURVEY



SECTION 18,
T 2 S - R 10 E



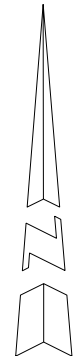
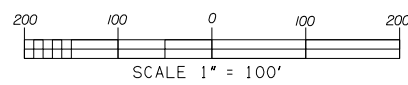
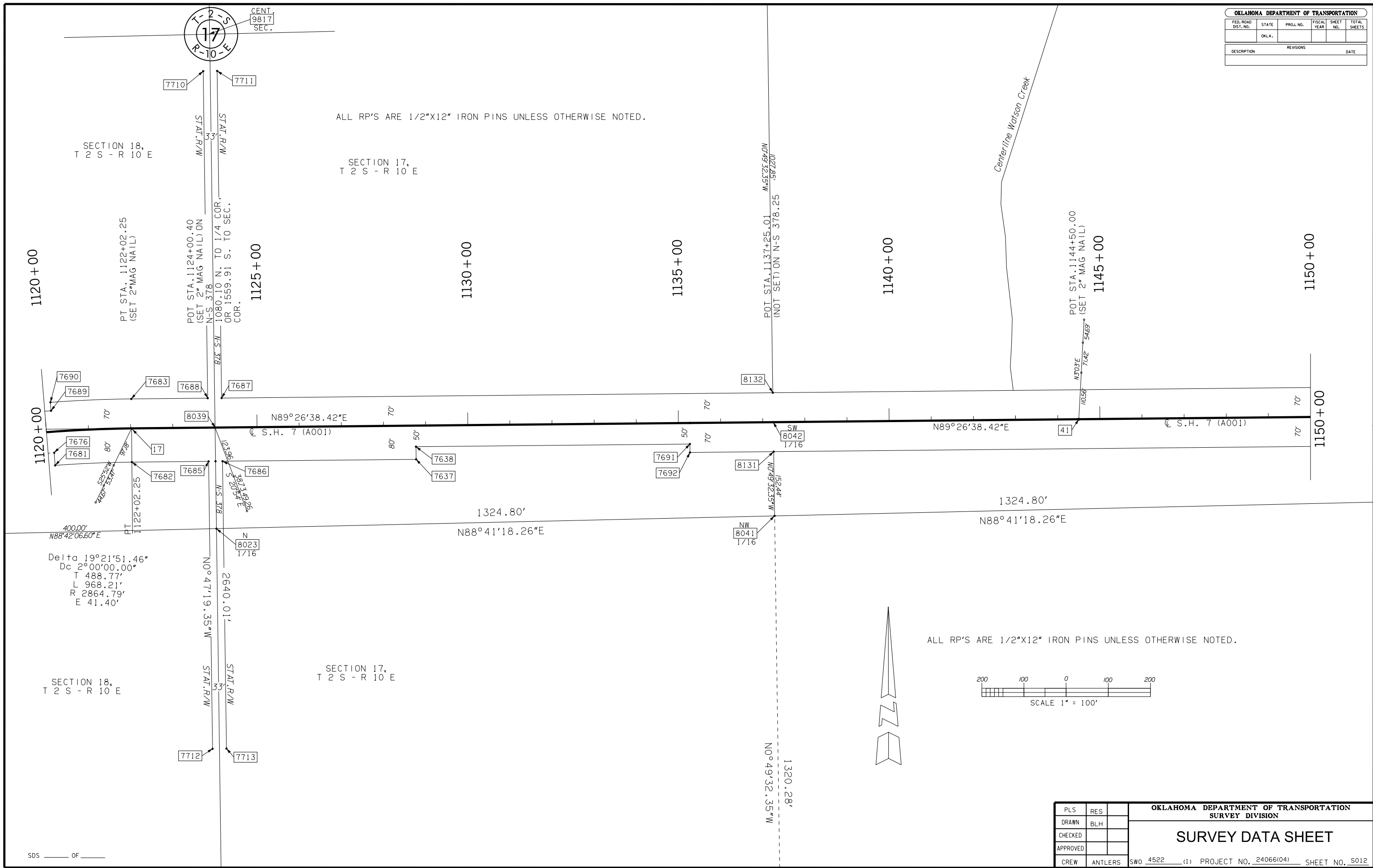
SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. S011	

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION		REVISIONS		DATE	

ALL RP'S ARE 1/2"X12" IRON PINS UNLESS OTHERWISE NOTED.

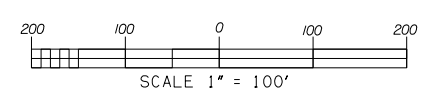
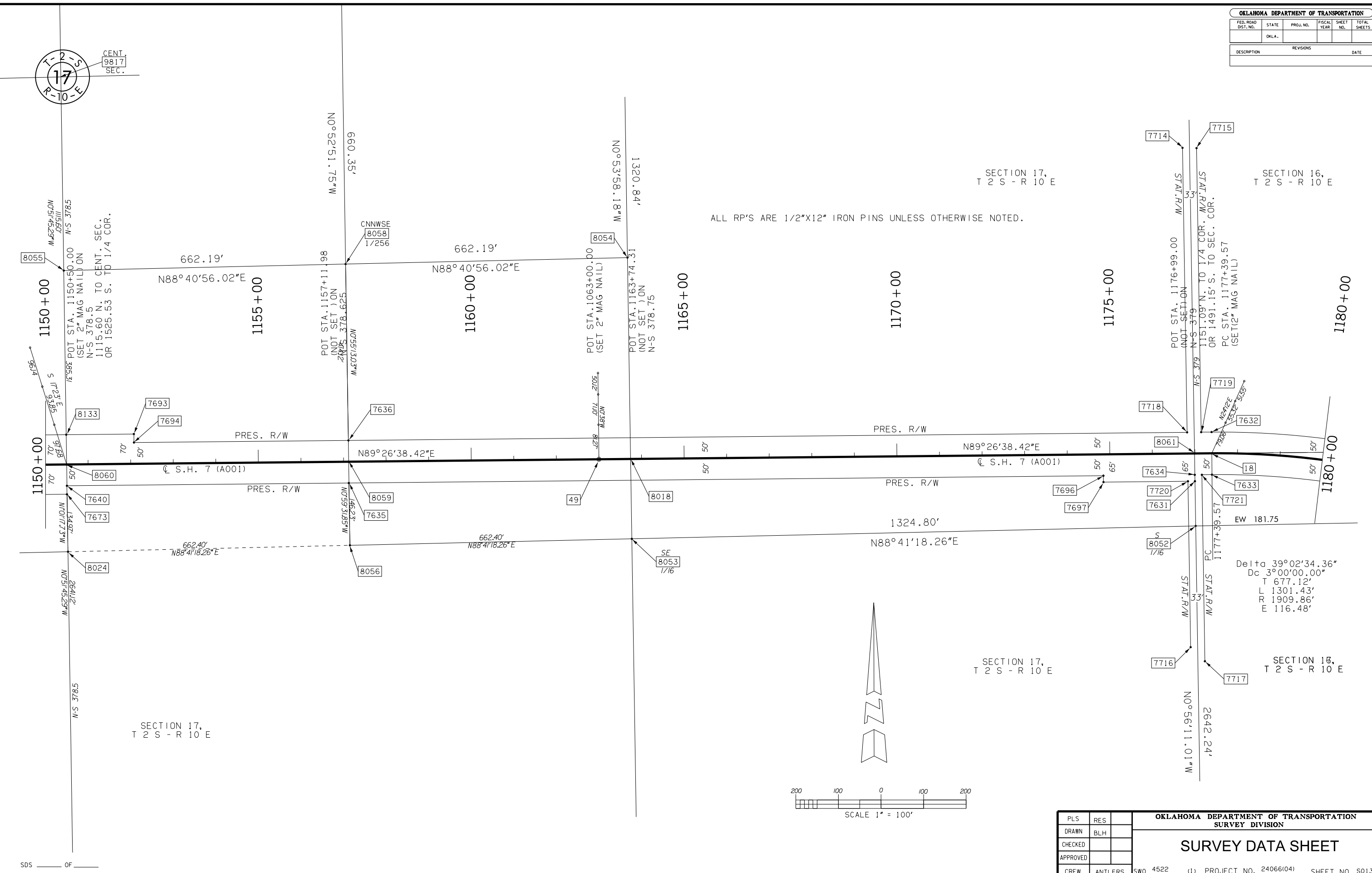
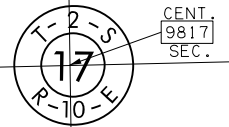
ALL RP'S ARE 1/2"X12" IRON PINS UNLESS OTHERWISE NOTED.



SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS		
SURVEY DATA SHEET			
SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. S012			

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION		REVISIONS		DATE	



OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION					
SURVEY DATA SHEET					
PLS	RES				
DRAWN	BLH				
CHECKED					
APPROVED					
CREW	ANTLERS	SWO 4522 (1)	PROJECT NO. 24066(04)	SHEET NO. 5013	

SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS	DATE	

ALL RP'S ARE 1/2"X12" IRON PINS UNLESS OTHERWISE NOTED.

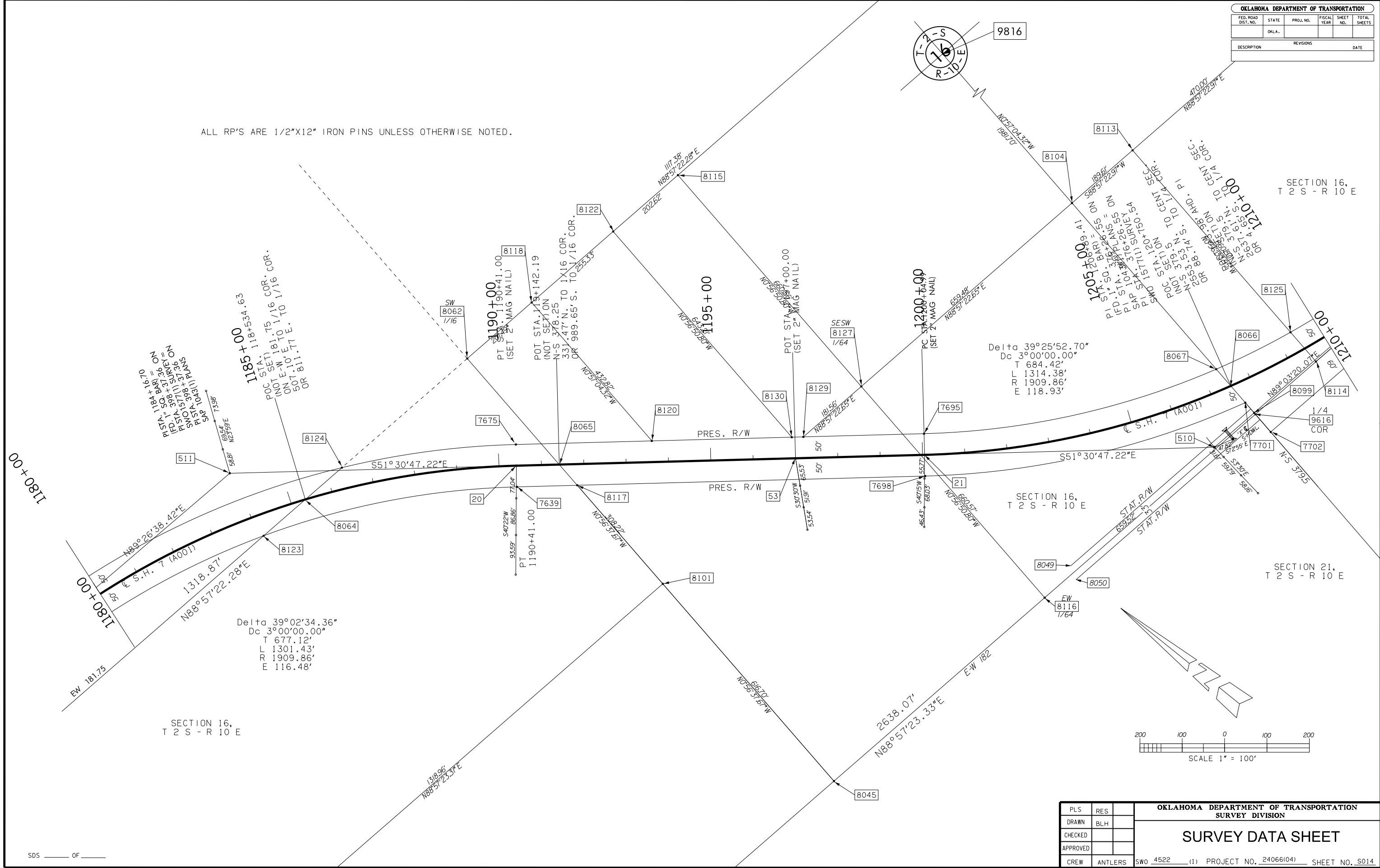
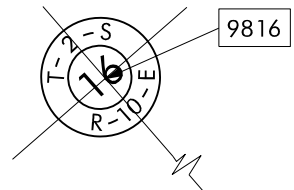
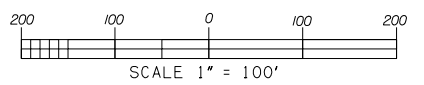
SECTION 16,
T 2 S - R 10 E

SECTION 21,
T 2 S - R 10 E

SECTION 16,
T 2 S - R 10 E

Delta 39°25'52.70"
Dc 3°00'00.00"
T 684.42'
L 1314.38'
R 1909.86'
E 118.93'

Delta 39°02'34.36"
Dc 3°00'00.00"
T 677.12'
L 1301.43'
R 1909.86'
E 116.48'

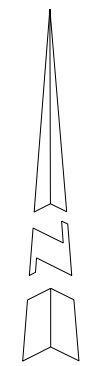
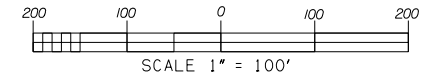
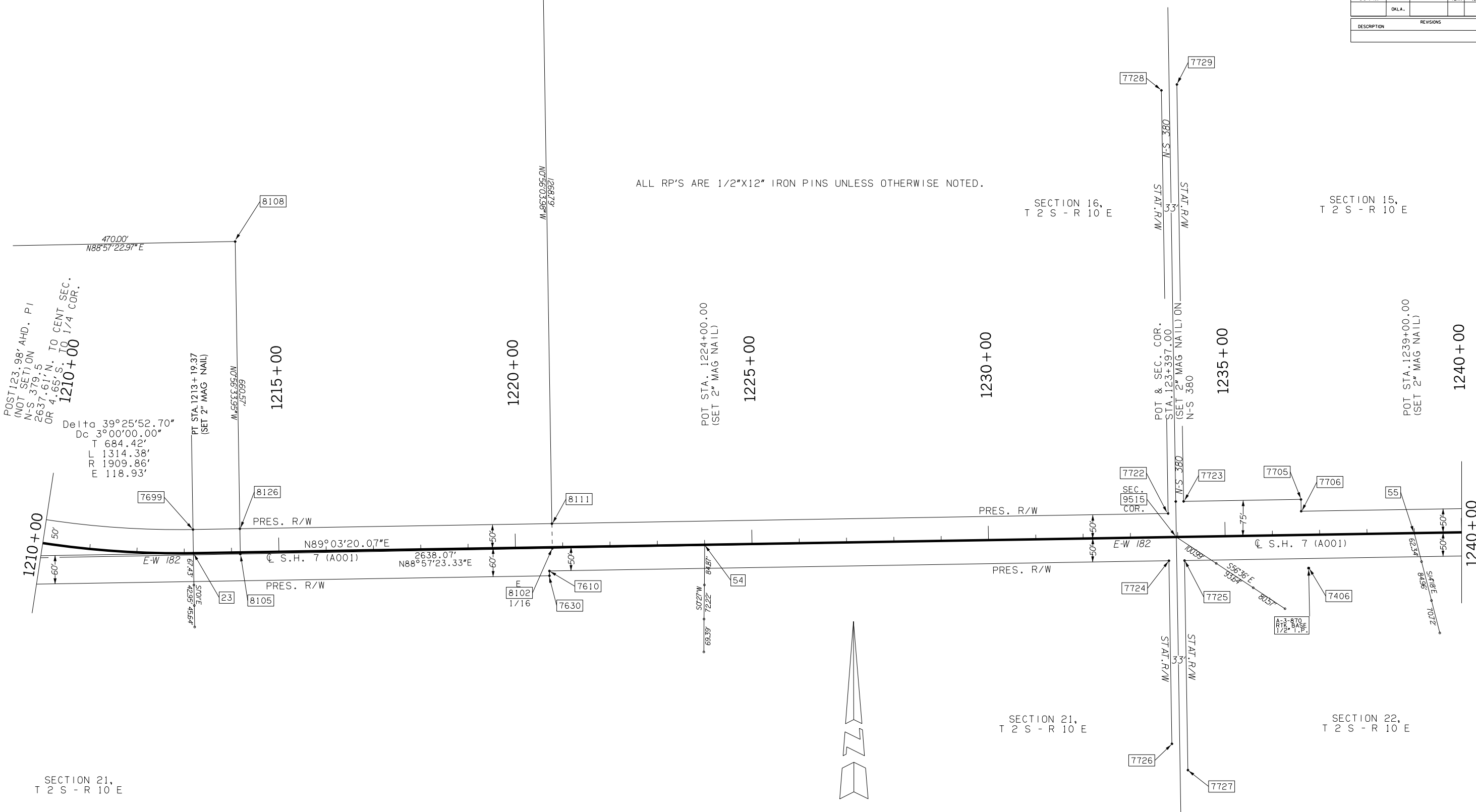


OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS		

SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. S014

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION		REVISIONS		DATE	

ALL RP'S ARE 1/2"x12" IRON PINS UNLESS OTHERWISE NOTED.



SECTION 21, T 2 S - R 10 E

SECTION 21, T 2 S - R 10 E

SECTION 22, T 2 S - R 10 E

SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SWO 4522 (1)	PROJECT NO. 24066(04) SHEET NO. S015

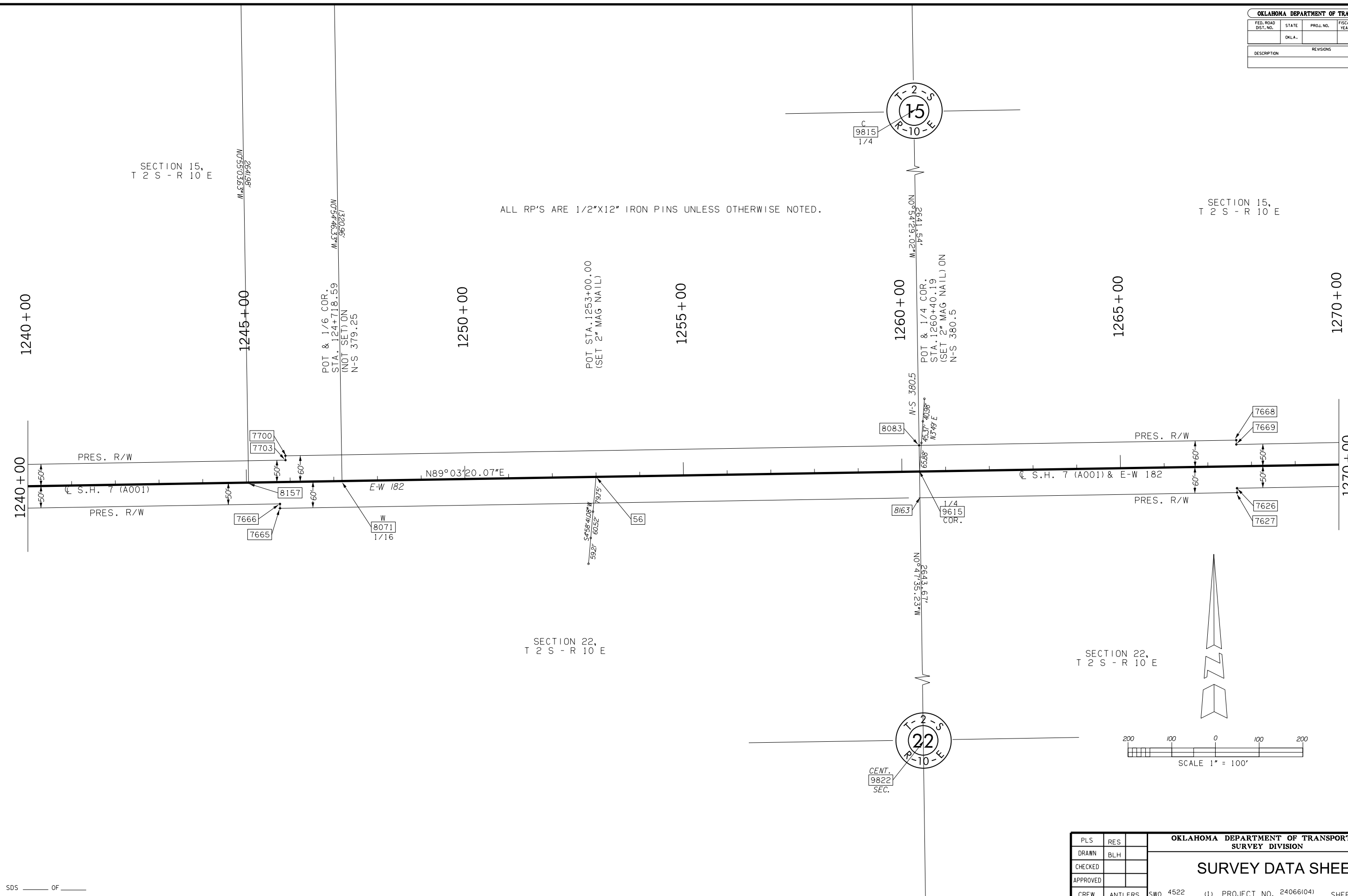
SURVEY DATA SHEET

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION		REVISIONS		DATE	

SECTION 15,
T 2 S - R 10 E

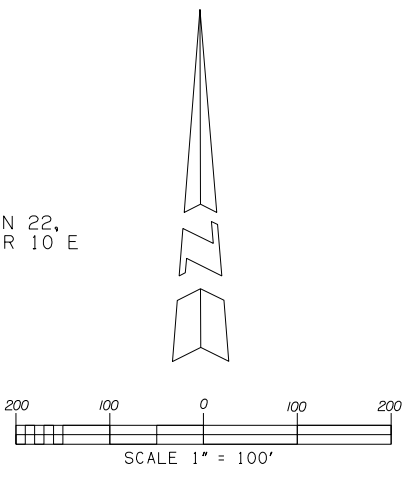
SECTION 15,
T 2 S - R 10 E

ALL RP'S ARE 1/2"x12" IRON PINS UNLESS OTHERWISE NOTED.



SECTION 22,
T 2 S - R 10 E

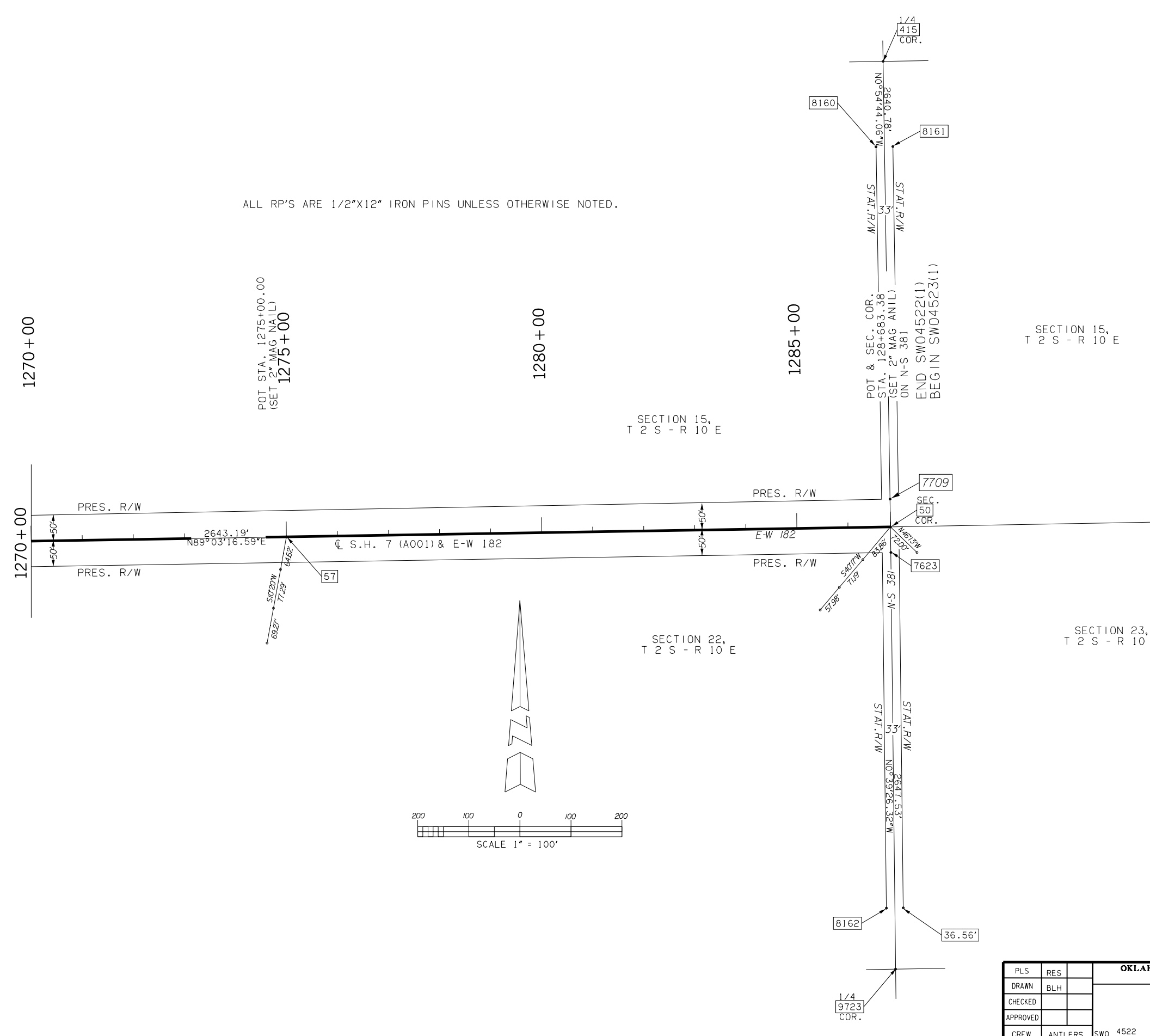
SECTION 22,
T 2 S - R 10 E



SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS		
SURVEY DATA SHEET			
SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. S016			

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION		REVISIONS		DATE	



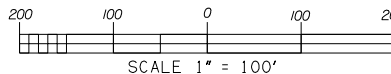
ALL RP'S ARE 1/2"X12" IRON PINS UNLESS OTHERWISE NOTED.

SECTION 15,
T 2 S - R 10 E

SECTION 15,
T 2 S - R 10 E

SECTION 22,
T 2 S - R 10 E

SECTION 23,
T 2 S - R 10 E



SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SWO 4522 (1)	PROJECT NO. 24066(04) SHEET NO. S017

ODOT MON. NO.
A-3-834
SECTION CORNER
Found 1/2" Iron Pin with ID Cap stamped CA#3949, agrees with identified existing identified corners 1 and 2 mi. north and 1 mi. south, used as found.

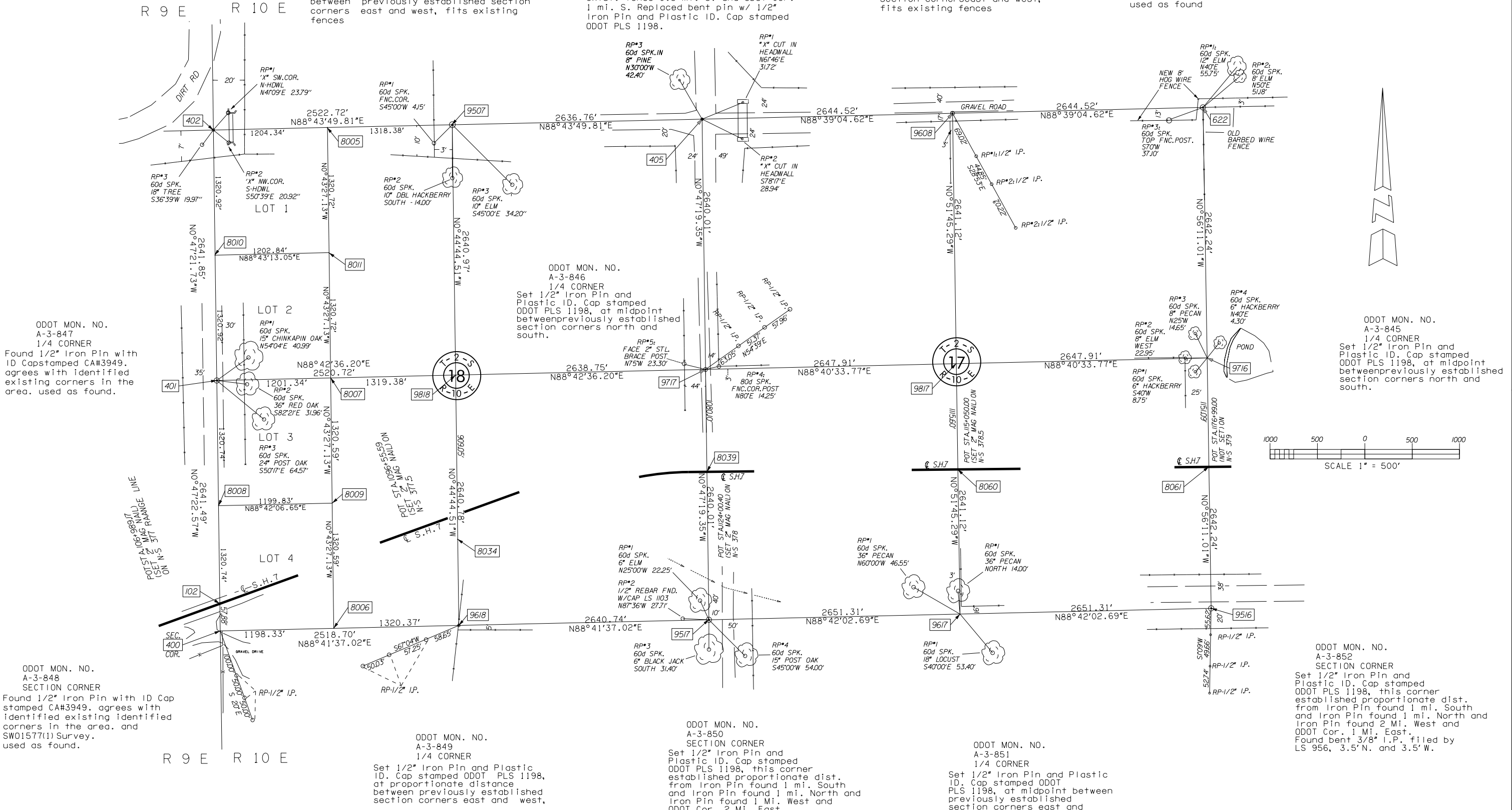
ODOT MON. NO.
A-3-835
1/4 CORNER
Iron Pin at proportionate distance between previously established section corners east and west, fits existing fences

ODOT MON. NO.
A-3-836
SECTION CORNER
Found bent 1/2" Iron Pin, agrees with identified existing fence 0.5 mi. N. and iron pin 1 mi. north and exist. fence 0.5 mi. east and Page Pin 1 mi. E and exist. fence 0.5 mi. S. and ODOT Cor. 1 mi. S. Replaced bent pin w/ 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198.

ODOT MON. NO.
A-3-837
1/4 CORNER
Set 1/2" Iron Pin at midpoint between previously established section corner east and west, fits existing fences

ODOT MON. NO.
A-3-838
SECTION CORNER
Found 1/2" Iron Pin filed by LS 1324, agrees with I.P. found 1 mi. west and I.P. found 1 mi. east and ODOT COR. 1 mi. south, used as found

OKLAHOMA DEPARTMENT OF TRANSPORTATION						
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS	DATE
	OKLA.					
DESCRIPTION		REVISIONS		DATE		



ODOT MON. NO.
A-3-847
1/4 CORNER
Found 1/2" Iron Pin with ID Cap stamped CA#3949, agrees with identified existing corners in the area, used as found.

ODOT MON. NO.
A-3-846
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners north and south.

ODOT MON. NO.
A-3-845
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners north and south.

ODOT MON. NO.
A-3-848
SECTION CORNER
Found 1/2" Iron Pin with ID Cap stamped CA#3949, agrees with identified existing identified corners in the area, and SW01577(1) Survey, used as found.

ODOT MON. NO.
A-3-849
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at proportionate distance between previously established section corners east and west,

ODOT MON. NO.
A-3-850
SECTION CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, this corner established proportionate dist. from Iron Pin found 1 mi. South and Iron Pin found 1 mi. North and Iron Pin found 1 mi. West and ODOT Cor. 2 mi. East. Found 1/2" I.P. w/ID cap stamped LS1103, 27.5' west and 1.5' north.

ODOT MON. NO.
A-3-851
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners east and west,

ODOT MON. NO.
A-3-852
SECTION CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, this corner established proportionate dist. from Iron Pin found 1 mi. South and Iron Pin found 1 mi. North and ODOT Cor. 1 mi. East. Found bent 3/8" I.P. filed by LS 956, 3.5' N. and 3.5' W.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS			
DRAWN			
CHECKED			
APPROVED			
CREW			
SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. S018			

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS	DATE	

ODOT MON. NO. A-3-848
SECTION CORNER
Found 1/2" Iron Pin with ID Cap stamped CA#3949, agrees with identified existing identified corners in the area. and SW01577(1) Survey, used as found.

ODOT MON. NO. A-3-849
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at proportionate distance between previously established section corners east and west,

ODOT MON. NO. A-3-850
SECTION CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, this corner established proportionate dist. from Iron Pin found 1 mi. South and Iron Pin found 1 mi. North and ODOT Cor. 2 Mi. East. Found 1/2" I.P. w/ID cap stamped LS1103, 27.5' west and 1.5' north.

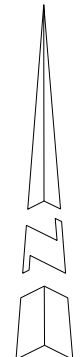
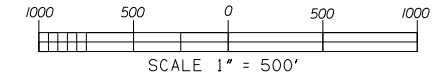
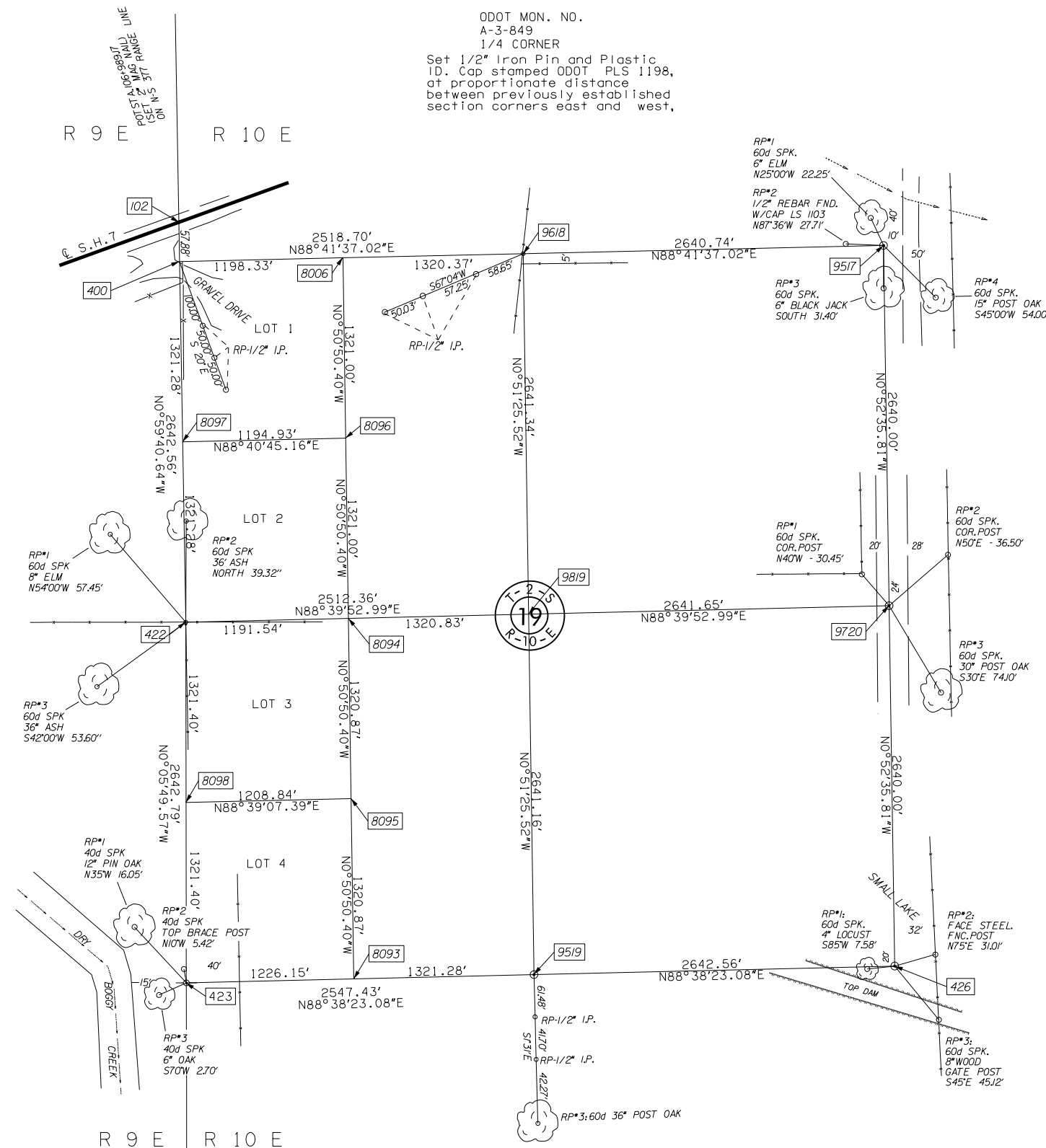
ODOT MON. NO. A-3-861
W-1/4 CORNER SEC. 19
Found 1/2" Galvanized Pipe in fence corner filed by LS 696 used as found.
Found 1/2" I.P. 2.5 North.

ODOT MON. NO. A-3-860
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners north and south.

ODOT MON. NO. A-3-864
SECTION CORNER
Found 1/2" I.P. w/ID cap stamped LS 1103 in place, this corner fits 1/2" I.P. found 1 mi. west and 3/8" I.P. found 1 mi. east and 1/2" I.P. found 1 mi. north and 1/2" I.P. found 1 mi. south, used as found.

ODOT MON. NO. A-3-862
SECTION CORNER
Found 1/2" Iron Pin with ID Cap stamped LS 1103, at old brace post, this corner agrees w/80D spk. fd. at township cor. 1 mi. south and 1/2" I.P. found 1 mi. east, and 1/2" I.P. filed 1 mi. west.
Found 1/2" I.P. filed by LS 969 - 41' west and 5' north.

ODOT MON. NO. A-3-863
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at proportionate distance between previously established section corners east and west,



OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS			
DRAWN			
CHECKED			
APPROVED			
CREW			

SURVEY DATA SHEET

SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. S019

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS		DATE

ODOT MON. NO. A-3-838
SECTION CORNER
Found 1/2" Iron Pin filed by LS 1324, agrees with I.P. found 1 mi. west and I.P. found 1 mi. east and ODOT COR. 1 mi. south, used as found

ODOT MON. NO. A-3-839
1/4 CORNER
Found 1/2" Iron Pin with ID Cap stamped PAGE LS 1324 agrees with I.P. found 0.5 mi. west and I.P. found 0.5 mi. east, used as found.

ODOT MON. NO. A-3-840
SECTION CORNER
Found 1/2" Iron Pin with ID Cap stamped PAGE LS 1324 agrees with I.P. found 0.5 mi. west and I.P. found 0.5 mi. east, used as found.

ODOT MON. NO. A-3-841
1/4 CORNER
Found 1/2" Iron Pin with ID Cap stamped PAGE LS 1324 agrees with I.P. found 0.5 mi. west and I.P. found 0.5 mi. east, used as found.

ODOT MON. NO. A-3-842
SECTION CORNER
Found 1/2" Iron Pin with ID Cap stamped PAGE LS 1324 agrees with I.P. found 0.5 mi. west and I.P. found 1.5 mi. east. and ODOT Cor. 1 mi. south used as found.

ODOT MON. NO. A-3-845
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners north and south.

ODOT MON. NO. A-3-844
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners north and south, fits fences.

ODOT MON. NO. A-3-843
1/4 CORNER
Found 1/2" Iron Pin with ID Cap stamped PAGE LS 1324 agrees with I.P. found 0.5 mi. north and ODOT Cor. 0.5 mi. south, used as found

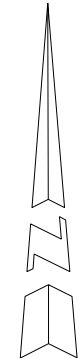
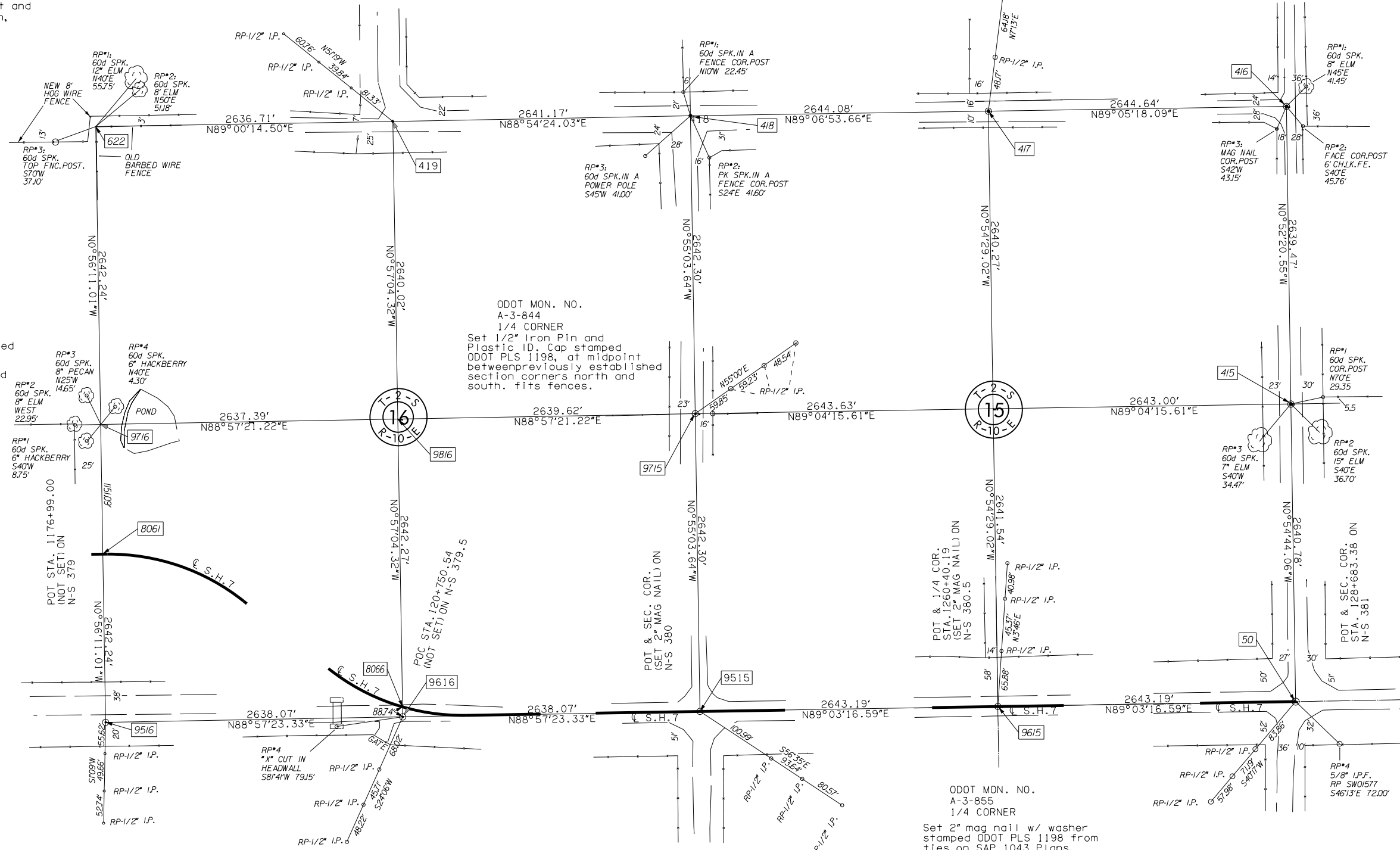
ODOT MON. NO. A-3-852
SECTION CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, this corner established proportionate dist. from Iron Pin found 1 mi. South and Iron Pin found 1 mi. North and Iron Pin found 2 Mi. West and ODOT Cor. 1 Mi. East. Found bent 3/8" I.P. filed by LS 956, 3.5' N. and 3.5' W.

ODOT MON. NO. A-3-853
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners east and west

ODOT MON. NO. A-3-854
PI & SECTION CORNER
Set 2" mag nail from ties on SAP 1043 Plans, agrees with I.P. found 1 mi. north and I.P. found 1 mi. south and I.P. found 3 mi. west and ODOT Cor. 0.5 mi. east

ODOT MON. NO. A-3-855
1/4 CORNER
Set 2" mag nail w/ washer stamped ODOT PLS 1198 from ties on SAP 1043 Plans, agrees with existing identified corners in the area.

ODOT MON. NO. A-3-856
POT & SECTION CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, from ties on SAP 1043 Plans, agrees with plans corners east and west and topography south.



PLS						OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET
DRAWN						
CHECKED						
APPROVED						
CREW						
SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. S020						

OKLAHOMA DEPARTMENT OF TRANSPORTATION						
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS	
	OKLA.					
DESCRIPTION				REVISIONS	DATE	

ODOT MON. NO. A-3-852
SECTION CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, this corner established proportionate dist. from Iron Pin found 1 mi. South and Iron Pin found 1 mi. North and Iron Pin found 2 mi. West and ODOT Cor. 1 Mi. East. Found bent 3/8" I.P. filed by LS 956, 3.5' N. and 3.5' W.

ODOT MON. NO. A-3-853
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners east and west

ODOT MON. NO. A-3-854
PI & SECTION CORNER
Set 2" mag nail from ties on SAP 1043 Plans, agrees with I.P. found 1 mi. north and I.P. found 1 mi. south and I.P. found 3 mi. west and I.P. found 0.5 mi. east

ODOT MON. NO. A-3-855
1/4 CORNER
Set 2" mag nail w/ washer stamped ODOT PLS 1198 from ties on SAP 1043 Plans, agrees with existing identified corners in the area.

ODOT MON. NO. A-3-856
POT & SECTION CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, from ties on SAP 1043 Plans, agrees with plans corners east and west and topography south.

ODOT MON. NO. A-3-858
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners north and south.

ODOT MON. NO. A-3-857
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners north and south.

ODOT MON. NO. A-3-859
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners north and south.

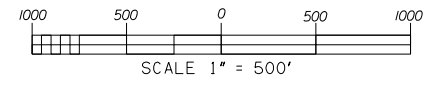
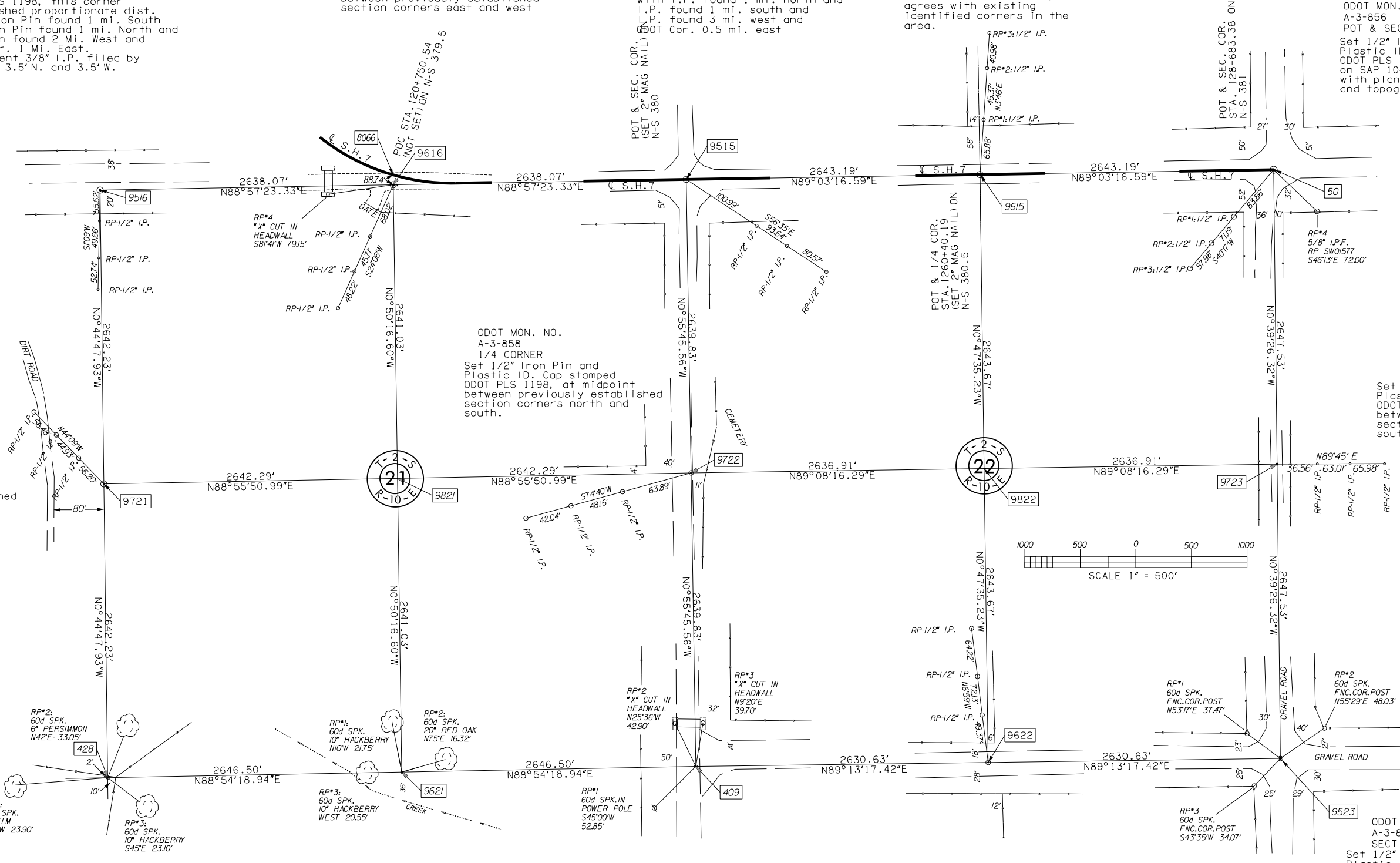
ODOT MON. NO. A-3-865
SECTION CORNER
Found 1/2" I.P. w/ID cap stamped LS 1103 in place, this corner fits 1/2" I.P. found 0.5 mi. west and 3/8" I.P. found 1 mi. east and I.P. found 2 mi. north used as found

ODOT MON. NO. A-3-866
1/4 CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners east and west

ODOT MON. NO. A-3-867
SECTION CORNER

ODOT MON. NO. A-3-868
1/4 CORNER
ODOT MON. NO. A-3-868
1/4 CORNER SECS. 22 & 27
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, at midpoint between previously established section corners east and west

ODOT MON. NO. A-3-869
SECTION CORNER
Set 1/2" Iron Pin and Plastic ID. Cap stamped ODOT PLS 1198, this corner established proportionate dist from: 3/8" I.P. found 1 mi. west and 80d SPK. found 2 mi. east and ODOT Cor. 1 mi. north and 1/2" I.P. found 1 mi. south.



OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	
DRAWN	
CHECKED	
APPROVED	
CREW	

SURVEY DATA SHEET

SWO 4522 (1) PROJECT NO. 24066(04) SHEET NO. S021

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.				

DESCRIPTION	REVISIONS	DATE

SURVEY DATA SHEETS

ATOKA COUNTY

S.H. 7

SWO 4523(1) STATE JOB NO. 24066(05)

From 11.4 miles East of the Johnston County Line, East 5 Miles to Atoka

SURVEY CONTROL DATA

1. HORIZONTAL CONTROL:

- A. HORIZONTAL CONTROL FOR THIS SURVEY IS THE NGS OKLAHOMA STATE PLANE COORDINATE SYSTEM, NAD83 (1993), LAMBERT PROJECTION (SOUTH ZONE).
- B. ACCURACY - THE PRIMARY CONTROL NETWORK, THE SECONDARY CONTROL NETWORK AND SECTION BOUNDARIES FOR THIS SURVEY ARE IN GENERAL COMPLIANCE WITH THE NGS SECOND ORDER, CLASS II STANDARDS FOR HORIZONTAL CONTROL (1:20,000).

2. BEARINGS:

THE BEARINGS SHOWN HEREIN OR HEREON ARE GRID BEARINGS DERIVED FROM THE NGS OKLAHOMA STATE PLANE COORDINATE SYSTEM AND ARE NOT ASTRONOMICAL. THE ANGLE OF VARIANCE BETWEEN GRID NORTH (GN) AND THE ASTRONOMICAL TRUE NORTH (TN) IS DEPICTED DIAGRAMMATICALLY.

3. VERTICAL CONTROLS:

- A. LEVEL DATUM IS NAVD 88.
- B. ACCURACY - VERTICAL CONTROL FOR THIS SURVEY IS WITHIN THE CLOSURE REQUIREMENT OF NOAA/NGS "CLASSIFICATION, STANDARDS OF ACCURACY, AND GENERAL SPECIFICATIONS OF GEODETIC CONTROL SURVEYS" (FEB. 1974, REPRINTED FEB. 1977) THIRD ORDER STANDARDS AS A MINIMUM.

SURVEY BEGAN: FEB. 1, 2010 .
SURVEY COMPLETED: MARCH 31, 2010.

PARTY PERSONNEL
RICKY E. STEELE PLS 1198
DANNY M. GREGG TRANS. SPEC. III
MICHAEL BOLING, TRANS. TECH II
CLAYTON MACK, TANS. TECH.
BRYAN L. HINDMAN, TRANS. TECH

UTILITY INFO:

CALL OKIE LOCATE NUMBERS
10030910550915
10030910560921
10030910570923
10030910580927
10030911000936

WATER LINES ON THIS SURVEY OWNED BY ATOKA CO. RURAL WATER DIST. #4 LINES WERE LOCATED BY SHELBY FUGATE. PH# 580-889-5949

WATER LINES ON THIS SURVEY OWNED BY CITY OF ATOKA ALL LINES WERE LOCATED BY STEPHEN SMITH PH# 580-889-2675

ALL POWER LINES ON THIS SURVEY ARE OWNED BY SOUTH EASTERN ELEC.

ALL TELEPHONE LINES ON THIS SURVEY ARE OWNED BY AT&T ALL LINES WERE LOCATED FROM PRINTS FURNISHED BY SMP A CONTRACT LOCATE CO. VERIFIED BY EVIDENCE FOUND IN FIELD DURING SURVEY.

GAS LINES ON THIS PROJECT
SCISSOR TAIL STEVE HENREY 580-320-3141
ENOGEX EDDIE HENRY 580-364-0392
ANTERO ADAM APPEL 580-927-9133
MARK WEST EDDIE SANDERS 918-389-5100
CENTER POINT ENERGY DENNIS POWELL 580-889-3429

SEWER LINES ON THIS PROJECT ARE OWNED BY CITY OF ATOKA LOCATED BY STEPHEN SMITH PH# 580-889-2675



SURVEY DATA SHEETS 1" = 100'
GEOMETRIC DATA SHEETS 1" = 500'

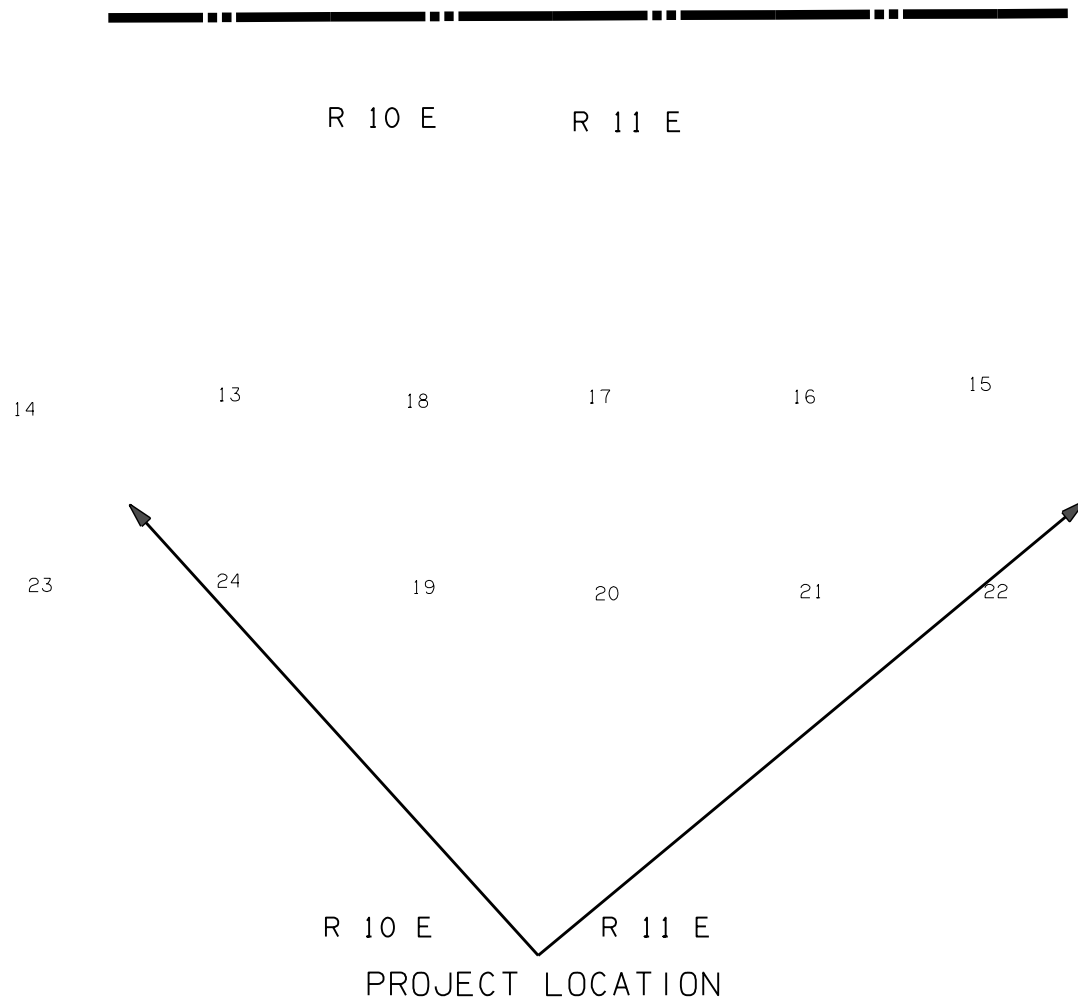
CONVENTIONAL SYMBOLS

- RAILROADS
- RANGE & TOWNSHIP
- SECTION LINES
- QUARTER SECTION LINES
- FENCES
- EXISTING ROADS
- BASE LINE
- TELEPHONE & TELEGRAPH
- POWER LINES
- BUILDINGS
- OILWELL
- DRAINAGE STRUCTURES - IN PLACE

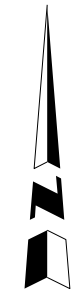
THIS SURVEY MEETS THE OKLAHOMA MINIMUM STANDARDS FOR THE PRACTICE OF LAND SURVEYING AS ADOPTED BY THE OKLAHOMA STATE BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS AND LAND SURVEYORS, JUNE 11, 2001.

SPECIFICATIONS FOR SURVEYS FOR PRIMARY AND SECONDARY HIGHWAYS DATED SEPTEMBER 11, 2001 GOVERN.

SDS _____ OF _____



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INDEX OF SURVEY SHEETS

- 1. TITLE SHEET
- 2. HORIZONTAL CONTROL DIAGRAM & HISTORICAL LETTER & WRITTEN REPORT
- 3. BENCHMARK LIST
- 4-5 COGO POINTS, ALIGNMENT REPORT,
- 6-18. SURVEY DATA SHEETS
- 19-23 GEOMETRIC DATA SHEETS

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

SWO_4523(1) Job/Piece_24066_ (04) Engr.Contract No. _____

LAND SURVEYOR'S CERTIFICATION

I hereby certify that all land and property sub-division distances, angles, corners and monumentation made or used in conjunction with this survey and depicted or recorded herein or hereon were recovered, established or re-established in substantial conformity with:

- applicable Instructions contained in the U.S. Government Bureau of Land Management publication "Manual of Surveying Instructions";
- its supplement, "Restoration of Lost or Obliterated Corners and Subdivision of Sections";
- "Oklahoma Minimum Standards for the Practice of Land Surveying" as adopted by the State Board of Registration for Professional Engineers and Land Surveyors; and
- sound land surveying practices;

Including a thorough search, study, analysis and consideration of all existing records and field evidence.

I further certify that all survey monuments depicted exist and that all land survey work was done by me or under my direct supervision and that it is true, accurate and correct to the best of my knowledge and belief.

Dated this 31st day of MARCH, 2010.

(seal)
Land Surveyor _____
Signature
RICKY E. STEELE
Printed Name
Oklahoma Registered Land Surveyor No. 1198

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	RES
DRAWN	BLH
CHECKED	
APPROVED	
CREW	ANTLERS

SURVEY DATA SHEET

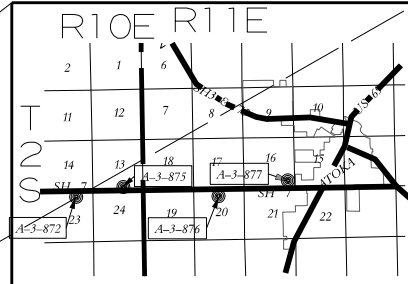
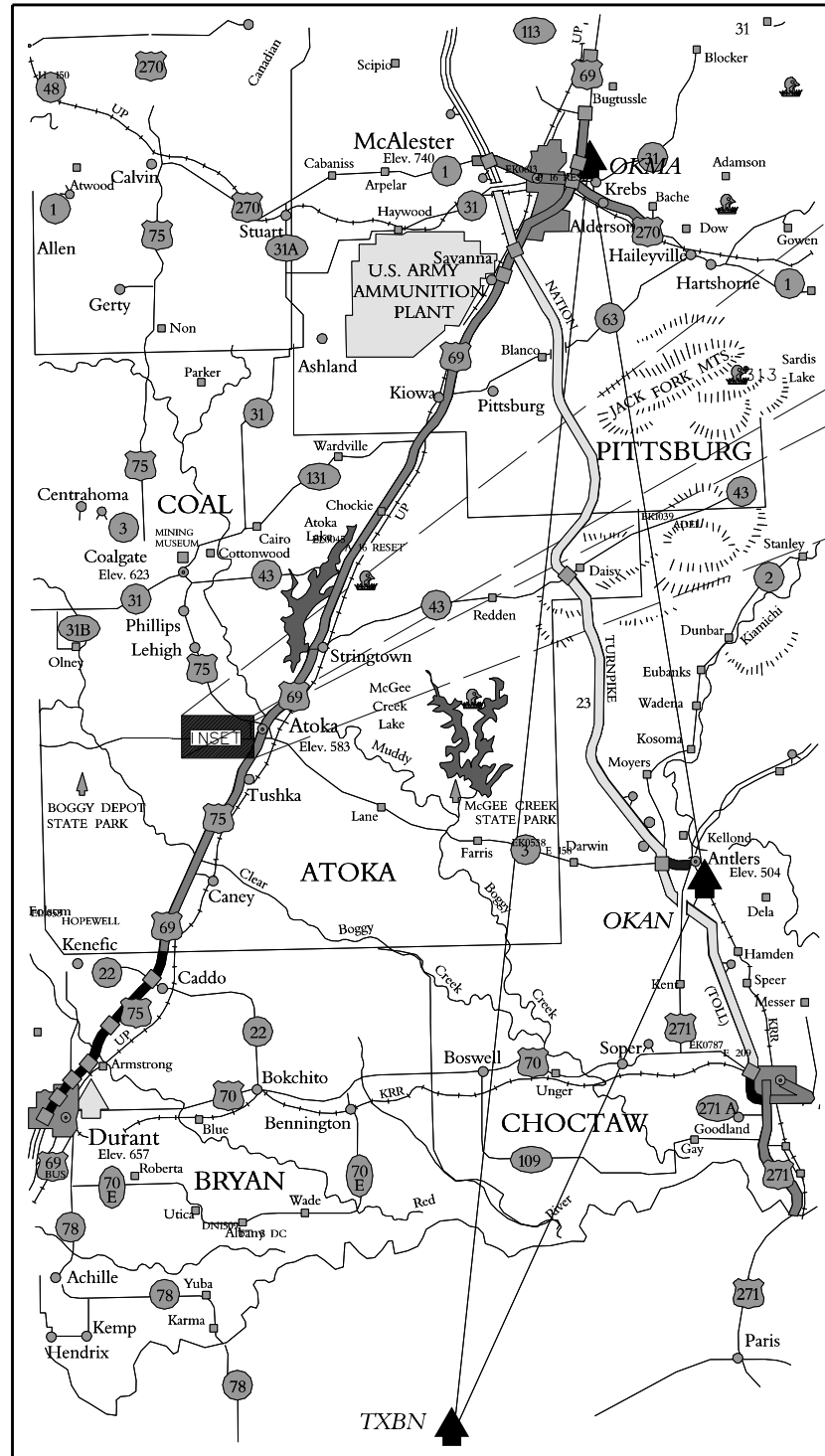
SWO 4523(1) STATE JOB NO. 24066(07) SHEET NO. 5022

HORIZONTAL CONTROL

INSET

OPUS REPORT

OKLAHOMA DEPARTMENT OF TRANSPORTATION						
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS	
	OKLA.					
DESCRIPTION		REVISIONS		DATE		



OPUS REPORT

A-3-874 - 7402 - ODOT STD.FENO MARKER
 FILE:74022440.09o 00010528

NGS OPUS SOLUTION REPORT

USER: rsteale@odot.org DATE: September 02, 2009
 RINEX FILE:74022440.09o TIME:17:36:55 UTC

SOFTWARE: page5 0908J3 master28.pl 081023 START: 2009/09/01 14:28:00
 EPHEMERIS: lgr15472.eph [Ultra-rapid] STOP: 2009/09/01 17:41:00
 NAV FILE: brdc2440.09n OBS USED: 7404 / 7578 : 98%
 ANT NAME: LEIAT502 NONE * FIXED AMB: 33 / 36 : 92%
 ARP HEIGHT: 1.653 OVERALL RMS: 0.012(m)

REF FRAME: NAD_83(CORS96)EPOCH:2002.0000 ITRF00 (EPOCH:2009.6676)

X: -570834.546(m) 0.090(m) -570835.263(m) 0.090(m)
 Y: -5239033.816(m) 0.097(m) -5239032.405(m) 0.097(m)
 Z: 3580937.676(m) 0.003(m) 3580937.513(m) 0.003(m)

LAT: 34 22 29.33908 0.048(m) 34 22 29.35899 0.048(m)
 E LON: 263 46 54.11172 0.000(m) 263 46 54.07784 0.000(m)
 W LON: 96 13 5.88828 0.000(m) 96 13 5.92216 0.000(m)
 EL HGT: 174.767(m) 0.072(m) 173.582(m) 0.072(m)
 ORTHO HGT: 202.369(m) 0.076(m) [NAVD88 (Computed using GEOID03)]

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Northing (Y) [meters] 3807223.529 116970.688
 Easting (X) [meters] 755786.696 763857.366
 Convergence [degrees] 1.5714107 1.01132133
 Point Scale 1.00040655 0.99994258
 Combined Factor 1.00037910 0.99991515

US NATIONAL GRID DESIGNATOR: 14SQD557807224(NAD 83)

PID DESIGNATION BASE STATIONS USED LATITUDE LONGITUDE DISTANCE(m)
 DL3498 TXBN BONHAM CORS ARP N333624.95 W0961031.56 85293.0
 DE7170 OKMA MCALESTER CORS ARP N345540.833 W0954414.056 75572.2
 DE7168 OKAN ANTLERS CORS ARP N341142.708 W0953716.847 58465.7

ODOT MON.NO.A-3-875 - 7403- ODOT STD.FENO MARKER
 FILE:68122390.000 000103526

NGS OPUS SOLUTION REPORT

USER: rsteale@odot.org DATE: August 31, 2009
 RINEX FILE:68122390.09o TIME:13:27:26 UTC

SOFTWARE: page5 0908J3 master.pl 0810233 START: 2009/08/27 18:37:00
 EPHEMERIS: lgr15464.eph [rapid] STOP: 2009/08/27 21:06:00
 NAV FILE: brdc2390.09n OBS USED: 5207 / 5294 : 98%
 ANT NAME: LEIAT502 NONE * FIXED AMB: 28 / 31 : 90%
 ARP HEIGHT: 2.00 OVERALL RMS: 0.013(m)

REF FRAME: NAD_83(CORS96)EPOCH:2002.0000 ITRF00 (EPOCH:2009.6543)

X: -569610.882(m) 0.022(m) -569611.599(m) 0.022(m)
 Y: -5239154.698(m) 0.005(m) -5239153.287(m) 0.005(m)
 Z: 3580954.093(m) 0.042(m) 3580953.930(m) 0.042(m)

LAT: 34 22 30.00288 0.027(m) 34 22 30.02280 0.027(m)
 E LON: 263 47 42.23566 0.033(m) 263 47 42.20179 0.033(m)
 W LON: 96 12 17.76434 0.033(m) 96 12 17.79821 0.033(m)
 EL HGT: 173.943(m) 0.008(m) 172.757(m) 0.008(m)
 ORTHO HGT: 201.622(m) 0.011(m) [NAVD88 (Computed using GEOID03)]

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Northing (Y) [meters] 3807277.796 117012.919
 Easting (X) [meters] 757015.699 765086.266
 Convergence [degrees] 1.57897835 1.01890909
 Point Scale 1.00041432 0.99994257
 Combined Factor 1.00038700 0.99991527

US NATIONAL GRID DESIGNATOR: 14SQD5701607278(NAD 83)

PID DESIGNATION BASE STATIONS USED LATITUDE LONGITUDE DISTANCE(m)
 DL3498 TXBN BONHAM CORS ARP N333624.95 W0961031.56 85264.8
 DE7170 OKMA MCALESTER CORS ARP N345540.833 W0954414.056 74846.9
 DE7168 OKAN ANTLERS CORS ARP N341142.708 W0953716.847 57317.2

ODOT MON.NO.A-3-876 - 7404 - ODOT STD.FENO MARKER
 FILE:74042440.09o 000105291

NGS OPUS SOLUTION REPORT

USER: rsteale@odot.org DATE: September 02, 2009
 RINEX FILE:74042440.09o TIME:17:38:13 UTC

SOFTWARE: page5 0908J3 master40.pl 081023 START: 2009/09/01 14:15:00
 EPHEMERIS: lgr15472.eph [Ultra-rapid] STOP: 2009/09/01 17:48:00
 NAV FILE: brdc2440.09n OBS USED: 7842 / 8082 : 97%
 ANT NAME: LEIAT502 NONE * FIXED AMB: 36 / 38 : 95%
 ARP HEIGHT: 1.710 OVERALL RMS: 0.012(m)

REF FRAME: NAD_83(CORS96)EPOCH:2002.0000 ITRF00 (EPOCH:2009.6676)

X: -566885.361(m) 0.042(m) -566886.078(m) 0.042(m)
 Y: -5239455.093(m) 0.017(m) -5239453.682(m) 0.017(m)
 Z: 3580920.915(m) 0.037(m) 3580920.752(m) 0.037(m)

LAT: 34 22 29.02698 0.028(m) 34 22 29.04691 0.028(m)
 E LON: 263 49 29.55618 0.043(m) 263 49 29.52234 0.043(m)
 W LON: 96 10 30.44382 0.043(m) 96 10 30.47766 0.043(m)
 EL HGT: 159.140(m) 0.031(m) 157.953(m) 0.031(m)
 ORTHO HGT: 186.990(m) 0.040(m) [NAVD88 (Computed using GEOID03)]

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Northing (Y) [meters] 3807323.713 117032.015
 Easting (X) [meters] 759758.589 767828.566
 Convergence [degrees] 1.59582694 1.03583046
 Point Scale 1.00043179 0.99994259
 Combined Factor 1.00040680 0.99991761

US NATIONAL GRID DESIGNATOR: 14SQD5975907324(NAD 83)

PID DESIGNATION BASE STATIONS USED LATITUDE LONGITUDE DISTANCE(m)
 DL3498 TXBN BONHAM CORS ARP N333624.95 W0961031.56 85190.8
 DE7170 OKMA MCALESTER CORS ARP N345540.833 W0954414.056 73340.7
 DE7168 OKAN ANTLERS CORS ARP N341142.708 W0953716.847 54741.3

ODOT MON.NO.A-3-877 - 7405 - ODOT STD.FENO MARKER
 FILE:66792391.000 000103518

NGS OPUS SOLUTION REPORT

USER: rsteale@odot.org DATE: August 31, 2009
 RINEX FILE:66792391.09o TIME:13:17:41 UTC

SOFTWARE: page5 0908J3 master.pl 0810233 START: 2009/08/27 18:28:00
 EPHEMERIS: lgr15464.eph [rapid] STOP: 2009/08/27 21:06:00
 NAV FILE: brdc2390.09n OBS USED: 5184 / 5455 : 95%
 ANT NAME: LEIAT502 NONE * FIXED AMB: 32 / 32 : 100%
 ARP HEIGHT: 1.716 OVERALL RMS: 0.013(m)

REF FRAME: NAD_83(CORS96)EPOCH:2002.0000 ITRF00 (EPOCH:2009.6543)

X: -564189.466(m) 0.086(m) -564190.183(m) 0.086(m)
 Y: -5239735.249(m) 0.076(m) -5239733.838(m) 0.076(m)
 Z: 3580934.755(m) 0.065(m) 3580934.592(m) 0.065(m)

LAT: 34 22 29.59491 0.071(m) 34 22 29.61484 0.071(m)
 E LON: 263 51 15.63880 0.094(m) 263 51 15.60499 0.094(m)
 W LON: 96 8 44.36120 0.094(m) 96 8 44.39501 0.094(m)
 EL HGT: 158.070(m) 0.048(m) 156.883(m) 0.048(m)
 ORTHO HGT: 186.094(m) 0.054(m) [NAVD88 (Computed using GEOID03)]

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Northing (Y) [meters] 3807417.121 117098.900
 Easting (X) [meters] 762468.554 770537.556
 Convergence [degrees] 1.61249905 1.05256664
 Point Scale 1.00044924 0.99994258
 Combined Factor 1.00042442 0.99991777

US NATIONAL GRID DESIGNATOR: 14SQD6246907417(NAD 83)

PID DESIGNATION BASE STATIONS USED LATITUDE LONGITUDE DISTANCE(m)
 DE8097 OKAD ADA CORS ARP N344801.213 W0964417.707 71997.1
 DE7170 OKMA MCALESTER CORS ARP N345540.833 W0954414.056 71882.7
 DE7168 OKAN ANTLERS CORS ARP N341142.708 W0953716.847 52230.0

PLS	RES	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	BLH	SURVEY DATA SHEET
CHECKED		
APPROVED		
CREW	ANTLERS	

SWO 4523 (1) PROJECT NO. 24066107 SHEET NO. S023

Table with columns: PAGE 1 OF 9, CHECK LEVELS SW04523(1), BENCH MARK LIST NAVD 88 DATUM. Includes columns for BM NO., RUN 1, RUN 2, MEAN DIFF., DIFF. USED, ADJ. ELEV., PUBLISHED ELEV., and BM DESCRIPTION. Rows include BM 7400, BM2, BM24, BM3, BM26, BM4, BM5, BM6, BM7, BM8, BM9, and BM10=7401.

Table with columns: PAGE 3 OF 9, CHECK LEVELS SW04523(1), BENCH MARK LIST NAVD 88 DATUM. Includes columns for BM NO., RUN 1, RUN 2, MEAN DIFF., DIFF. USED, ADJ. ELEV., PUBLISHED ELEV., and BM DESCRIPTION. Rows include BM22, BM23, BM24, BM25, BM26, BM27, BM28, BM29, BM30, BM31, BM32 7406, and BM33.

Table with columns: PAGE 2 OF 9, CHECK LEVELS SW04523(1), BENCH MARK LIST NAVD 88 DATUM. Includes columns for BM NO., RUN 1, RUN 2, MEAN DIFF., DIFF. USED, ADJ. ELEV., PUBLISHED ELEV., and BM DESCRIPTION. Rows include BM11, BM 12, BM13, BM14, BM15, BM16, BM17, BM18, BM19, BM20, and BM21.

Table with columns: PAGE 4 OF 9, CHECK LEVELS SW04523(1), BENCH MARK LIST NAVD 88 DATUM. Includes columns for BM NO., RUN 1, RUN 2, MEAN DIFF., DIFF. USED, ADJ. ELEV., PUBLISHED ELEV., and BM DESCRIPTION. Rows include BM34, BM35, BM36, BM37, BM38, BM39, BM40, BM41, BM42, and BM43.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET. Includes checkboxes for PLS, RES, DRAWN, BLH, CHECKED, APPROVED, CREW, ANTLERS. Project info: SWO 4523 (1) PROJECT NO. 240661071 SHEET NO. S024.

PAGE 5 OF 9 CHECK LEVELS SW04523(1) BENCH MARK LIST NAVD 88 DATUM							
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED:	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM44	-14.53	-14.53	-14.53	-14.53	676.96		TOP DEL. POST IN FENCE LINE 92.31' RT. STA. 1313+28.83
BM45	5.36	5.36	5.36	5.36	662.43		TOP DEL. POST 99.99' RT. STA. 1320+93.82
BM46	-3.98	-3.98	-3.98	-3.98	667.79		BOX ON CL S-HDWL. CULVT. 17.64' RT. STA. 1323+66.01
BM47=7402	-22.48	-22.48	-22.48	-22.48	663.81	OPUS 663.814	ODOT FENO MKR. SET IN CONC. 46.76' RT. STA. 1330+68.37
BM48	-1.20	-1.20	-1.20	-1.20	641.33		BOX ON CL N-HDWL. CULVT. 18.22' LT. STA. 1339+56.07
BM49	-2.89	-2.89	-2.89	-2.89	640.13		TOP DEL. POST 110.11' LT. STA. 1340+22.96
BM50	6.13	6.13	6.13	6.13	637.24		BOX ON CL S-HDWL. CULVT. 22.77' RT. STA. 1342+40.36
BM51	-14.42	-14.41	-14.42	-14.42	643.37		TOP DEL. POST 93.74' RT. STA. 1348+43.98
BM52	33.50	33.50	33.50	33.50	628.95		BOX ON CL S-HDWL. CULVT. 31.35' RT. STA. 1353+41.32
BM53	-13.79	-13.79	-13.79	-13.79	662.45		TOP DEL. POST 113.00' LT. STA. 1360+47.55'
BM54	12.78	12.78	12.78	12.78	648.65		BOX ON CL S-HDWL. CULVT. 30.24' RT. STA. 1365+83.82

PAGE 6 OF 9 CHECK LEVELS SW04523(1) BENCH MARK LIST NAVD 88 DATUM							
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED:	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM55=7403	-10.96	-10.96	-10.96	-10.96	661.43	OPUS 661.488	ODOT FENO MKR. SET IN CONC. 27.65' LT. STA. 1371+02.27
BM56	-3.20	-3.20	-3.20	-3.20	650.47		BOX ON CL S-HDWL. CULVT. 17.11' RT. STA. 1378+30.10
BM57	5.89	5.88	5.88	5.88	647.28		TOP DEL. POST 107.90' LT. STA. 1381+25.38
BM58	5.71	5.72	5.72	5.72	653.16		TOP DEL. POST 93.80' LT. STA. 1389+83.96
BM59	-5.94	-5.94	-5.94	-5.94	658.88		BOX ON CL N-HDWL. CULVT. 18.36' LT. STA. 1391+86.06
BM60	-9.85	-9.84	-9.84	-9.84	652.94		TOP DEL. POST 102.80' LT. STA. 1398+39.90
BM61	-0.75	-0.76	-0.76	-0.76	643.09		BOX ON CL S-HDWL. CULVT. 25.22' RT. STA. 1405+17.10
BM62	-12.84	-12.84	-12.84	-12.84	642.34		TOP DEL. POST 99.34' RT. STA. 1407+26.28
BM63	-3.62	-3.62	-3.62	-3.62	629.49		TOP DEL. POST 112.37' RT. STA. 1417+22.98
BM64	-3.35	-3.35	-3.35	-3.35	625.87		BOX ON CL S-HDWL. CULVT. 24.33' RT. STA. 1418+43.41
BM65	-3.30	-3.31	-3.31	-3.31	622.52		TOP DEL. POST 84.28' RT. STA. 1423+41.21

PAGE 7 OF 9 CHECK LEVELS SW04523(1) BENCH MARK LIST NAVD 88 DATUM							
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED:	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM66	-11.10	-11.11	-11.11	-11.10	619.21		TOP DEL. POST 69.82' RT. STA. 1431+75.34
BM67	2.40	2.41	2.41	2.41	608.11		TOP DEL. POST 111.08' RT. STA. 1440+91.08
BM68	7.38	7.37	7.37	7.38	610.51		BOX ON N.W. WINGWALL 17.88' LT. STA. 1444+27.72
BM69	-4.46	-4.45	-4.45	-4.45	617.89		TOP DEL. POST 92.66' RT. STA. 1449+58.68
BM70=7404	-9.22	-9.22	-9.22	-9.22	613.44	OPUS 613.50	ODOT FENO MKR. SET IN CONC. 51.82' RT. STA. 1461+02
BM71	-12.10	-12.10	-12.10	-12.10	604.22		TOP DEL. POST 78.95' RT. STA. 1469+95.67
BM72	-3.64	-3.64	-3.64	-3.64	592.12		TOP DEL. POST 114' RT. STA. 1480+26
BM73	0.95	0.95	0.95	0.95	588.48		BOX ON CL N-HDWL. CULVT. 19.97' LT. STA. 1484+54.82
BM74	-4.92	-4.92	-4.92	-4.92	589.42		TOP DEL. POST IN CH.LK.FE.90.30' RT. STA. 1489+77.60
BM75	-1.63	-1.63	-1.63	-1.63	584.51		BOX ON CL S-HDWL. CULVT. 20.97' RT. STA. 1492+89.73
BM76	0.38	0.39	0.39	0.39	582.88		BOX ON CL N-HDWL. CULVT. 21.69' LT. STA. 1497+57.98
BM77					583.27		BOX ON CL N-HDWL. CULVT. 16.52' LT. STA. 1497+93.83

PAGE 8 OF 9 CHECK LEVELS SW04523(1) BENCH MARK LIST NAVD 88 DATUM							
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED:	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
BM78	-4.12	-4.12	-4.12	-4.12	579.15		TOP DEL. POST 109.32' RT. STA. 1501+91.19
BM79	-11.33	-11.34	-11.34	-11.34	567.81		TOP DEL. POST 123.46' RT. STA. 1510+72.33
BM80	3.91	3.92	3.91	3.91	571.72		BOX ON CL S-HDWL. CULVT. 34.00' RT. STA. 1514+63.45
BM81	-3.52	-3.52	-3.52	-3.52	568.20		TOP DEL. POST 81.39' RT. STA. 1518+38.35
BM82	8.00	8.01	8.00	8.00	576.20		TOP DEL. POST 71.91' RT. STA. 1529+39.43
BM83	0.92	0.91	0.91	0.91	577.12		BOX ON CL S-HDWL. CULVT. 13.76' RT. STA. 1534+40.87
BM84	6.39	6.39	6.39	6.39	583.51		TOP DEL. POST 55.70' RT. STA. 1539+98.10
BM85=7405	27.02	27.03	27.03	27.03	610.54	OPUS 610.54	ODOT FENO MKR. SET IN CONC. 22.64' LT. STA. 1549+94.76
	-23.56	-23.55	-23.56	-23.56			

PLS	RES	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	BLH	
CHECKED		
APPROVED		
CREW	ANTLERS	SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S025

PAGE 9 OF 9 CHECK LEVELS SW04523(1)						BENCH MARK LIST NAVD 88 DATUM	
BM NO.	RUN 1	RUN 2	MEAN DIFF.	DIFF. USED	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
TBM					586.98	356.39	BOX ON CONC. S.W. AT S.E. COR. DOLLAR STORE
	-19.61	-19.60	-19.60	-19.60			
TBM					567.38		BOX ON CL W-HDWL
	0.03	0.03	0.03	0.03			
BM					567.41		ODOT BRASS MON. NO. A-3-672 CL W-HDWL, CULVT. STA. 824+70 ON F-219 (6) US 69 PLANS.
	-2.53	-2.53	-2.53	-2.53			
BM					564.88		ODOT BRASS MON. NO. A-3-673 CL EAST END CONC. RET. WALL RT. APPROX. STA. 84+14 US 69 PLANS.

9. Topography: Topographic information was obtained by field conventional methods. Surface improvements and underground utilities such as; fences, drives, mail boxes, signs etc. have been labeled and can be found in the project (TOPO) file.

10. Surface features: The surface features on this project were obtained by the field conventional method, a Surface Feature File (SFF), which is a Microstation V8i Design File containing breaklines and random ground points for contouring and quantity computations has been created and archived as per instructions.

11. Land & Property Ties: Complete land ties were obtained on Sections 13, 14, 23, & 24, Township 2-South, Range 10 East and Sections 16, 17, 18, 19, 20, & Township 2-South, Range 11 East the following is a complete history of the PLSS corners established on this project.

12. Right-of-Way: The Right-of-Way on this survey is taken from SAP1043(1) Plans and verified using R/W deeds provided by Right-of-Way Division.

13. Utilities: The underground utilities on this project were flagged or taken from digital records by the owning companies or their representatives with the help of crew personnel. Utilities were located by conventional methods and placed in the project design file at zero elevation and labeled with owner name, size and depth, when provided. ODOT Form SD-7, "Public and Privately Owned Utilities List," have been submitted with this project. Contact information for utility owners can be found on the 1st Survey Data Sheet.

14. Hazardous Materials or Waste Sites: There were no hazardous waste sites found during this survey. There were no abandoned underground storage tanks found on this project.

15. Drainage Information:
a. Drainage areas for all drains crossing the Survey Centerline were taken from digital USGS Quad. maps provided by the Aerial Survey Branch, the divide lines between these drainage areas have been placed in a Microstation V8 Design File and archived as per instructions.
b. Highwater information was obtained by interviewing longtime residents in the area, this information can be found placed in the Microstation V8 Drainage Design File.
c. No Ravine sections for drains crossing the Survey Centerline were required on this survey.
d. Flowline profiles have been obtained on drains crossing Centerline of Survey 500' Upstream and 1000' downstream.

16. Submission of Survey Data: All digital survey data has been placed in the appropriate project folder on the ODOT Intranet storage system as per archiving instruction dated May 20, 2003. For a complete listing of computer files created and archived on this project see: http://intranet/engrgrp/survey/fsvarch/SW04523_1/index.txt In addition to the computer files submitted, hard copies of the following have been submitted to the Central Survey Office:
A. Historical Letter & Written Report
B. Form SD-1, Transmittal Letter w/FSVARCH.INDEX attached. Note: the FSVARCH.INDEX is a listing of all computer files archived during the course of the survey, as well as a written description of what is contained in each of the files, and the date the files were archived.
C. Form SD-7, Public and Privately Owned Utilities List w/ vicinity map on back.
D. Form SD-9, Final Cost Report of Survey
E. (4) Form SD-11, Position and Description of Survey Monuments (GPS control monuments, Brass/Aluminum Caps for benchmarks, etc. if applicable).
F. Form SD-20, Survey Control Data Statement.
H. Form SD-41, Surveyor's Certification.
I. Logo Data (coordinate list with alignments).
J. Benchmarks & Check Levels list, including the SWD and description of the project.
K. (38) Original and reduced copies (8" x 11") of Certified Land Corner Forms.
Personnel:
Ricky E. Steele * Prof. Land Surveyor II
Danny M. Gregg * Transportation Specialist III
Michael W. Boling 1/32 Transportation Tech. II
Clayton R. Mack 1/32 Transportation Tech. I
Bryan L. Hindman 1/32 Transportation Tech. I

OKLAHOMA DEPARTMENT OF TRANSPORTATION
Survey Division 14051521-2621 Fax 405-522-0364
Date: March 31, 2010

To: Mr. Larry Reser, Chief of Surveys
From: Ricky E. Steele, Professional Land Surveyor
Subject: SW0 SW04523(1) - J/P24066(04) - S.H 7 1/2 Atoka County
From 11.4 miles East of the Johnston Co. Line, East approx. 5 miles to Atoka.
HISTORICAL LETTER AND WRITTEN REPORT
Method of Survey - Conventional Survey Methods
Units of Measurement - U.S. Survey Foot.
Survey Began 1/32 February 1, 2010
Survey Completed 1/32 March 31, 2010

1. General
Previous Surveys and plans used on this project.
Surveys:
SW0 1577(1)-S.H.7 From U.S. 69 at Atoka, west toward Wapanucka.
Plans:
S.A.P. No. 1043(1) 1/32 S.H.7 From U.S. 69 at Atoka, west toward Wapanucka.

2. Survey Assignment:
This project was assigned to the Antlers Survey Crew, under my direct supervision by letter detailing the survey scope and special provisions dated August 27, 2009 by Mr. Jeff King Transportation Survey Manager.

3. Purpose of Survey:
The purpose of this survey is to obtain adequate information for the design and construction of Upgraded roadway and shoulders.

4. Survey Limits:
This survey begins at PI & SEC. COR. STA. 1286+83.38 on N-S 381= POT STA. 1286+83.38 ON SW04522(1) J/P24066(05)= PI & SEC. COR. STA. 242+76.7 ON SAP 1043(1) PLANS. And extends easterly along the existing SH 7 to approximately 500' East of NS 386 Section Line crossing.

5. Alignment:
The alignment for this survey is along and identical to Present S.H. 7 as depicted in SW0 1577(1) Survey and SAP 1043(1) Plans. There are 7 tangents on this survey, the following original monumentation was used to establish Centerline of Survey:
A. PI STA. 1206+89.41 (FOUND 1" SQUARE BAR) = PI STA. 376+26.55 ON SAP 1043(1) PLANS.
B. FOUND ORIG. RP 72 5/32 SE. OF PI STA. 295+10.1 ON SW01577(1) = PI STA. 1286+83.38 ON THIS SURVEY
C. FOUND ORIG. RP 50 S. PI STA. 250+63.8 1 ON SW01577(1) = PI STA. 1331+99.11 ON THIS SURVEY.
D. FOUND ORIG. RP 50 5/32 N. PI STA. 192+74.83 ON SW01577(1).
E. FOUND ORIG. RP 50 5/32 N. PC STA. 187+91.5 ON SW01577(1).
F. FOUND ORIG. RP 96 09/32 SW. OF PC STA. 178+69.8 SW01577(1).
G. FOUND ORIG. RP 50 5/32 S. POT STA. 148+00 ON SW01577(1).
H. FOUND ORIG. RP 60 5/32 N. POT STA 111+00 ON SW01577(1).

6. Stationing:
Stationing on this survey is taken from PI & SEC. COR. STA. 1286+83.38 ON SW0 4522(1) SURVEY and carried forward without equation to PI & SEC. COR. STA. 1550+56.86 = PI STA. 48+864.152 ON SW03759 SURVEY = 0°08'44.06" LT.

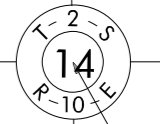
7. Horizontal Control:
Horizontal control for this survey is the Oklahoma State Plane Coordinate System, NAD 83 (1993) Lambert Projection South Zone derived submitting Leica GPS rawdata from 3 hour static sessions to NGS OPUS for processing. Four project control monuments were established for this survey; two near the beginning, and end of the project. The project control monuments on this project are ODOT Std. Feno markers set flush with the ground in concrete. Secondary Control for this survey is Centerline of Survey POT, POST and Curve Points set and referenced using GPS Real Time Kinematic methods, following accepted ODOT Survey Division methods and techniques. The primary control network, the secondary control network and the section boundaries for this survey are in general compliance with NGS Second Order, Class II Standards for horizontal control (1:20,000). It is assumed that the GPS positional accuracies obtained have met or exceeded this standard.

8. Vertical Control:
Level datum for this survey is (NGS) N.A.V.D. 88 taken from OPUS Solution for Primary Control Monument A-3-872 (The most westerly control point) and a double set of spirit levels were run with a Leica NA 2002 Digital Level through the project benchmarks and tied to ODOT BRASS MON. NO. A-3-672 CL W-HDWL, CULVT. STA. 824+70 ON F-219 (6) US 69 PLANS. And ODOT BRASS MON. NO. A-3-673 CL EAST END CONC. RET. WALL RT. APPROX. STA. 84+14 US 69 PLANS. These monuments were established under SW0 3915(1) Benchmark densification project. This survey meets the requirements of N.G.S. 3rd order standards as a minimum.

SDS _____ OF _____

PLS	RES		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SW0 4523 (1)	PROJECT NO. 24066(07) SHEET NO. S026

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION		REVISIONS		DATE	



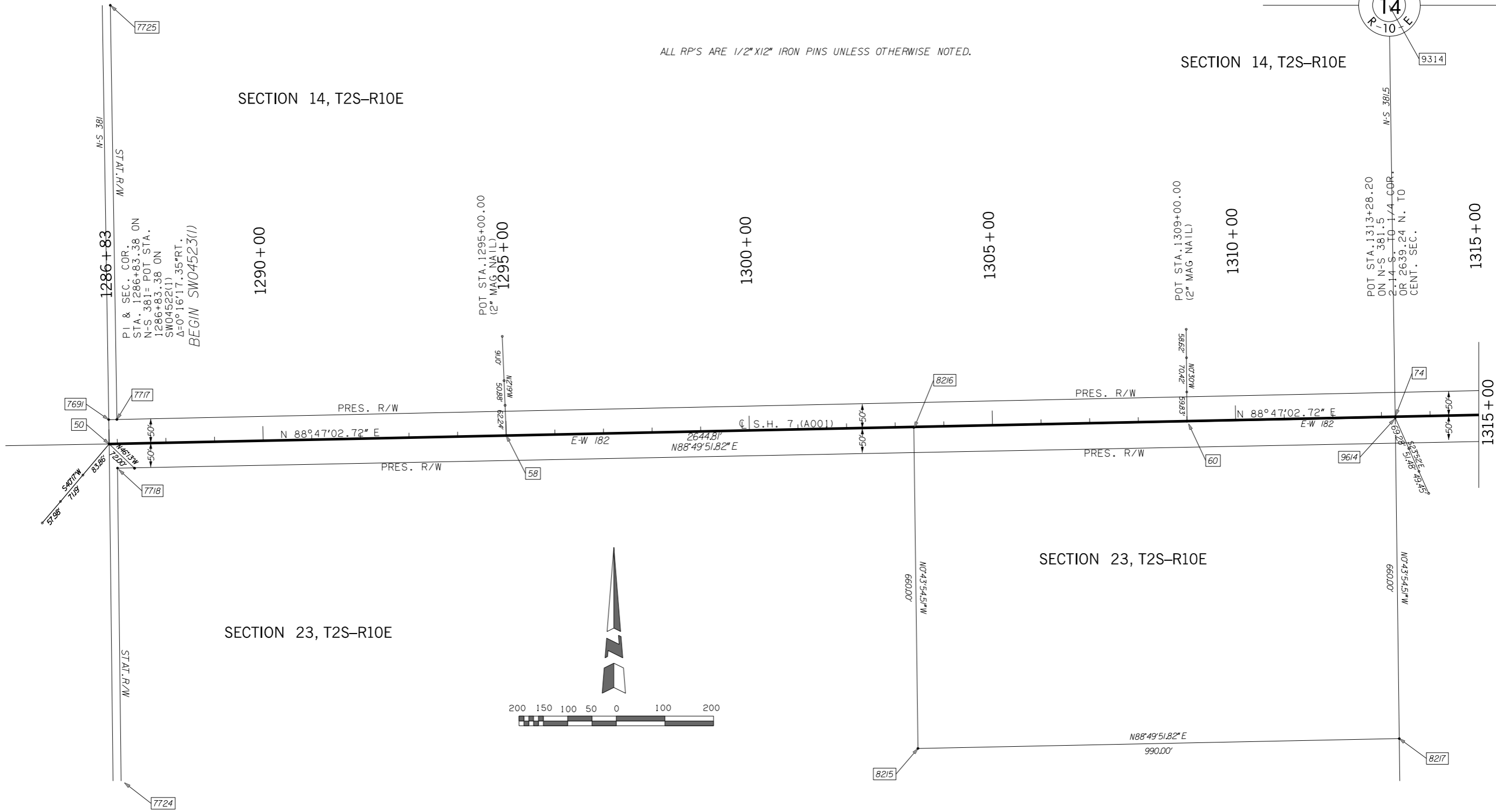
SECTION 14, T2S-R10E

ALL RP'S ARE 1/2" X 12" IRON PINS UNLESS OTHERWISE NOTED.

SECTION 14, T2S-R10E

SECTION 23, T2S-R10E

SECTION 23, T2S-R10E



PI & SEC. COR.
 STA. 1286+83.38 ON
 N-S 381- POT STA.
 1286+83.38 ON
 SW04522(1)
 $\Delta = 0^\circ 16' 17.35''$ RT.
 BEGIN SW04523(1)

POT STA. 1295+00.00
 (2" MAG NAIL)
 1295+00

POT STA. 1309+00.00
 (2" MAG NAIL)

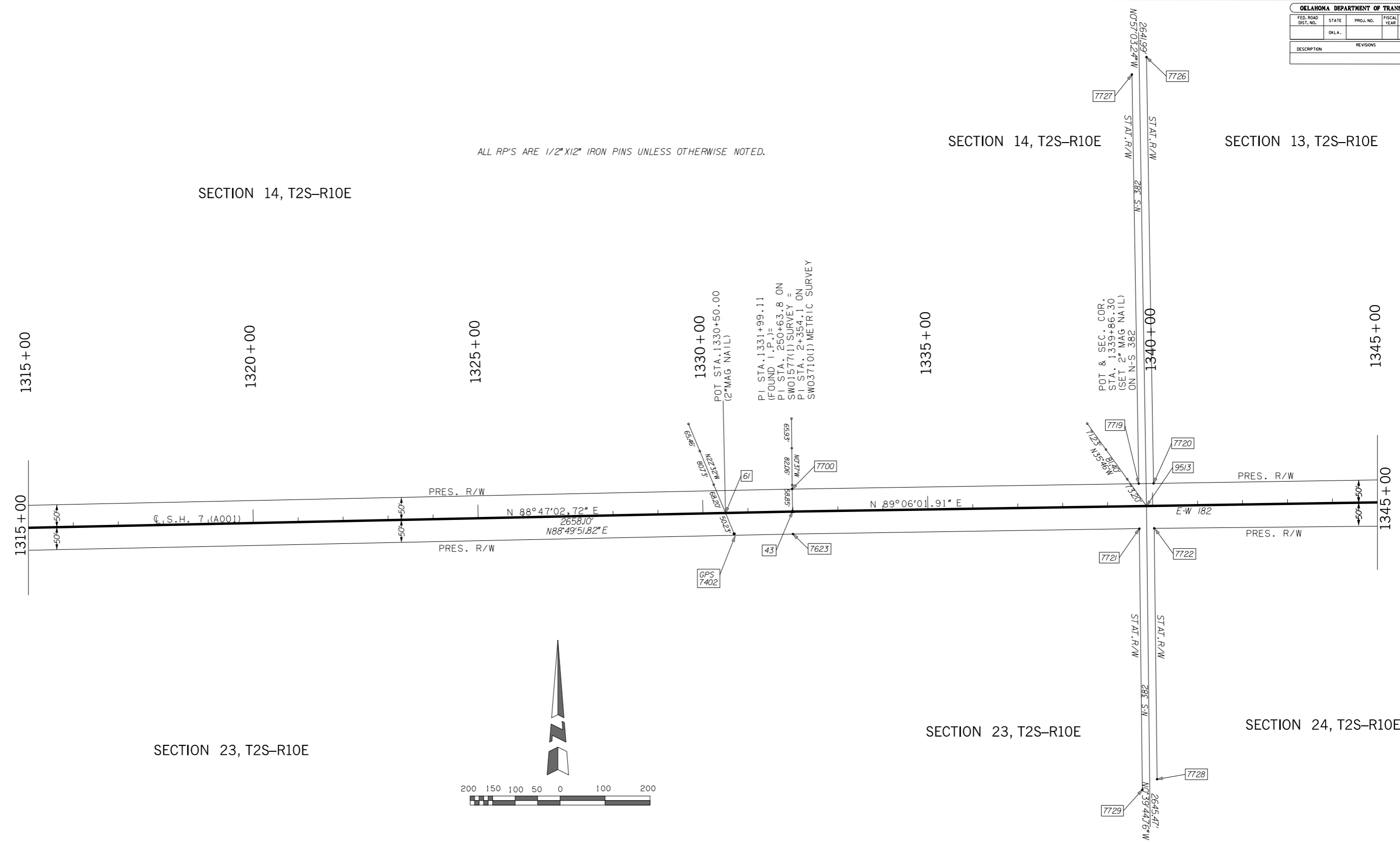
POT STA. 1313+28.20
 ON N-S 381.5
 2.14 S. TO 1/4 COR.
 OR 2639.24 N. TO
 CENT. SEC.

SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SWO 4523 (1)	PROJECT NO. 24066(07) SHEET NO. S027

OKLAHOMA DEPARTMENT OF TRANSPORTATION				
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.
	OKLA.			
DESCRIPTION		REVISIONS		DATE

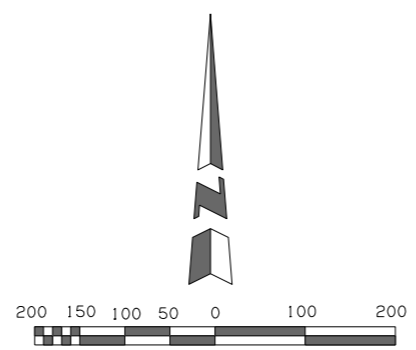
ALL RP'S ARE 1/2" X 1/2" IRON PINS UNLESS OTHERWISE NOTED.



SECTION 23, T2S-R10E

SECTION 23, T2S-R10E

SECTION 24, T2S-R10E



SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SWO 4523 (1)	PROJECT NO. 24066(07) SHEET NO. S028

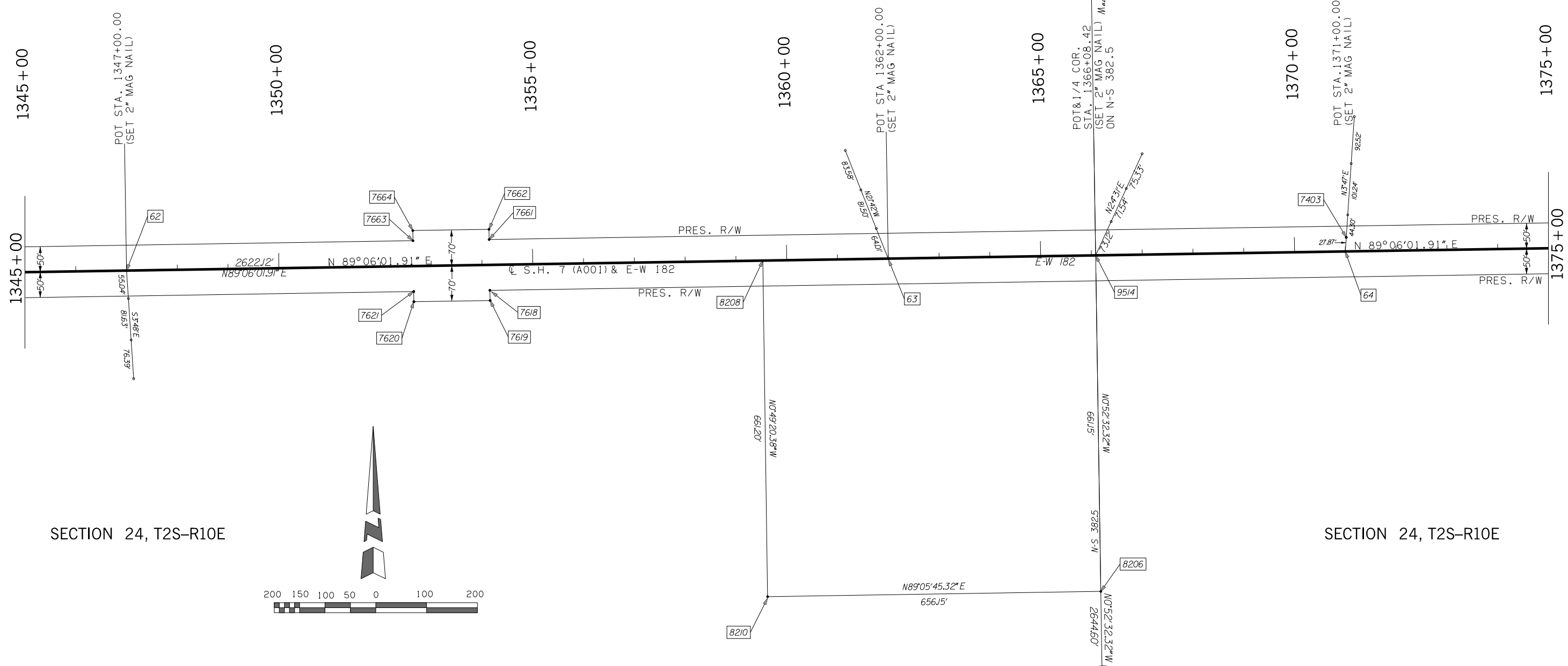
SURVEY DATA SHEET

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS		DATE

SECTION 13, T2S-R10E

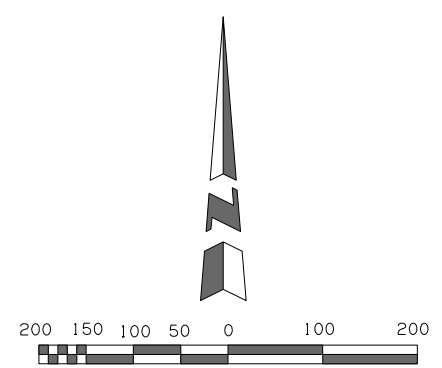
ALL RP'S ARE 1/2" X 1/2" IRON PINS UNLESS OTHERWISE NOTED.

SECTION 13, T2S-R10E



SECTION 24, T2S-R10E

SECTION 24, T2S-R10E

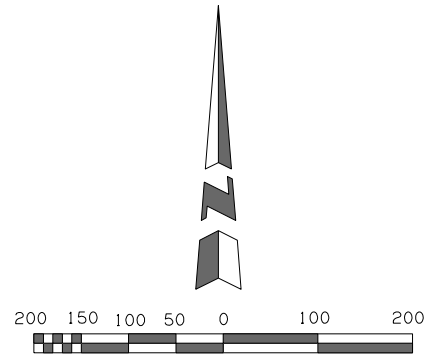
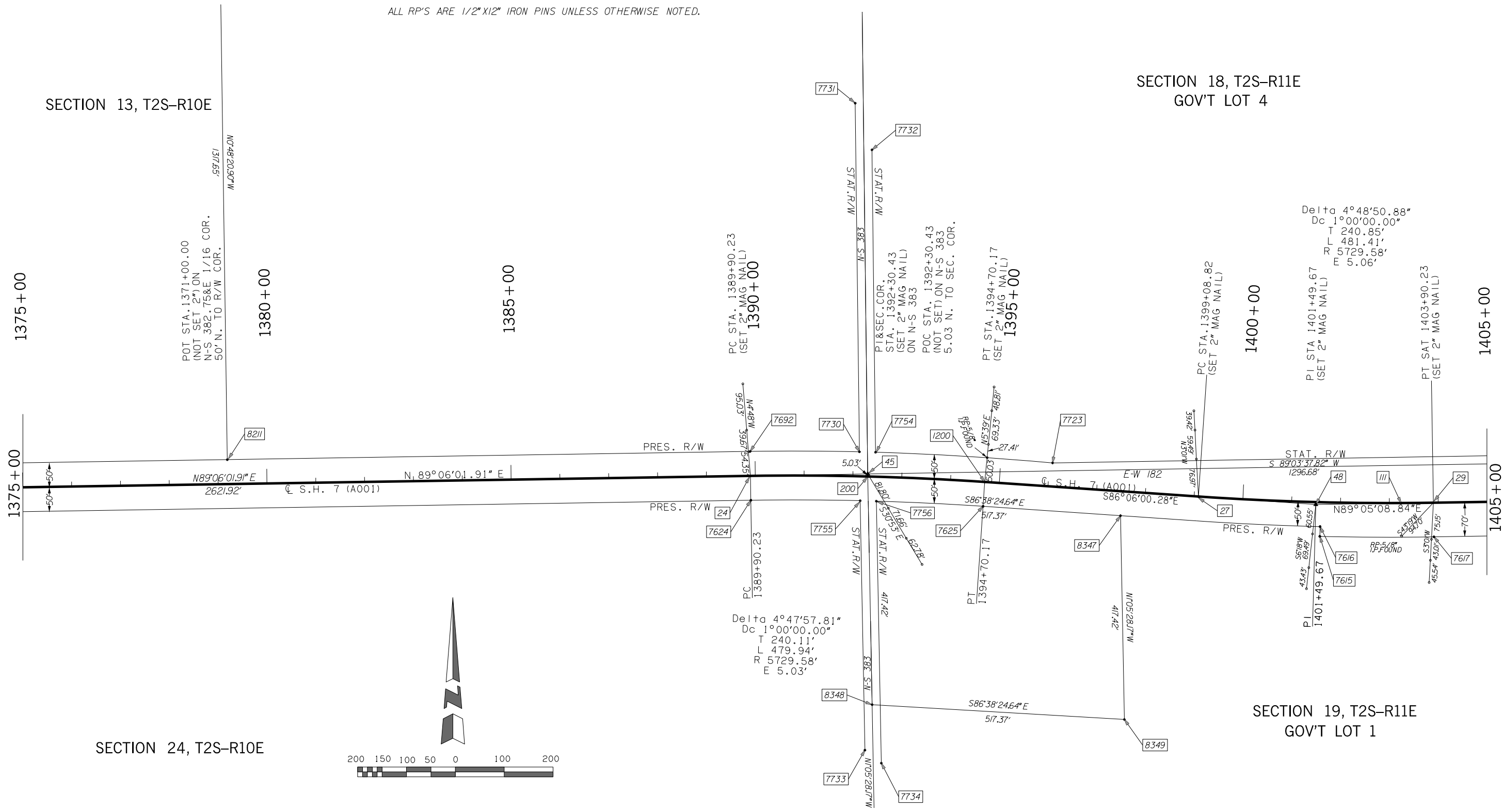


SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SWO 4523 (1)	PROJECT NO. 24066(07) SHEET NO. S029

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS		DATE

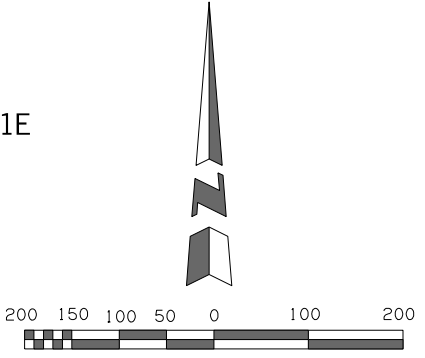
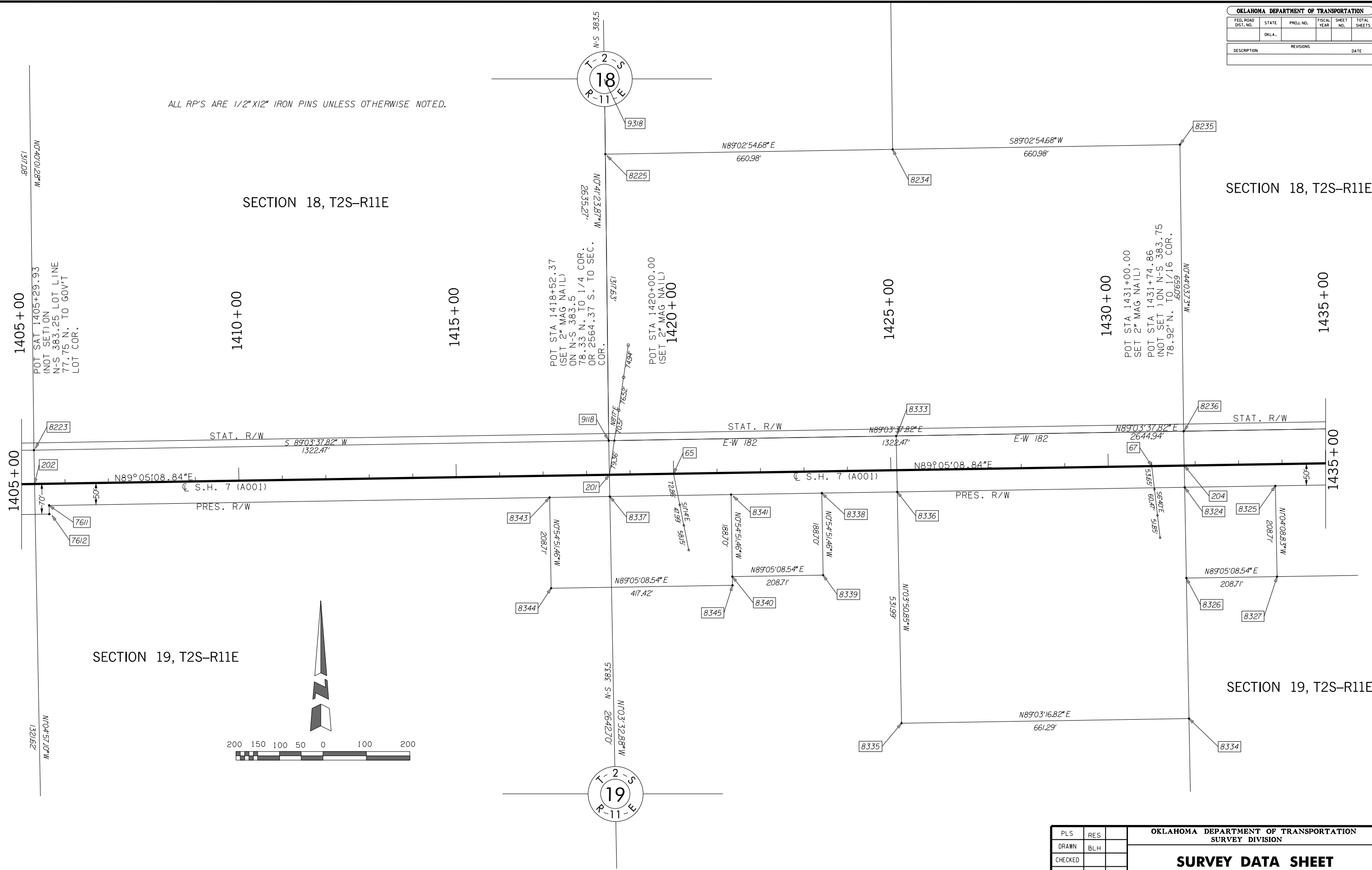
ALL RP'S ARE 1/2" X 12" IRON PINS UNLESS OTHERWISE NOTED.



PLS	RES		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S030
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS		

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS	DATE	

ALL RP'S ARE 1/2" X 1/2" IRON PINS UNLESS OTHERWISE NOTED.



OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SWO 4523 (1)	PROJECT NO. 24066(07) SHEET NO. S031

SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION		REVISIONS		DATE	

ALL RP'S ARE 1/2"X12" IRON PINS UNLESS OTHERWISE NOTED.

SECTION 17, T2S-R11E

SECTION 18, T2S-R11E

1435 + 00

1440 + 00

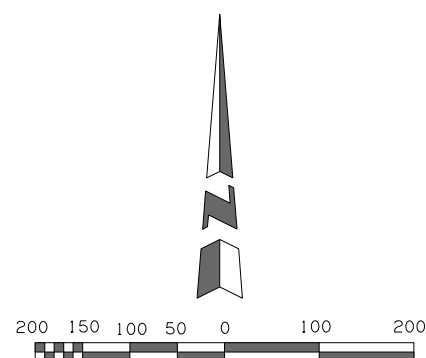
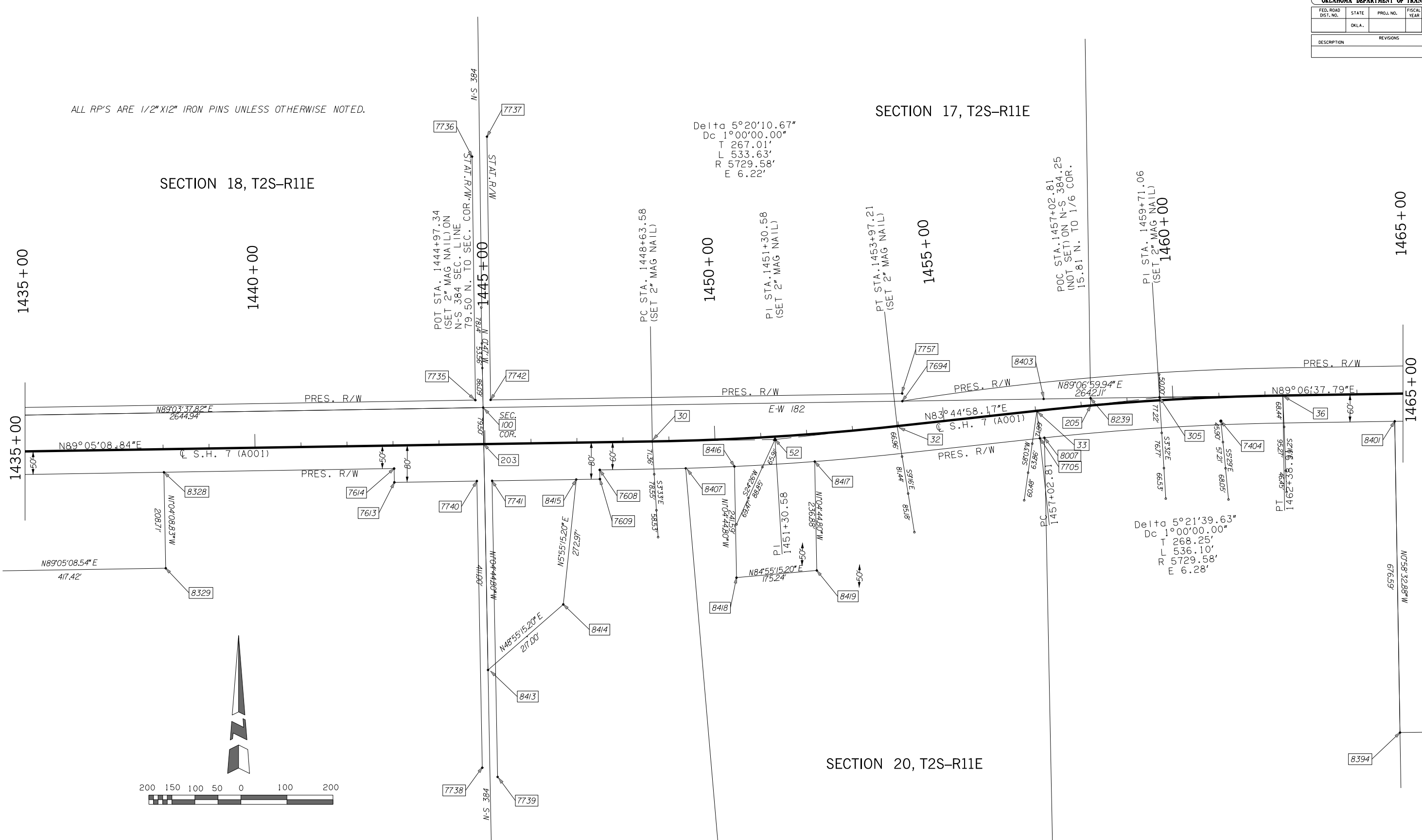
1450 + 00

1455 + 00

1465 + 00

Delta 5°20'10.67"
Dc 1°00'00.00"
T 267.01'
L 533.63'
R 5729.58'
E 6.22'

Delta 5°21'39.63"
Dc 1°00'00.00"
T 268.25'
L 536.10'
R 5729.58'
E 6.28'



SECTION 19, T2S-R11E

SECTION 20, T2S-R11E

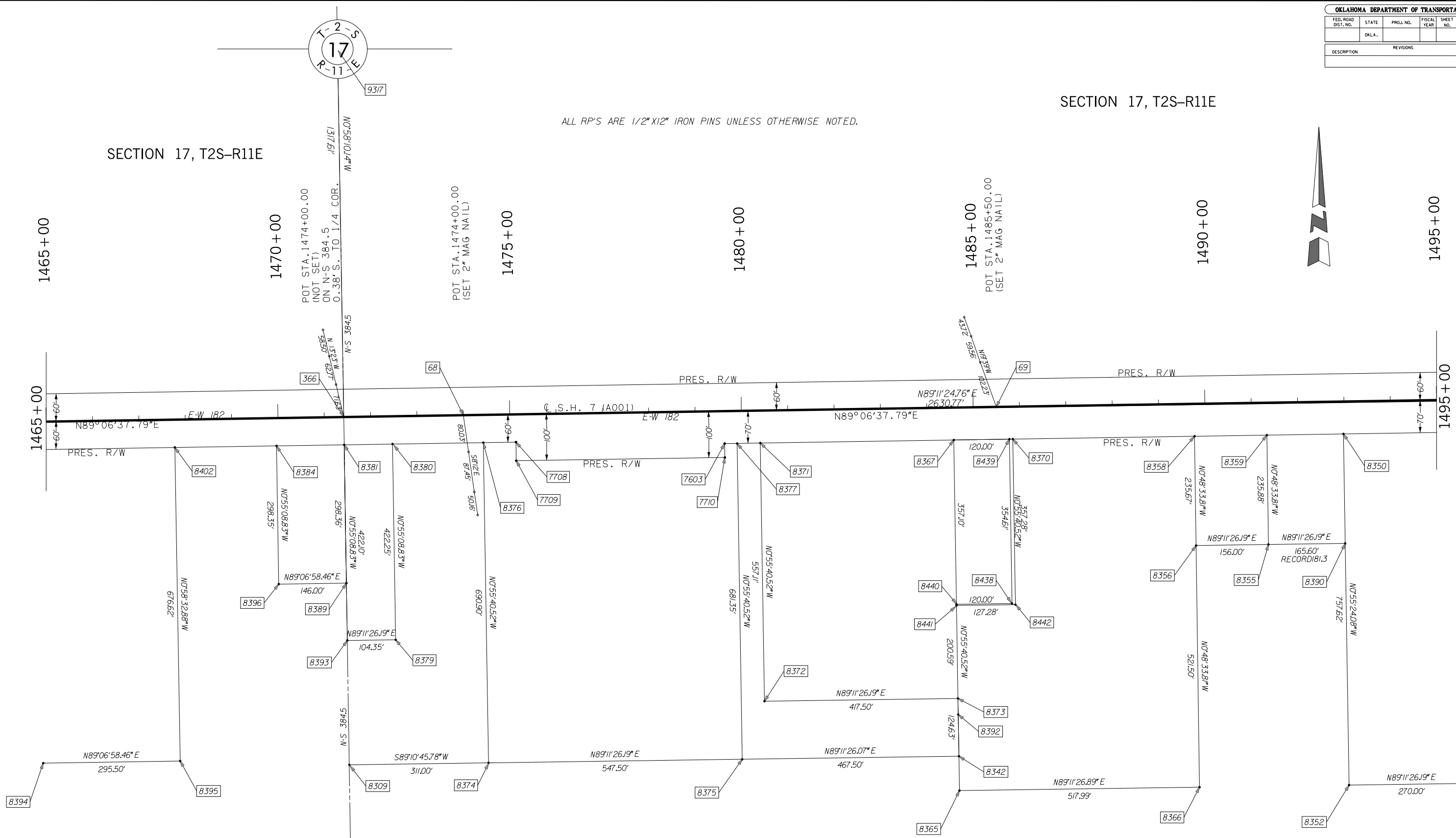
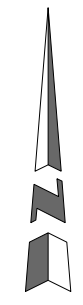
PLS	RES	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
DRAWN	BLH	SURVEY DATA SHEET			
CHECKED					
APPROVED					
CREW	ANTLERS				
SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S032					

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS	DATE	

SECTION 17, T2S-R11E

ALL RP'S ARE 1/2" X 1/2" IRON PINS UNLESS OTHERWISE NOTED.

SECTION 17, T2S-R11E



SECTION 20, T2S-R11E

SECTION 20, T2S-R11E

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS	RES		
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS	SWO 4523 (1)	PROJECT NO. 24066(07) SHEET NO. S033

SURVEY DATA SHEET

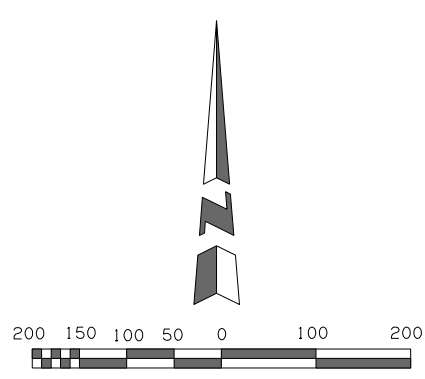
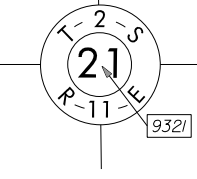
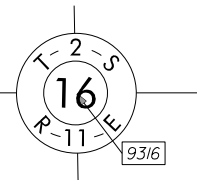
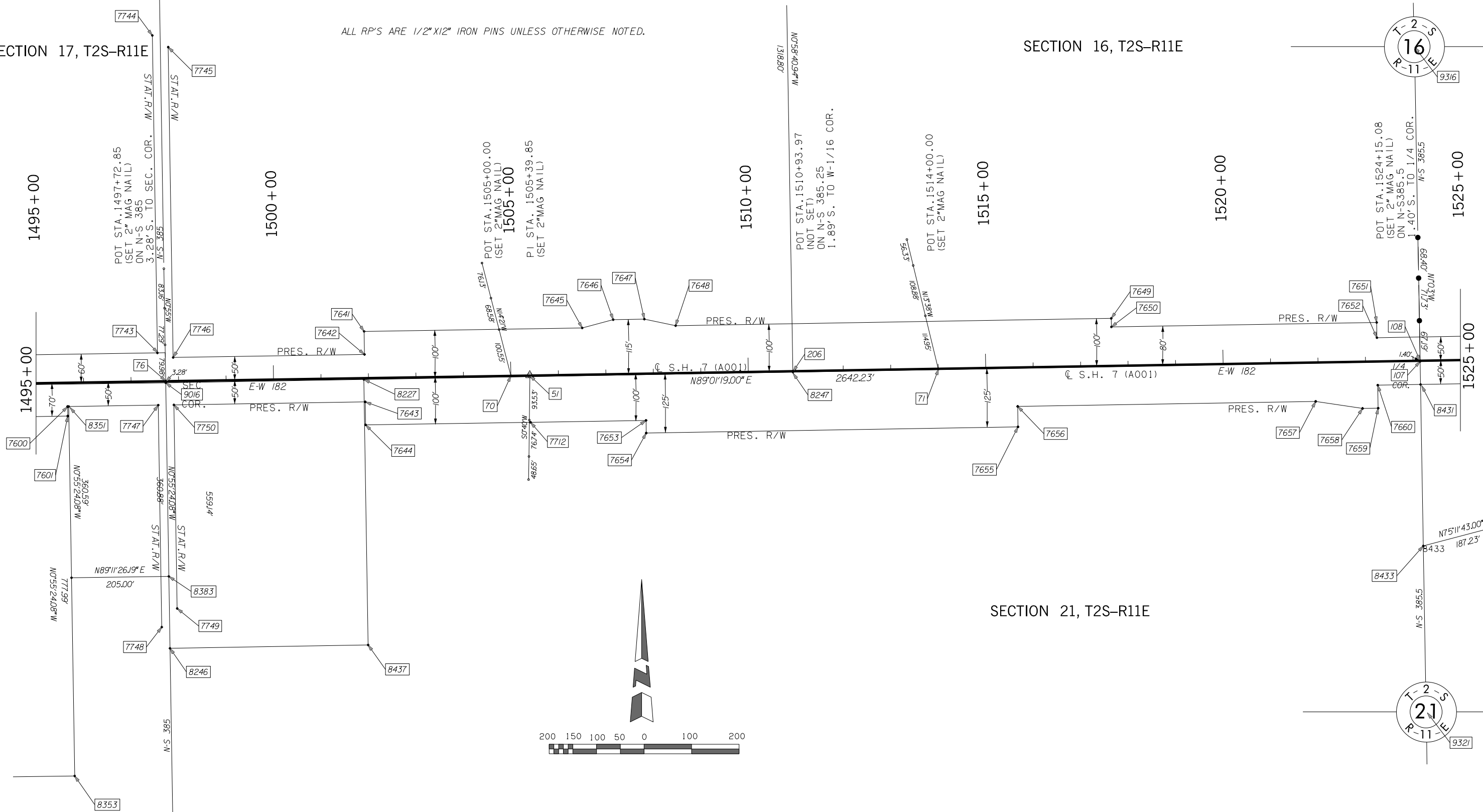
SOS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS		DATE

SECTION 17, T2S-R11E

SECTION 16, T2S-R11E

ALL RP'S ARE 1/2" X 1/2" IRON PINS UNLESS OTHERWISE NOTED.



SECTION 20, T2S-R11E

SECTION 21, T2S-R11E

SDS _____ OF _____

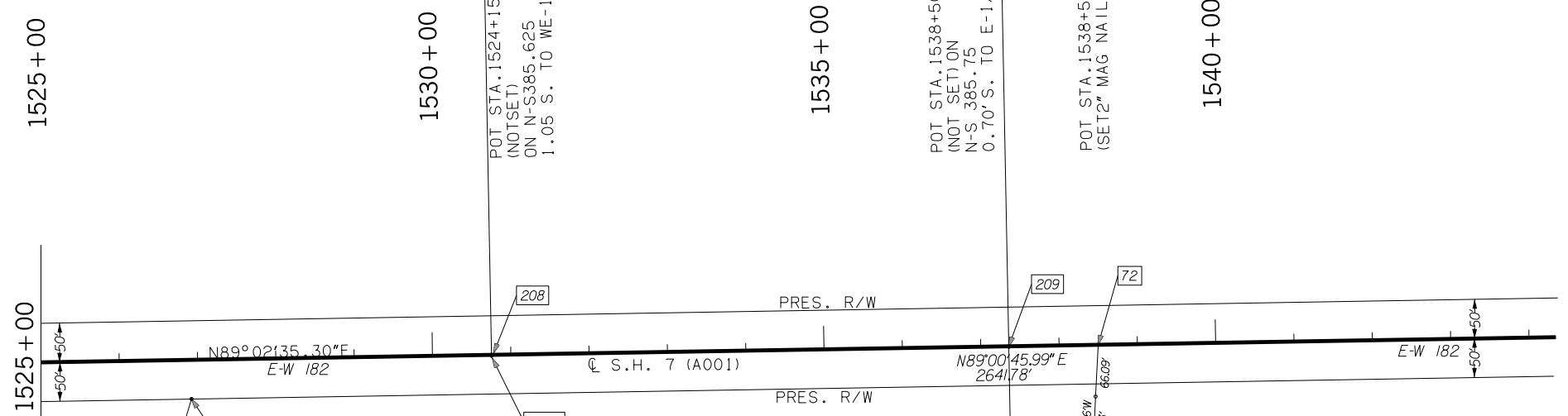
OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	RES
DRAWN	BLH
CHECKED	
APPROVED	
CREW	ANTLERS

SURVEY DATA SHEET

SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S034

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION		REVISIONS		DATE	

SECTION 16, T2S-R11E



ALL RP'S ARE 1/2" X 12" IRON PINS UNLESS OTHERWISE NOTED.

1525 + 00

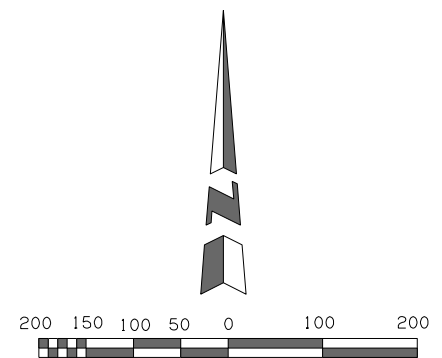
1525 + 00

1530 + 00

1535 + 00

1540 + 00

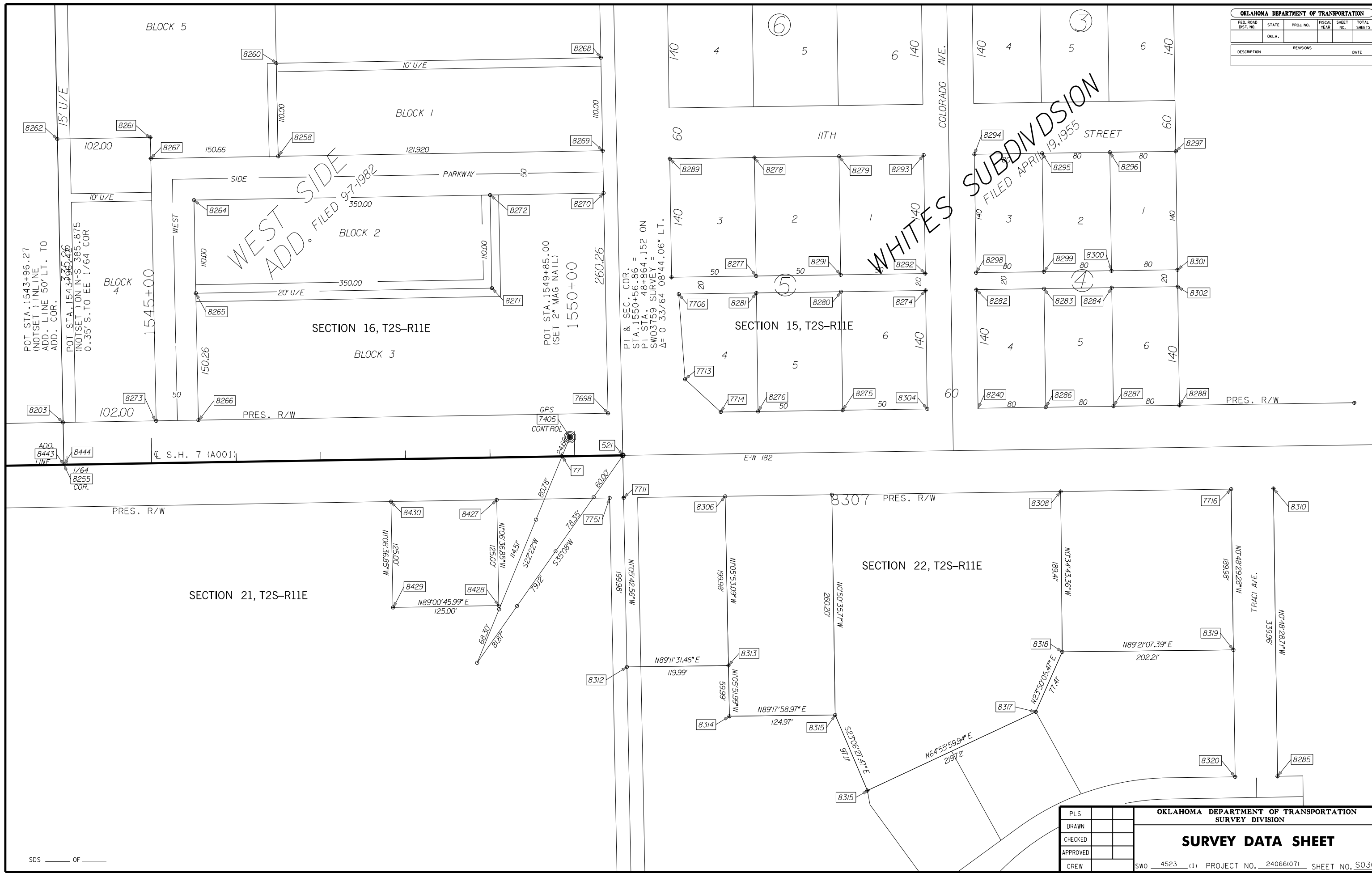
SECTION 21, T2S-R11E



SDS _____ OF _____

PLS	RES		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S035
DRAWN	BLH		
CHECKED			
APPROVED			
CREW	ANTLERS		

OKLAHOMA DEPARTMENT OF TRANSPORTATION						
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS	
	OKLA.					
DESCRIPTION					REVISIONS	DATE



PLS				OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S036
DRAWN				
CHECKED				
APPROVED				
CREW				

SDS _____ OF _____

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION				REVISIONS	DATE

SECTION CORNER
ODOT MON. NO.
A-3-842
Found 1/2" Iron Pin with ID Cap stamped PAGE LS 1324, agrees with I.P. found 0.5 mi. west and I.P. found 1.5 mi. east, and ODOT Cor. 1 mi. south, used as found.

1/4 CORNER
ODOT MON. NO.
A-3-880
SET 1/2" IRON PIN W/ PLASTIC ID CAP STAMPED ODOT PLS 1198 AT MIDPOINT BETWEEN PREVIOUSLY ESTABLISHED SECTION CORNERS EAST AND WEST.

SECTION CORNER
ODOT MON. NO.
A-3-881
THIS CORNER ESTABLISHED USING OLD POST IN PLACE IN CENTER N-S LANE FITS IP. 1/2 MI. EAST AND STONE 1 MI. EAST AND IP IN FENCE 1/2 MI. W AND I.P. 1 MI. WEST AND 1/4 FENCE 1/2 MI. SOUTH AND PLANS SEC. COR. 1 MI. SOUTH AND GOOD 1/4 FENCE 1 MI. NORTH.

1/4 CORNER
ODOT MON. NO.
A-3-882
FOUND 1/2" I.P. W/ ID CAP INPLACE, THIS COR. FILED BY LS 1324 IN 1994 FITS TOPO AGREES W/ IDENTIFIED EXISTING CORNERS IN THE AREA, USED AS FOUND.

SECTION CORNER
ODOT MON. NO.
A-3-883
ORIGINAL STONE FOUND AND FILED BY LS 1324 IN 1994 NOW OBLITERATED BY ROCK CAIRN, SET 1/2" IRON PIN W/ PLASTIC ID CAP STAMPED ODOT PLS 1198 IN TOP OF CAIRN FROM 2 REMAINING RPS.

1/4 CORNER
ODOT MON. NO.
A-3-884
FOUND 1/2" IRON PIN FILED BY LS1324 IN 1994 REFILED BY LS1467 IN 2006 AGREES W/ EXISTING IDENTIFIED CORNERS IN THE AREA, USED AS FOUND.

SECTION CORNER
ODOT MON. NO.
A-3-889
SET 1/2" IRON PIN W/ PLASTIC ID CAP STAMPED ODOT PLS 1198 THIS CORNER SET FROM TIES ON SAP 1043 PLANS USING ORIG. RPS FOUND IN THE AREA AGREES W/ IDENTIFIED EXISTING CORNERS IN THE AREA, DETERMINED TO BE THE BEST AVAILABLE EVIDENCE OF THE ORIGINAL POSITION.

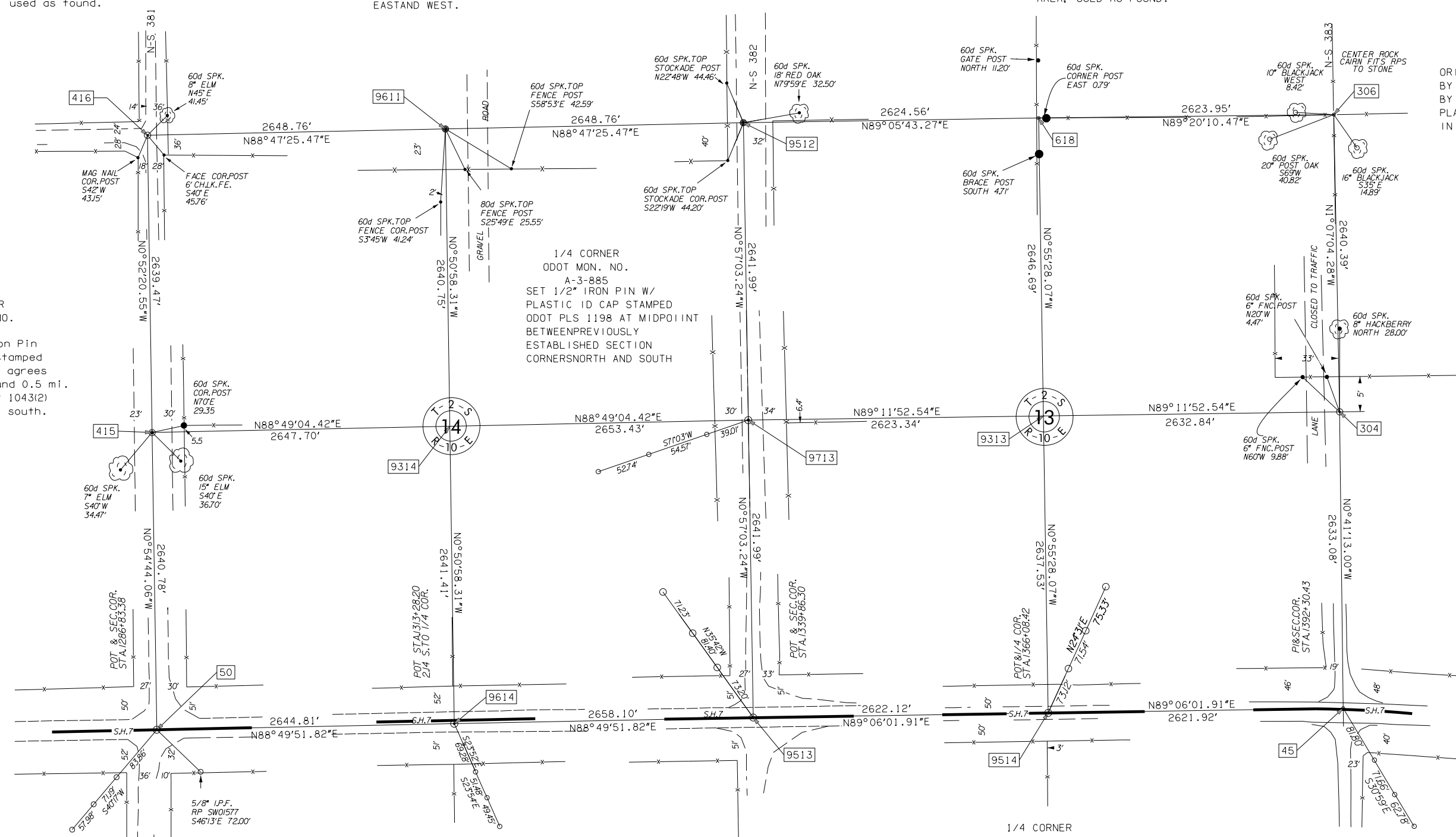
1/4 CORNER
ODOT MON. NO.
A-3-843
Found 1/2" Iron Pin with ID Cap stamped PAGE LS 1324, agrees with I.P. found 0.5 mi. north and SAP 1043(2) Plans 0.5 mi. south, used as found

SECTION CORNER
ODOT MON. NO.
A-3-856
SET 1/2" IRON PIN W/ PLASTIC ID CAP STAMPED ODOT PLS 1198 THIS CORNER ESTABLISHED FROM SAP 1043 PLANS, AGREES W/ IDENTIFIED EXISTING CORNERS IN THE AREA.

1/4 CORNER
ODOT MON. NO.
A-3-886
SET 2" MAG NAIL W/WASHER STAMPED ODOT PLS 1198 THIS CORNER ESTABLISHED FROM SAP 1043 PLANS, AGREES W/ IDENTIFIED EXISTING CORNERS IN THE AREA.

SECTION CORNER
ODOT MON. NO.
A-3-887
SET 2" MAG NAIL W/WASHER STAMPED ODOT PLS 1198 THIS CORNER ESTABLISHED FROM SAP 1043 PLANS, AGREES W/ IDENTIFIED EXISTING CORNERS IN THE AREA.

1/4 CORNER
ODOT MON. NO.
A-3-888
SET 2" MAG NAIL W/WASHER STAMPED ODOT PLS 1198 THIS CORNER ESTABLISHED FROM SAP 1043 PLANS, AGREES W/ IDENTIFIED EXISTING CORNERS IN THE AREA.



OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
PLS			
DRAWN			
CHECKED			
APPROVED			
CREW			

SURVEY DATA SHEET

SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S037

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				

DESCRIPTION	REVISIONS	DATE

SECTION CORNER
ODOT MON. NO.
A-3-856
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198
THIS CORNER ESTABLISHED
FROM SAP 1043 PLANS, AGREES
W/ IDENTIFIED EXISTING
CORNERS IN THE AREA.

1/4 CORNER
ODOT MON. NO.
A-3-886
SET 2" MAG NAIL W/WASHER STAMPED
ODOT PLS 1198
THIS CORNER ESTABLISHED
FROM SAP 1043 PLANS, AGREES
W/ IDENTIFIED EXISTING
CORNERS IN THE AREA.

SECTION CORNER
ODOT MON. NO.
A-3-887
SET 2" MAG NAIL W/WASHER
STAMPED ODOT PLS 1198
THIS CORNER ESTABLISHED
FROM SAP 1043 PLANS, AGREES
W/ IDENTIFIED EXISTING
CORNERS IN THE AREA.

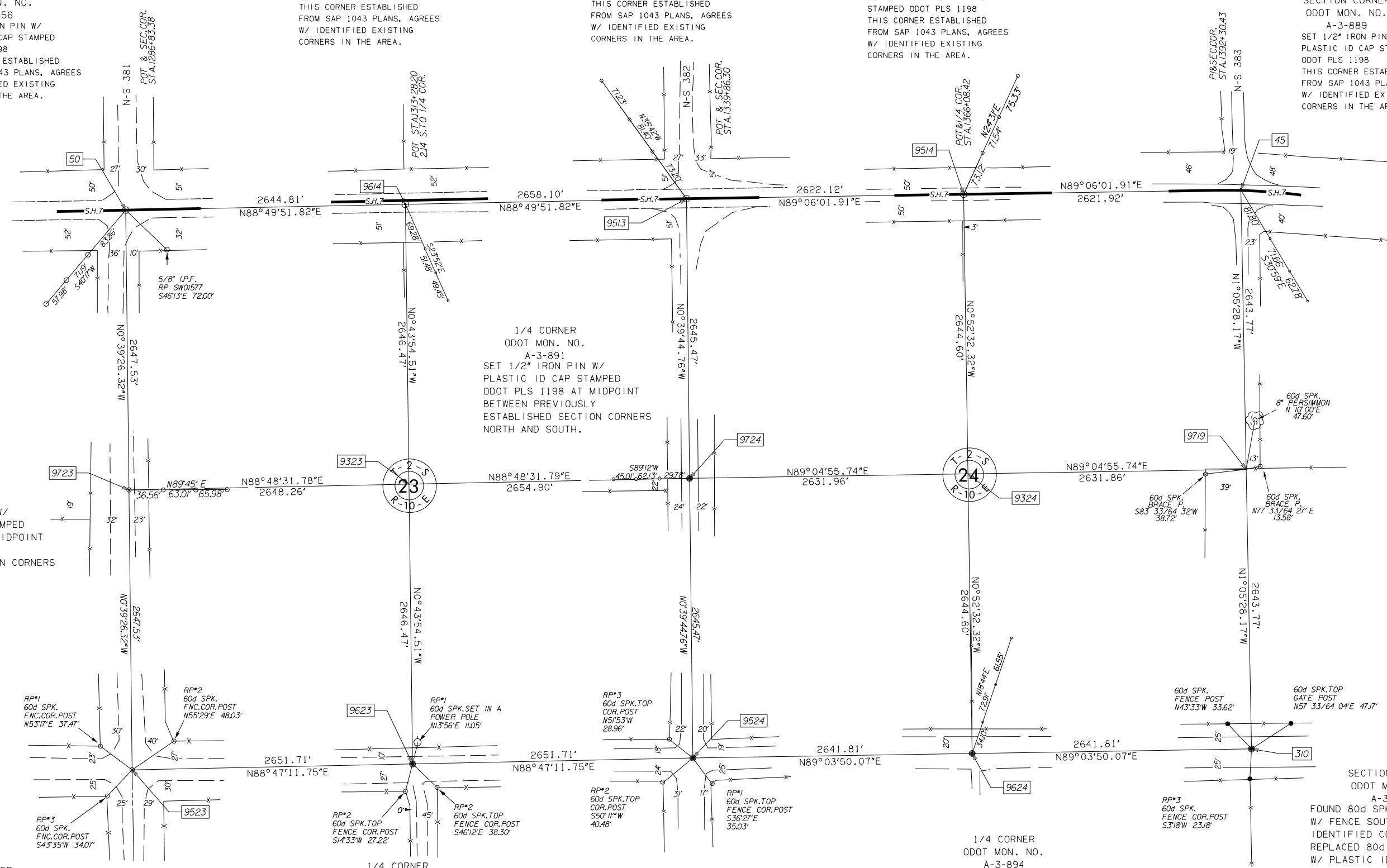
1/4 CORNER
ODOT MON. NO.
A-3-888
SET 2" MAG NAIL W/WASHER
STAMPED ODOT PLS 1198
THIS CORNER ESTABLISHED
FROM SAP 1043 PLANS, AGREES
W/ IDENTIFIED EXISTING
CORNERS IN THE AREA.

SECTION CORNER
ODOT MON. NO.
A-3-889
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198
THIS CORNER ESTABLISHED
FROM SAP 1043 PLANS, AGREES
W/ IDENTIFIED EXISTING
CORNERS IN THE AREA.

1/4 CORNER
ODOT MON. NO.
A-3-891
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198 AT MIDPOINT
BETWEEN PREVIOUSLY
ESTABLISHED SECTION CORNERS
NORTH AND SOUTH.

1/4 CORNER
ODOT MON. NO.
A-3-890
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198 AT MIDPOINT
BETWEEN PREVIOUSLY
ESTABLISHED SECTION CORNERS
NORTH AND SOUTH.

1/4 CORNER
ODOT MON. NO.
A-3-857
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198 AT MIDPOINT
BETWEEN PREVIOUSLY
ESTABLISHED SECTION CORNERS
NORTH AND SOUTH.



SECTION CORNER
ODOT MON. NO.
A-3-869
SET 1/2" IRON PIN W/ PLASTIC ID CAP STAMPED
ODOT PLS 1198 PROPORTIONATE DIST. FROM
1/2" I.P. 1 MI. SOUTH AND PLANS CORNER 1 MI.
NORTH AND 3/8" I.P. 1 MI. WEST AND 80d SPK.
2 MI. EAST. FITS TOPO AND GOV'T RECORD

1/4 CORNER
ODOT MON. NO.
A-3-892
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198 AT MIDPOINT
BETWEEN PREVIOUSLY
ESTABLISHED SECTION CORNERS
EAST AND WEST.

SECTION CORNER
ODOT MON. NO.
A-3-893
SET 1/2" IRON PIN W/ PLASTIC ID CAP STAMPED
ODOT PLS 1198 PROPORTIONATE DIST. FROM
TWNSHIP LINE, 1 MI. SOUTH AND PLANS CORNER 1 MI.
NORTH AND 3/8" I.P. 2 MI. WEST AND 80d SPK.
1 MI. EAST. FITS TOPO AND GOV'T RECORD

1/4 CORNER
ODOT MON. NO.
A-3-894
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198 AT MIDPOINT
BETWEEN PREVIOUSLY
ESTABLISHED SECTION CORNERS
EAST AND WEST.

SECTION CORNER
ODOT MON. NO.
A-3-895
FOUND 80d SPK. INPLACE IN LINE
W/ FENCE SOUTH AGREES W/
IDENTIFIED CORNERS IN THE AREA
REPLACED 80d SPK W/ 1/2" IRON PIN
W/ PLASTIC ID CAP STAMPED ODOT
PLS 1198.

PLS	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	SURVEY DATA SHEET
CHECKED	
APPROVED	
CREW	

SECTION CORNER
ODOT MON. NO.
A-3-883
ORIGINAL STONE FOUND AND FILED
BY LS 1324 IN 1994 NOW OBLITERATED
BY ROCK CAIRN, SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED ODOT PLS 1198
IN TOP OF CAIRN FROM 2 REMAINING RPS.

1/4 CORNER
ODOT MON. NO.
A-3-896
FOUND ORIG. STONE INPLACE
LOOSE IN GROUND, THIS POSITION
AGREES W/ EXISTING IDENTIFIED
CORNERS IN THE AREA, SET IN GROUND
AT SAME POSITION AND USED.

SECTION CORNER
ODOT MON. NO.
A-3-897
FOUND 1/2" IRON PIN
INPLACE, FILED BY LS 1324
AGREES W/ EXISTING IDENTIFIED
CORNERS IN THE AREA, USED BY
LOCAL SURVEYORS, USED AS FOUND.

1/4 CORNER
ODOT MON. NO.
A-3-898
FOUND 1/2" IRON PIN IN PLACE AGREES
W/ EXISTING IDENTIFIED CORNERS IN THE AREA,
PLACED ID CAP STAMPED ODOT PLS 1198 ON I.P.

SECTION CORNER
ODOT MON. NO.
A-3-600
FOUND 1/2" IRON PIN INPLACE
THIS CORNER ESTABLISHED
1965 UNDER SW02683 SURVEY
FILED IN 1980 BY O.R. SISK
REFILED 1993 BY J.L. PERRY
USED AS FOUND.
FD. 1" PIPE 2.5' N. & 3.5' W.

1/4 CORNER
ODOT MON. NO.
A-3-599
FOUND ODOT STD.
FENO MARKER INPLACE
THIS CORNER CORRECTED
UNDER SW03710(1) USED
AS FOUND.

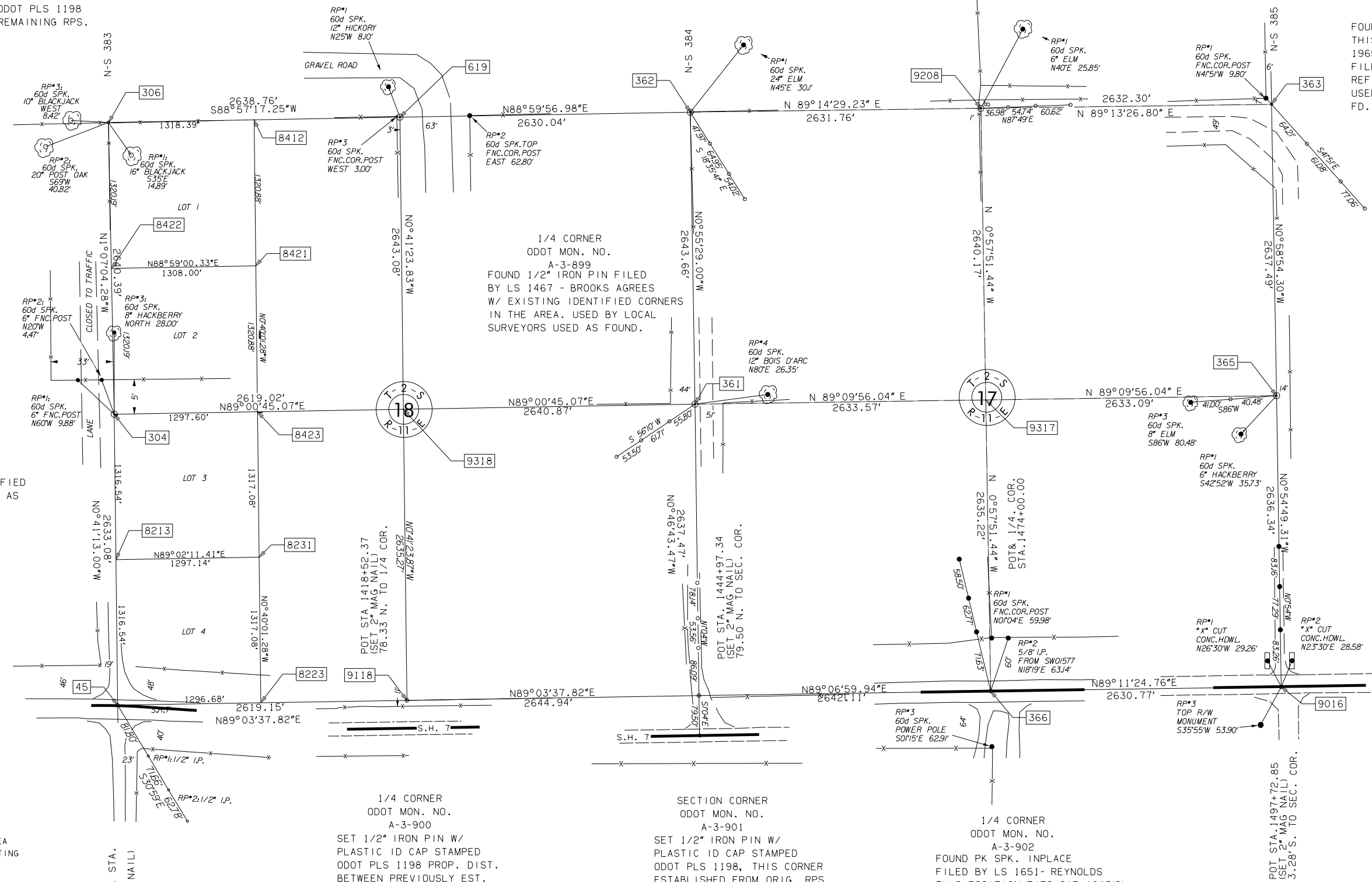
SECTION CORNER
ODOT MON. NO.
A-3-598
FOUND PK SPK. INPLACE
FILED BY LS 1651 - REYNOLDS.
THIS POSITION AGREES W/CORNER
ESTABLISHED UNDER SW03445(1)
REPLACED PK SPK. W/2" MAG NAIL
& WASHER STAMPED ODOT PLS 1198.

1/4 CORNER
ODOT MON. NO.
A-3-884
FOUND 1/2" IRON PIN
FILED BY LS1324 IN 1994
REFILED BY LS1467 IN 2006
AGREES W/ EXISTING IDENTIFIED
CORNERS IN THE AREA, USED AS
FOUND.

1/4 CORNER
ODOT MON. NO.
A-3-900
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198 PROP. DIST.
BETWEEN PREVIOUSLY EST.
SECTION CORNERS EAST AND WEST.
FOUND I.P. FILED BY LS 1593
6' SOUTH & 10' EAST.

SECTION CORNER
ODOT MON. NO.
A-3-901
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198, THIS CORNER
ESTABLISHED FROM ORIG. RPS
ON SAP 1043(2) PLANS.
FOUND I.P. FILED BY LS 1593
6' SOUTH & 7' EAST.

1/4 CORNER
ODOT MON. NO.
A-3-902
FOUND PK SPK. INPLACE
FILED BY LS 1651- REYNOLDS
THIS POSITION FITS SAP 1043(2)
PLANS, REPLACED PK SPK. W/
2" MAG NAIL & WASHER STAMPED
ODOT PLS 1198



SECTION CORNER
ODOT MON. NO.
A-3-889
SET 2" MAG NAIL W/WASHER
STAMPED ODOT PLS 1198
THIS CORNER SET FROM TIES
ON SAP 1043 PLANS USING
ORIG. RPS FOUND IN THE AREA
AGREES W/ IDENTIFIED EXISTING
CORNERS IN THE AREA.
DETERMINED TO BE THE
BEST AVAILABLE EVIDENCE
OF THE ORIGINAL POSITION.
CORNER SET BY LS1324 IN
1998 NOT FOUND.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	
DRAWN	
CHECKED	
APPROVED	
CREW	

SURVEY DATA SHEET

SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S039

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS	DATE	

SECTION CORNER
ODOT MON. NO.
A-3-889

SET 2" MAG NAIL W/WASHER
STAMPED ODOT PLS 1198
THIS CORNER SET FROM TIES
ON SAP 1043 PLANS USING
ORIG. RPS FOUND IN THE AREA
AGREES W/ IDENTIFIED EXISTING
CORNERS IN THE AREA.
DETERMINED TO BE THE
BEST AVAILABLE EVIDENCE
OF THE ORIGINAL POSITION.
CORNER SET BY LS1324 IN
1998 NOT FOUND.

1/4 CORNER
ODOT MON. NO.

A-3-900
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198 PROP. DIST.
BETWEEN PREVIOUSLY EST.
SECTION CORNERS EAST AND WEST.
FOUND I.P. FILED BY LS 1593
6' SOUTH & 10' EAST.

SECTION CORNER
ODOT MON. NO.

A-3-901
SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198, THIS CORNER
ESTABLISHED FROM ORIG. RPS
ON SAP 1043(2) PLANS.
FOUND I.P. FILED BY LS 1593
6' SOUTH & 7' EAST.

1/4 CORNER
ODOT MON. NO.

A-3-902
FOUND PK SPK. INPLACE
FILED BY LS 1651- REYNOLDS
THIS POSITION FITS SAP 1043(2)
PLANS, REPLACED PK SPK. W/
2" MAG NAIL & WASHER STAMPED
ODOT PLS 1198

SECTION CORNER
ODOT MON. NO.

A-3-598
FOUND PK SPK. INPLACE
FILED BY LS 1651 - REYNOLDS.
THIS POSITION AGREES W/CORNER
ESTABLISHED UNDER SW03445(1)
REPLACED PK SPK. W/2" MAG NAIL
& WASHER STAMPED ODOT PLS 1198.

1/4 CORNER
ODOT MON. NO.

A-3-651
THIS CORNER ESTABLISHED UNDER SW03710(1)
FOUND ALUM. CAP INPLACE
FILED BY LS 1651 - REYNOLDS
FITS ODOT RPS USED AS FOUND.

SECTION CORNER
ODOT MON. NO.

A-3-497
FOUND ODOT STD. FENO MARKER
ESTABLISHED UNDER SW03710
USED AS FOUND.

1/4 CORNER
ODOT MON. NO.
A-3-890

SET 1/2" IRON PIN W/
PLASTIC ID CAP STAMPED
ODOT PLS 1198 AT MIDPOINT
BETWEEN PREVIOUSLY EST.
SECTION CORNERS NORTH AND
SOUTH, AGREES W/ IDENTIFIED
EXISTING CORNERS IN THE AREA,
FITS TOPO.

1/4 CORNER
ODOT MON. NO.

A-3-903
FOUND 1/2" IRON PIN INPLACE
FILED BY LS 1061 -JEFFERIES
AGREES W/ EXISTING IDENTIFIED
CORNERS IN THE AREA, USED AS
FOUND.

20

SECTION CORNER
ODOT MON. NO.
A-3-895

FOUND 80d SPK. INPLACE
FILED BY LS1061-JEFFERIES
FITS TOPO, USED BY LOCAL
SURVEYORS REPLACED 80d
W/ 1/2" IRON PIN & PLASTIC
ID CAP STAMPED ODOT PLS 1198

1/4 CORNER
ODOT MON. NO.

A-3-904
FOUND 1/2" IRON PIN INPLACE
FILED BY LS 1324-PAGE
AGREES W/ EXISTING IDENTIFIED
CORNERS IN THE AREA, USED AS
FOUND.

SECTION CORNER
ODOT MON. NO.

A-3-905
FOUND 1/2" IRON PIN INPLACE
FILED BY LS1061 -JEFFERIES
AGREES W/ EXISTING IDENTIFIED
CORNERS IN THE AREA, USED AS
FOUND.

1/4 CORNER
ODOT MON. NO.

A-3-906
FOUND 1/2" IRON PIN AT MIDPOINT
BETWEEN PREVIOUSLY
ESTABLISHED SECTION CORNERS EASTAND WEST, AGREES W/
IDENTIFIEDEXISTING CORNERS IN THE AREA, FITS TOPO.
PLACED PLASTIC ID CAP STAMPED ODOT PLS 1198 ON IRON
PIN FOUND.
FOUND 80d SPK. 17.5' SOUTH
FILED BY LS 956- KEY BRO.

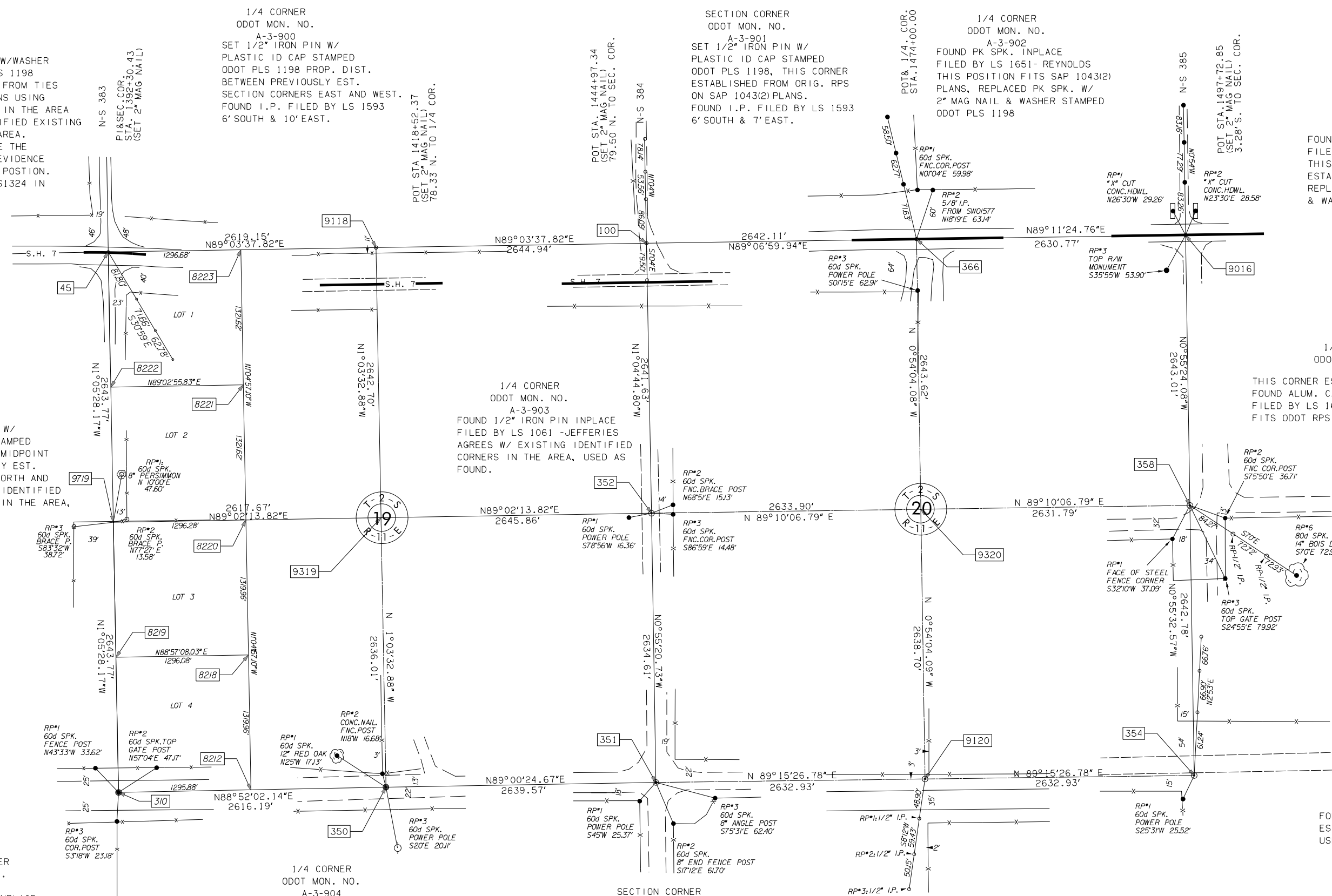
PLS	
DRAWN	
CHECKED	
APPROVED	
CREW	

OKLAHOMA DEPARTMENT OF TRANSPORTATION
SURVEY DIVISION

SURVEY DATA SHEET

SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S040

SDS _____ OF _____



SECTION CORNER
 ODOT MON. NO.
 A-3-600
 FOUND 1/2" IRON PIN INPLACE
 THIS CORNER ESTABLISHED
 1965 UNDER SW02683 SURVEY
 FILED IN 1980 BY O.R. SISK
 REFILED 1993 BY J.L. PERRY
 USED AS FOUND.
 FD. 1" PIPE 2.5' N. & 3.5' W.

1/4 CORNER
 ODOT MON. NO.
 A-3-601
 FOUND 1/2" IRON PIN INPLACE
 NO CAP, FILED BY:
 SISK - 1980
 PERRY - 1993
 PAGE SR. 2002
 MARQUARDT 05
 PLACED PLASTIC ID CAP ON
 I.P. USED AS FOUND.

1/4 CORNER
 ODOT MON. NO.
 A-3-602
 FOUND CONC. NAIL WITH BRASS TAG
 STAMPED PLS. 1566 INPLACE
 THIS CORNER ESTABLISHED BY ODOT
 AGREES W/ ODOT RPS
 USED AS FOUND.

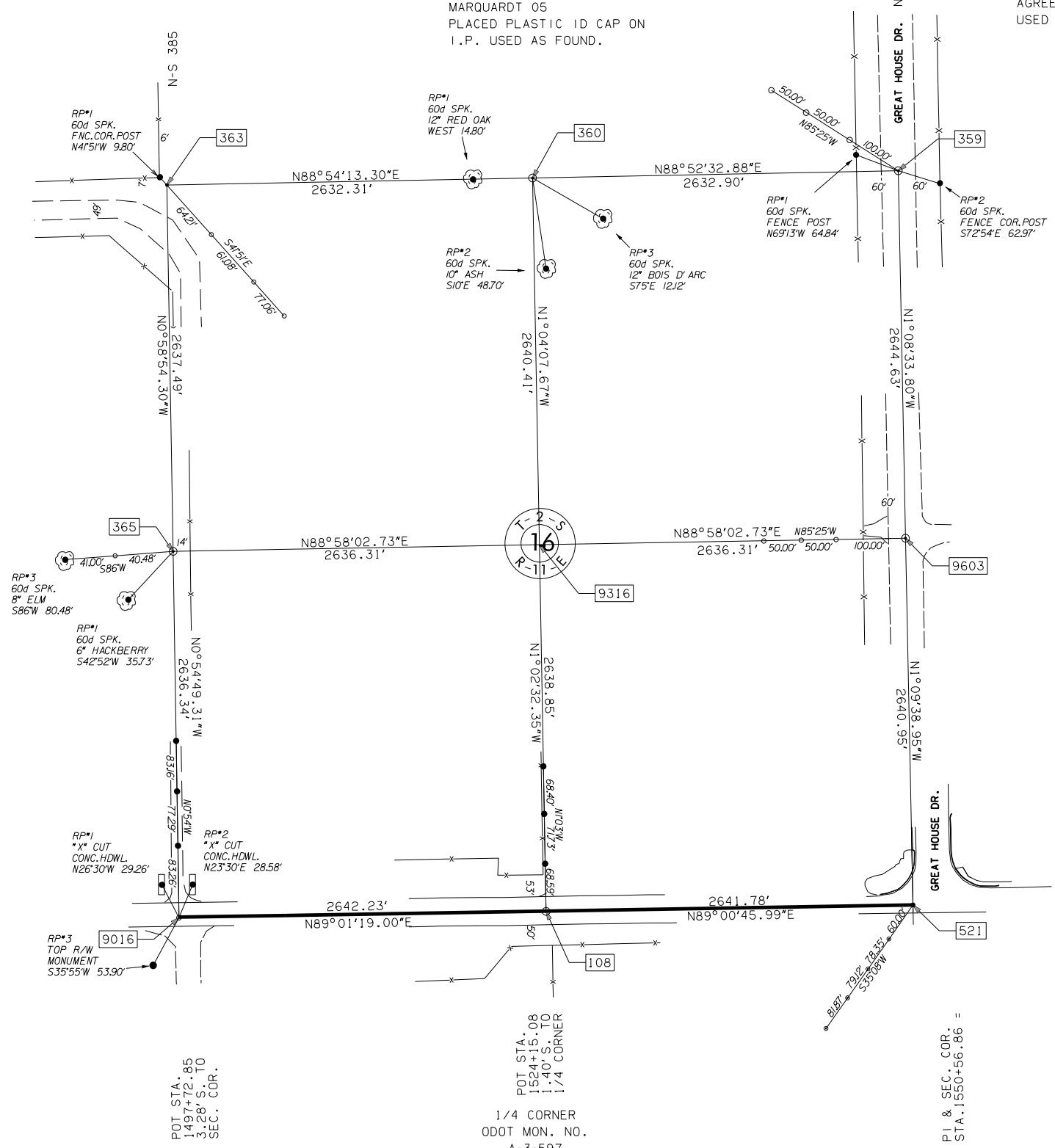
1/4 CORNER
 ODOT MON. NO.
 A-3-599
 FOUND ODOT STD.
 FENO MARKER INPLACE
 THIS CORNER CORRECTED
 UNDER SW03710(1) USED
 AS FOUND.

1/4 CORNER
 ODOT MON. NO.
 A-3-603
 SET 2" MAG NAIL AND
 WASHER STAMPED
 ODOT PLS 1198, FROM
 EXIST ODOT RPS.

SECTION CORNER
 ODOT MON. NO.
 A-3-598
 FOUND PK SPK. INPLACE
 FILED BY LS 1651 - REYNOLDS.
 THIS POSITION AGREES W/CORNER
 ESTABLISHED UNDER SW03445(1)
 REPLACED PK SPK. W/2" MAG NAIL
 & WASHER STAMPED ODOT PLS 1198.

1/4 CORNER
 ODOT MON. NO.
 A-3-597
 THIS CORNER ESTABLISHED UNDER SW03445(1)
 SET 2" MAG NAIL AND WASHER STAMPED ODOT
 PLS 1198 FROM RPS IN SW03710(1) SURVEY

SECTION CORNER
 ODOT MON. NO.
 A-3-20

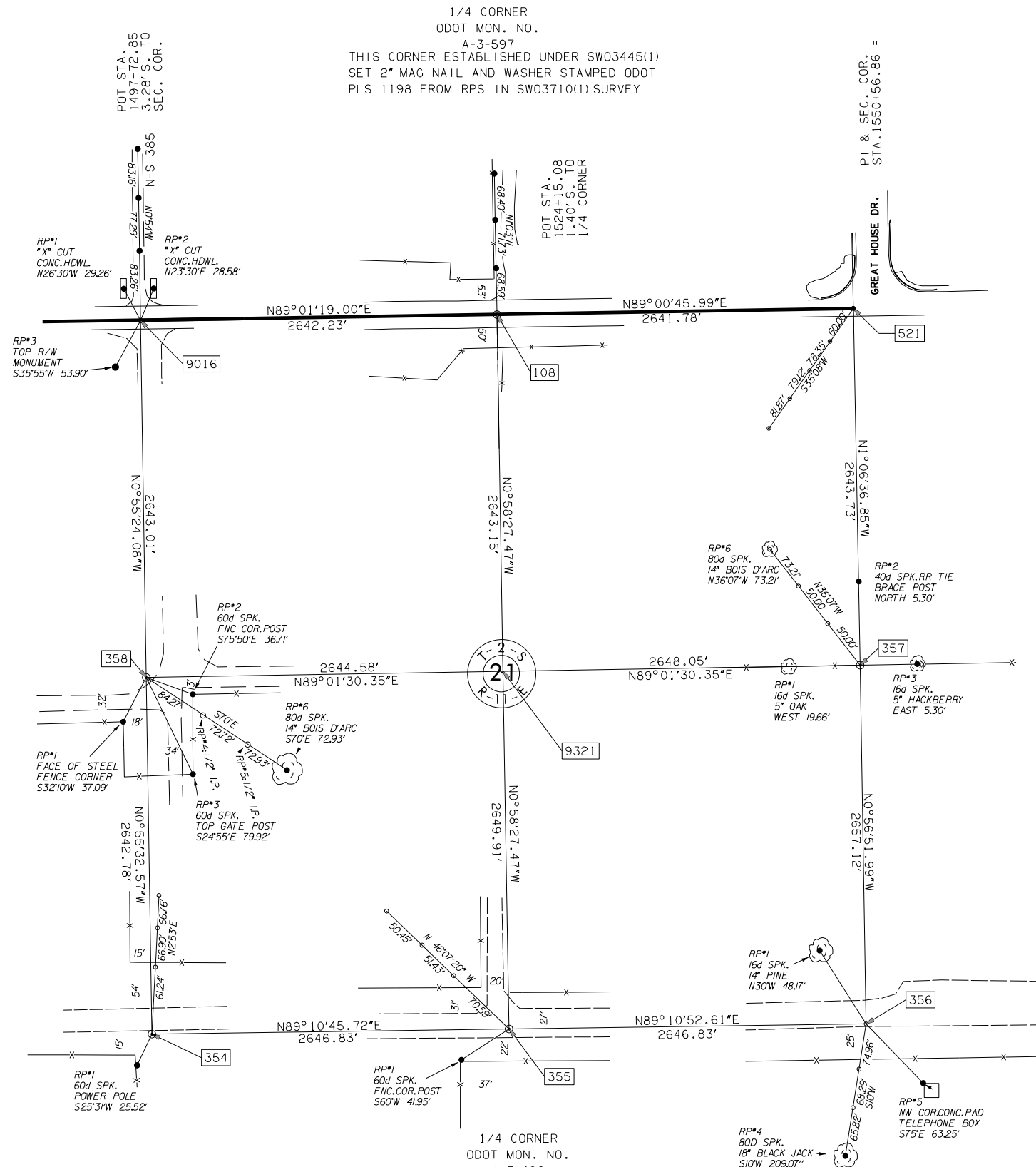


OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	OKLA.				
DESCRIPTION			REVISIONS	DATE	

SECTION CORNER
 ODOT MON. NO.
 A-3-598
 FOUND PK SPK. INPLACE
 FILED BY LS 1651 - REYNOLDS.
 THIS POSITION AGREES W/CORNER
 ESTABLISHED UNDER SW03445(1)
 REPLACED PK SPK. W/2" MAG NAIL
 & WASHER STAMPED ODOT PLS 1198.

1/4 CORNER
 ODOT MON. NO.
 A-3-597
 THIS CORNER ESTABLISHED UNDER SW03445(1)
 SET 2" MAG NAIL AND WASHER STAMPED ODOT
 PLS 1198 FROM RPS IN SW03710(1) SURVEY

SECTION CORNER
 ODOT MON. NO.
 A-3-20



1/4 CORNER
 ODOT MON. NO.
 A-3-651
 THIS CORNER ESTABLISHED UNDER SW03710(1)
 FOUND ALUM. CAP INPLACE
 FILED BY LS 1651 - REYNOLDS
 FITS ODOT RPS USED AS FOUND.

1/4 CORNER
 ODOT MON. NO.
 A-3-648
 FOUND 1/2" IRON PIN IN PLACE.
 ESTABLISHED UNDER SW02793
 USED AS FOUND.

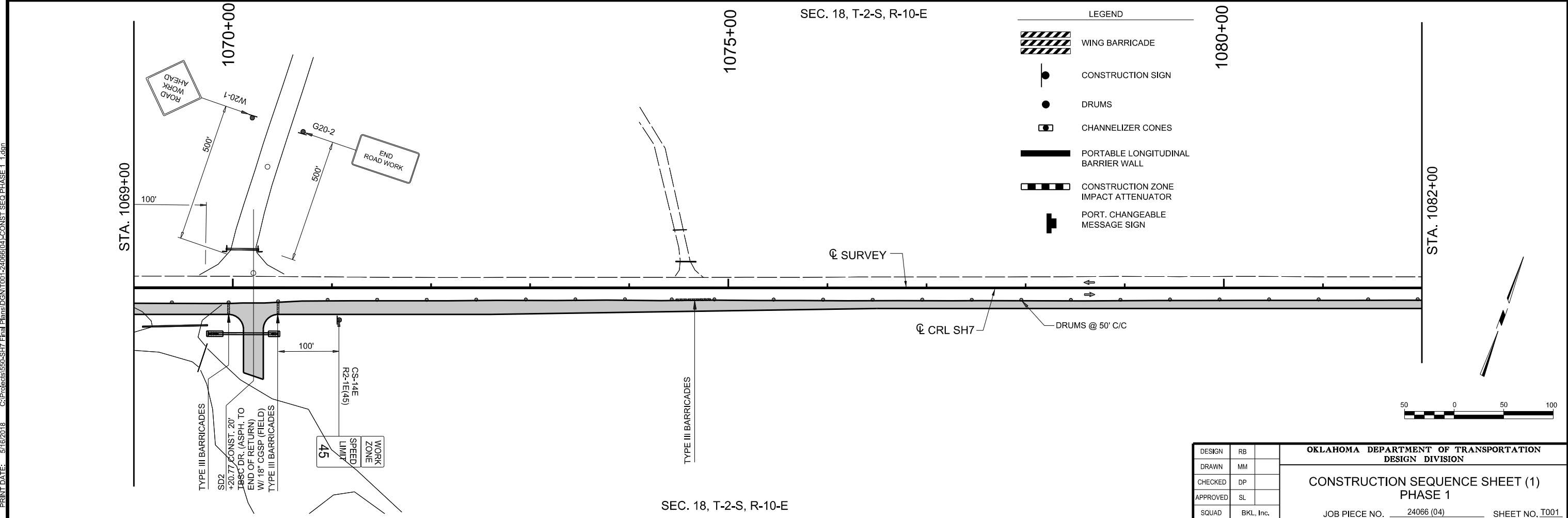
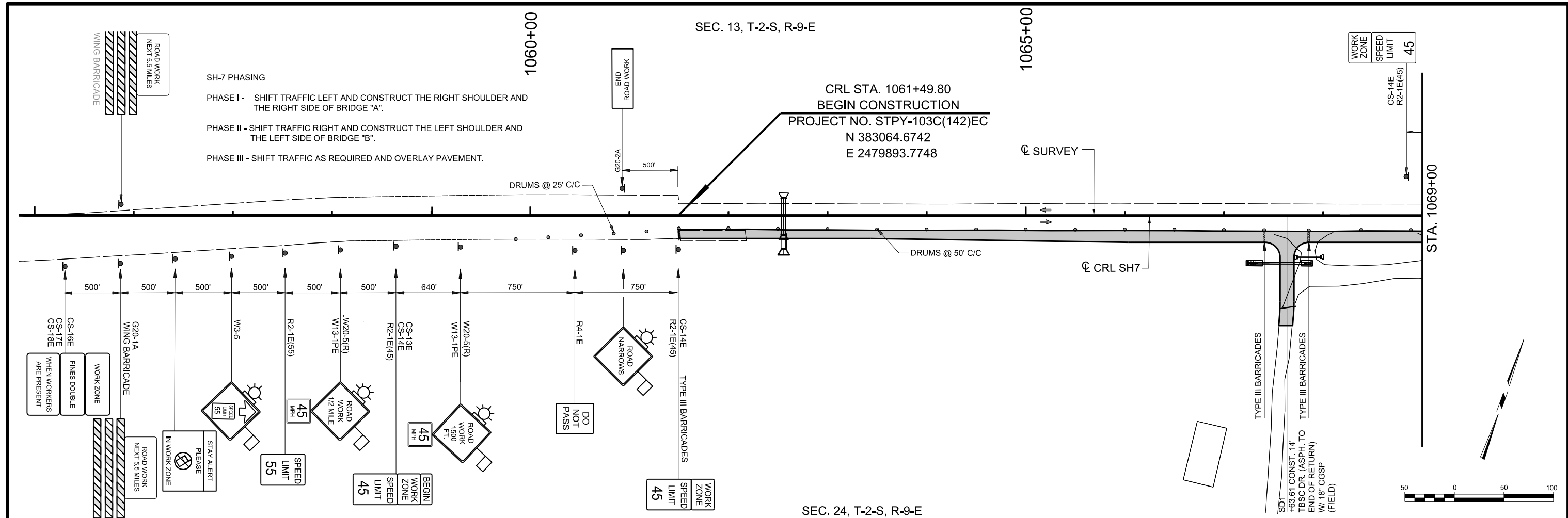
SECTION CORNER
 ODOT MON. NO.
 A-3-497
 FOUND ODOT STD. FENO MARKER
 ESTABLISHED UNDER SW03710
 USED AS FOUND.

1/4 CORNER
 ODOT MON. NO.
 A-3-496
 FOUND PK SPK INPLACE
 FITS ODOT RPS
 REPLACED W/ 2" MAG
 NAIL AND WASHER STAMPED
 ODOT PLS 1198

SECTION CORNER
 ODOT MON. NO.
 A-3-495
 FOUND ODOT STD.
 FENO MARKER
 EST. UNDER SW03710
 USED AS FOUND.

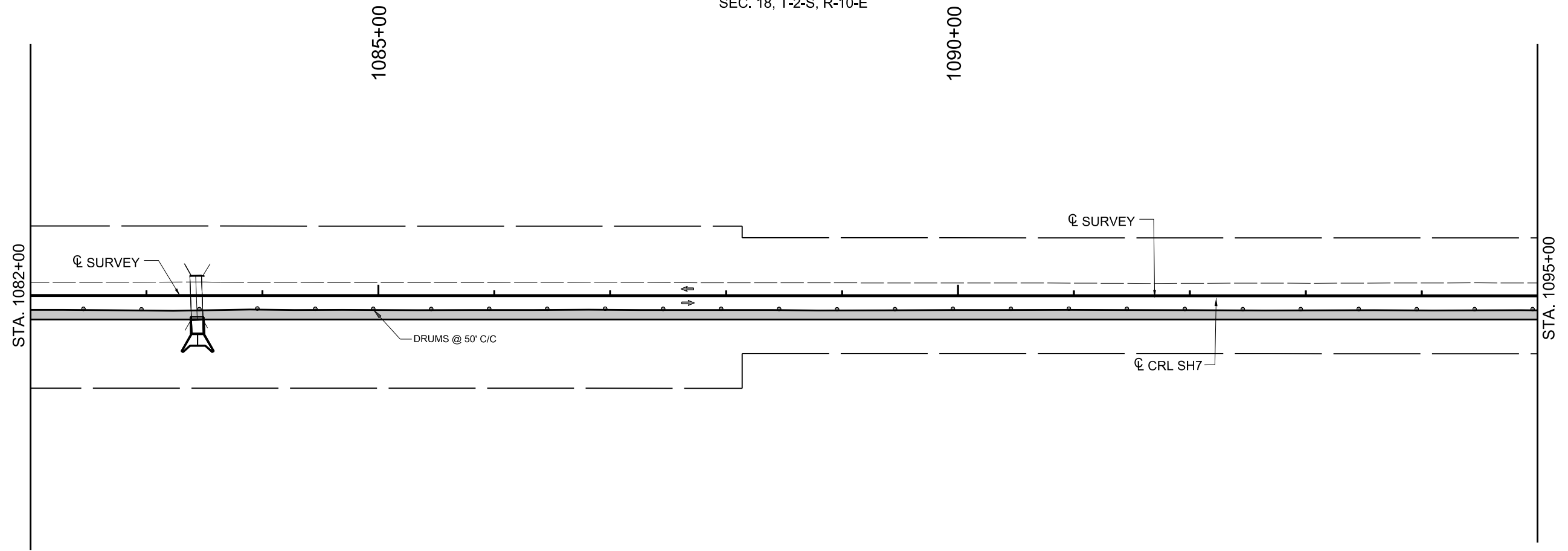
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DRAWN				SURVEY DATA SHEET	
CHECKED					
APPROVED					
CREW					
				SWO 4523 (1) PROJECT NO. 24066(07) SHEET NO. S042	

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\DOANT001-24066(04)-CONST SEQ PHASE 1.dgn

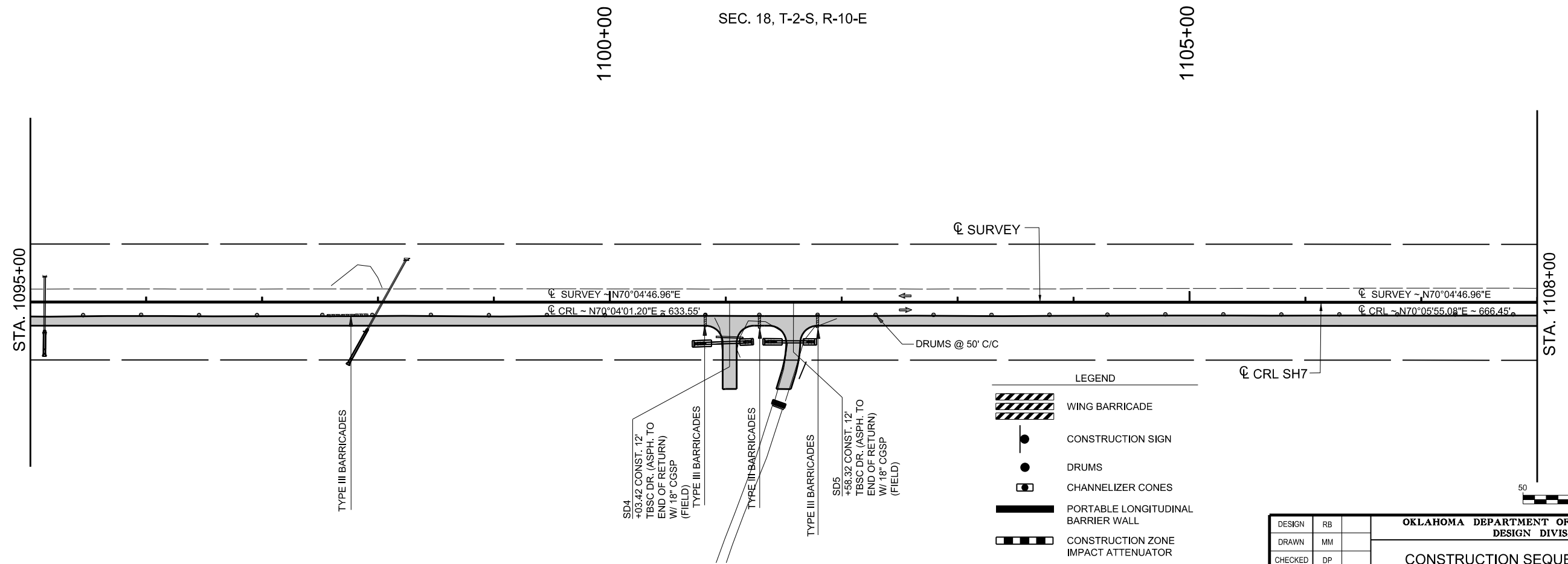


DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (1) PHASE 1 JOB PIECE NO. 24066 (04) SHEET NO. T001
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

SEC. 18, T-2-S, R-10-E



SEC. 18, T-2-S, R-10-E



SEC. 18, T-2-S, R-10-E

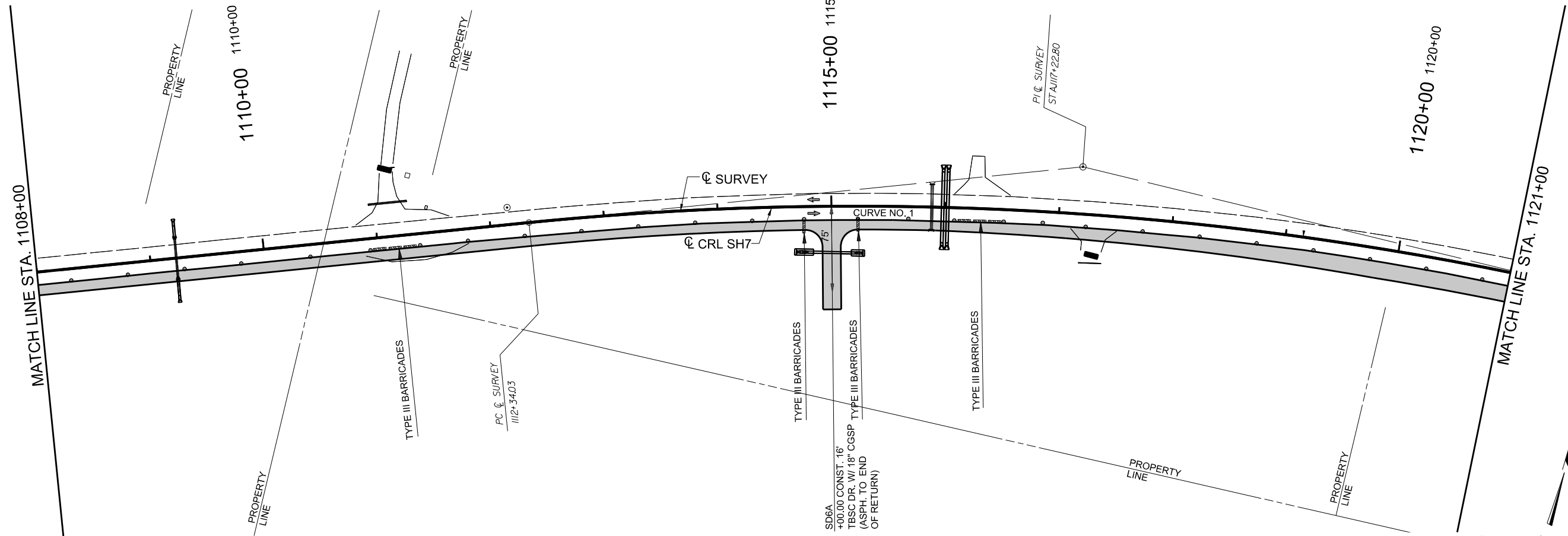
- LEGEND
- WING BARRICADE
 - CONSTRUCTION SIGN
 - DRUMS
 - CHANNELIZER CONES
 - PORTABLE LONGITUDINAL BARRIER WALL
 - CONSTRUCTION ZONE IMPACT ATTENUATOR
 - PORT. CHANGEABLE MESSAGE SIGN

DESIGN	RB	
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
 CONSTRUCTION SEQUENCE SHEET (2)
 PHASE 1
 JOB PIECE NO. 24066 (04) SHEET NO. T002

PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\002-24066(04)-CONST SEQ PHASE 1_2.dgn

SEC. 18, T-2-S, R-10-E

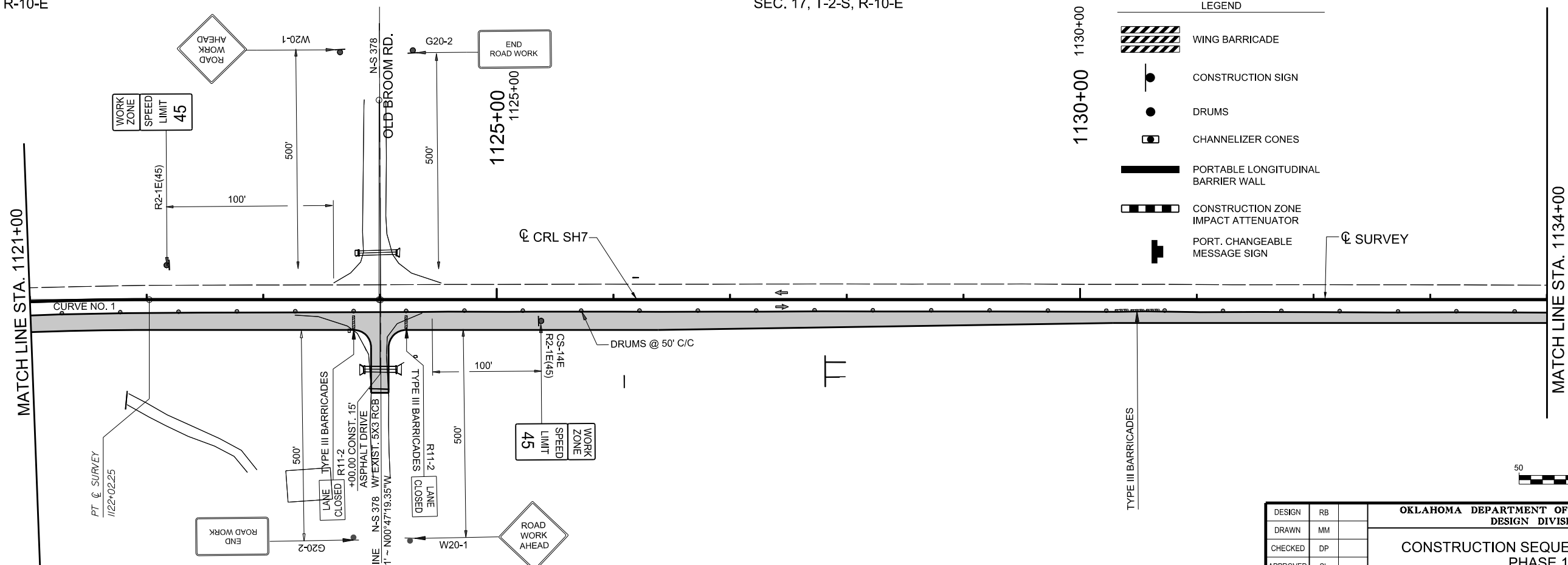


SEC. 18, T-2-S, R-10-E



SEC. 18, T-2-S, R-10-E

SEC. 17, T-2-S, R-10-E



- LEGEND
- WING BARRICADE
 - CONSTRUCTION SIGN
 - DRUMS
 - CHANNELIZER CONES
 - PORTABLE LONGITUDINAL BARRIER WALL
 - CONSTRUCTION ZONE IMPACT ATTENUATOR
 - PORT. CHANGEABLE MESSAGE SIGN

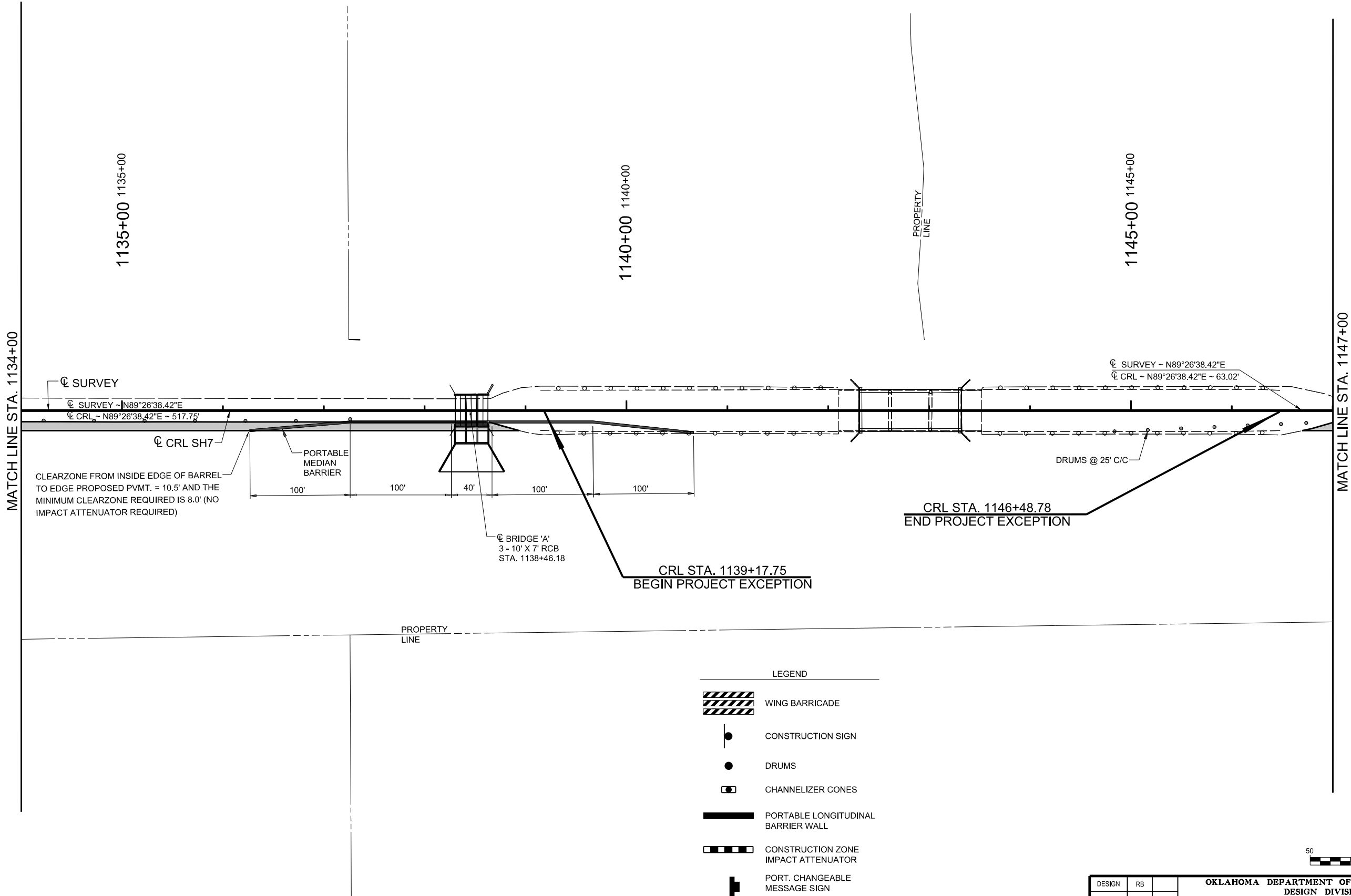









SEC. 18, T-2-S, R-10-E

SEC. 17, T-2-S, R-10-E

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (3) PHASE 1 JOB PIECE NO. 24066 (04) SHEET NO. T003
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

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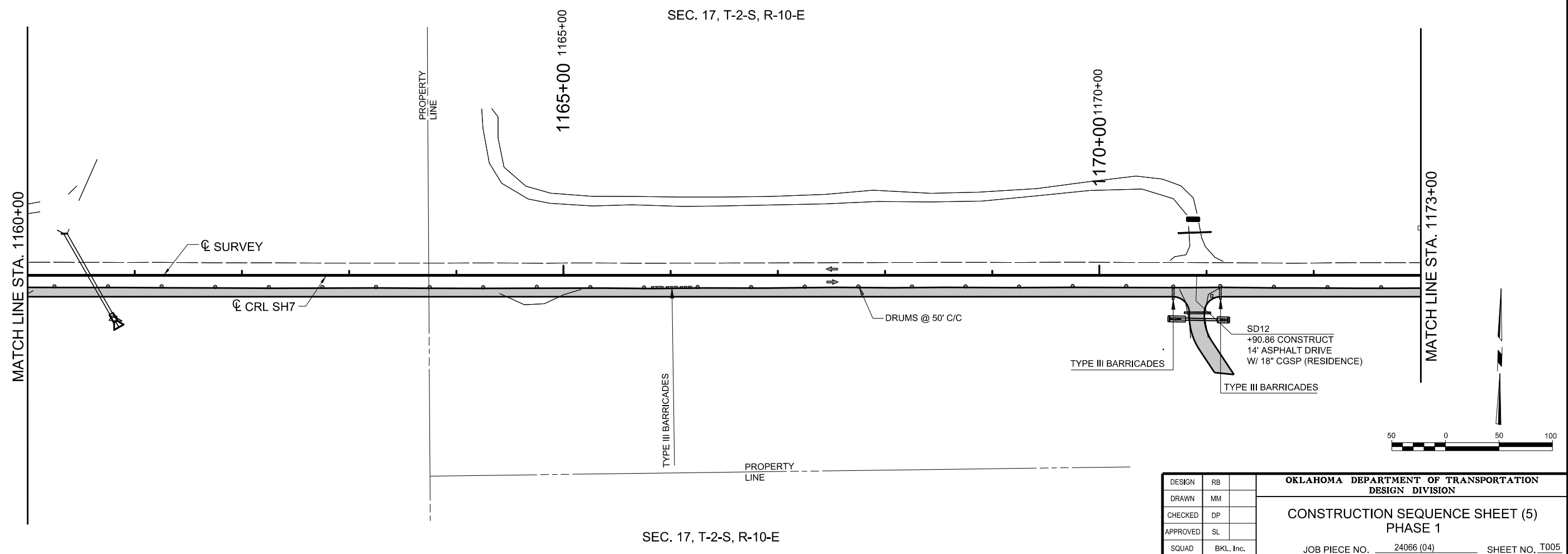
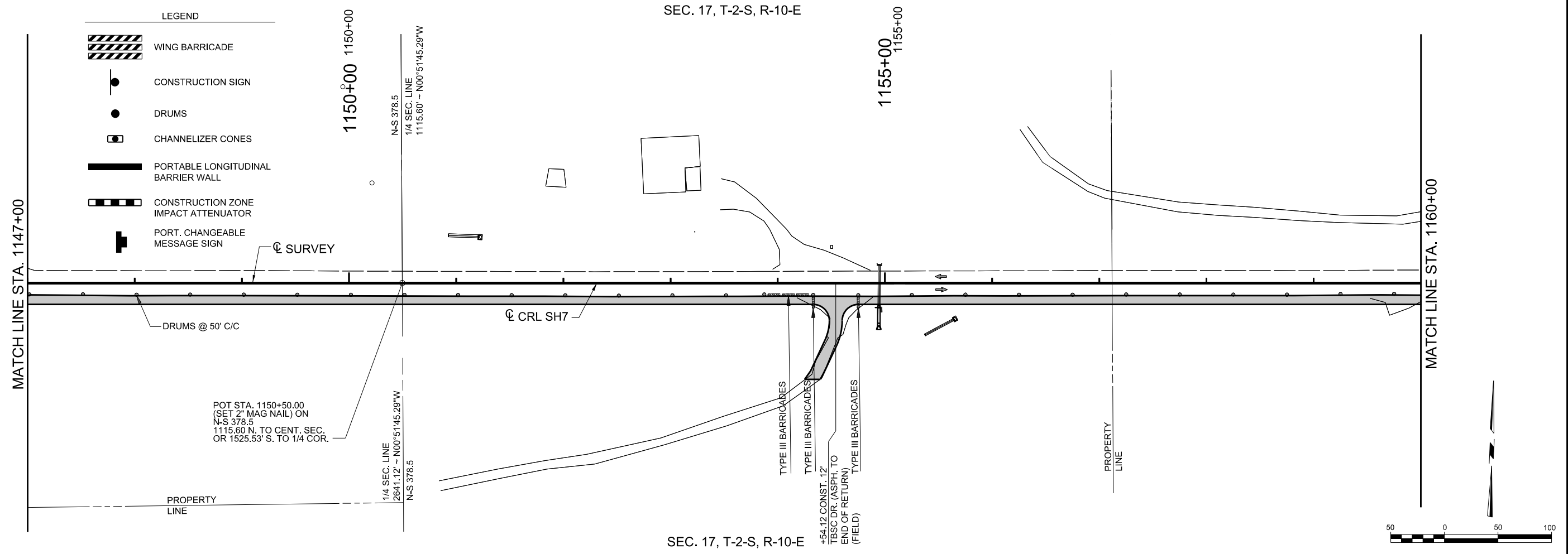


- LEGEND
-  WING BARRICADE
 -  CONSTRUCTION SIGN
 -  DRUMS
 -  CHANNELIZER CONES
 -  PORTABLE LONGITUDINAL BARRIER WALL
 -  CONSTRUCTION ZONE IMPACT ATTENUATOR
 -  PORT. CHANGEABLE MESSAGE SIGN



PRINT DATE: 5/18/2018 C:\Projects\550-SHT Final Plans\DGNT\004-24066(04)-Const. Sec Phase1_4.dgn

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (4) PHASE 1 JOB PIECE NO. 24066 (04) SHEET NO. T004
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CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

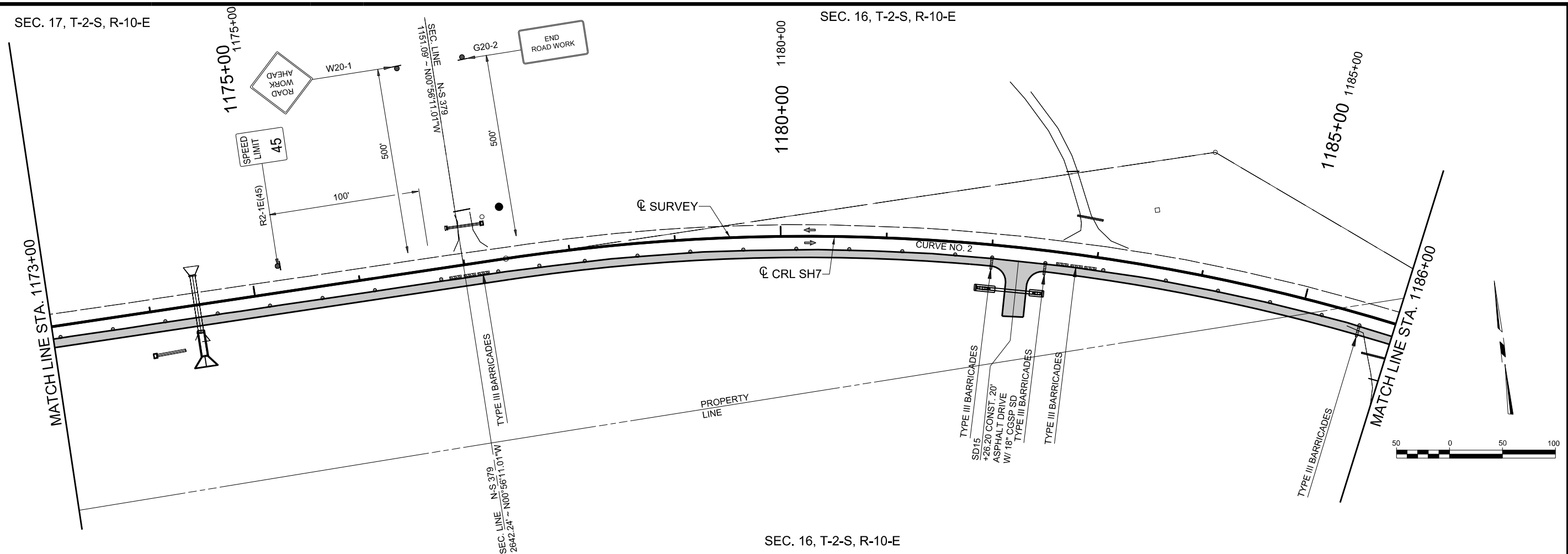


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DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

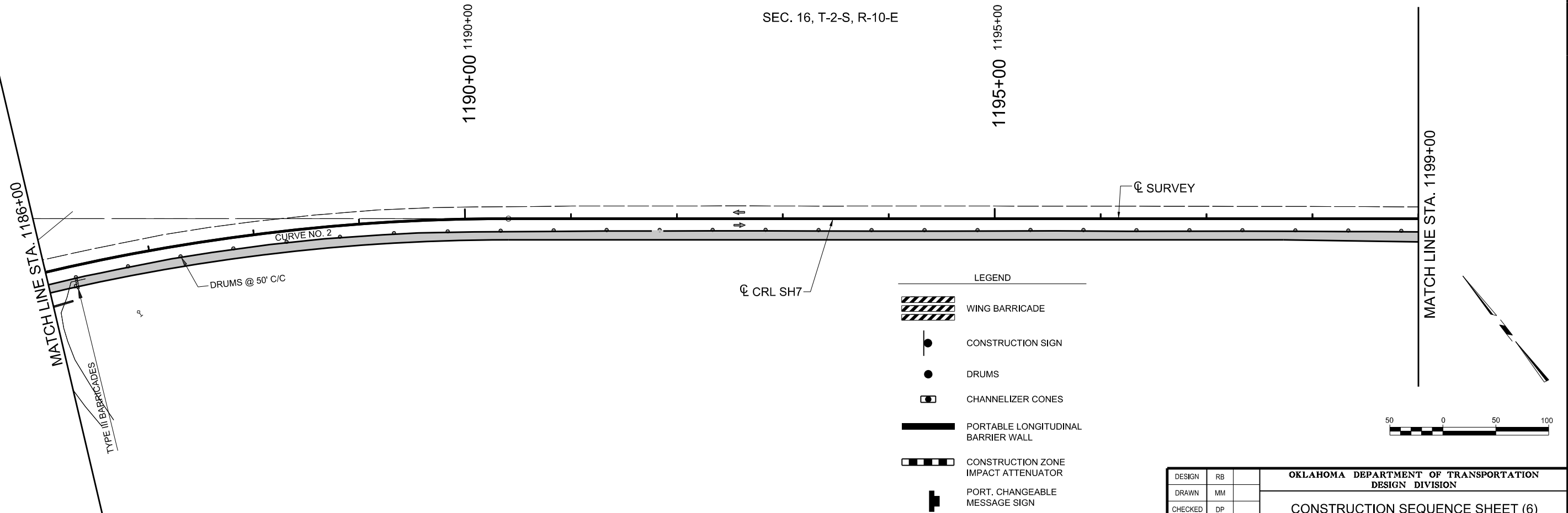
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SEC. 17, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E



SEC. 16, T-2-S, R-10-E



SEC. 16, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E

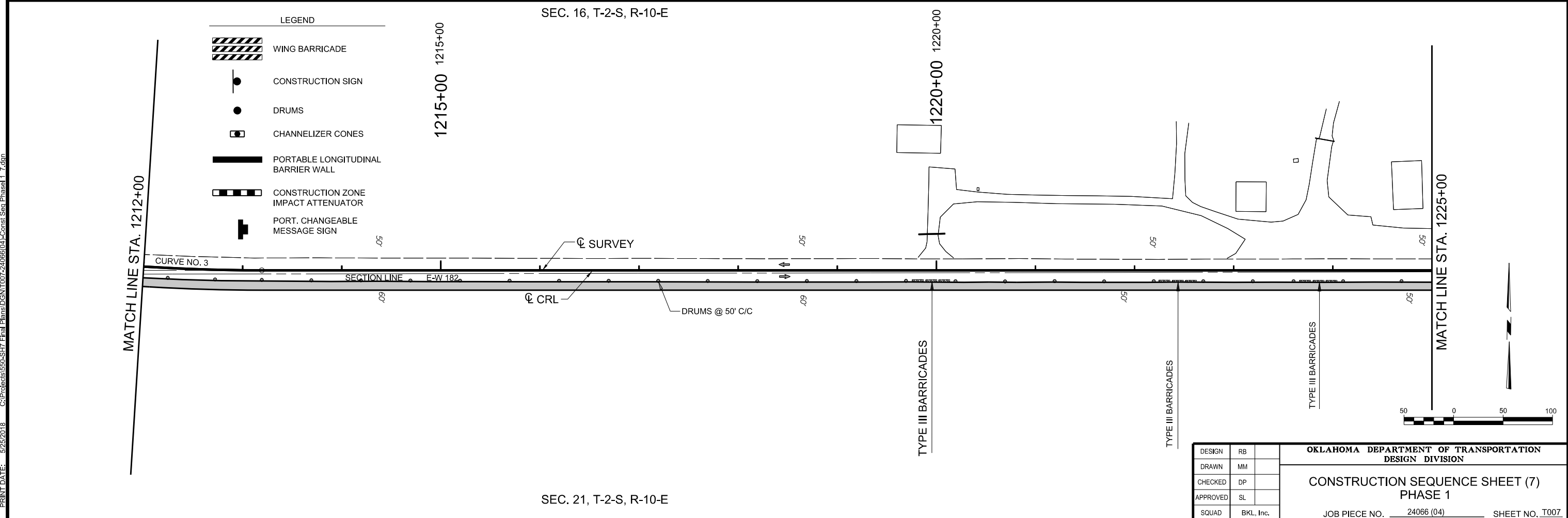
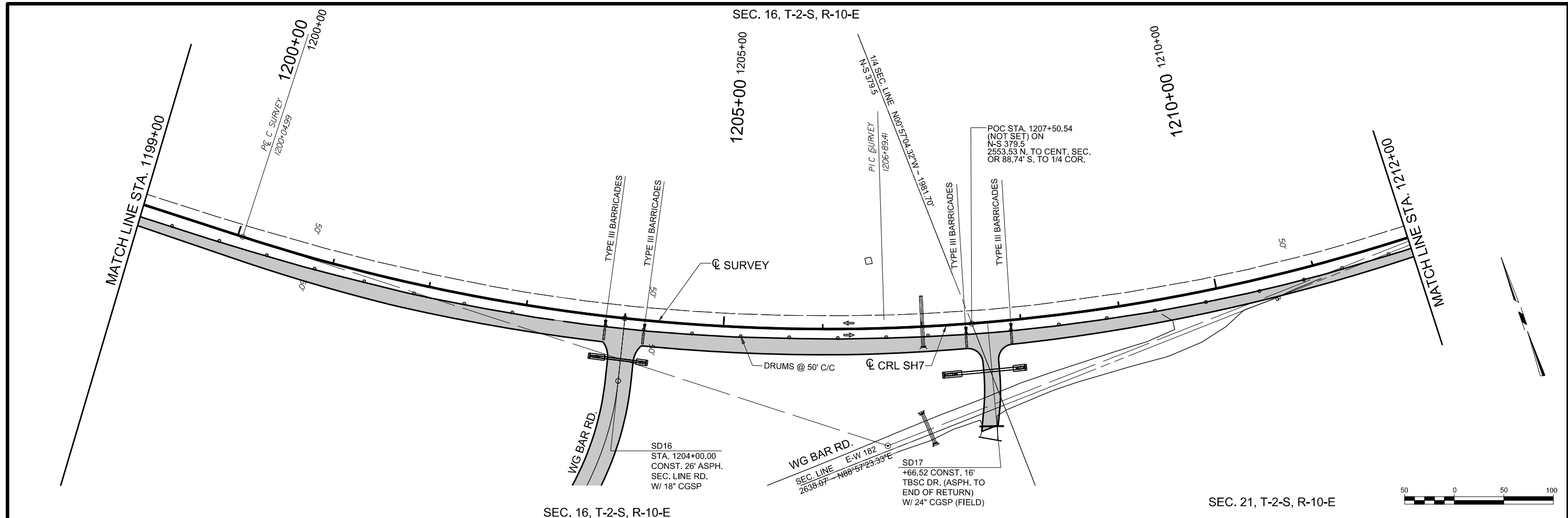
LEGEND

	WING BARRICADE
	CONSTRUCTION SIGN
	DRUMS
	CHANNELIZER CONES
	PORTABLE LONGITUDINAL BARRIER WALL
	CONSTRUCTION ZONE IMPACT ATTENUATOR
	PORT. CHANGEABLE MESSAGE SIGN

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (6) PHASE 1 JOB PIECE NO. 24066 (04) SHEET NO. T006
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

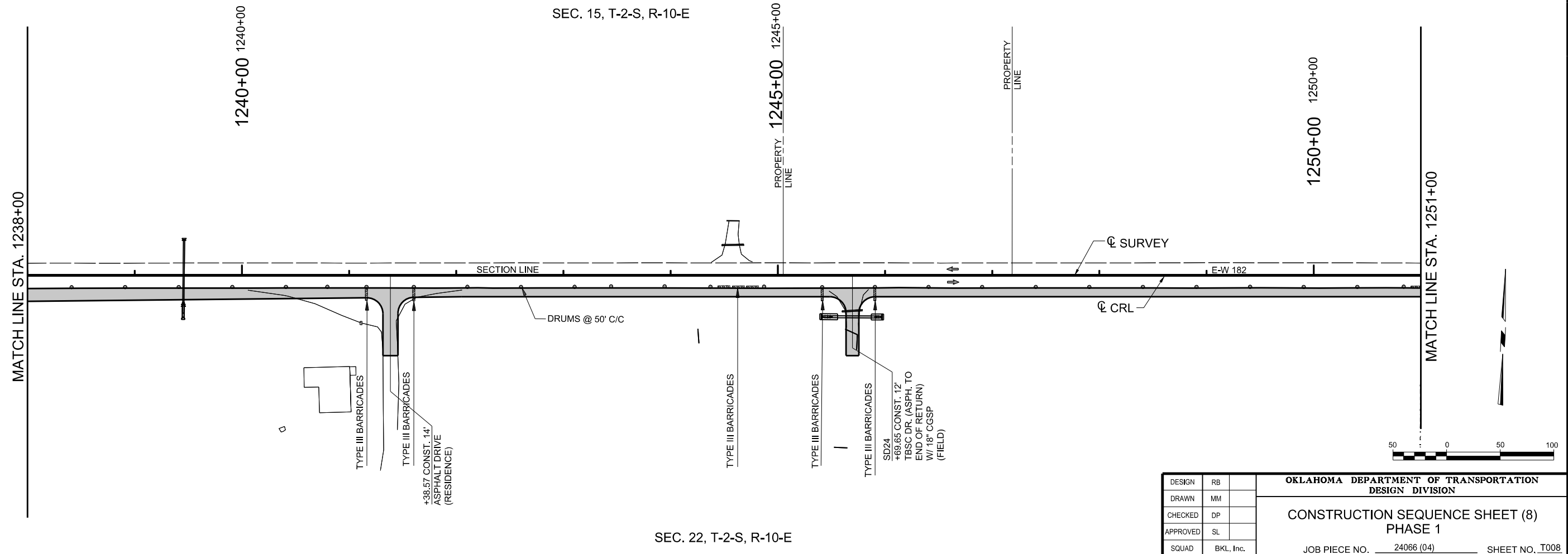
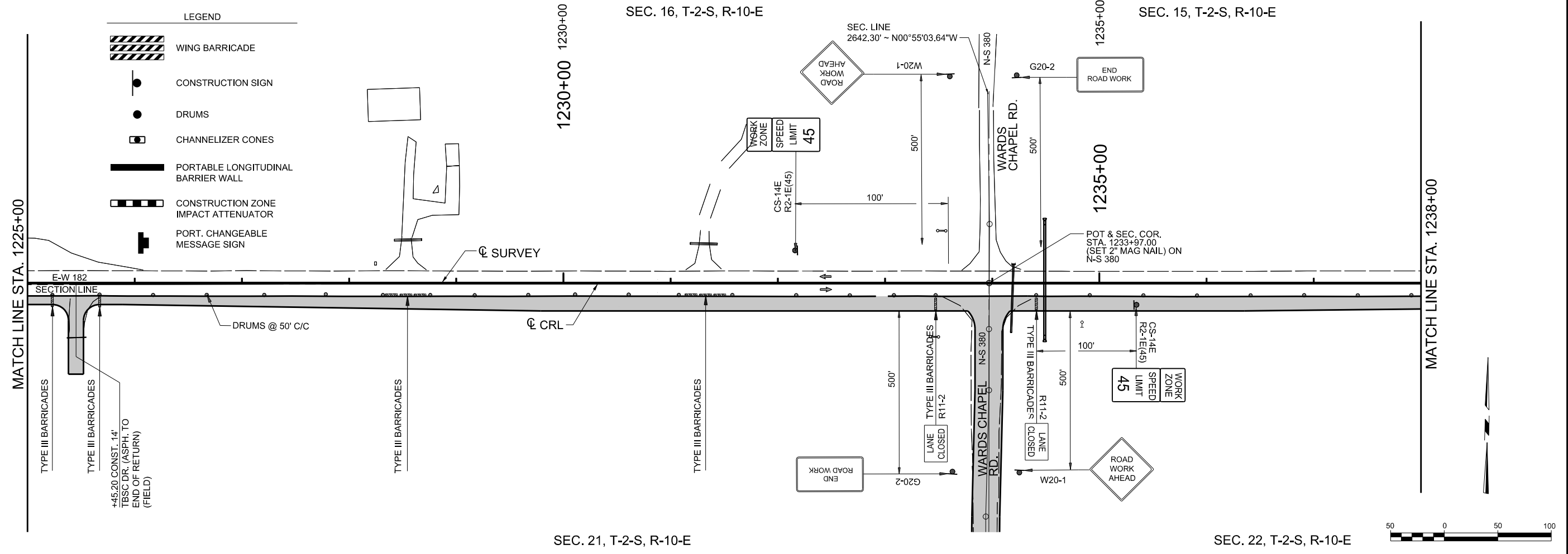
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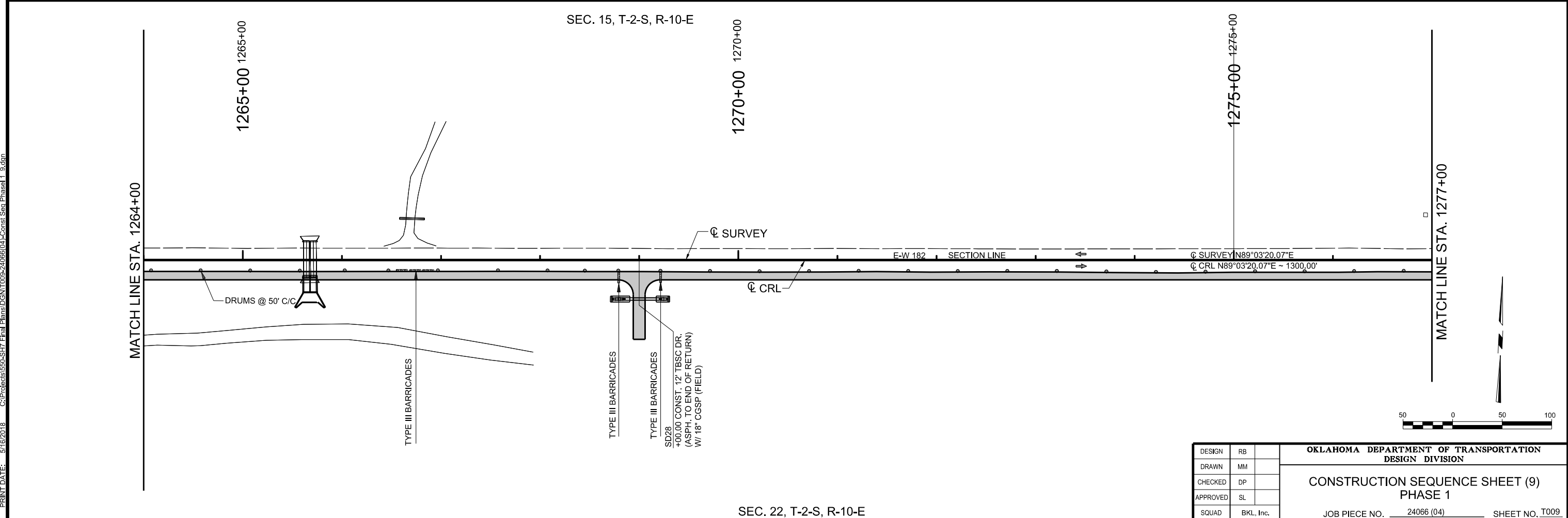
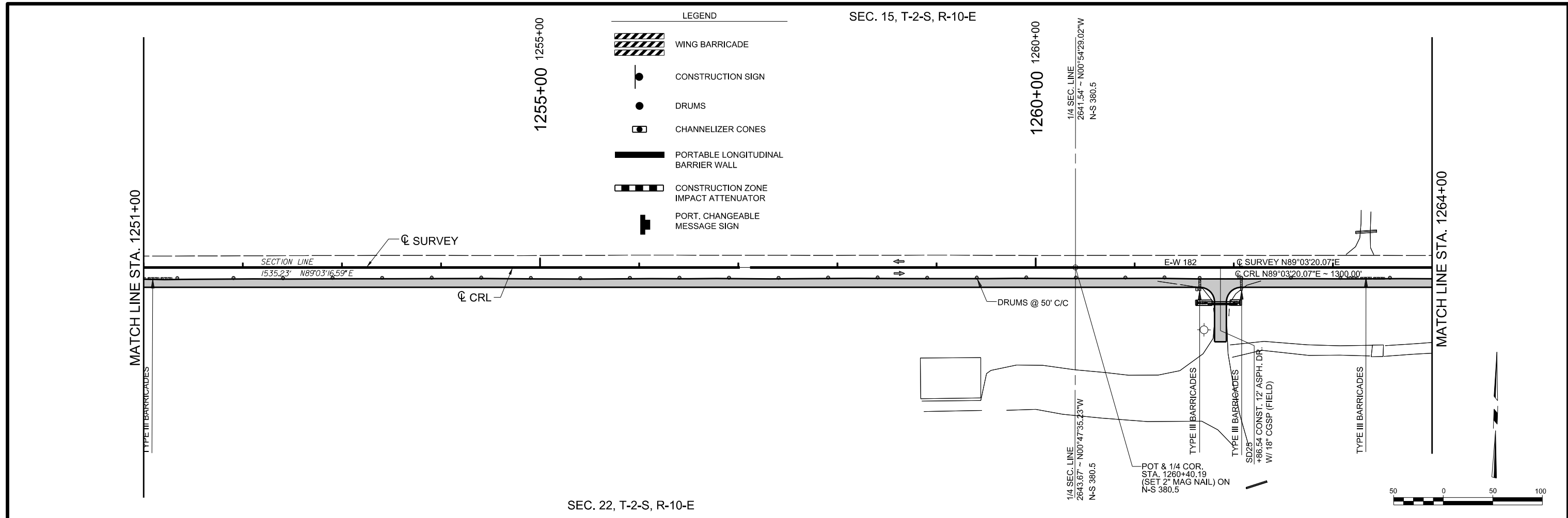
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DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\DGANT\009-24066(04)-Const-Seq Phase1_8.dgn



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (8) PHASE 1 JOB PIECE NO. 24066 (04) SHEET NO. T008
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\DGNT\009-240666(04)-Const-Seq Phase1_9.dgn



SEC. 22, T-2-S, R-10-E

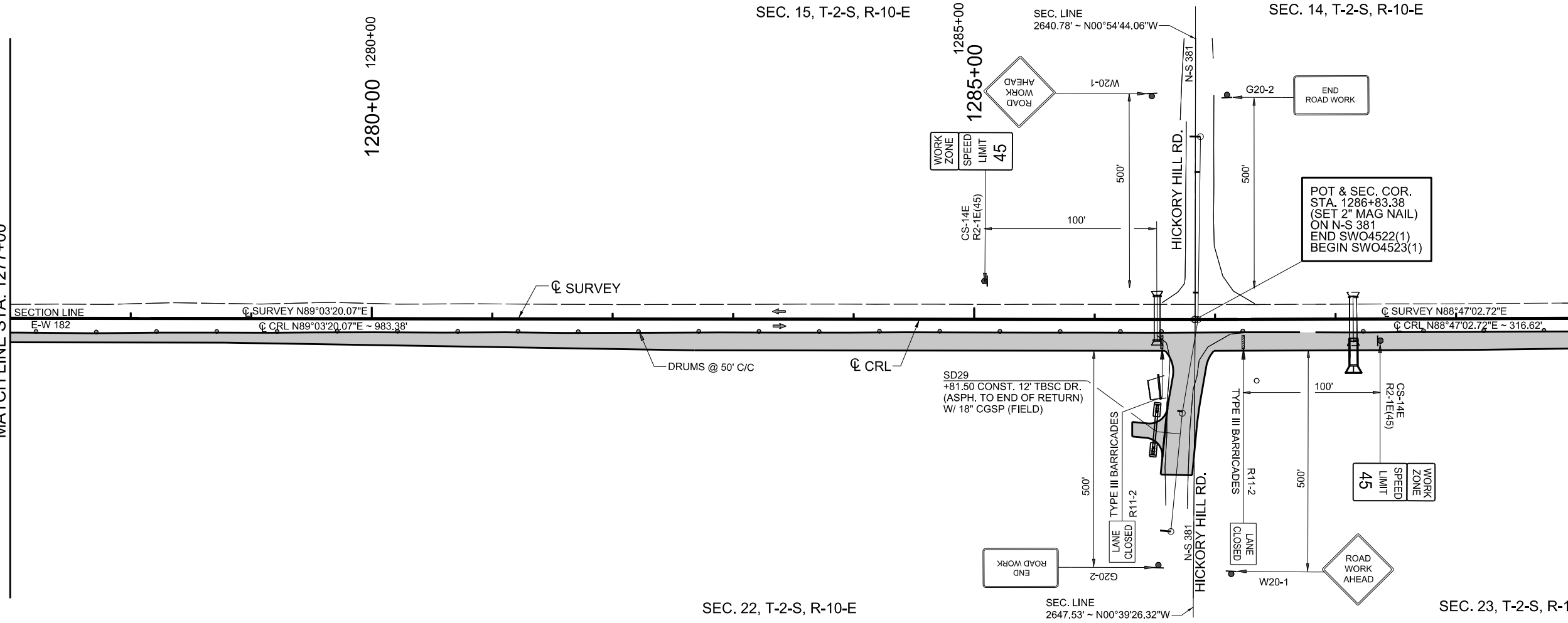
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DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

SEC. 15, T-2-S, R-10-E

SEC. 14, T-2-S, R-10-E

MATCH LINE STA. 1277+00

MATCH LINE STA. 1290+00

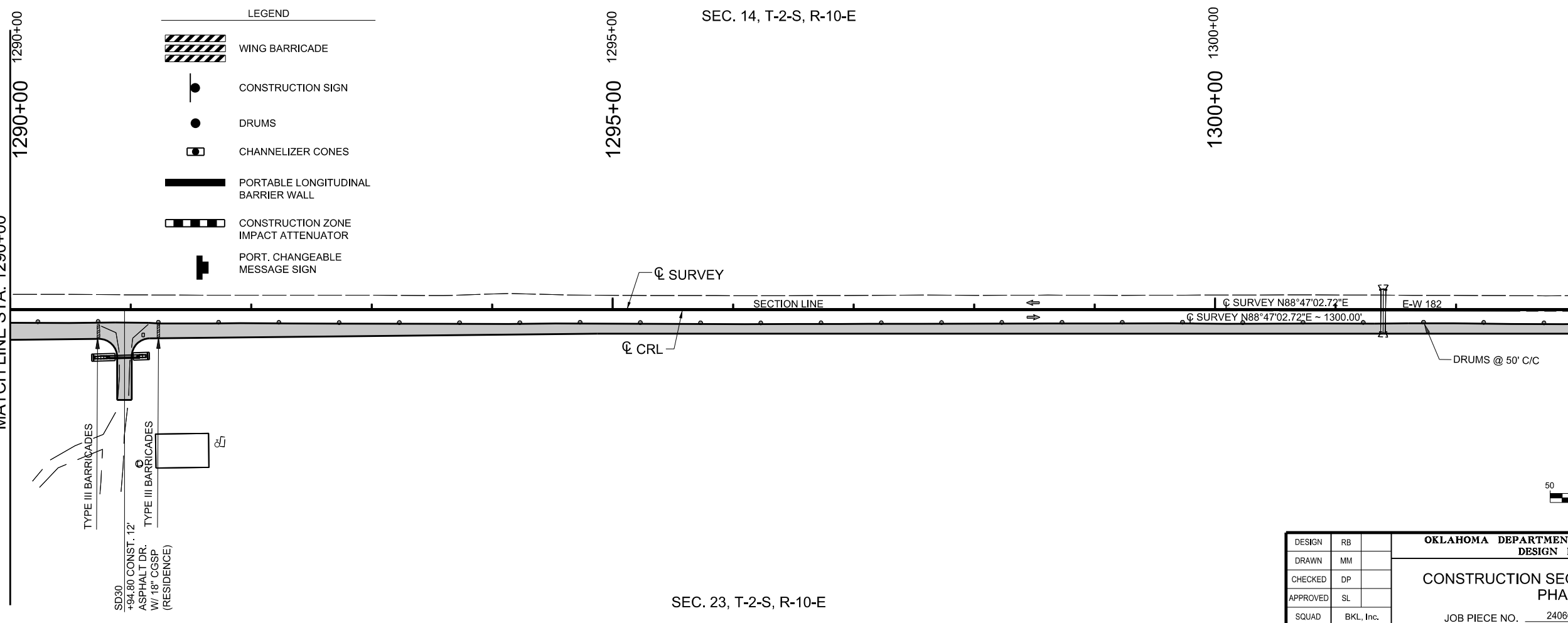


SEC. 22, T-2-S, R-10-E

SEC. 23, T-2-S, R-10-E

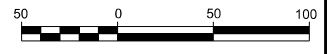
MATCH LINE STA. 1290+00

MATCH LINE STA. 1303+00



SEC. 23, T-2-S, R-10-E

- LEGEND
- WING BARRICADE
 - CONSTRUCTION SIGN
 - DRUMS
 - CHANNELIZER CONES
 - PORTABLE LONGITUDINAL BARRIER WALL
 - CONSTRUCTION ZONE IMPACT ATTENUATOR
 - PORT. CHANGEABLE MESSAGE SIGN



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (10) PHASE 1 JOB PIECE NO. 24066 (04) SHEET NO. T010
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\DGNT\010-24066(04)-Const. Seq. Phase 1_10.dgn

PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\150-SHT Const\Sec Phase 1 11.dgn

SEC. 14, T-2-S, R-10-E

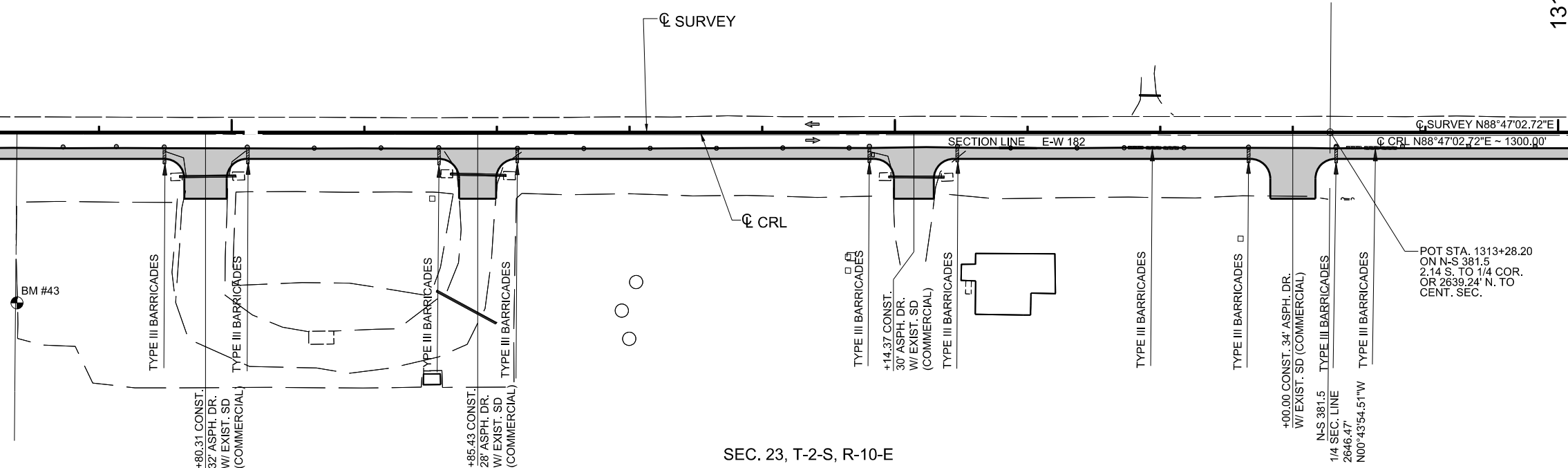
MATCH LINE STA. 1303+00

1305+00 1305+00

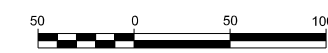
1310+00 1310+00

1315+00 1315+00

MATCH LINE STA. 1316+00



SEC. 23, T-2-S, R-10-E



SEC. 14, T-2-S, R-10-E

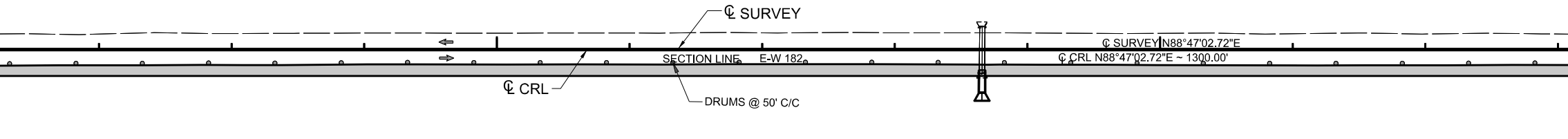
MATCH LINE STA. 1316+00

1320+00 1320+00

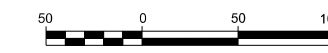
1325+00 1325+00

MATCH LINE STA. 1329+00

- LEGEND
- WING BARRICADE
 - CONSTRUCTION SIGN
 - DRUMS
 - CHANNELIZER CONES
 - PORTABLE LONGITUDINAL BARRIER WALL
 - CONSTRUCTION ZONE IMPACT ATTENUATOR
 - PORT. CHANGEABLE MESSAGE SIGN

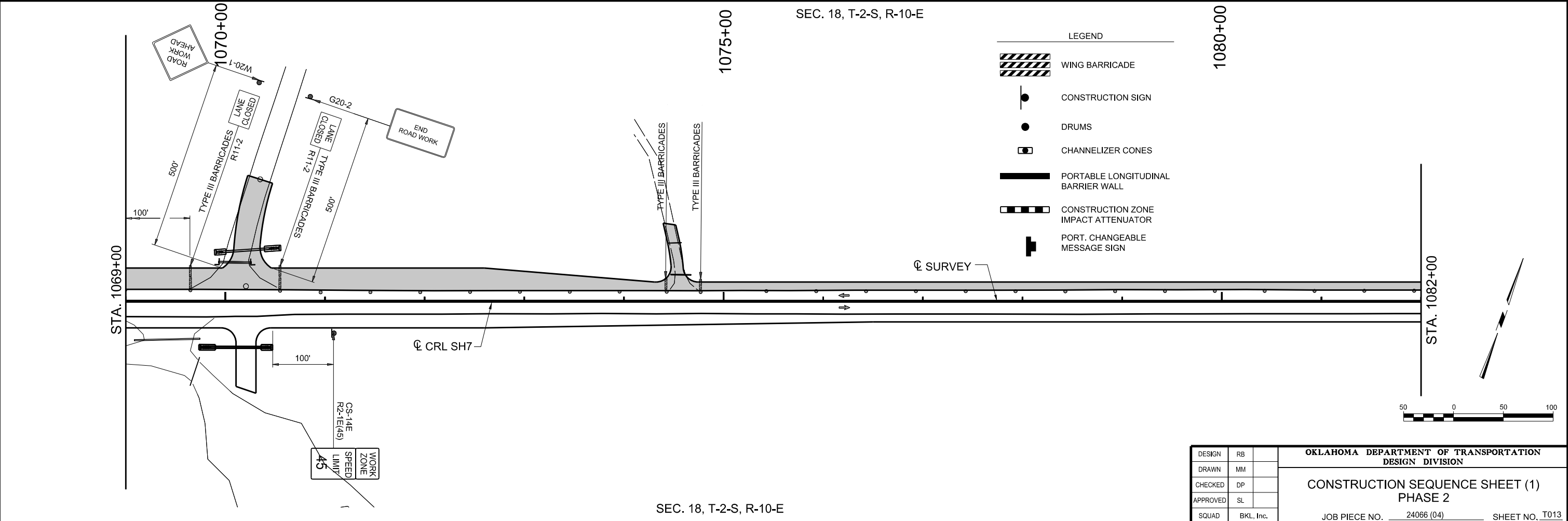
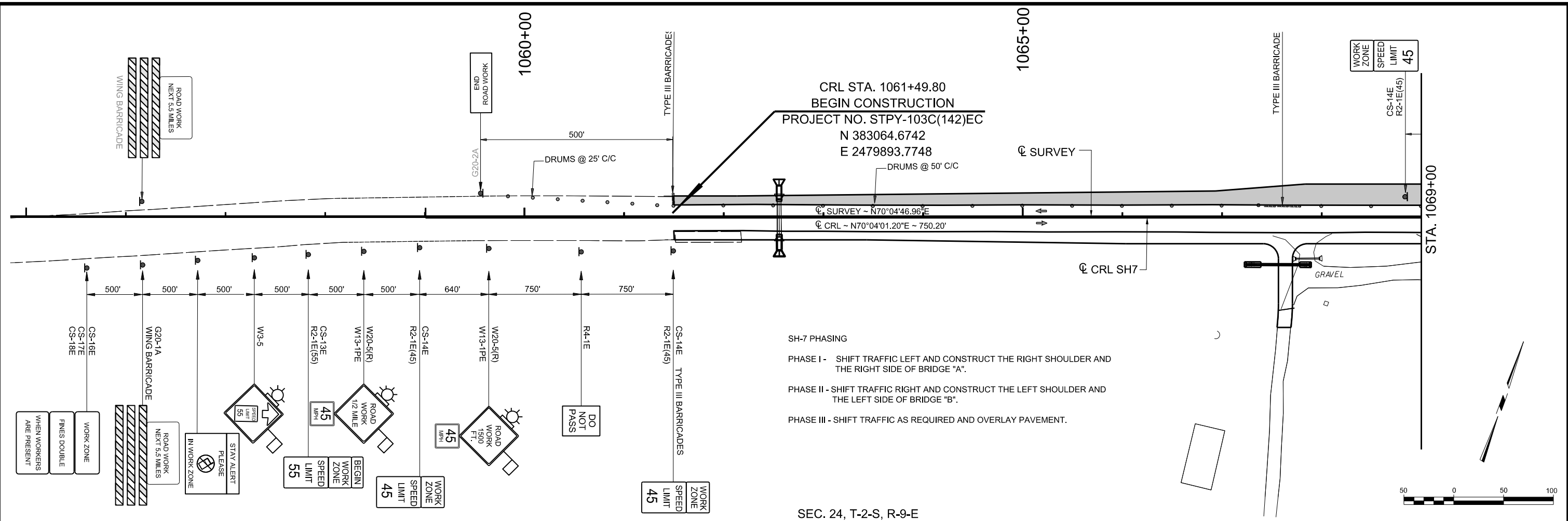


SEC. 23, T-2-S, R-10-E



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (11) PHASE 1 JOB PIECE NO. 24066 (04) SHEET NO. T011
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/18/2018 C:\Projects\650-SHT Final Plans\DGNT\013-24066(04)-Const Seq Phase 2_1.dgn



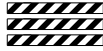






DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (1) PHASE 2 JOB PIECE NO. 24066 (04) SHEET NO. T013
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

SEC. 18, T-2-S, R-10-E

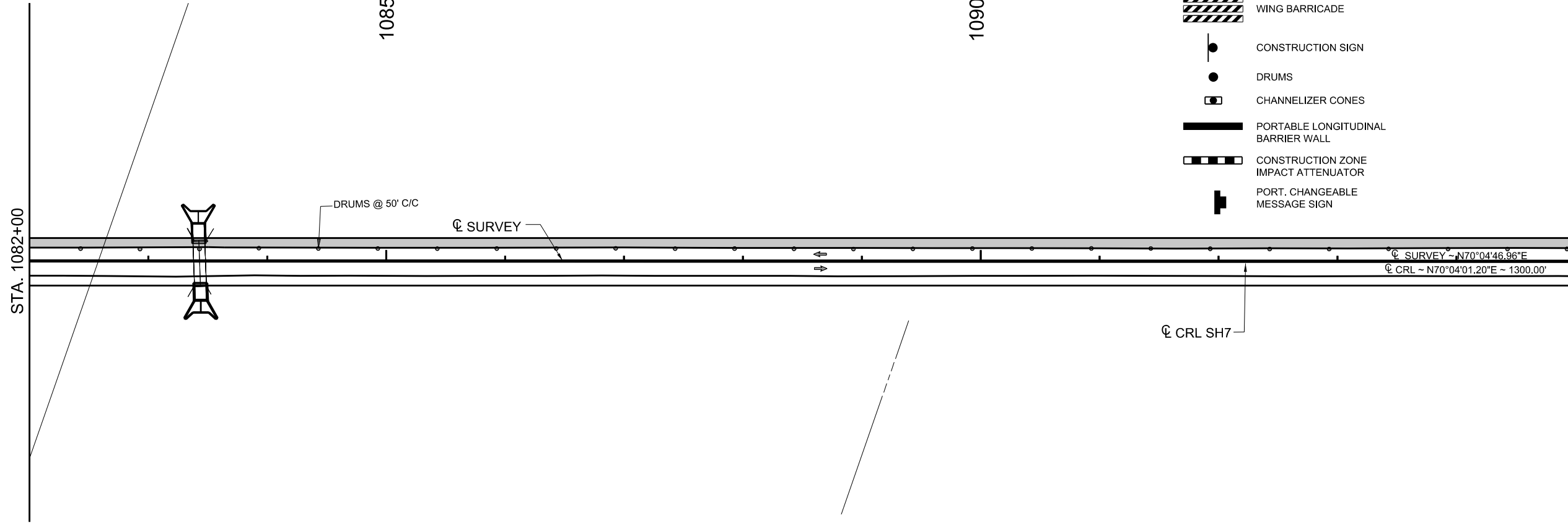
1090+00

1085+00

LEGEND

-  WING BARRICADE
-  CONSTRUCTION SIGN
-  DRUMS
-  CHANNELIZER CONES
-  PORTABLE LONGITUDINAL BARRIER WALL
-  CONSTRUCTION ZONE IMPACT ATTENUATOR
-  PORT. CHANGEABLE MESSAGE SIGN

STA. 1082+00



STA. 1095+00



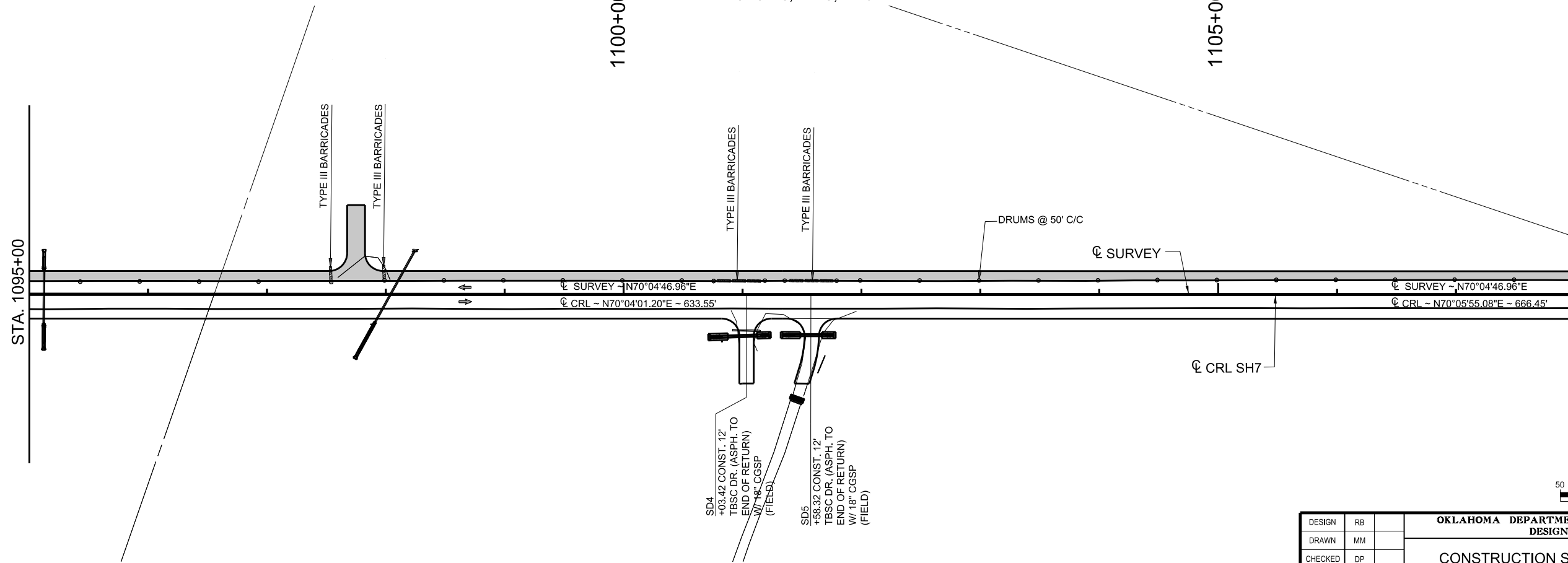
SEC. 18, T-2-S, R-10-E

1100+00

1105+00

SEC. 18, T-2-S, R-10-E

STA. 1095+00



STA. 1108+00



SEC. 18, T-2-S, R-10-E

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\04-240666\04-Const\Seq Phase 2_2.dgn

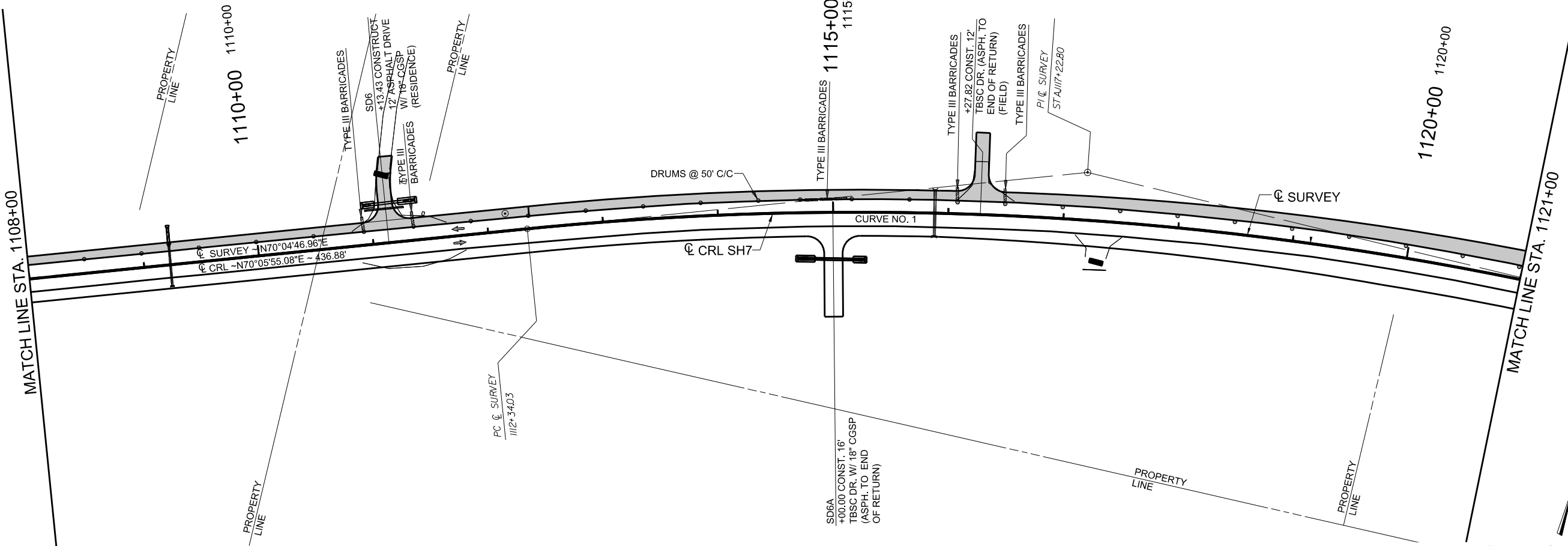
DESIGN	RB	
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

OKLAHOMA DEPARTMENT OF TRANSPORTATION
DESIGN DIVISION

CONSTRUCTION SEQUENCE SHEET (2)
PHASE 2

JOB PIECE NO. 24066 (04) SHEET NO. T014

SEC. 18, T-2-S, R-10-E

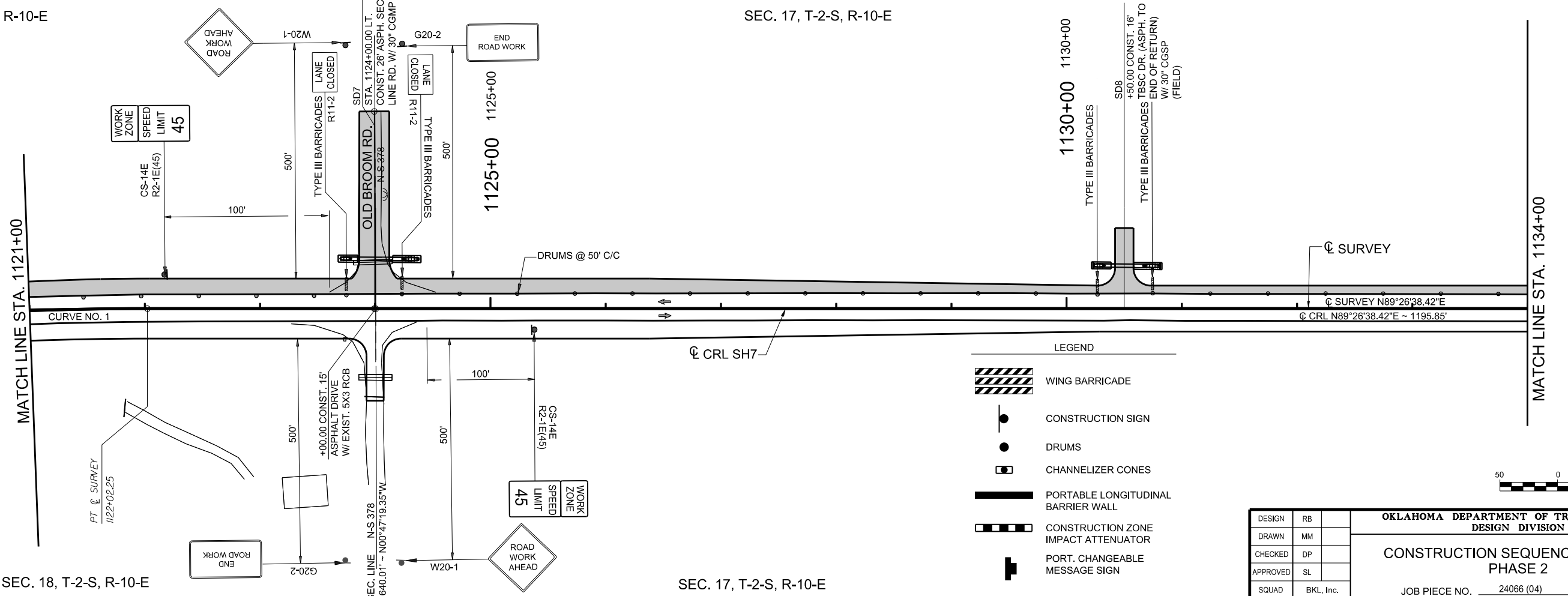


SEC. 18, T-2-S, R-10-E



SEC. 18, T-2-S, R-10-E

SEC. 17, T-2-S, R-10-E



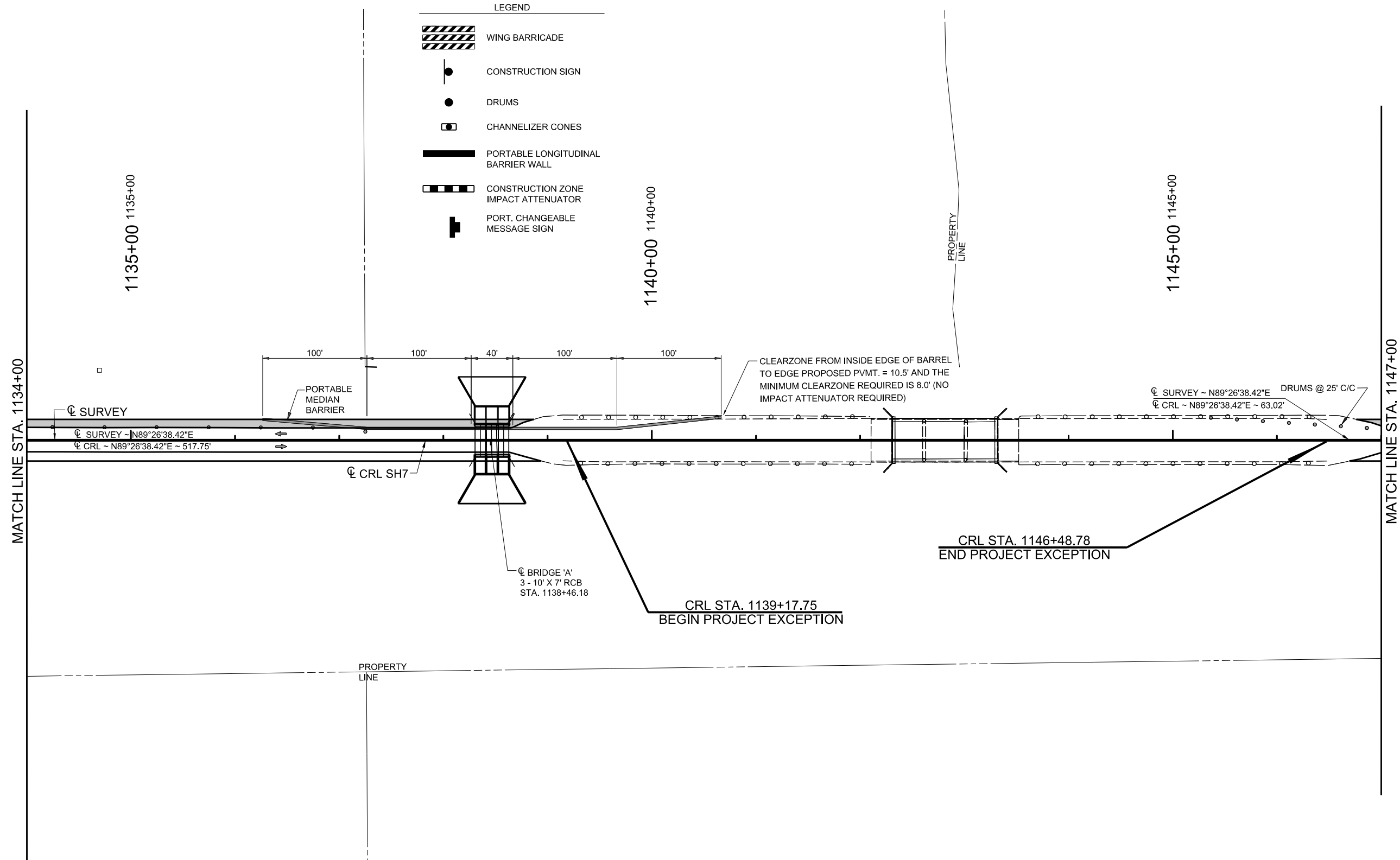
SEC. 17, T-2-S, R-10-E



- LEGEND
- WING BARRICADE
 - CONSTRUCTION SIGN
 - DRUMS
 - CHANNELIZER CONES
 - PORTABLE LONGITUDINAL BARRIER WALL
 - CONSTRUCTION ZONE IMPACT ATTENUATOR
 - PORT. CHANGEABLE MESSAGE SIGN

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (3) PHASE 2 JOB PIECE NO. 24066 (04) SHEET NO. T015
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\DG\NT015-24066(04)-Const Seq Phase 2_3.dgn



MATCH LINE STA. 1134+00

1135+00 1135+00

1140+00 1140+00

1145+00 1145+00

MATCH LINE STA. 1147+00

☉ SURVEY
 ☉ SURVEY - N89°26'38.42"E
 ☉ CRL - N89°26'38.42"E ~ 517.75'

☉ CRL SH7

☉ BRIDGE 'A'
 3 - 10' X 7' RCB
 STA. 1138+46.18

CRL STA. 1139+17.75
 BEGIN PROJECT EXCEPTION

CLEARZONE FROM INSIDE EDGE OF BARREL
 TO EDGE PROPOSED PVMT. = 10.5' AND THE
 MINIMUM CLEARZONE REQUIRED IS 8.0' (NO
 IMPACT ATTENUATOR REQUIRED)

☉ SURVEY - N89°26'38.42"E DRUMS @ 25' C/C
 ☉ CRL - N89°26'38.42"E ~ 63.02'

CRL STA. 1146+48.78
 END PROJECT EXCEPTION

PROPERTY LINE

PROPERTY LINE

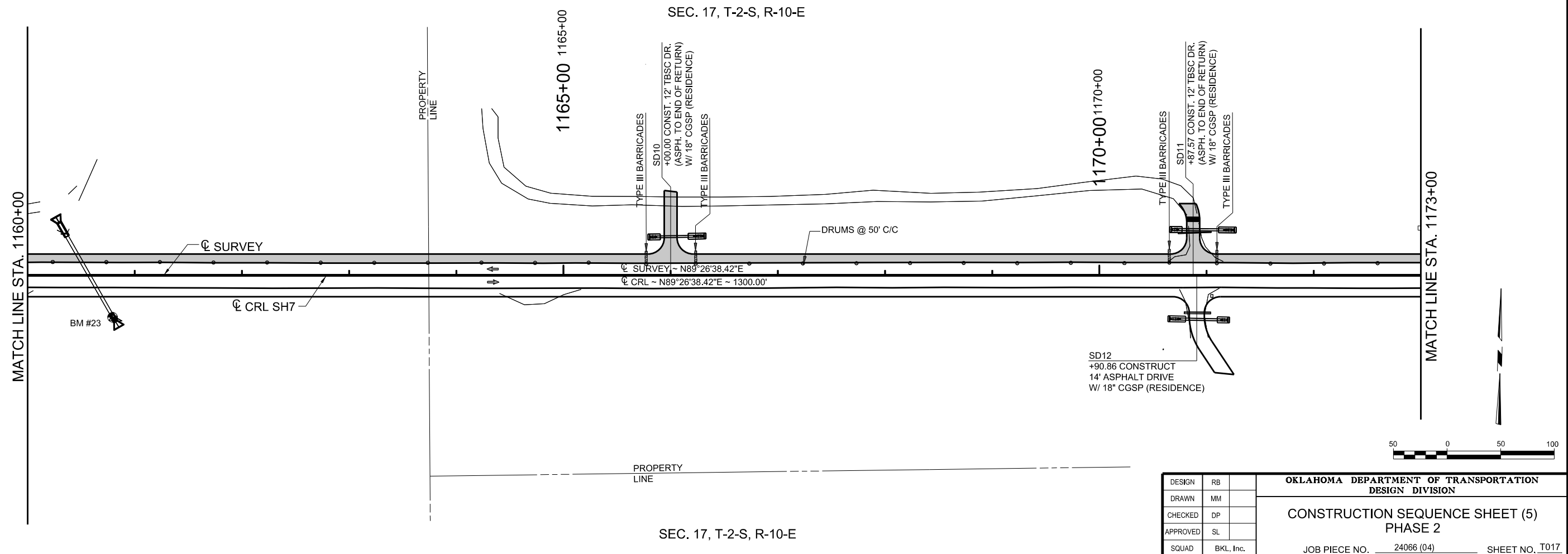
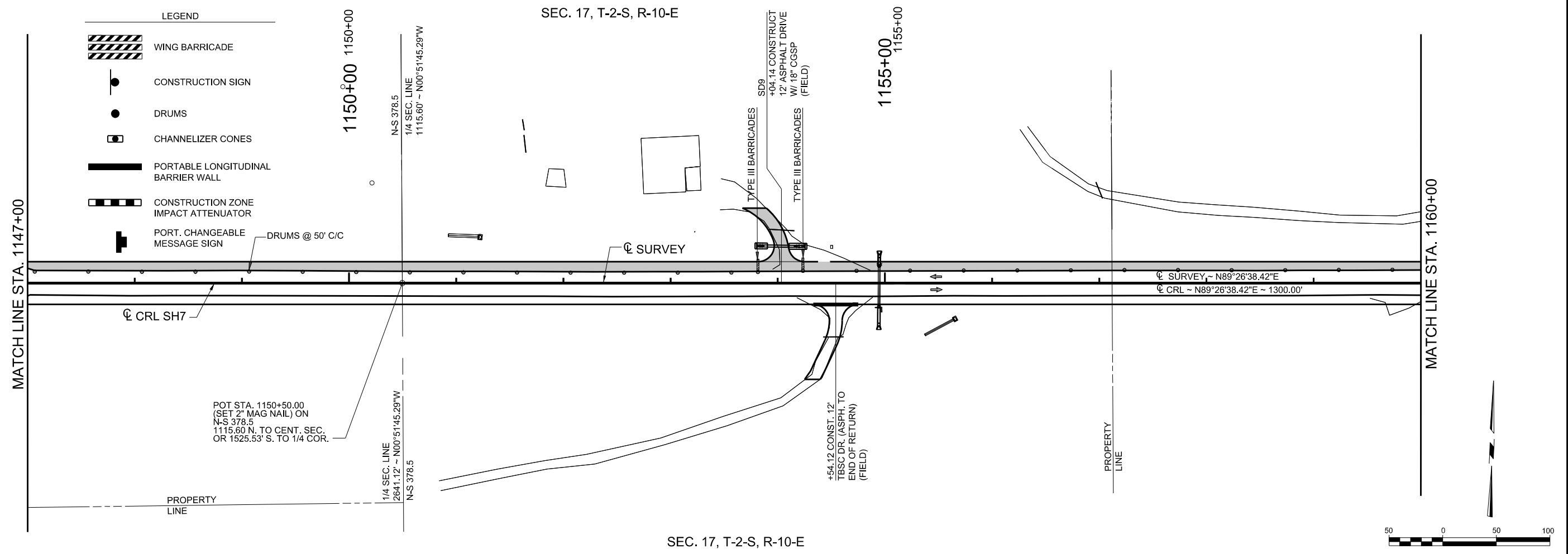
PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\DGNT\016-24066(04)-Const\Sec Phase 2_4.dgn

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (4) PHASE 2 JOB PIECE NO. 24066 (04) SHEET NO. T016
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\650-SH7_Final Plans\DWG\T017-24066(04)-Const_Seq Phase 2_5.dwg

LEGEND

	WING BARRICADE
	CONSTRUCTION SIGN
	DRUMS
	CHANNELIZER CONES
	PORTABLE LONGITUDINAL BARRIER WALL
	CONSTRUCTION ZONE IMPACT ATTENUATOR
	PORT. CHANGEABLE MESSAGE SIGN



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (5) PHASE 2 JOB PIECE NO. 24066 (04) SHEET NO. T017
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

SEC. 17, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E

MATCH LINE STA. 1173+00

1175+00

1180+00

1185+00

MATCH LINE STA. 1186+00

SEC. 17, T-2-S, R-10-E

SEC. 16, T-2-S, R-10-E



MATCH LINE STA. 1186+00

1190+00

SEC. 16, T-2-S, R-10-E

1195+00

MATCH LINE STA. 1199+00

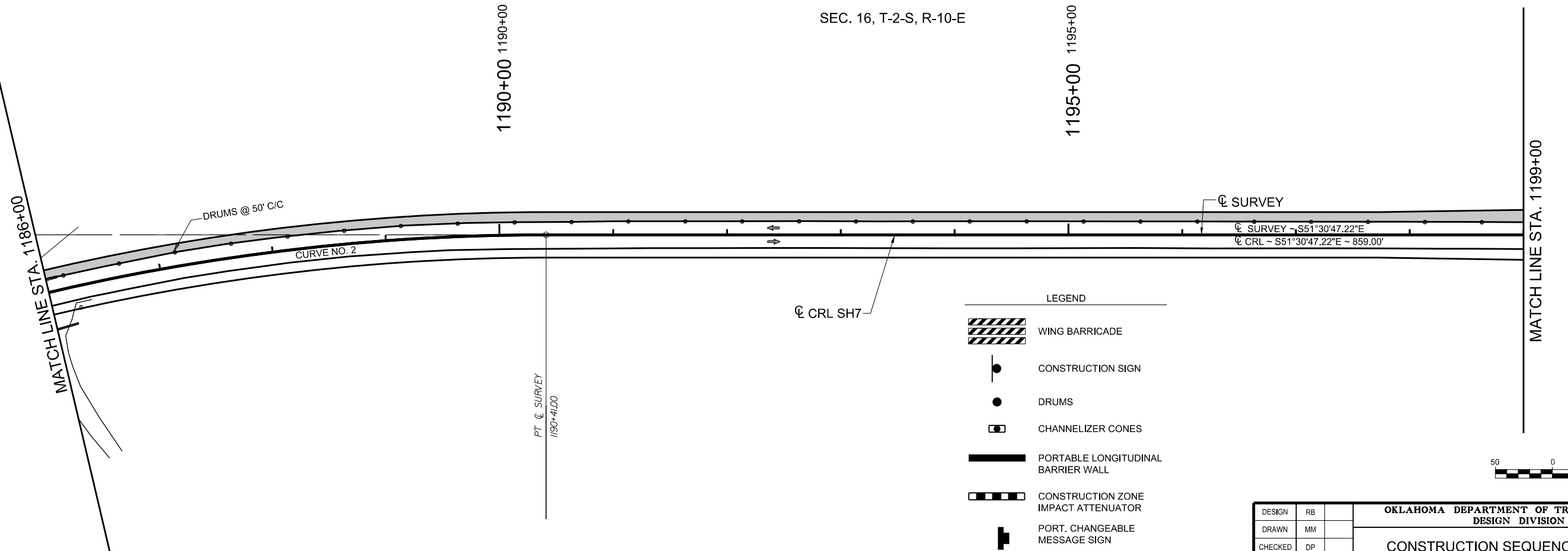
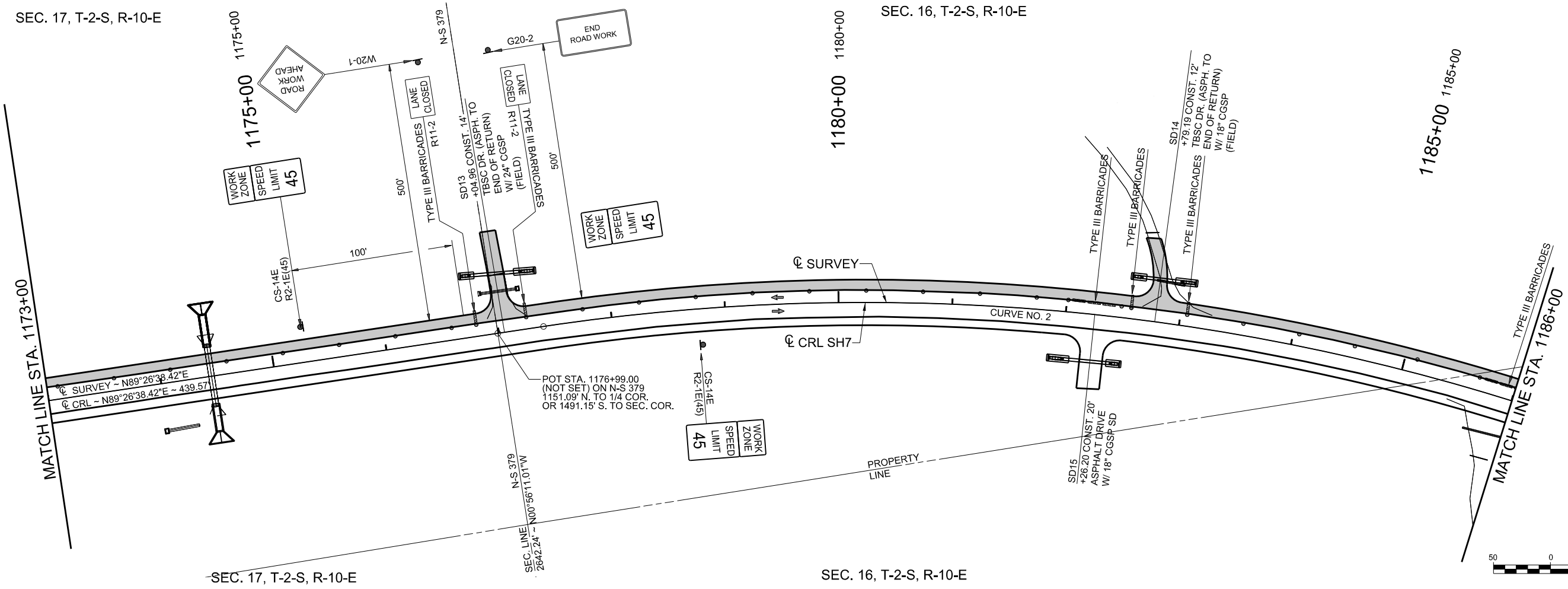
SEC. 16, T-2-S, R-10-E



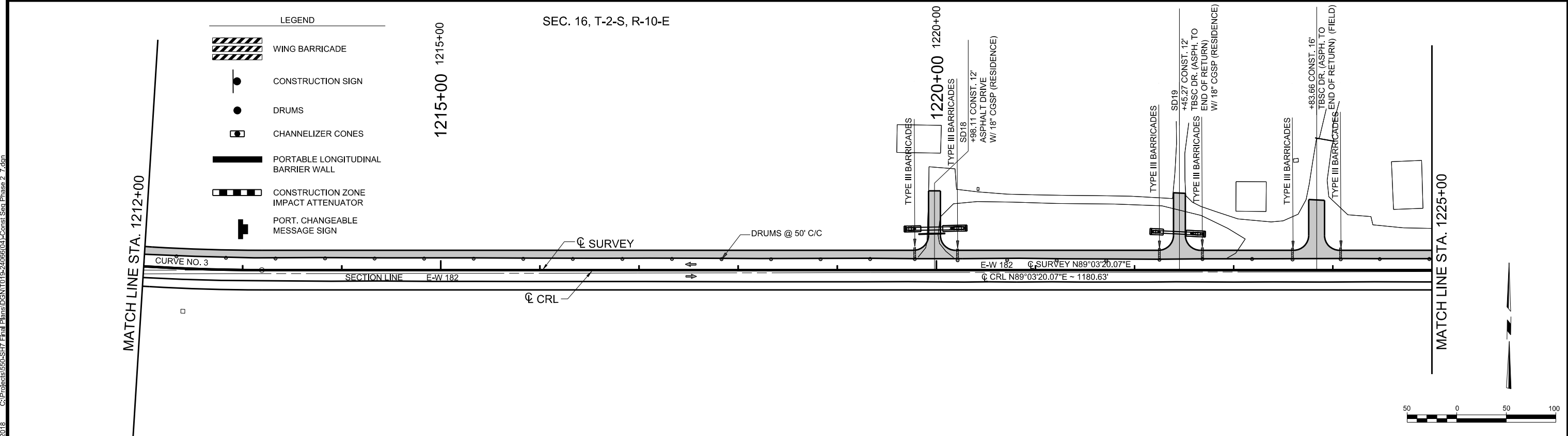
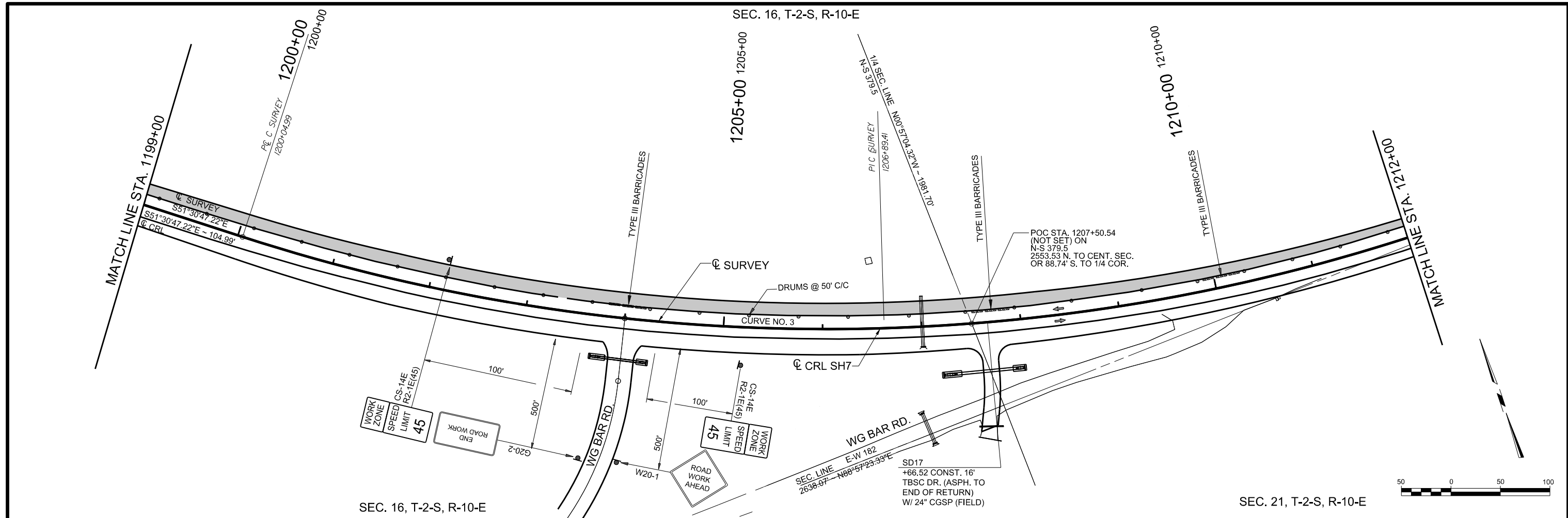
- LEGEND
- WING BARRICADE
 - CONSTRUCTION SIGN
 - DRUMS
 - CHANNELIZER CONES
 - PORTABLE LONGITUDINAL BARRIER WALL
 - CONSTRUCTION ZONE IMPACT ATTENUATOR
 - PORT. CHANGEABLE MESSAGE SIGN

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (6) PHASE 2 JOB PIECE NO. 24066 (04) SHEET NO. T018
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\550-SHT Final Plans\DGNT\018-24066(04)-Const_Seq Phase 2_6.dgn



PRINT DATE: 5/18/2018 C:\Projects\150-SHT Final Plans\DGNT\019-24066(04)-Const. Seq. Phase 2_7.dgn

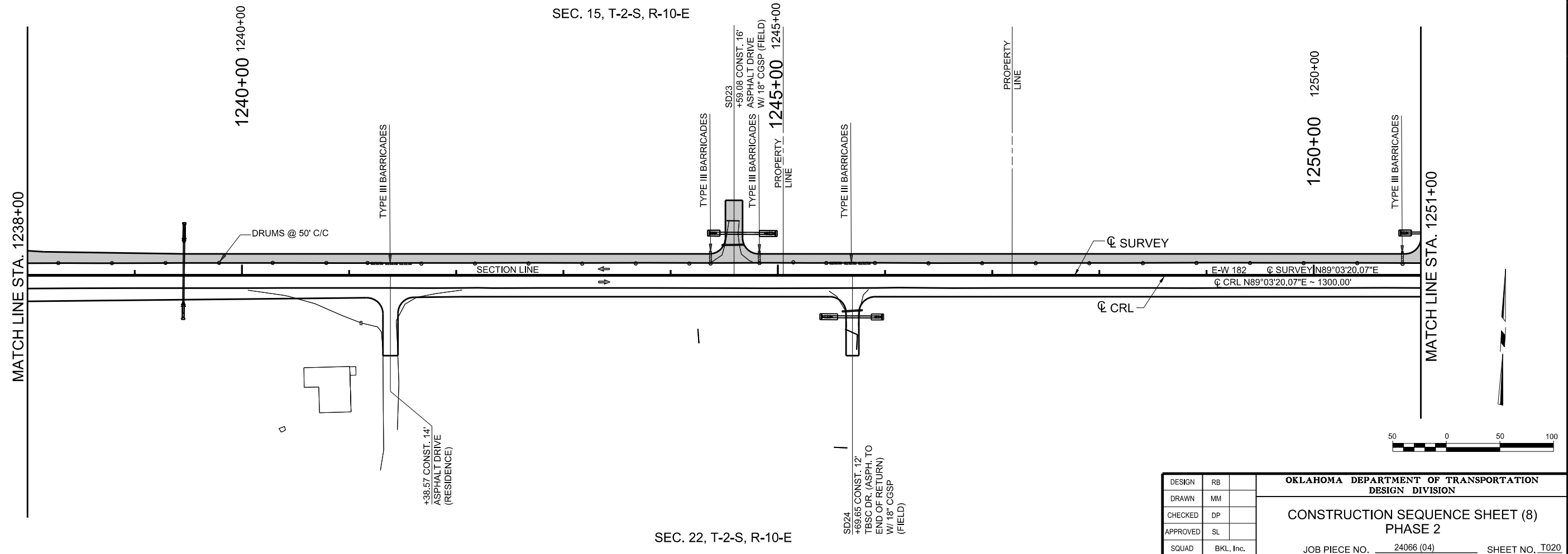
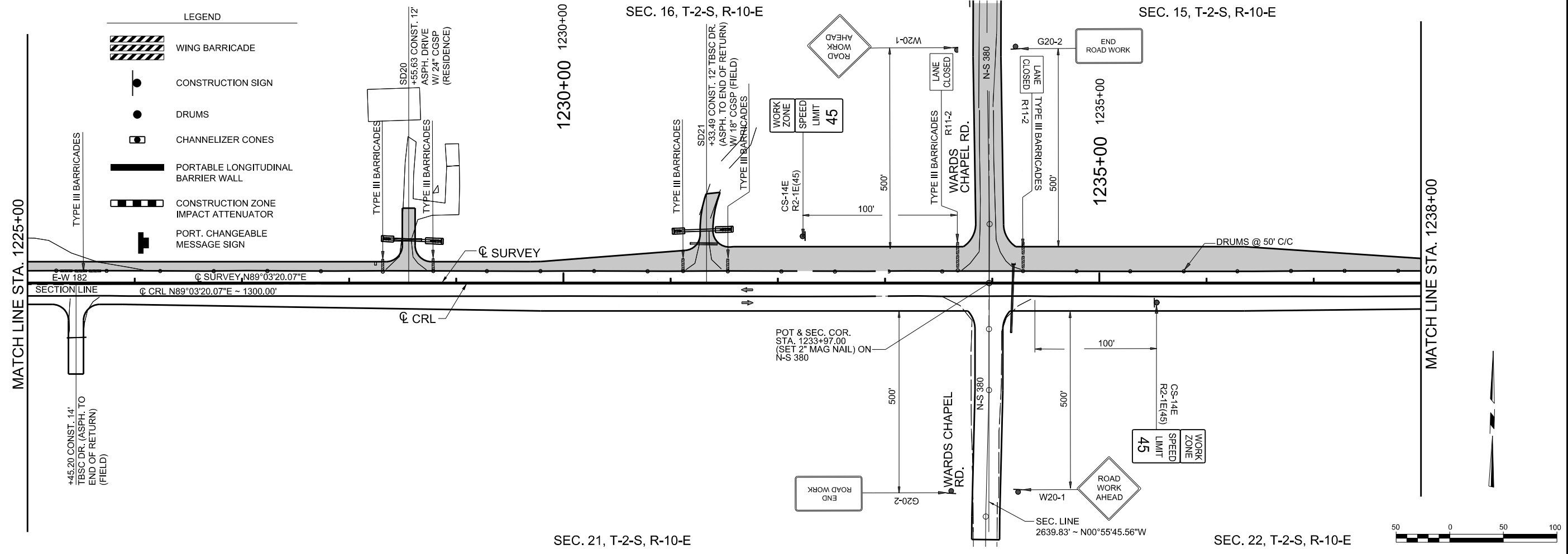


LEGEND

	WING BARRICADE
	CONSTRUCTION SIGN
	DRUMS
	CHANNELIZER CONES
	PORTABLE LONGITUDINAL BARRIER WALL
	CONSTRUCTION ZONE IMPACT ATTENUATOR
	PORT. CHANGEABLE MESSAGE SIGN

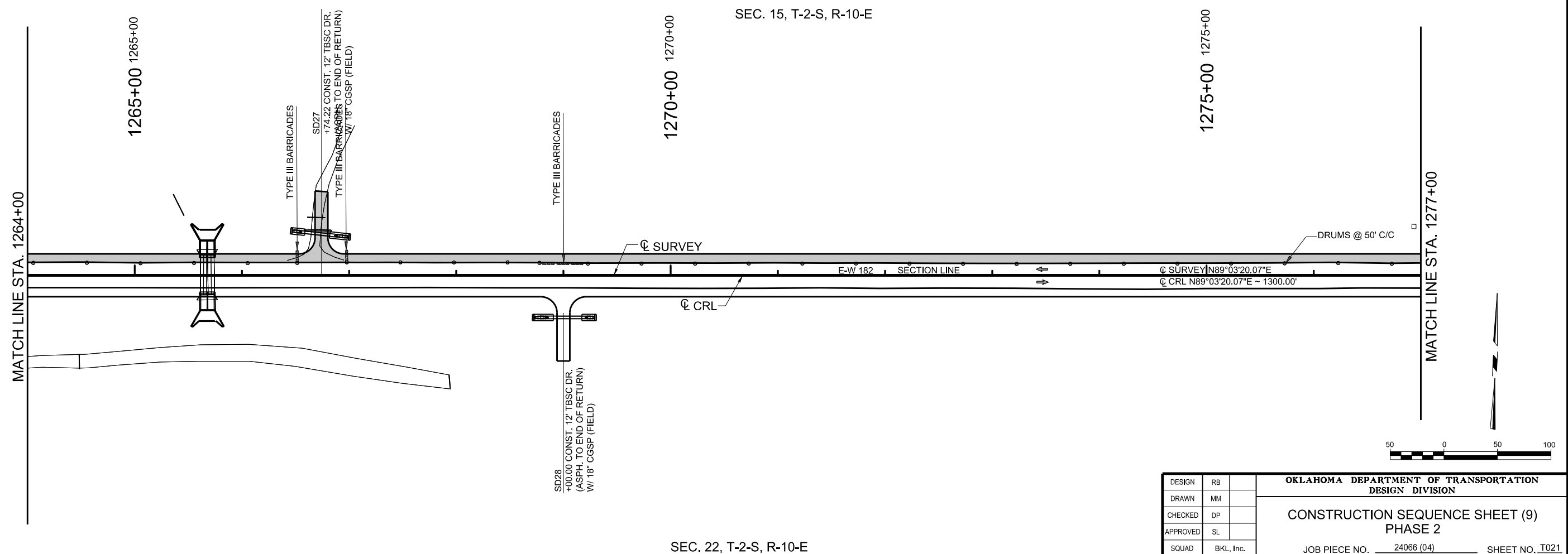
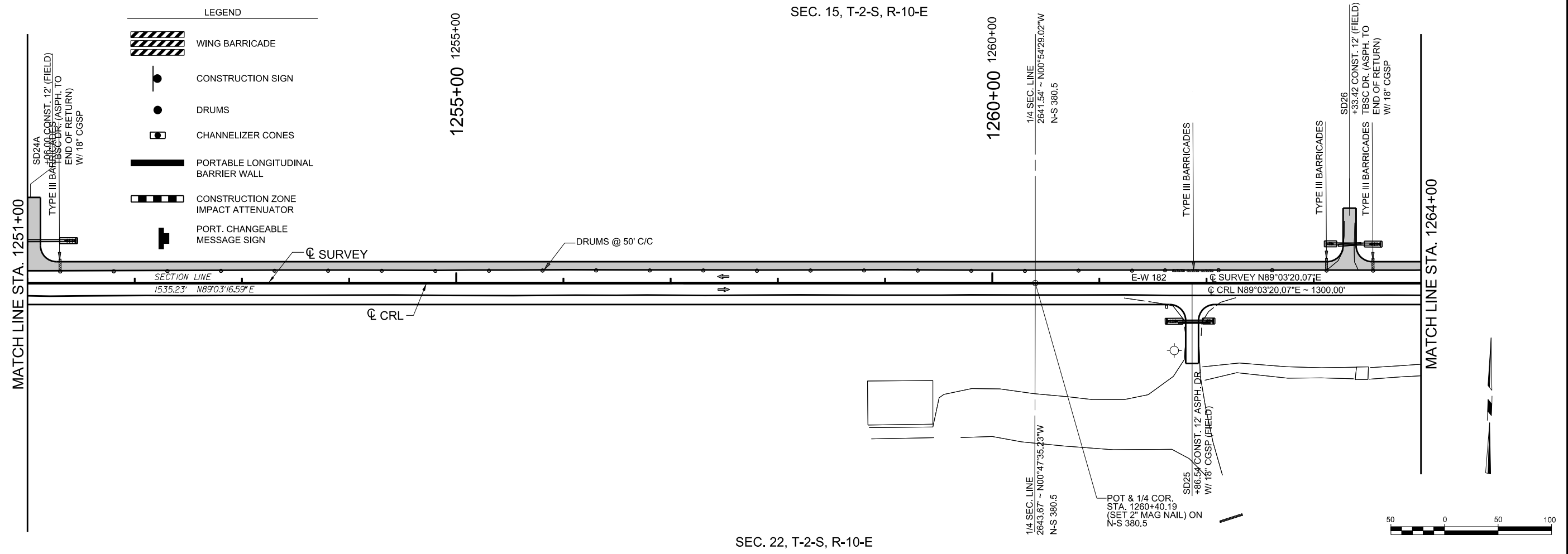
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DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/18/2018 C:\Projects\150-SHT Final Plans\DGANT\02-240666\04-Const Seq Phase 2_8.dgn



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (8) PHASE 2 JOB PIECE NO. 24066 (04) SHEET NO. T020
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

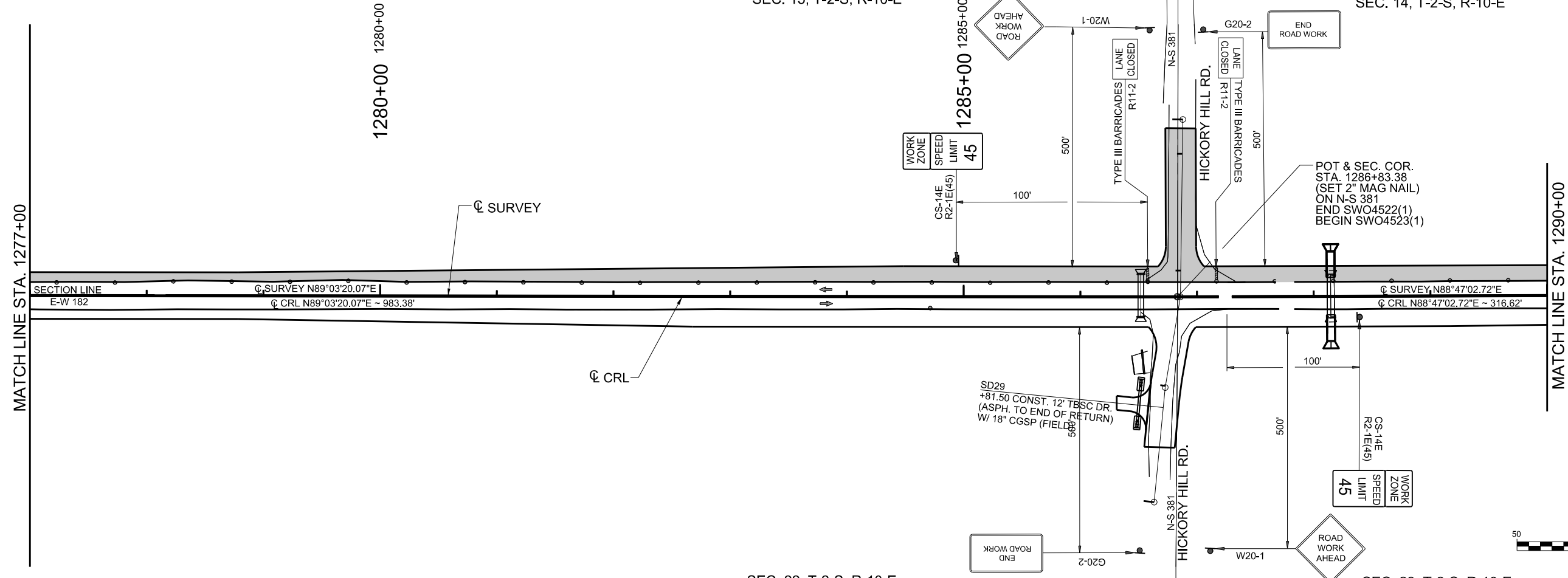
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DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (9) PHASE 2 JOB PIECE NO. 24066 (04) SHEET NO. T021
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

SEC. 15, T-2-S, R-10-E

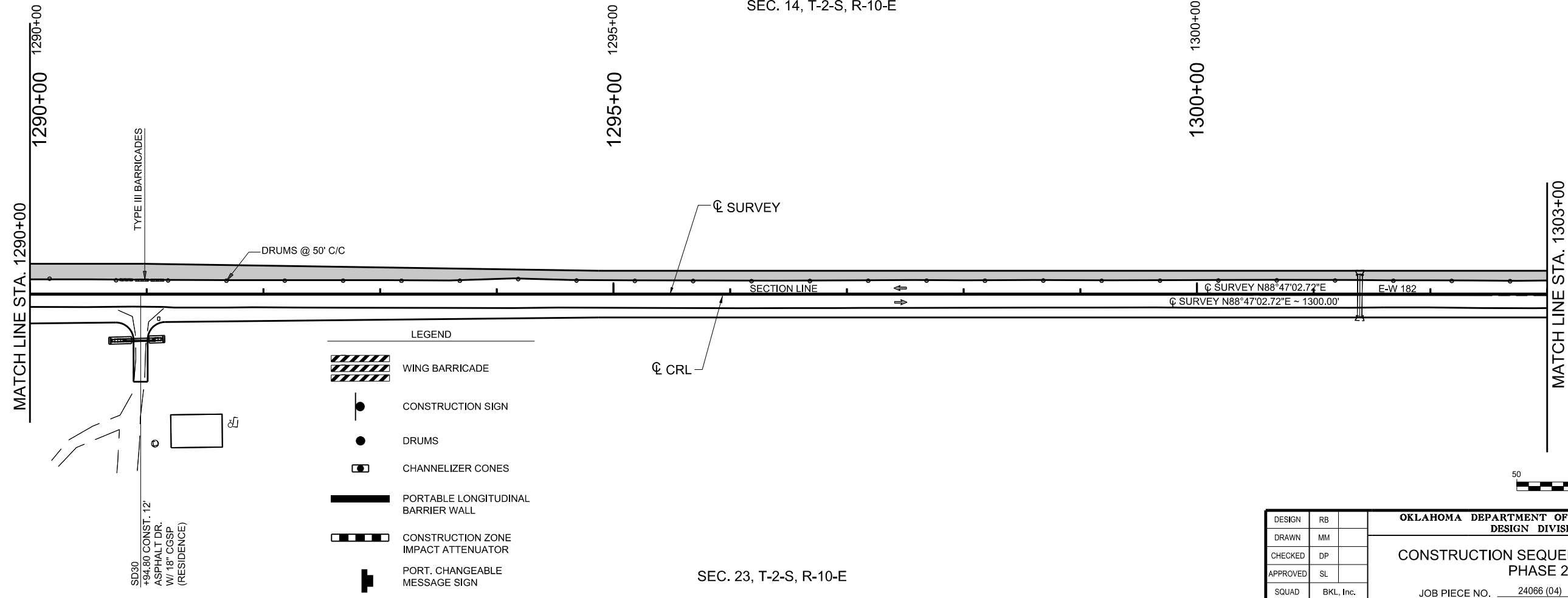
SEC. 14, T-2-S, R-10-E



SEC. 22, T-2-S, R-10-E

SEC. 23, T-2-S, R-10-E

SEC. 14, T-2-S, R-10-E



SEC. 23, T-2-S, R-10-E

- LEGEND
- WING BARRICADE
 - CONSTRUCTION SIGN
 - DRUMS
 - CHANNELIZER CONES
 - PORTABLE LONGITUDINAL BARRIER WALL
 - CONSTRUCTION ZONE IMPACT ATTENUATOR
 - PORT. CHANGEABLE MESSAGE SIGN

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (10) PHASE 2 JOB PIECE NO. 24066 (04) SHEET NO. T022
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\022-24066(04)-Const Seq Phase2_10.dgn

SEC. 14, T-2-S, R-10-E

MATCH LINE STA. 1303+00

1305+00 1305+00

TYPE III BARRICADES

☉ SURVEY

DRUMS @ 50' C/C

1310+00 1310+00

TYPE III BARRICADES

SECTION LINE E-W 182

TYPE III BARRICADES

SD31
+90.90 CONST. 12" TBSC DR.
(ASPH. TO END OF RETURN)
W/ 2 - 18" RCP (FIELD)

TYPE III BARRICADES

TYPE III BARRICADES

+40.00 CONST. 12" TBSC DR.
(ASPH. TO END OF RETURN)
W/ 2 - 18" RCP (FIELD)

TYPE III BARRICADES

SD32

+40.00 CONST. 12" TBSC DR.
(ASPH. TO END OF RETURN)
W/ 2 - 18" RCP (FIELD)

TYPE III BARRICADES

1315+00 1315+00

MATCH LINE STA. 1316+00

+80.31 CONST.
32' ASPH. DR.
W/ EXIST. SD
(COMMERCIAL)

+85.43 CONST.
28' ASPH. DR.
W/ EXIST. SD
(COMMERCIAL)

+14.37 CONST.
30' ASPH. DR.
W/ EXIST. SD
(COMMERCIAL)

+00.00 CONST. 34' ASPH. DR.
W/ EXIST. SD (COMMERCIAL)








1/4 SEC. LINE N-S 381.5
2646.47' - N00°43'54.51"W

POT STA. 1313+28.20
ON N-S 381.5
2.14 S. TO 1/4 COR.
OR 2639.24' N. TO
CENT. SEC.

SEC. 23, T-2-S, R-10-E



LEGEND

-  WING BARRICADE
-  CONSTRUCTION SIGN
-  DRUMS
-  CHANNELIZER CONES
-  PORTABLE LONGITUDINAL BARRIER WALL
-  CONSTRUCTION ZONE IMPACT ATTENUATOR
-  PORT. CHANGEABLE MESSAGE SIGN

1320+00 1320+00

SEC. 14, T-2-S, R-10-E

1325+00 1325+00

MATCH LINE STA. 1316+00

☉ SURVEY

DRUMS @ 50' C/C

SECTION LINE E-W 182

☉ SURVEY N88°47'02.72"E
☉ CRL N88°47'02.72"E ~ 1300.00'

☉ CRL

MATCH LINE STA. 1329+00

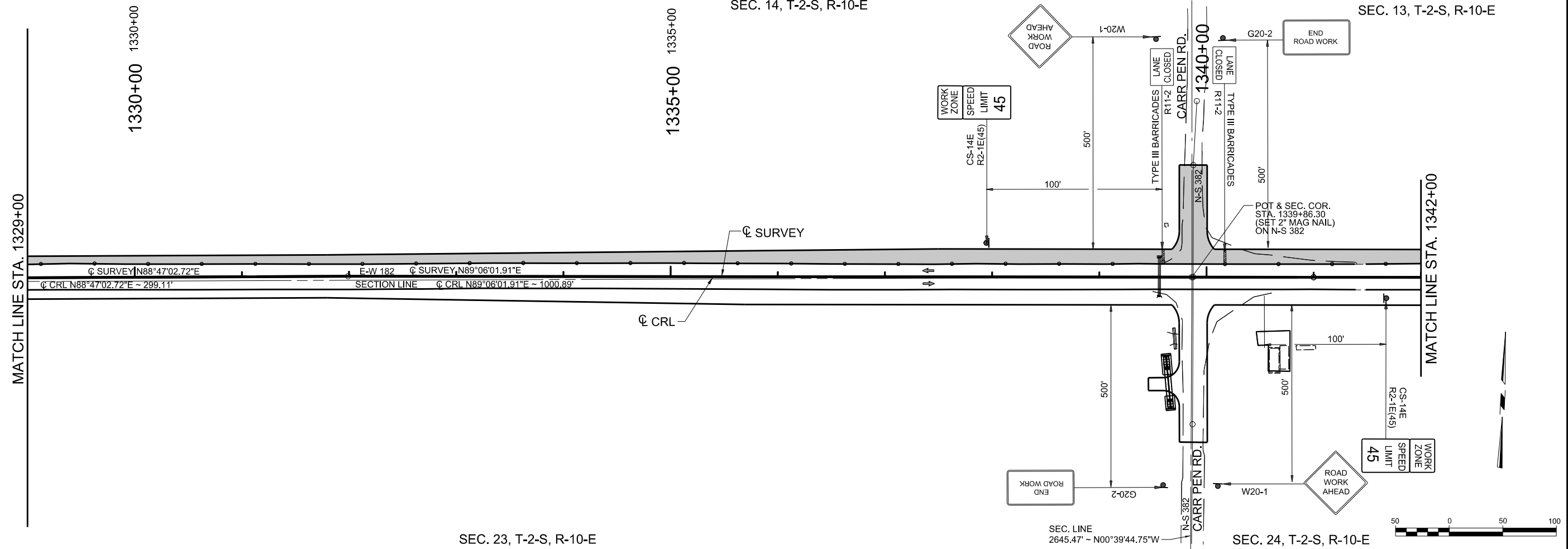


SEC. 23, T-2-S, R-10-E

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CONSTRUCTION SEQUENCE SHEET (11) PHASE 2 JOB PIECE NO. 24066 (04) SHEET NO. T023
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

SEC. 14, T-2-S, R-10-E

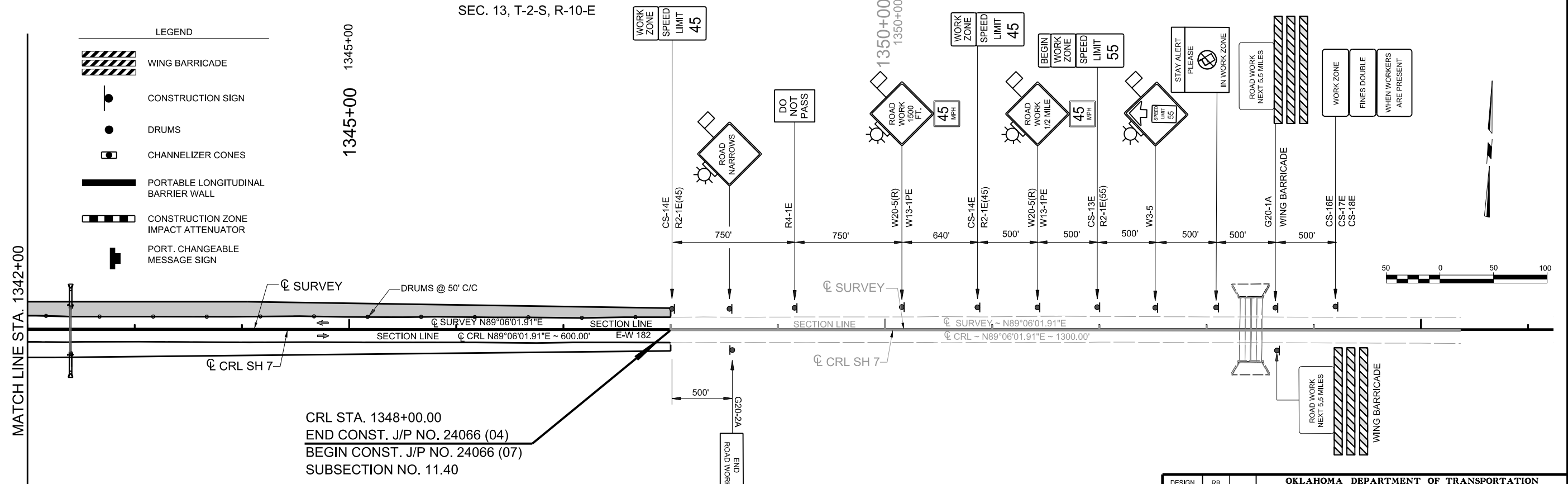
SEC. 13, T-2-S, R-10-E



SEC. 23, T-2-S, R-10-E

SEC. LINE 2645.47' - N00°39'44.75"W

SEC. 24, T-2-S, R-10-E



SEC. 13, T-2-S, R-10-E

CRL STA. 1348+00.00
 END CONST. J/P NO. 24066 (04)
 BEGIN CONST. J/P NO. 24066 (07)
 SUBSECTION NO. 11.40

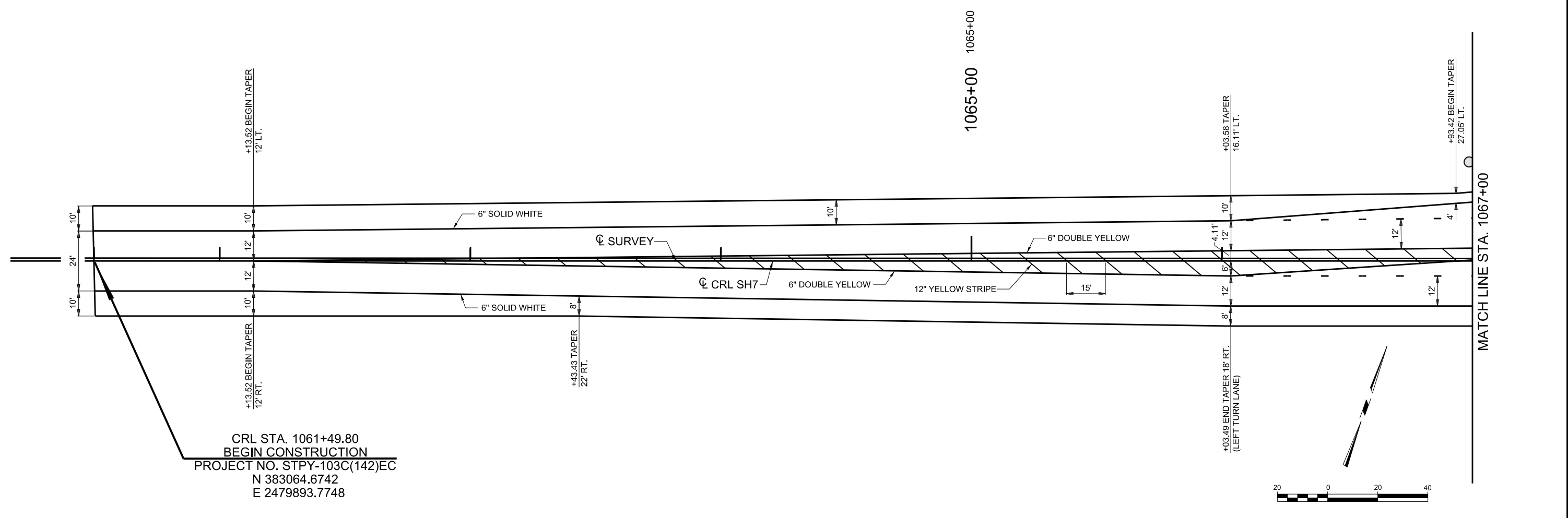
SEC. 24, T-2-S, R-10-E



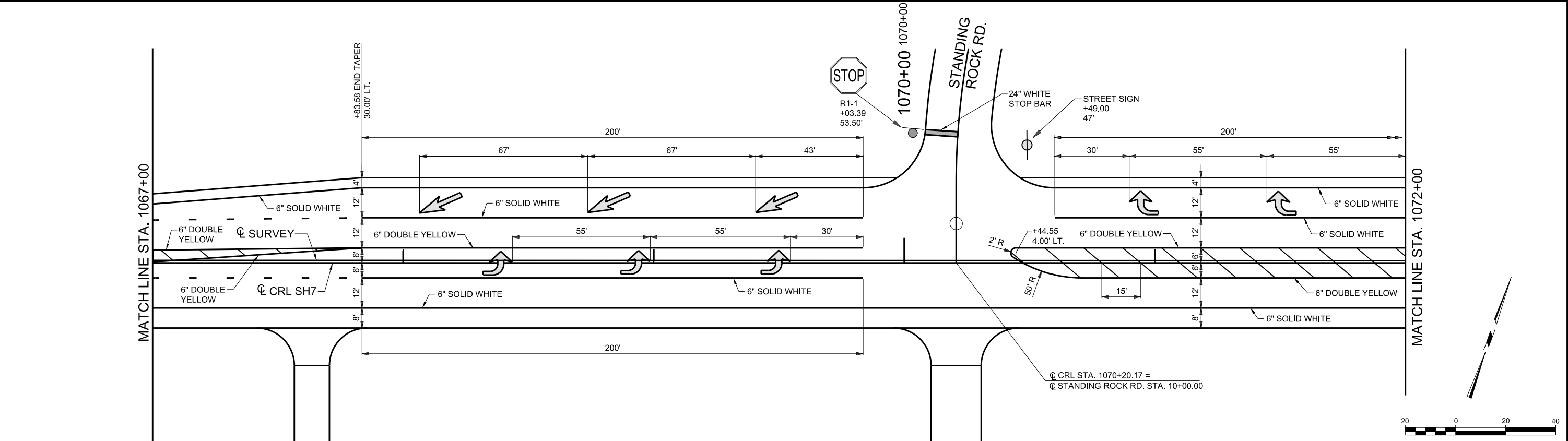
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DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/18/2018 C:\Projects\550-SHT Final Plans\OGNIT024-24066(04)-Const Seq Phase 2_12.dgn

PRINT DATE: 5/18/2018 C:\Projects\150-SHT Final Plans\025-24066(04)-STRIPING STA 1070_1.dgn



CRL STA. 1061+49.80
 BEGIN CONSTRUCTION
 PROJECT NO. STPY-103C(142)EC
 N 383064.6742
 E 2479893.7748



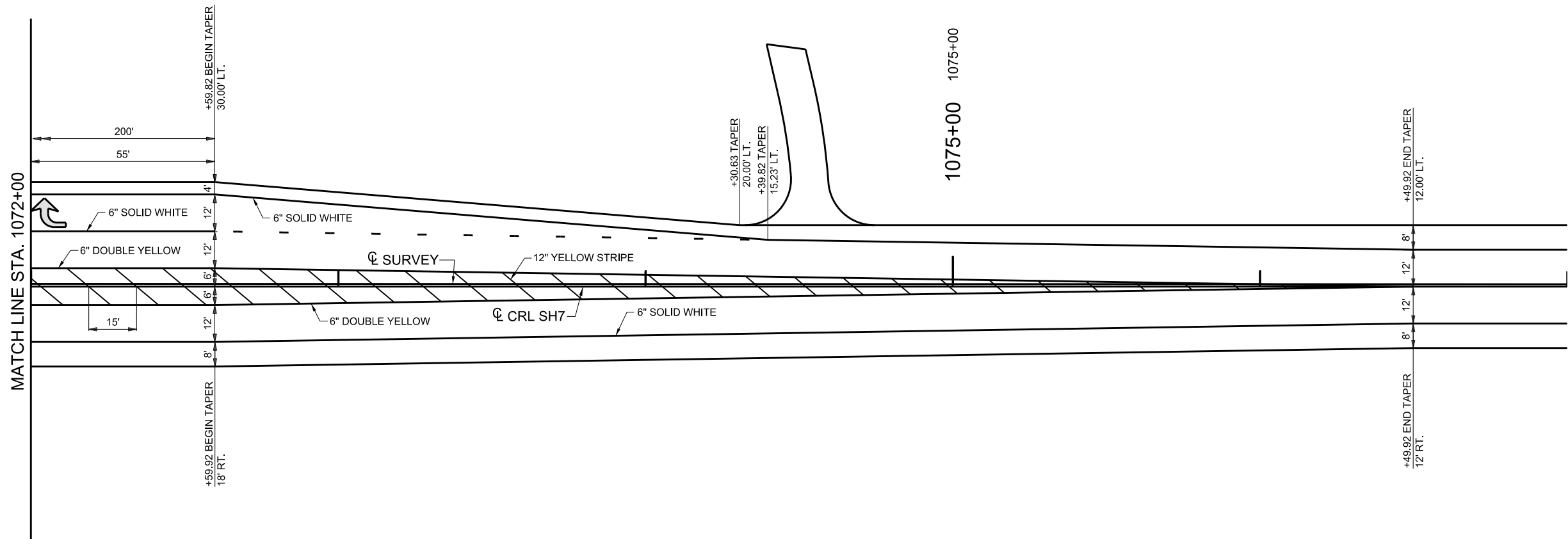
STOP
 R1-1
 +03.39
 53.50'

STANDING
 ROCK RD.

☉ CRL STA. 1070+20.17 =
 ☉ STANDING ROCK RD. STA. 10+00.00

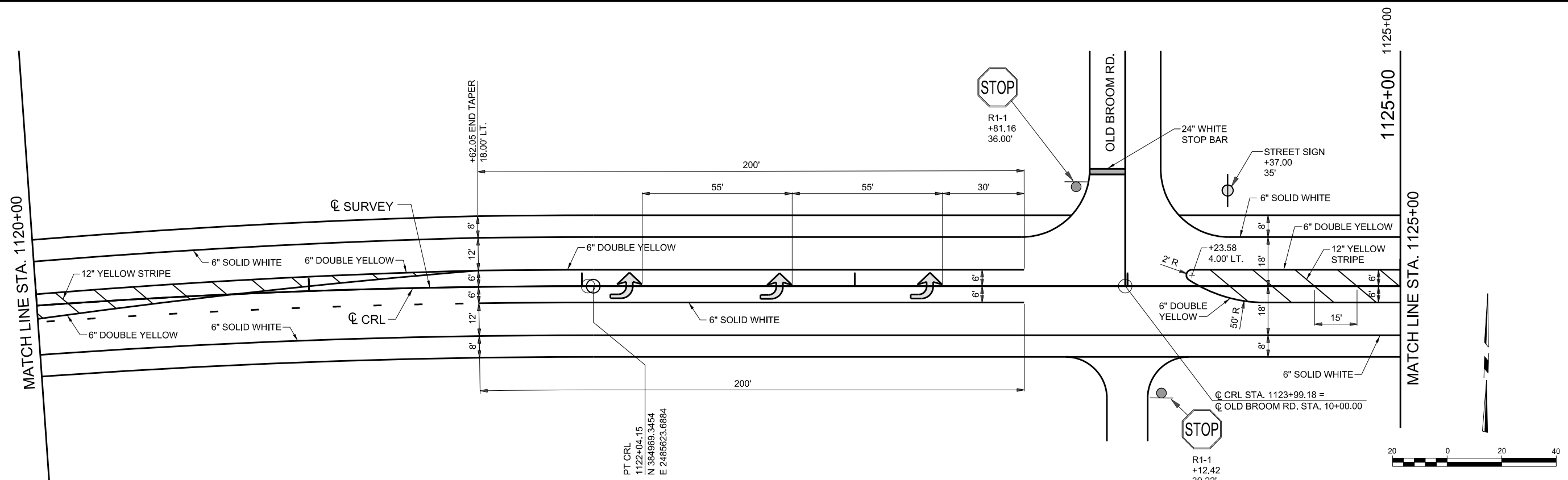
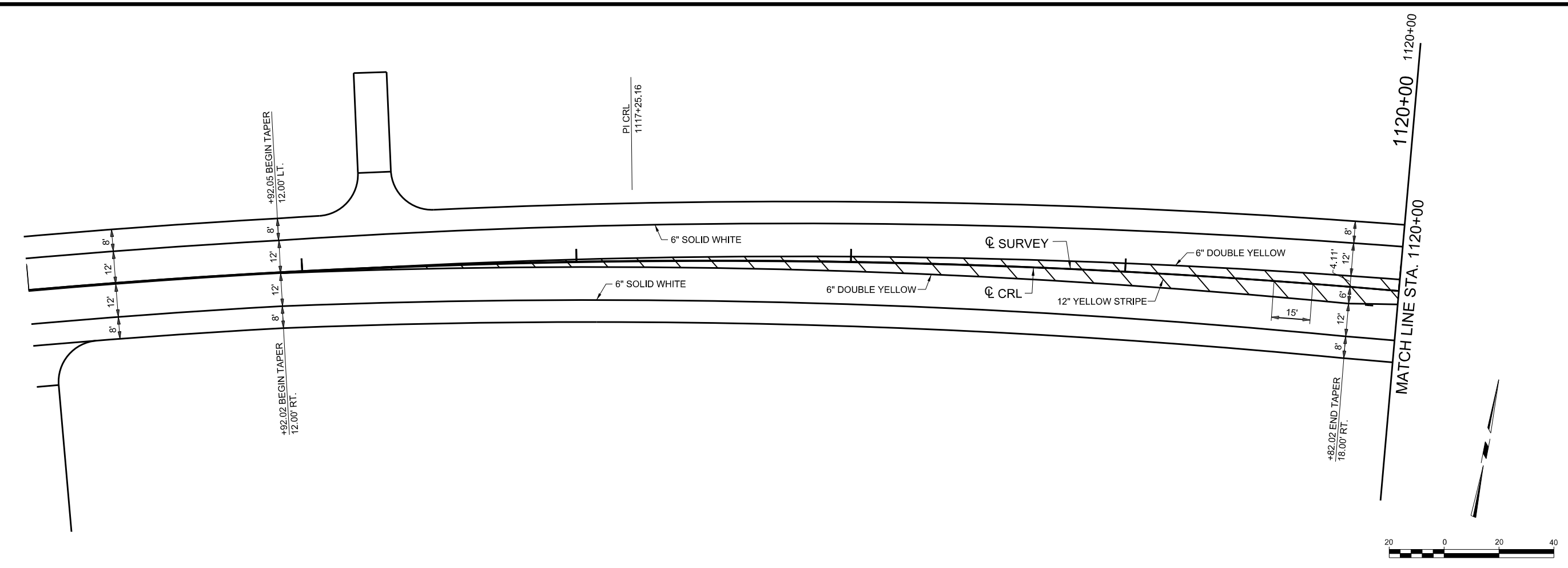
DESIGN	RB
DRAWN	MM
CHECKED	DP
APPROVED	SL
SQUAD	BKL, Inc.

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
 STRIPING DETAIL (1)
 STANDING ROCK RD.
 JOB PIECE NO. 24066 (04) SHEET NO. T025



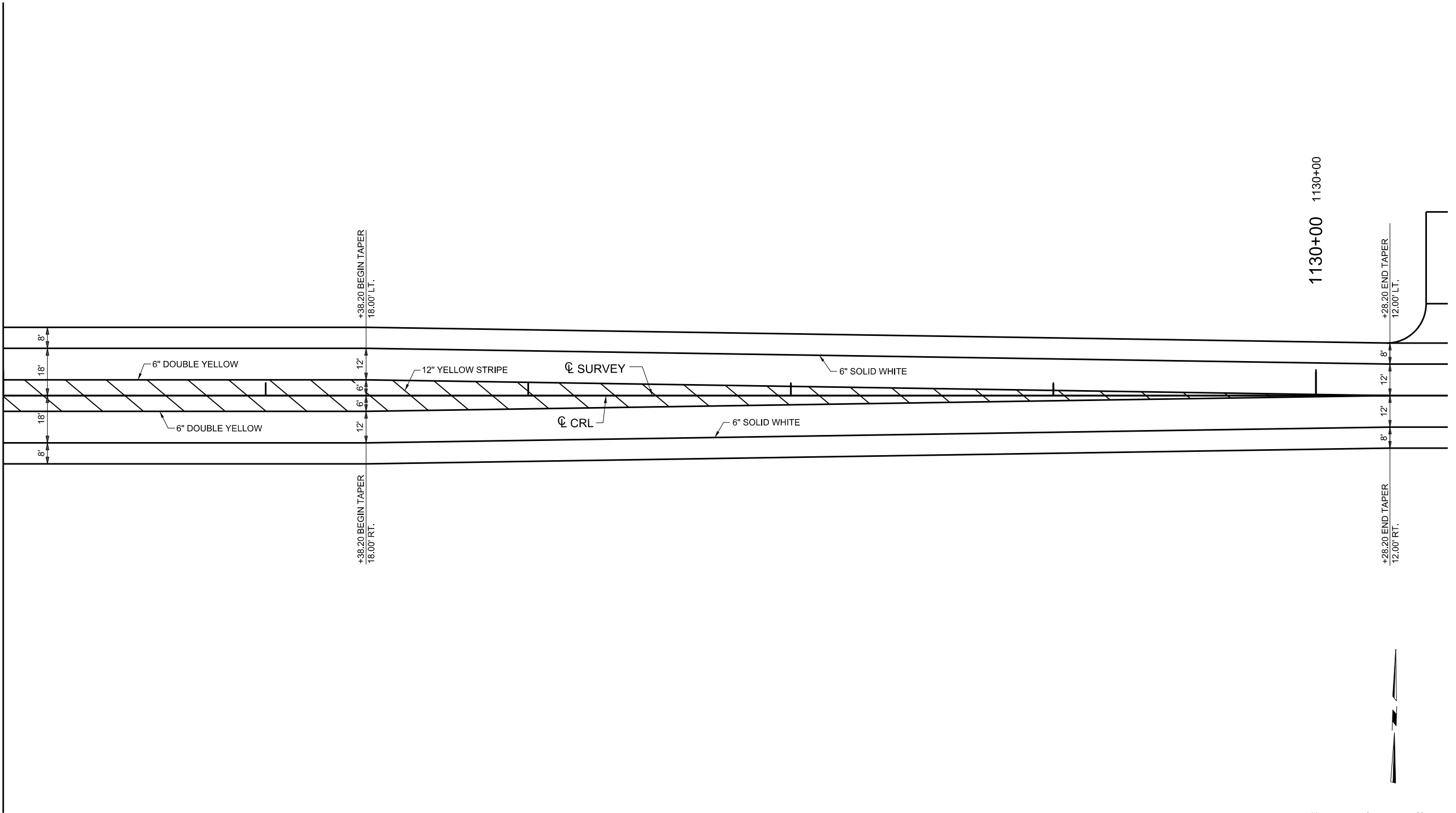
DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION STRIPING DETAIL (2) STANDING ROCK RD. JOB PIECE NO. 24066 (04) SHEET NO. T026
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\DO\1027-24066(04)-STRIPING STA 1124_1.dgn



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION STRIPING DETAIL (3) OLD BROOM RD. JOB PIECE NO. 24066 (04) SHEET NO. T027
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

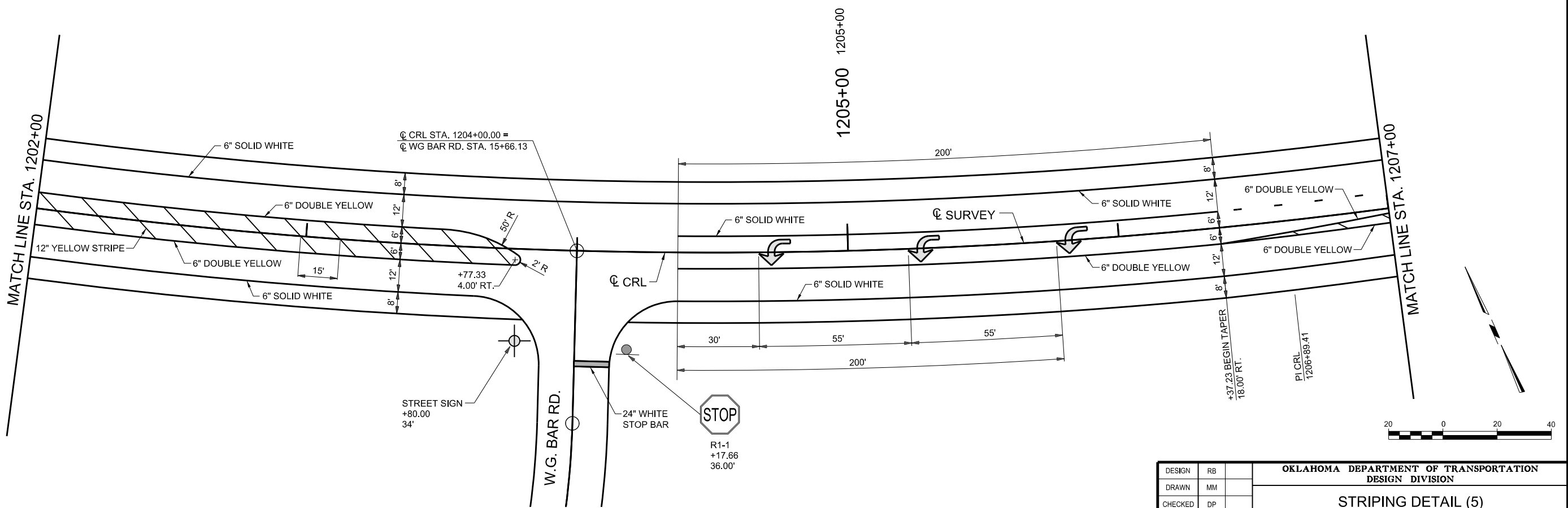
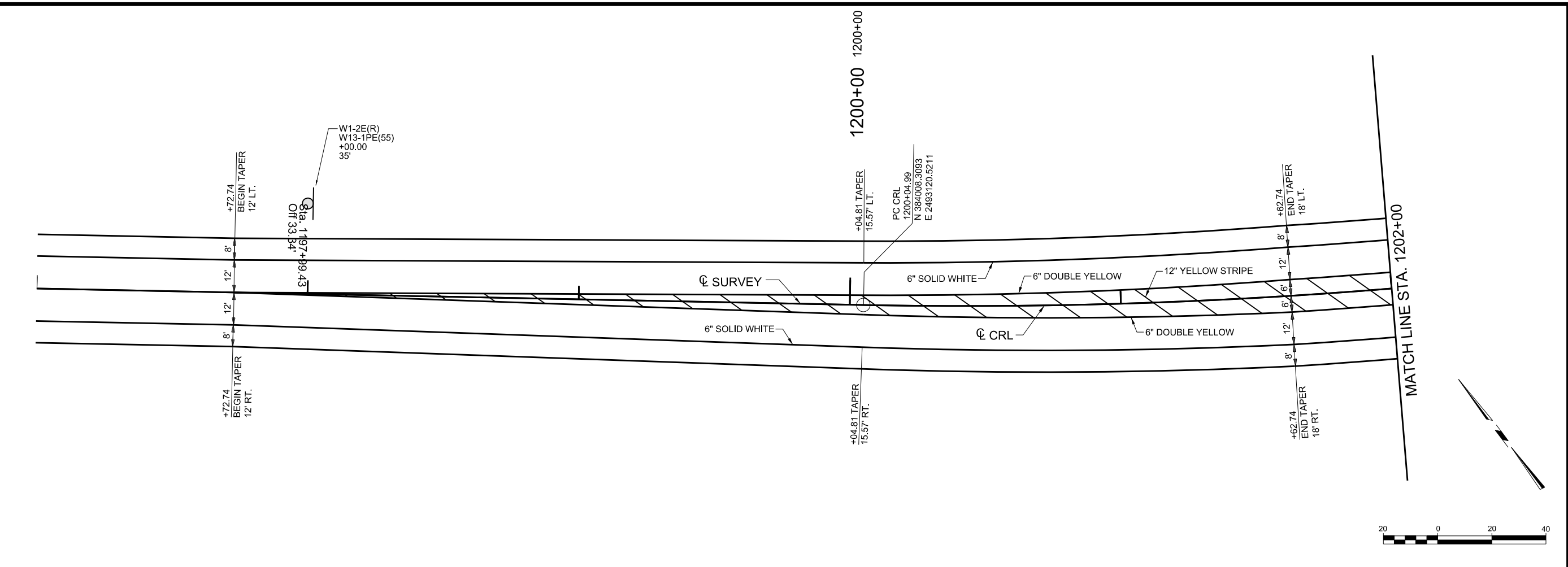
MATCH LINE STA. 1125+00



DESIGN	RB
DRAWN	MM
CHECKED	DP
APPROVED	SL
SQUAD	BKL, Inc.

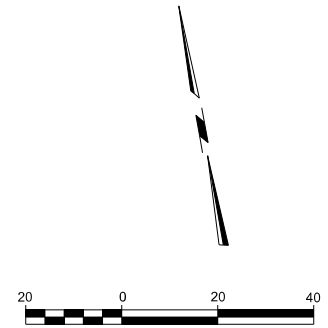
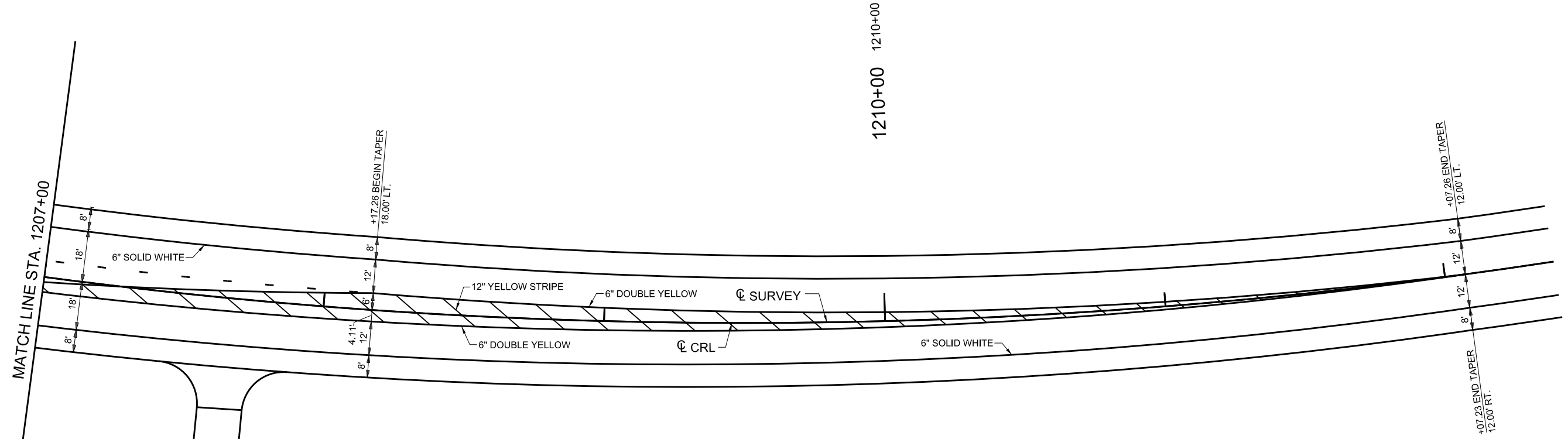
OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
STRIPING DETAIL (4)
 OLD BROOM RD.
 JOB PIECE NO. 24066 (04) SHEET NO. T028

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\DOT\029-240666(04)-STRIPING STA 1204_1.dgn



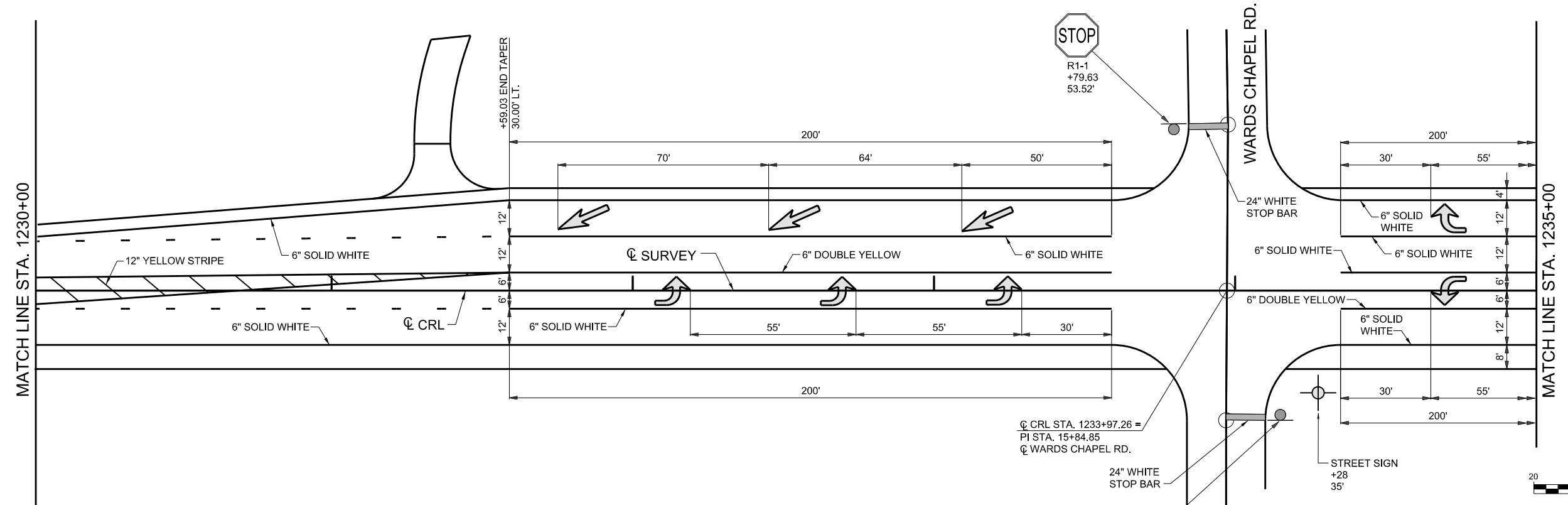
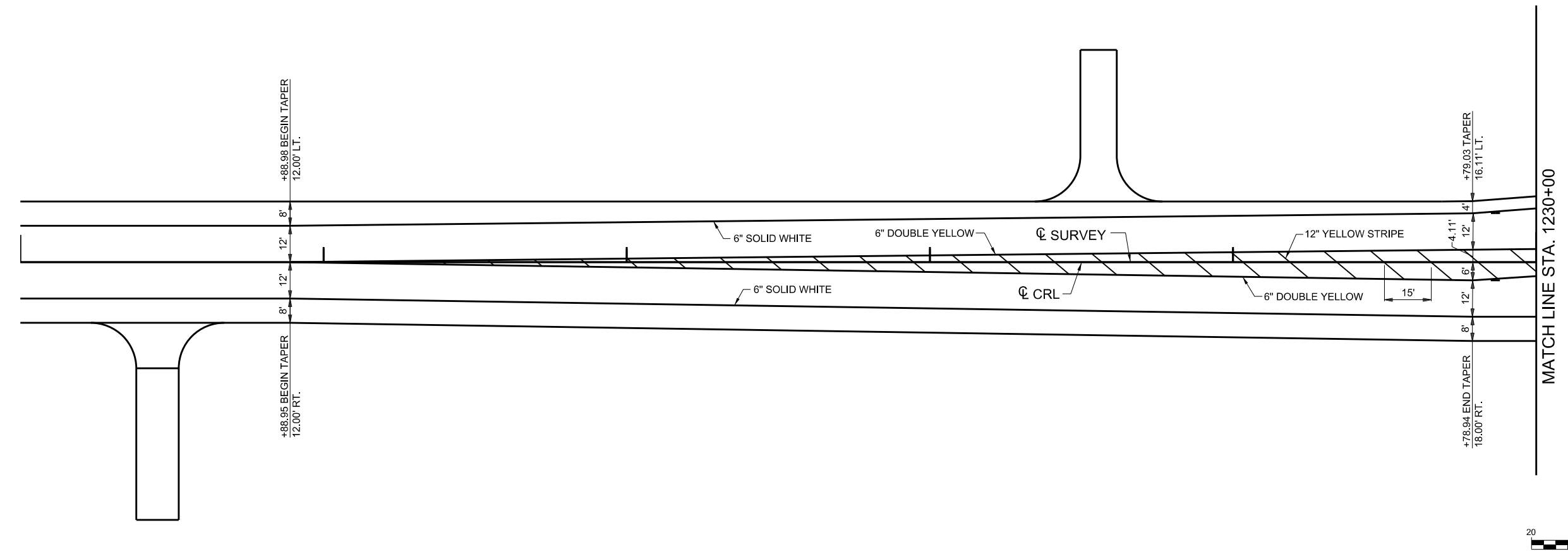
DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION STRIPING DETAIL (5) W.G. BAR RD. JOB PIECE NO. 24066 (04) SHEET NO. T029
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/25/2018 C:\Projects\650-SHT Final Plans\OGN\T030-24066(04)-STRIPING STA 1204_2.dgn



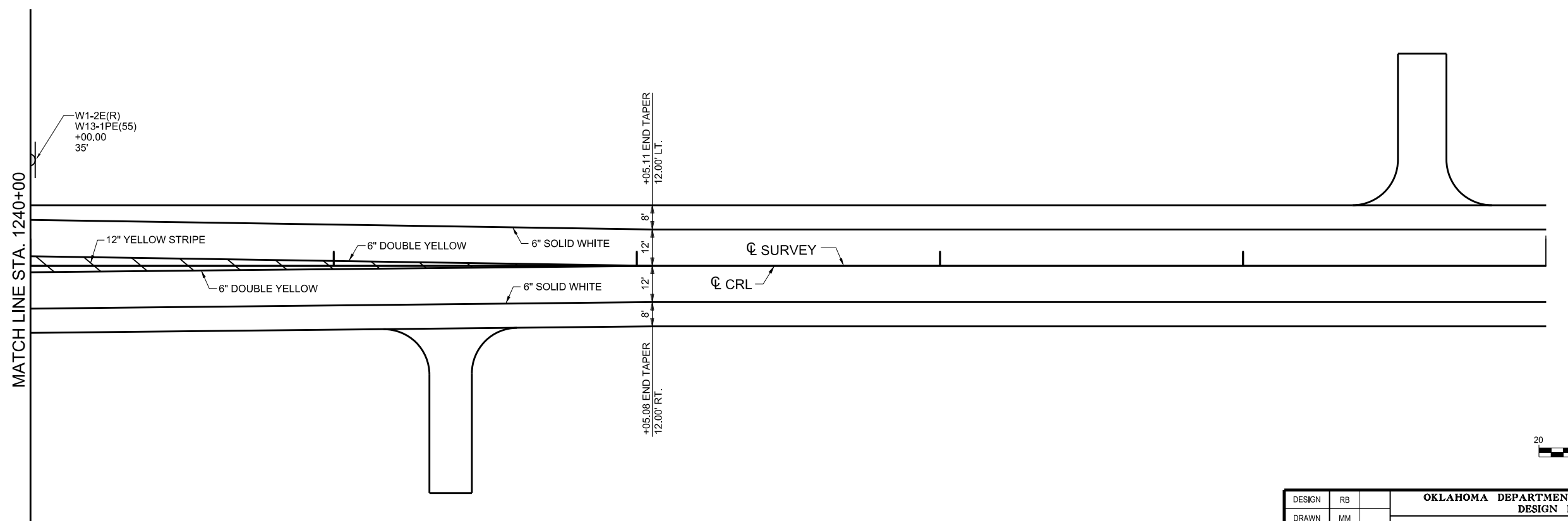
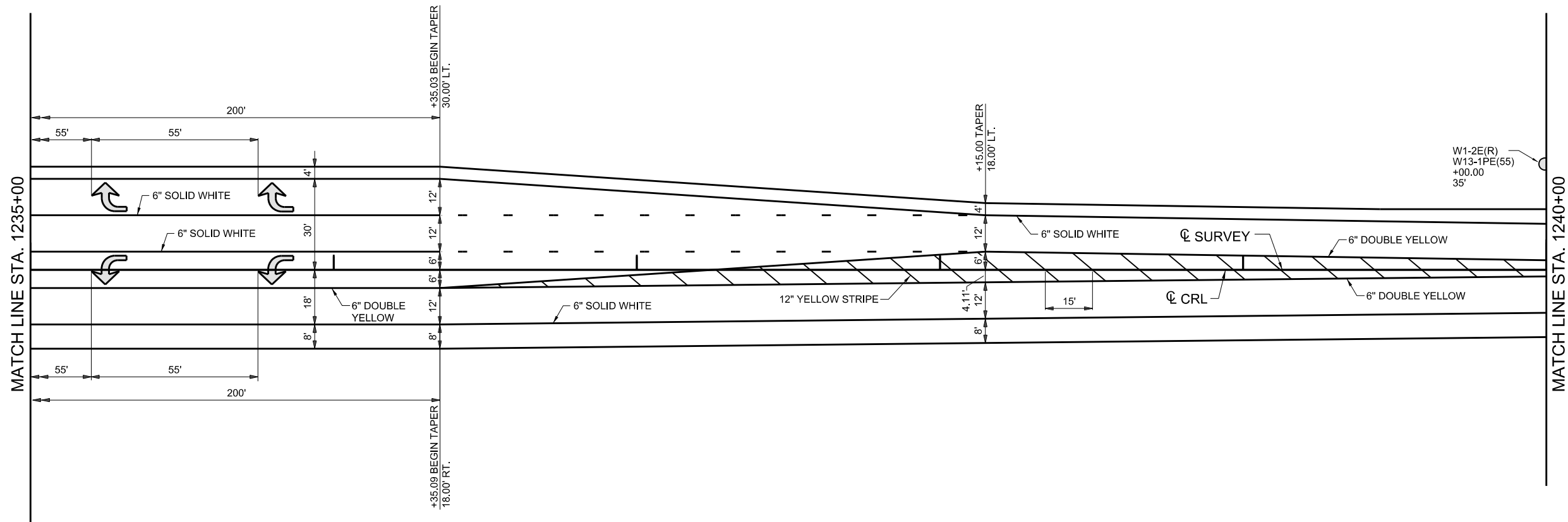
DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION STRIPING DETAIL (6) W.G. BAR RD. JOB PIECE NO. 24066 (04) SHEET NO. T030
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/18/2018 C:\Projects\500-SHT Final Plans\04-STRIPING STA 1234_1.dwg



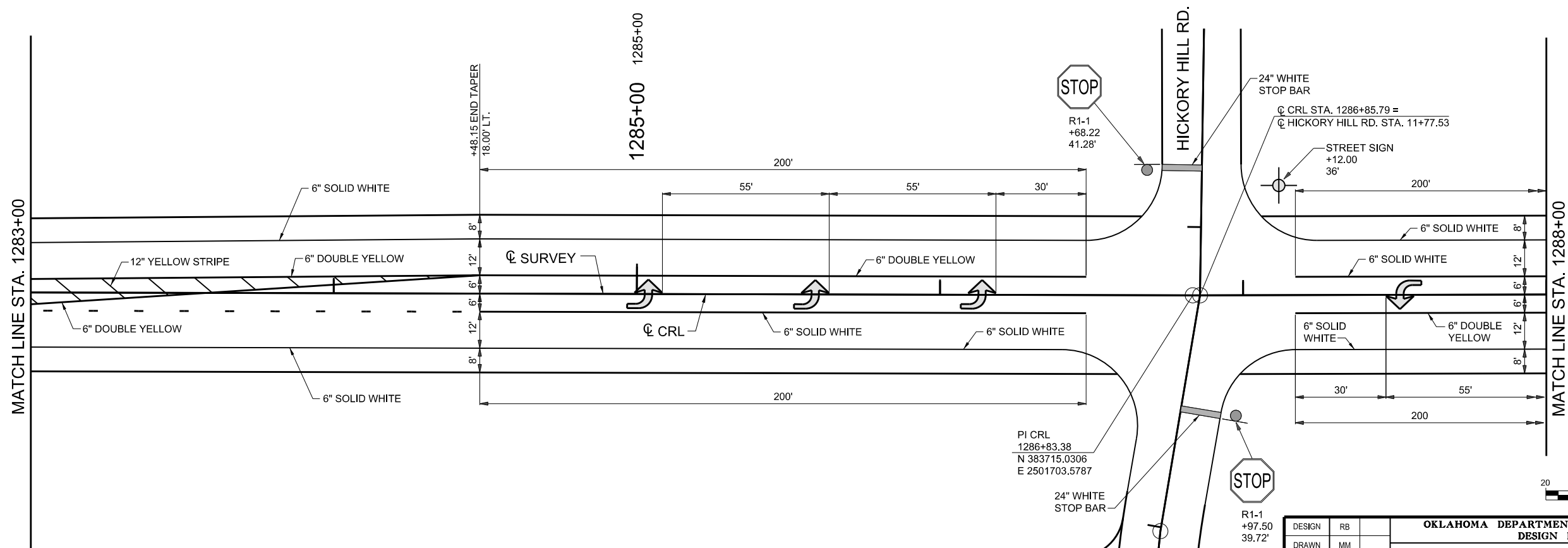
DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION STRIPING DETAIL (7) WARDS CHAPEL RD. JOB PIECE NO. 24066 (04) SHEET NO. T031
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\DO\NT\032-240666(04)-STRIPING STA 1234 2.dgn



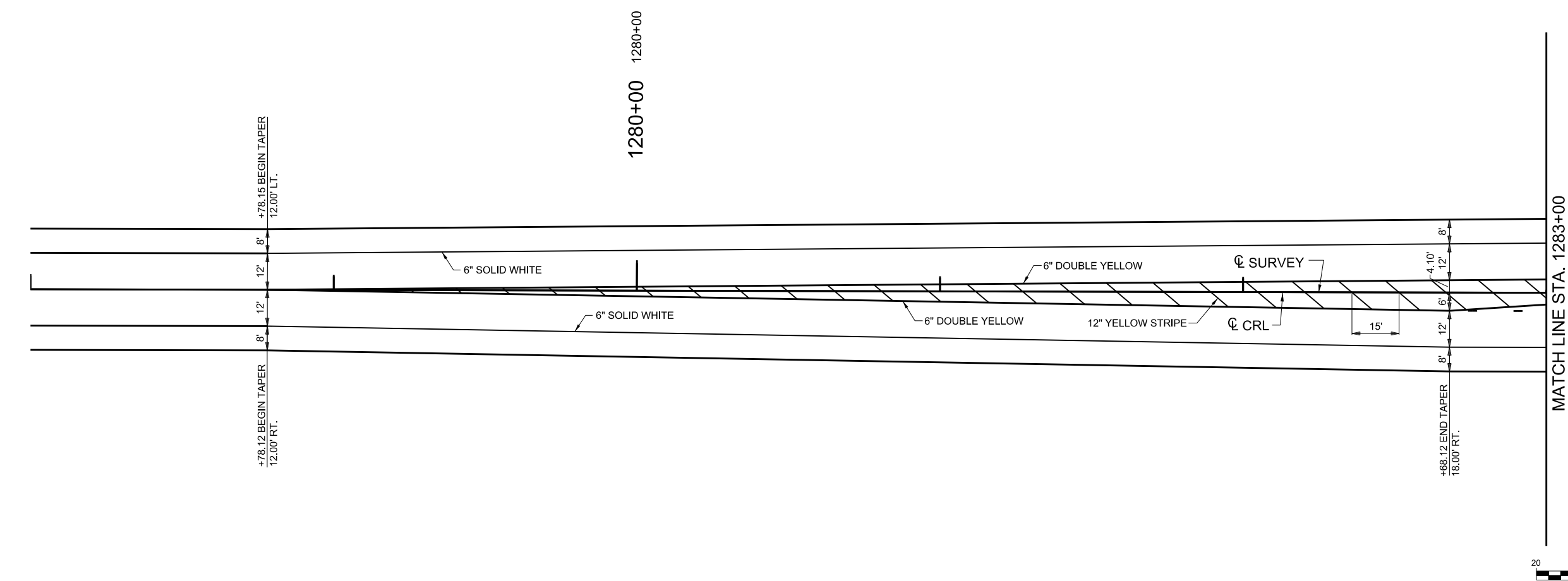
DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION STRIPING DETAIL (8) WARDS CHAPEL RD. JOB PIECE NO. 24066 (04) SHEET NO. T032
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\03-24066(04)-STRIPING STA 1287_1.dwg



DESIGN	RB	
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
STRIPING DETAIL (9)
HICKORY HILL RD.
 JOB PIECE NO. 24066 (04) SHEET NO. T033



1280+00 1280+00

MATCH LINE STA. 1283+00

MATCH LINE STA. 1283+00

MATCH LINE STA. 1288+00

HICKORY HILL RD.

STOP

STOP

PI CRL
 1286+83.38
 N 383715.0306
 E 2501703.5787

R1-1
 +97.50
 39.72'

24" WHITE
 STOP BAR

☉ CRL STA. 1286+85.79 =
 ☉ HICKORY HILL RD. STA. 11+77.53

STREET SIGN
 +12.00
 36'

+78.12 BEGIN TAPER
 12.00' RT.

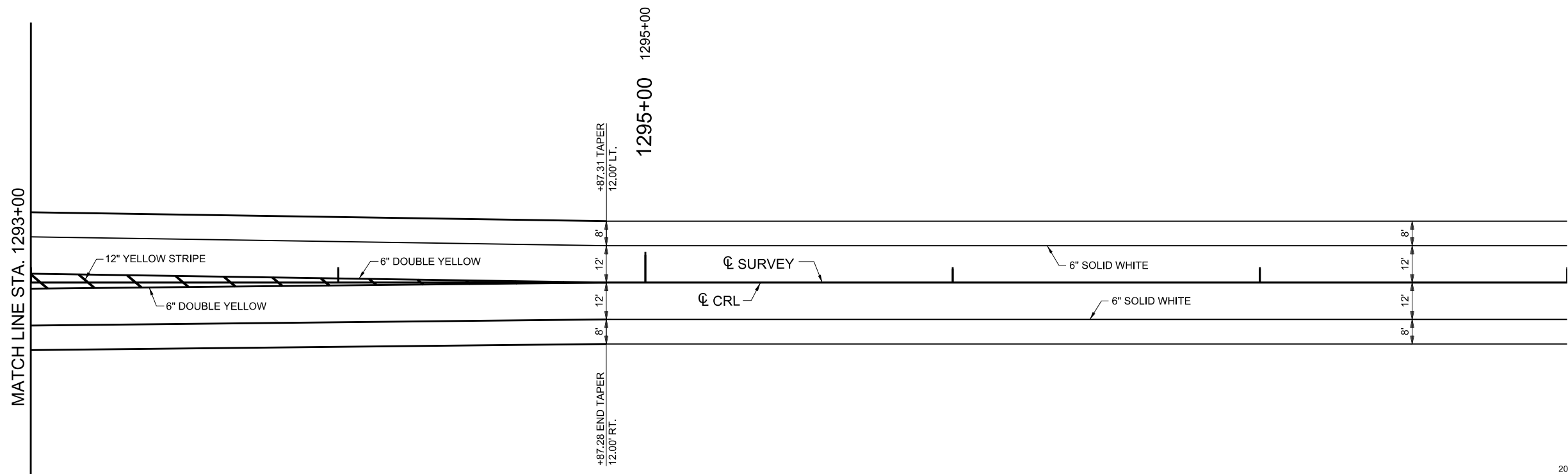
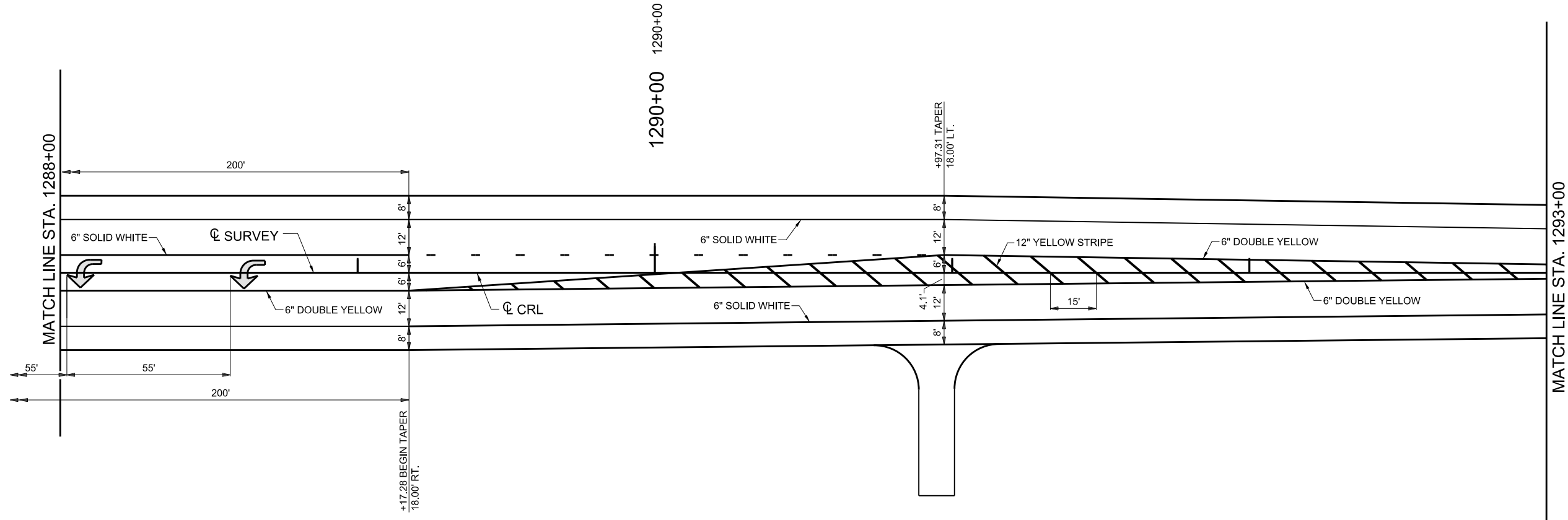
+78.15 BEGIN TAPER
 12.00' LT.

+68.12 END TAPER
 18.00' RT.

+48.15 END TAPER
 18.00' LT.

1285+00 1285+00

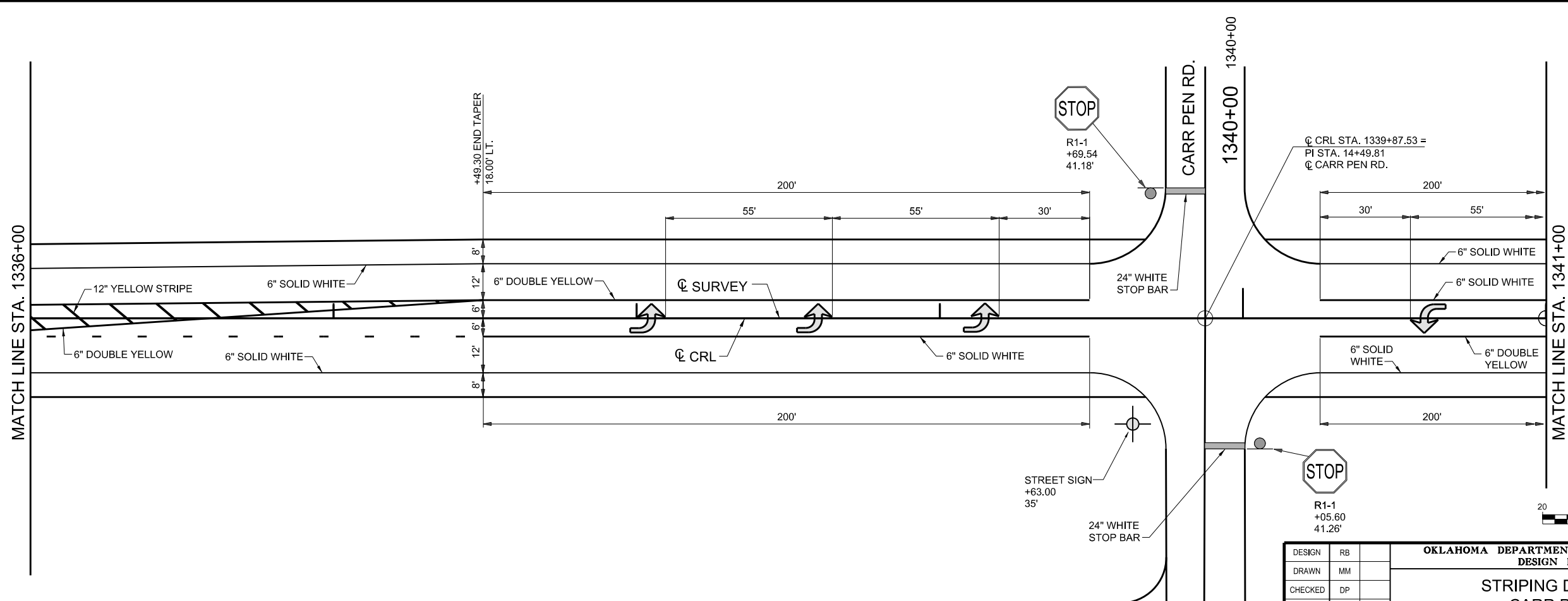
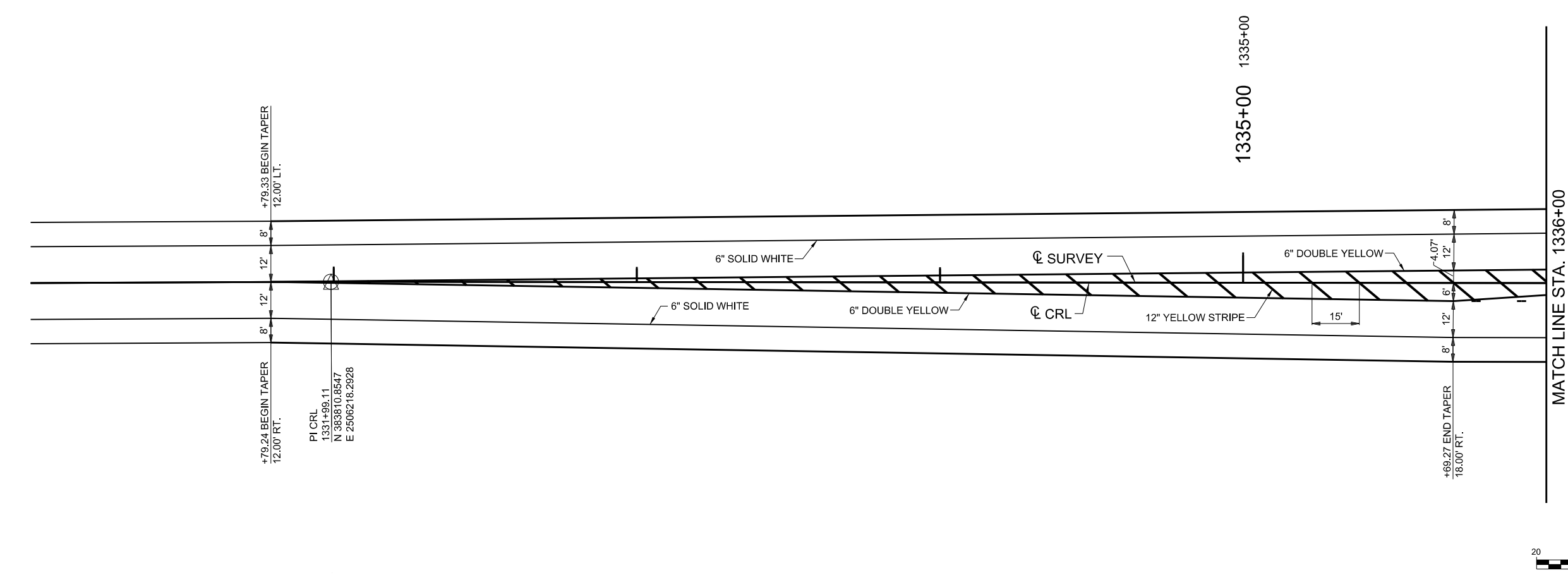
PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\034-24066(04)-STRIPING STA 1287_2.dgn



DESIGN	RB	
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

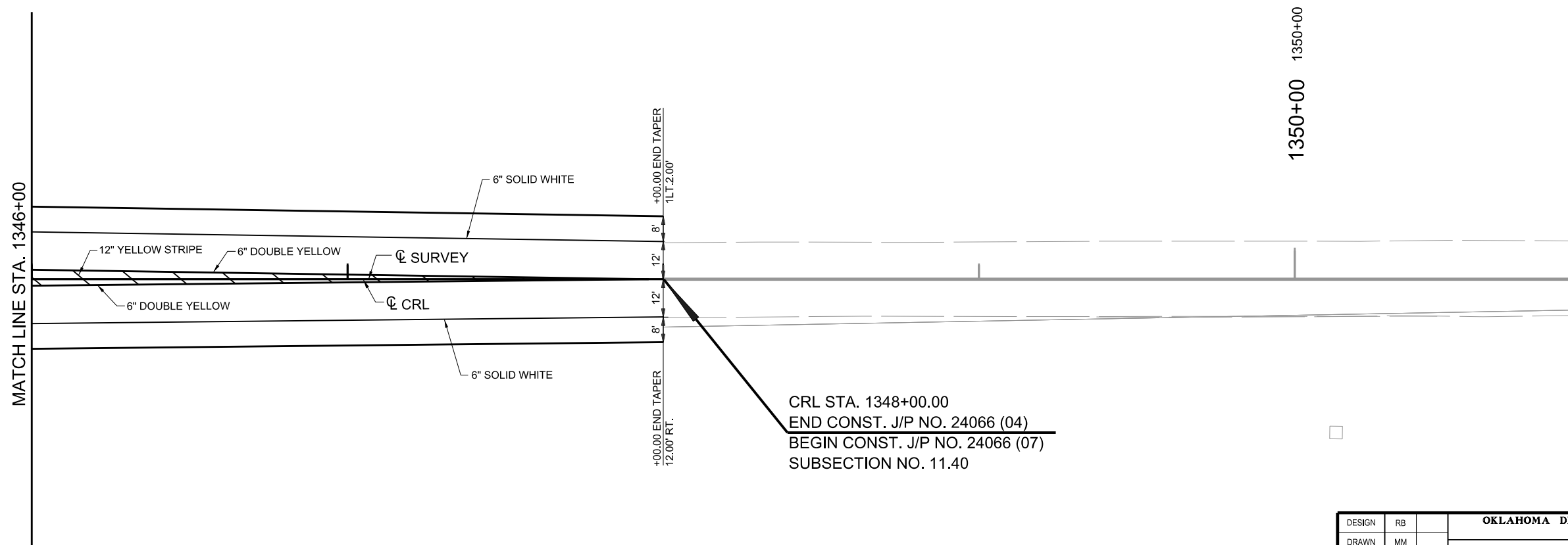
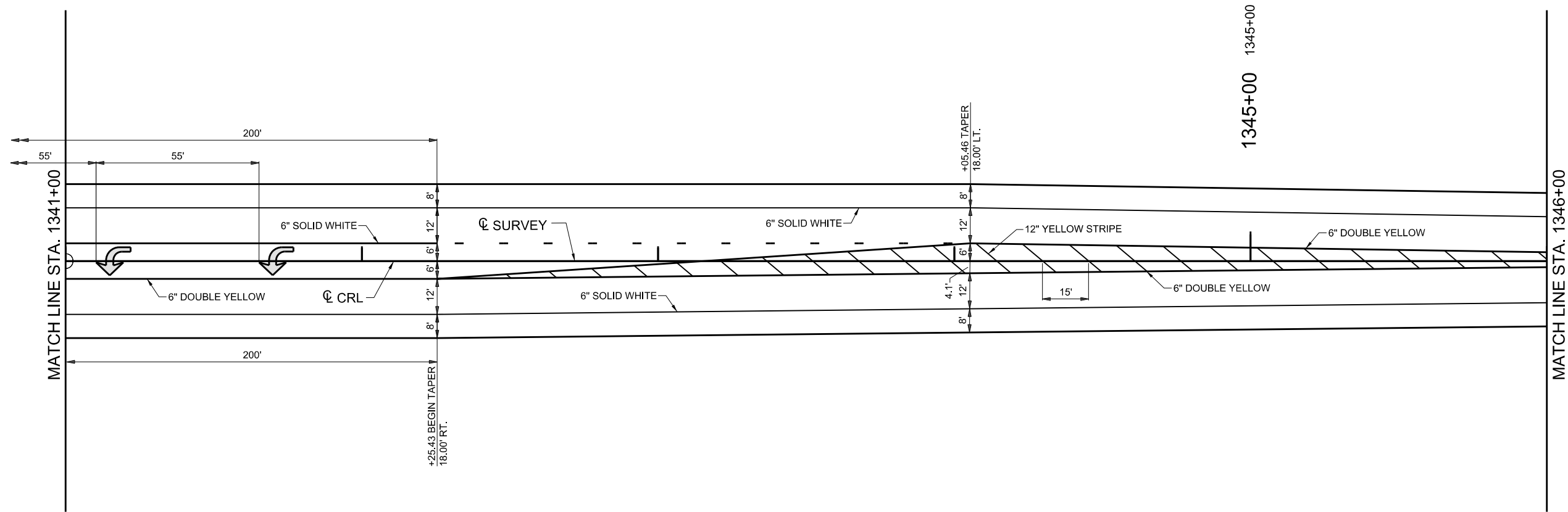
OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
STRIPING DETAIL (10)
HICKORY HILL RD.
 JOB PIECE NO. 24066 (04) SHEET NO. T034

PRINT DATE: 5/16/2018 C:\Projects\650-SHT Final Plans\035-24066(04)-STRIPING STA 1340_1.dgn



DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION STRIPING DETAIL (11) CARR PEN RD. JOB PIECE NO. 24066 (04) SHEET NO. T035
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

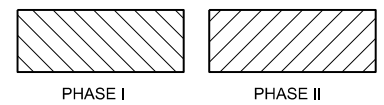
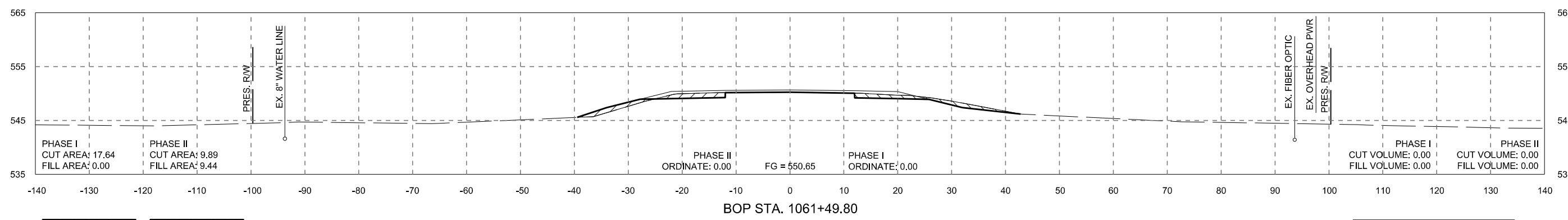
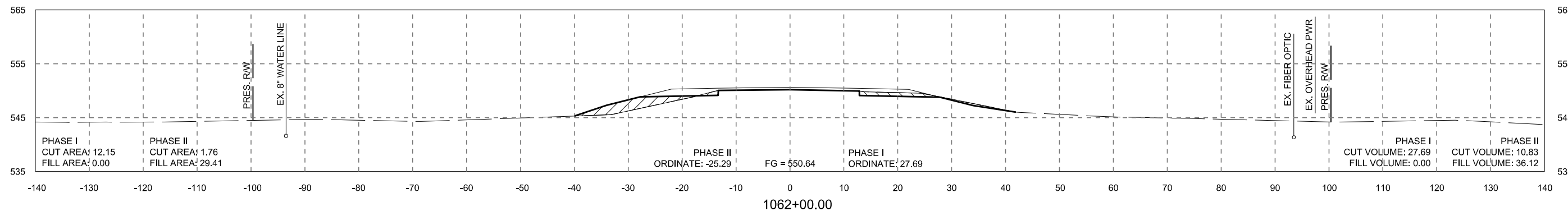
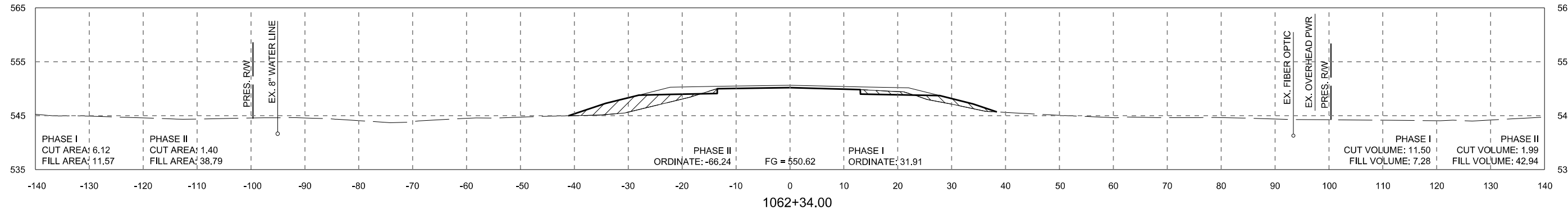
PRINT DATE: 5/16/2018 C:\Projects\150-SHT Final Plans\036-24066(04)-STRIPING STA 1340_2.dgn



CRL STA. 1348+00.00
 END CONST. J/P NO. 24066 (04)
 BEGIN CONST. J/P NO. 24066 (07)
 SUBSECTION NO. 11.40

DESIGN	RB
DRAWN	MM
CHECKED	DP
APPROVED	SL
SQUAD	BKL, Inc.

OKLAHOMA DEPARTMENT OF TRANSPORTATION
 DESIGN DIVISION
STRIPING DETAIL (12)
CARR PEN RD.
 JOB PIECE NO. 24066 (04) SHEET NO. T036



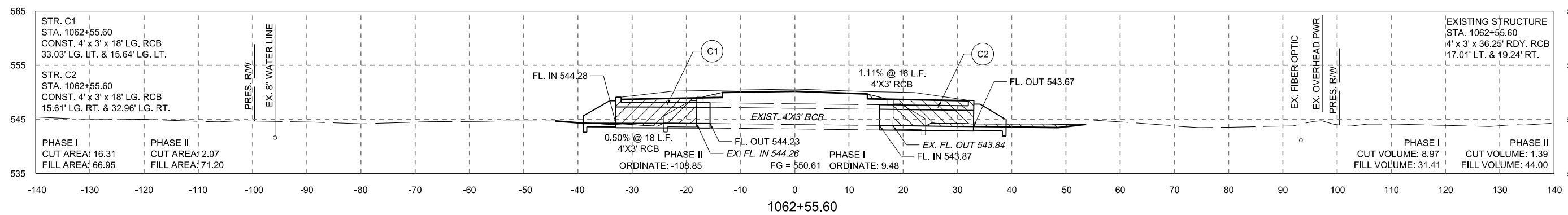
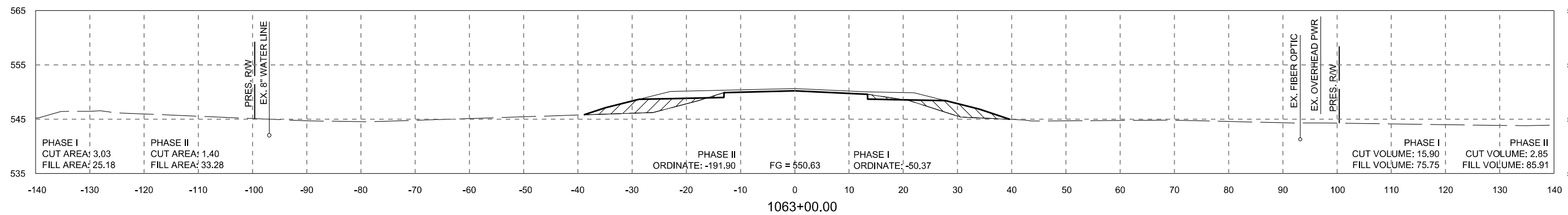
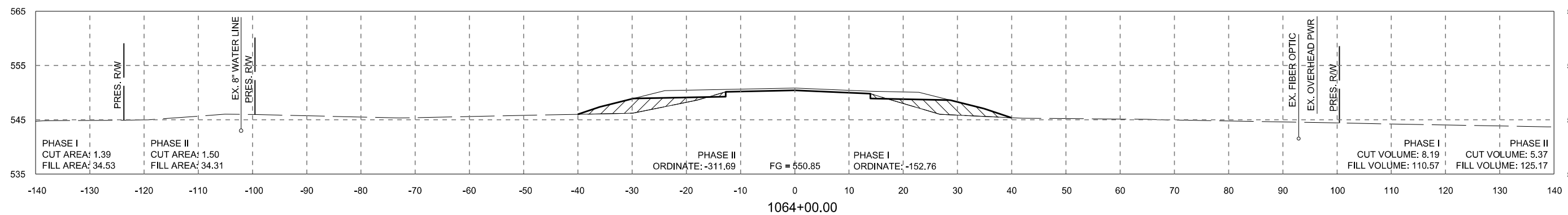
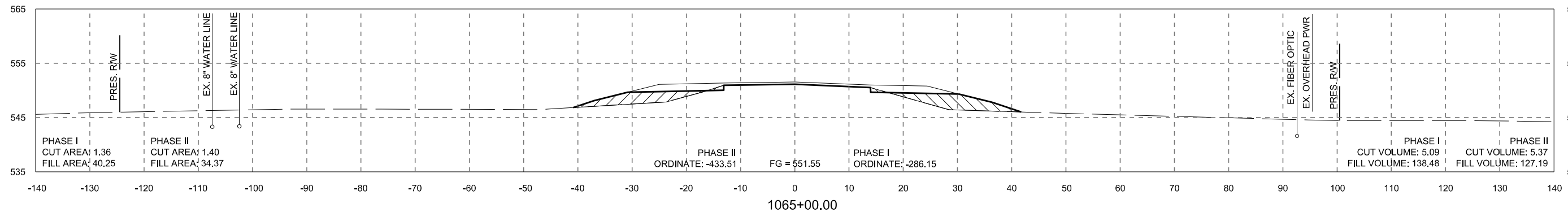
NOTE:
MATCH EXISTING CROSS SLOPE
1 1/2" MIN. TO 2% MAX

PRINT DATE: 8/22/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTION MAINLINE.dgn

SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X001
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

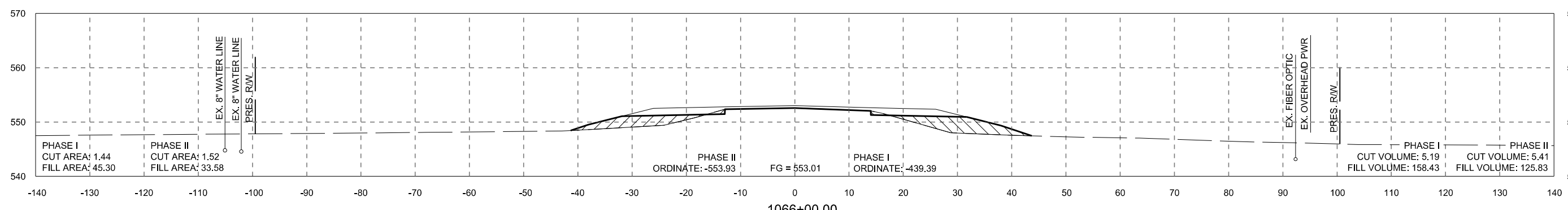
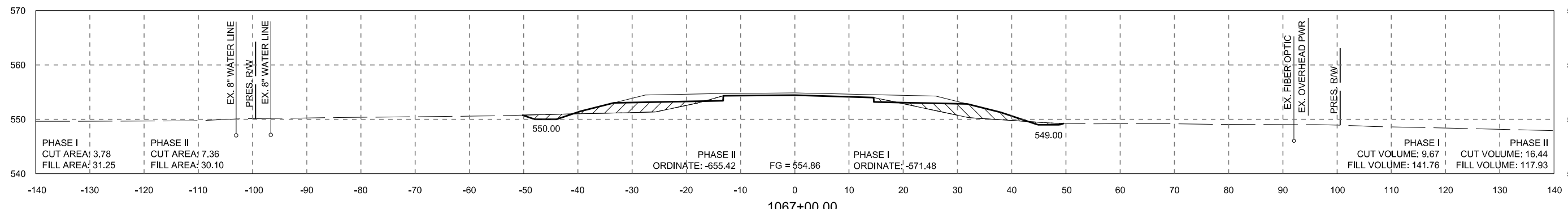
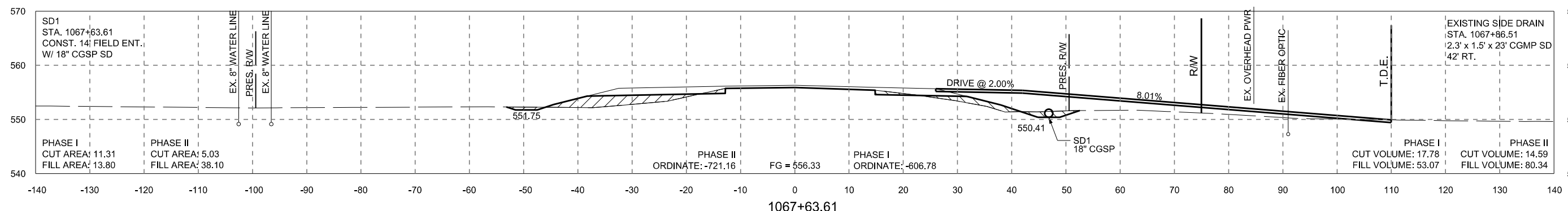
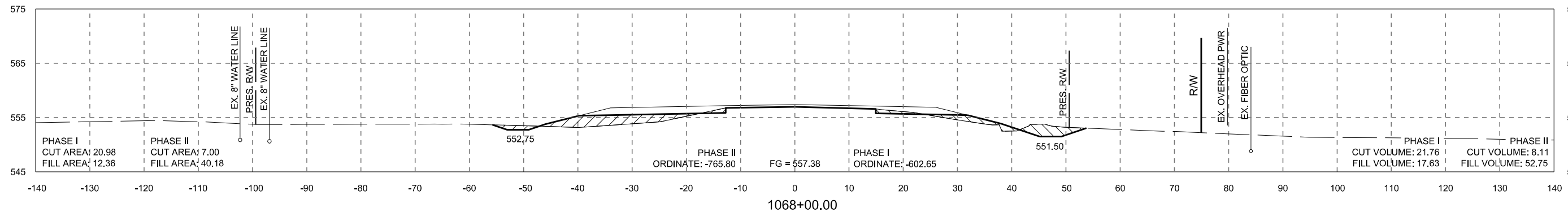
PRINT DATE: 8/22/2018 C:\Projects\650-SHT Final Plans\650-000-24066(04)-X-SECTION MAINLINE.dgn



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X002
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

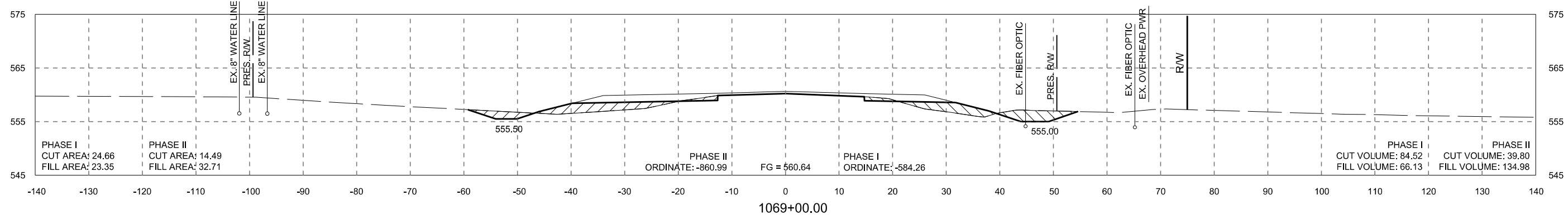
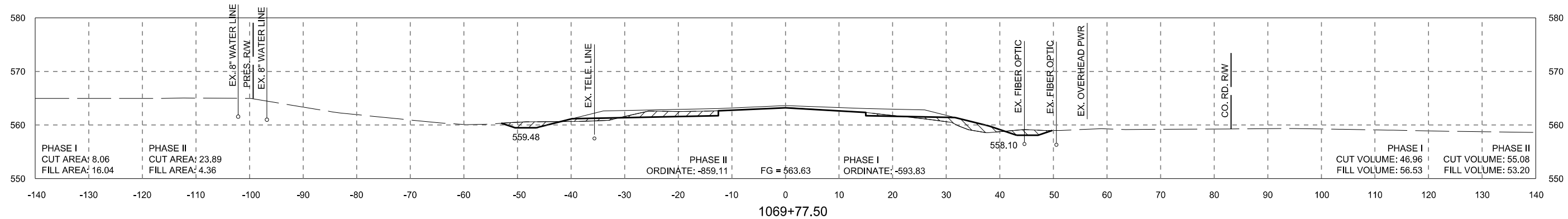
PRINT DATE: 8/22/2018 C:\Projects\650-SHT Final Plans\650\000-24066(04)-X-SECTION MAINLINE.dgn



SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X003
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

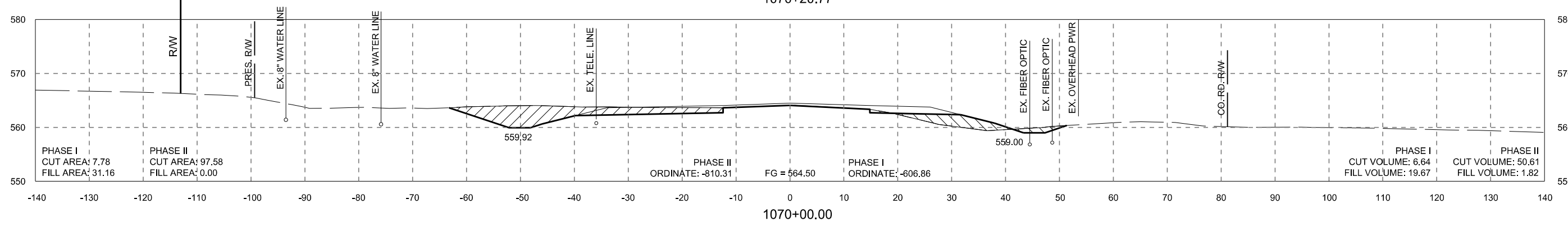
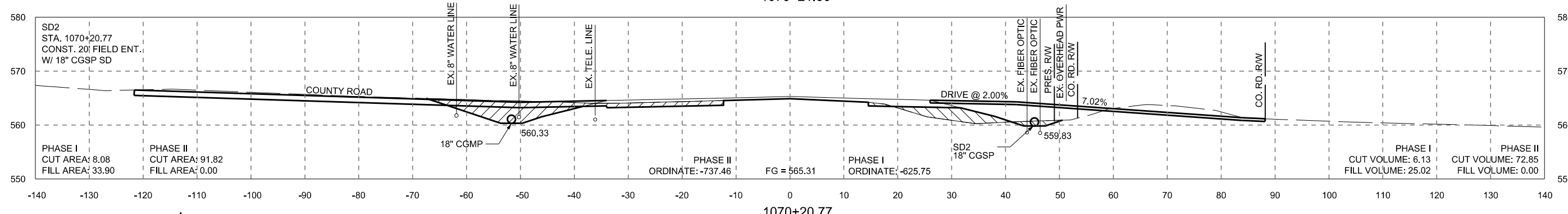
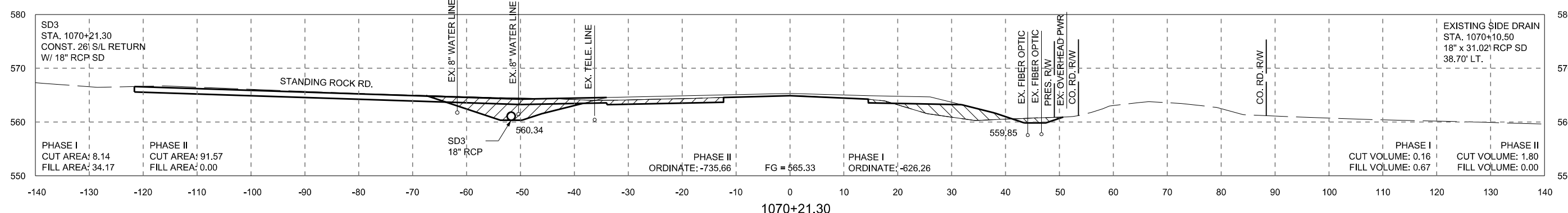
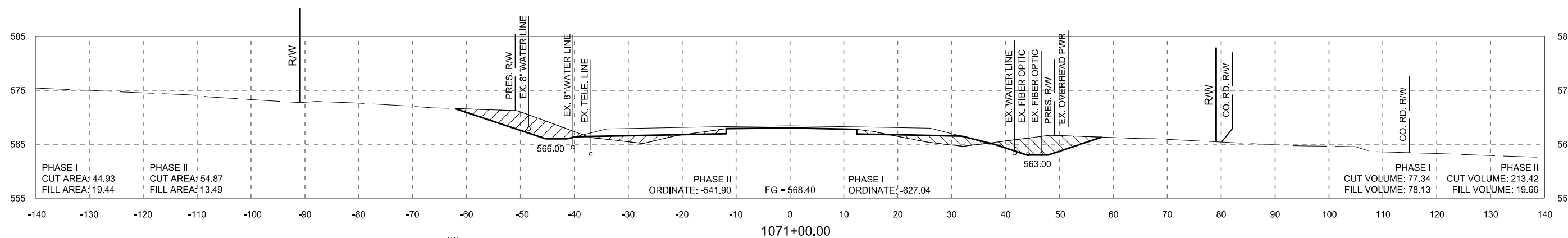
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X004
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

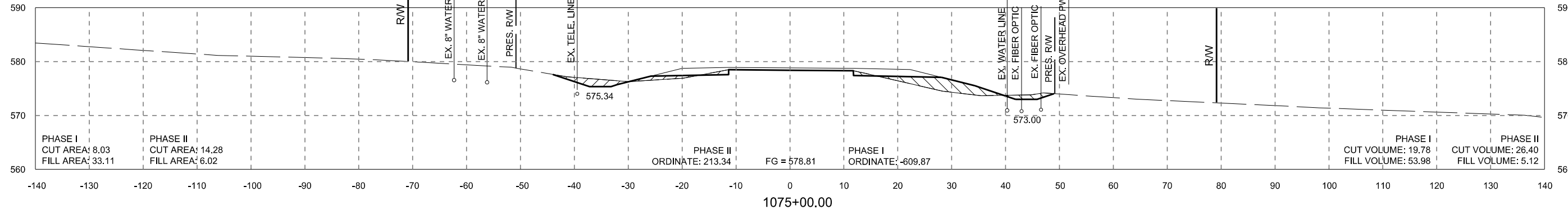
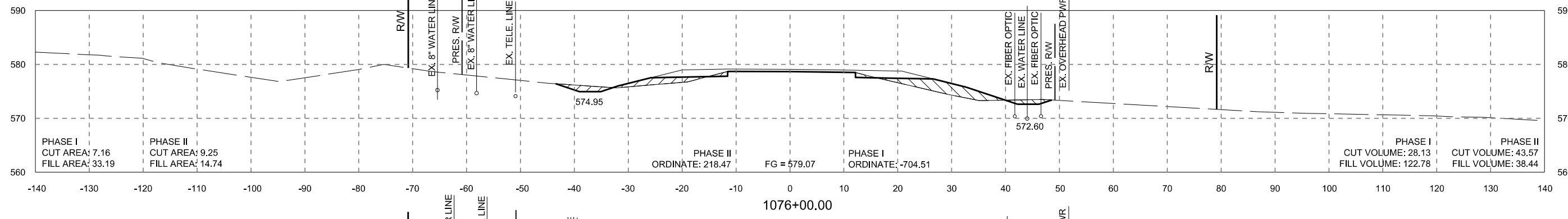
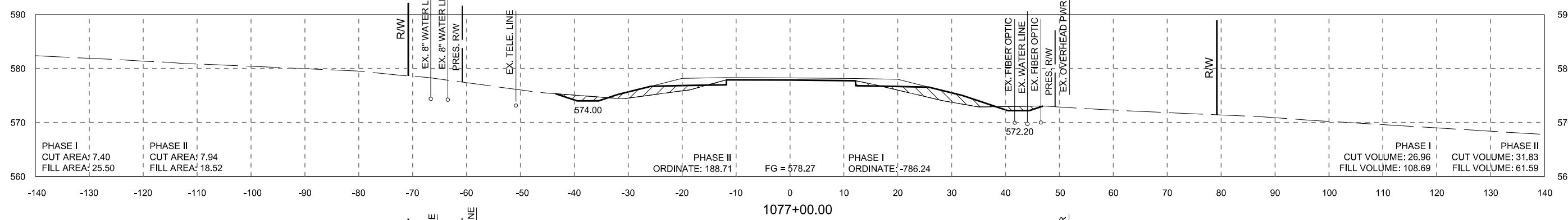
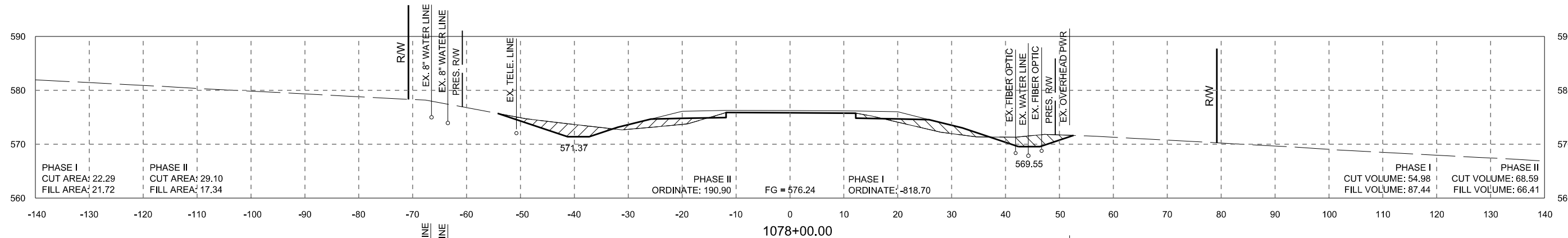
PRINT DATE: 8/22/2018 C:\Projects\650-SHT Final Plans\650-SHT-24066(04)-SECTION MAINLINE.dgn



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X005
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

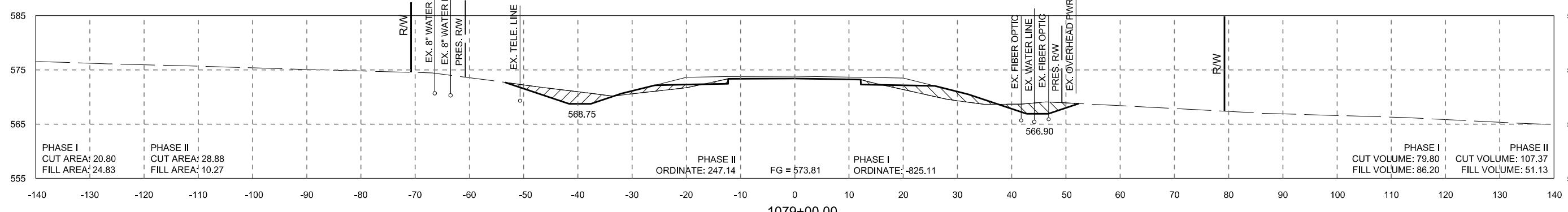
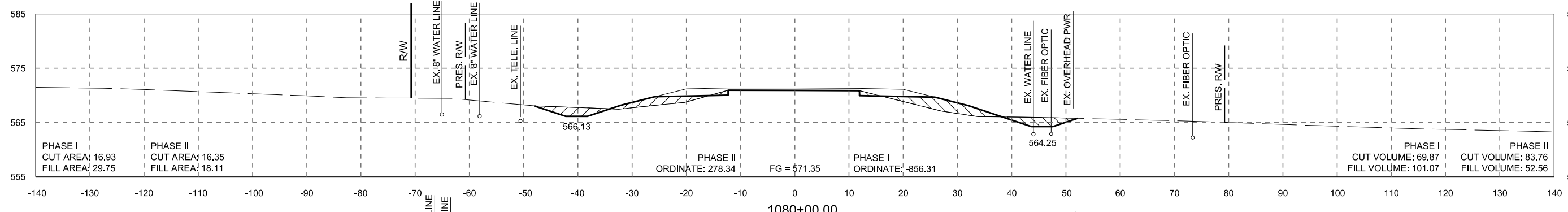
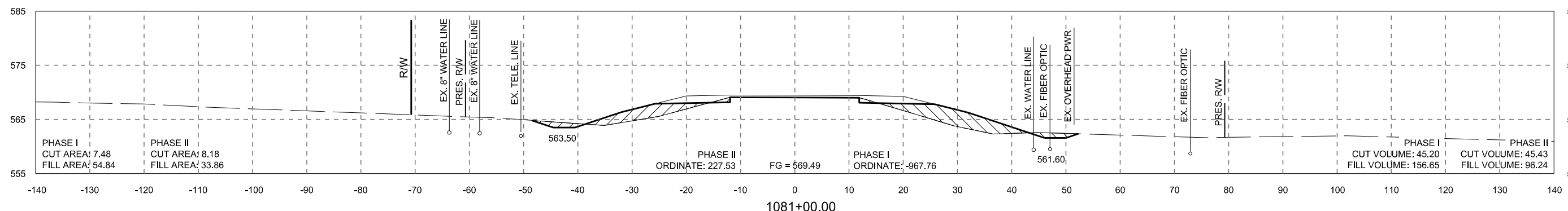
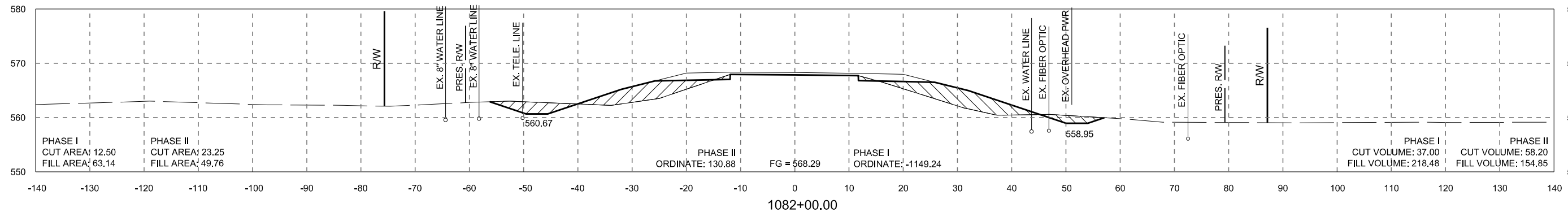
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X007
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

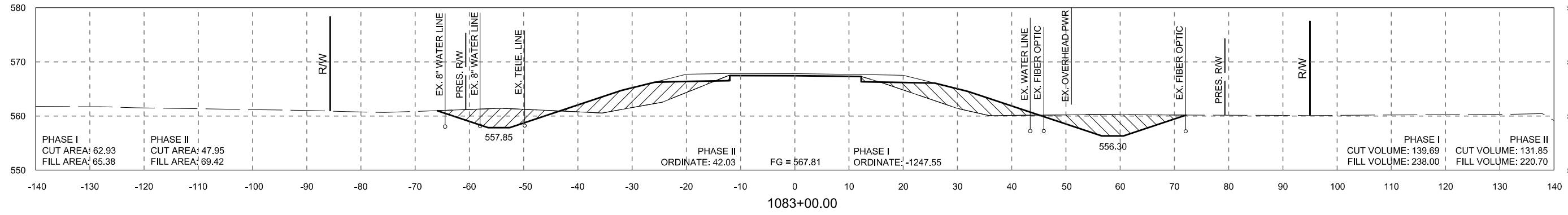
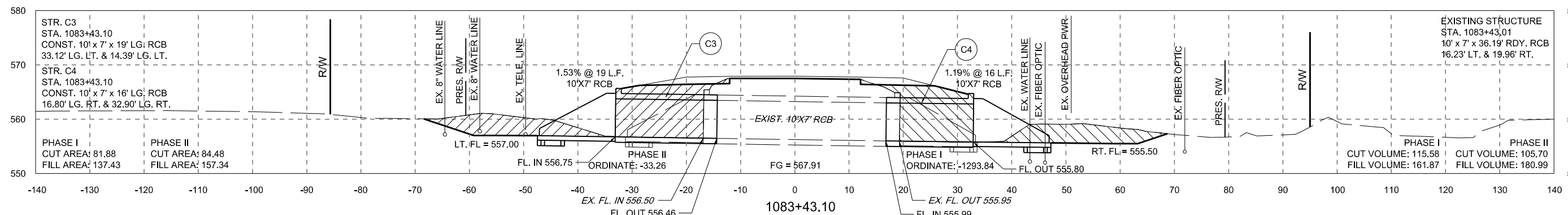
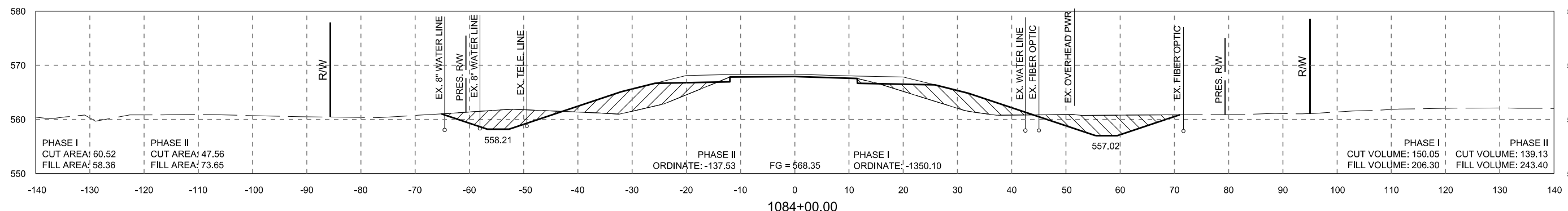
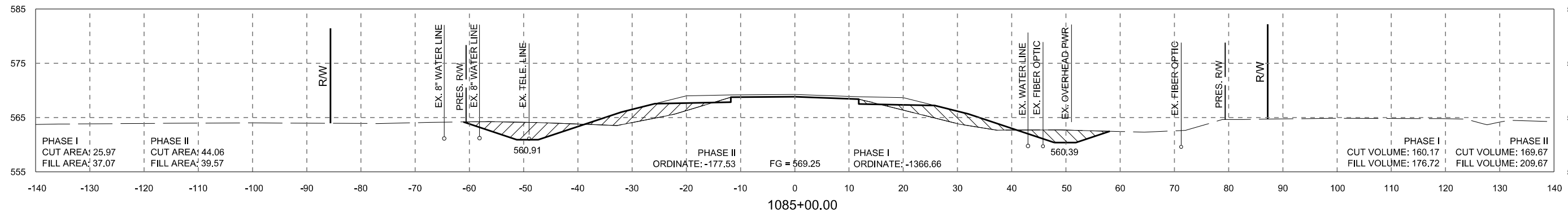
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X008
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

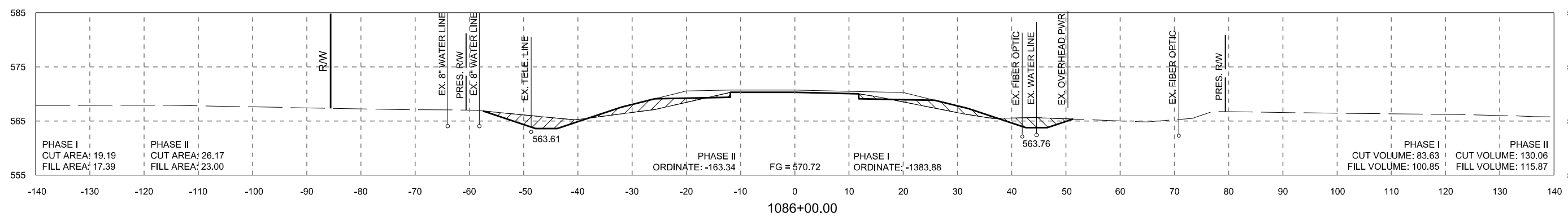
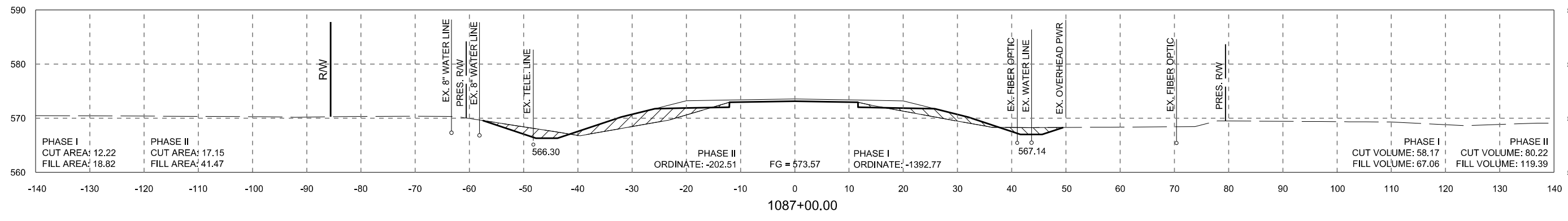
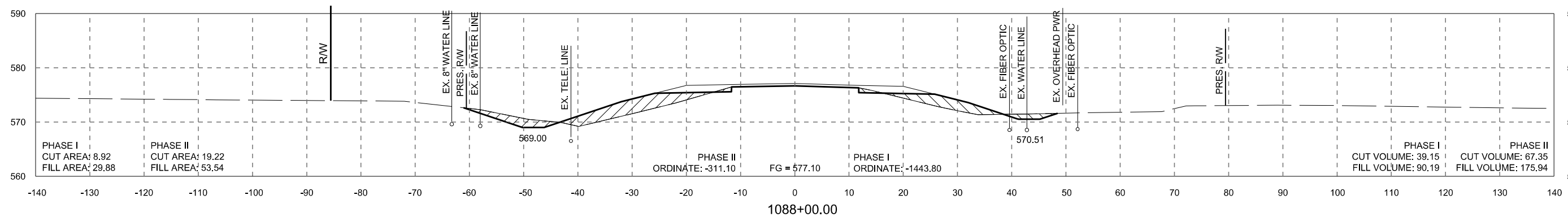
PRINT DATE: 8/22/2018 C:\Projects\650-SHT Final Plans\650-SHT-24066(04)-SECTION MAINLINE.dgn



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X009
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

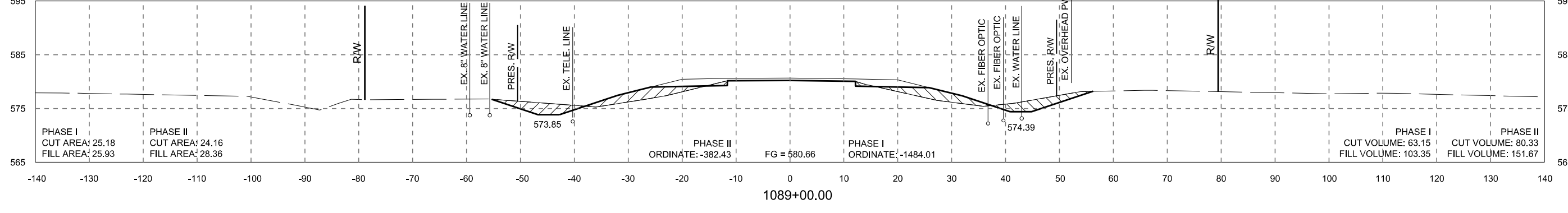
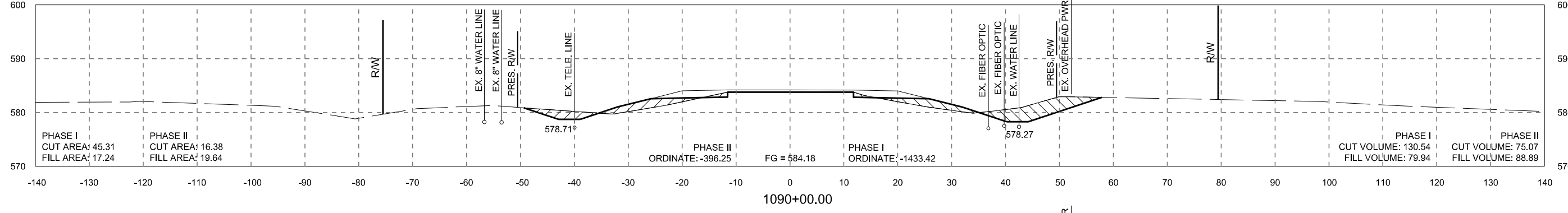
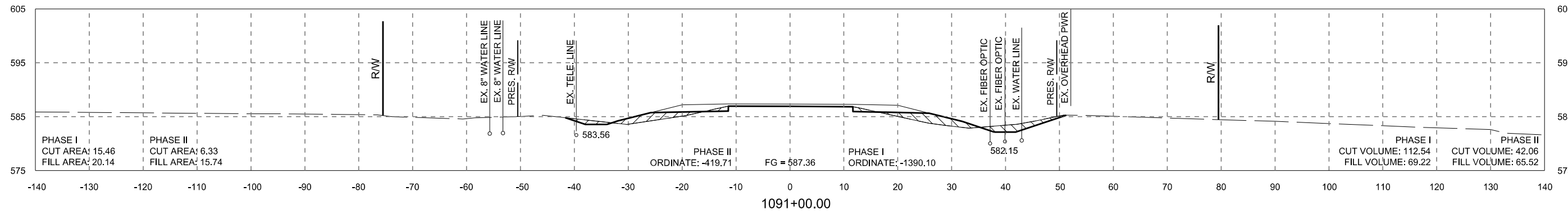
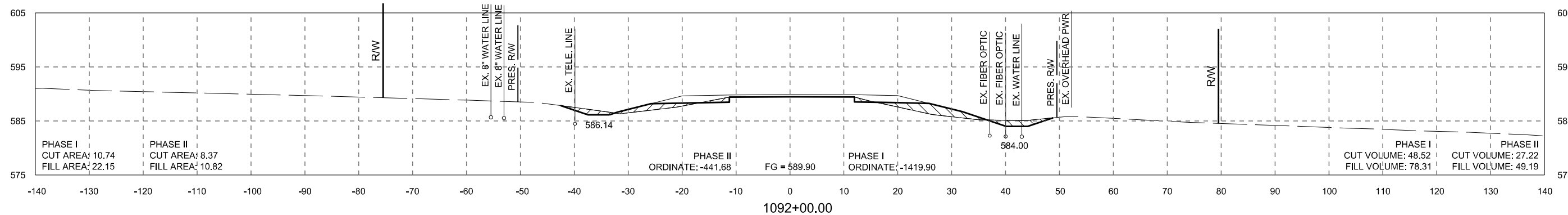
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X010
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

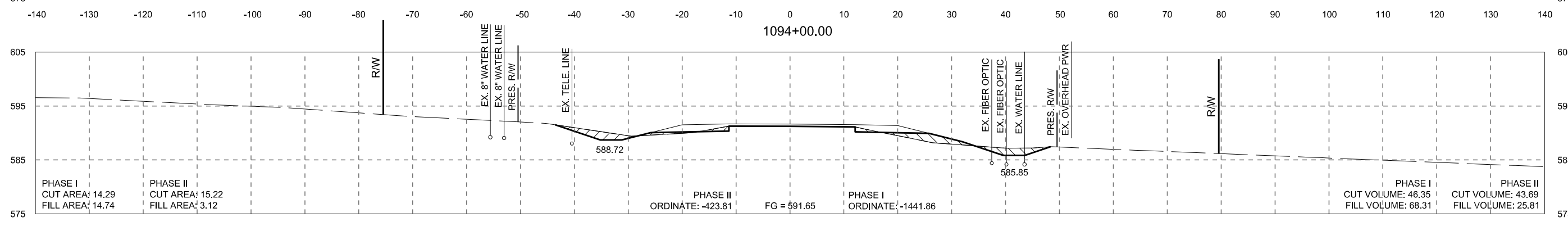
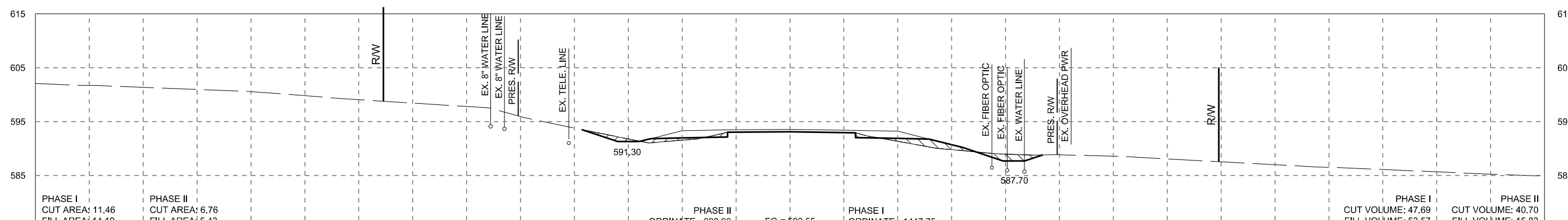
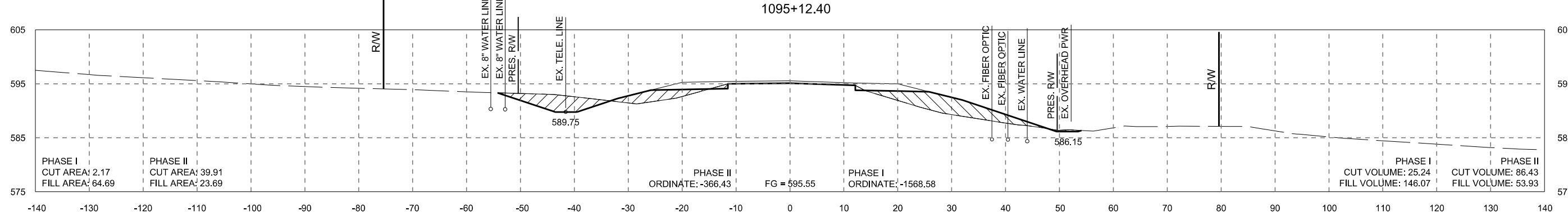
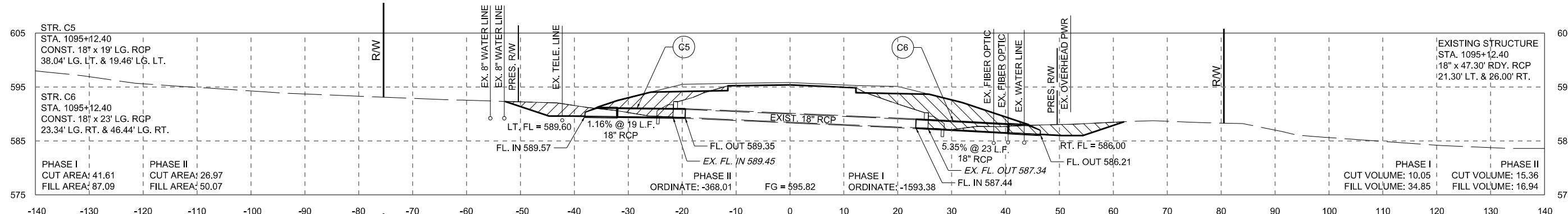
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X011
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

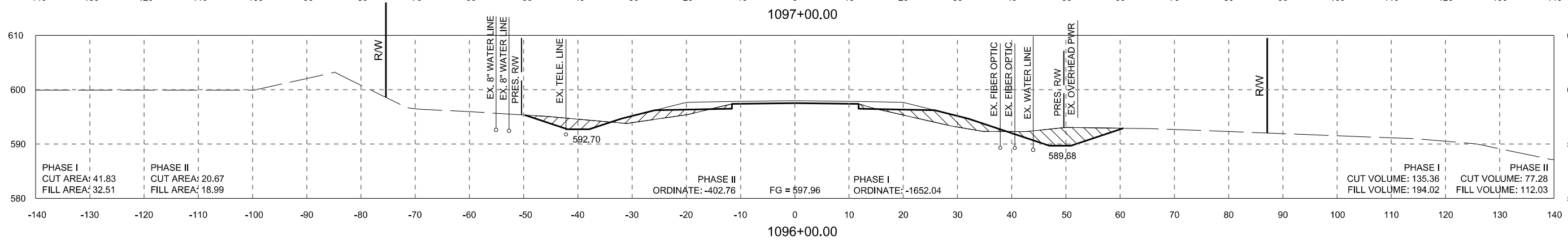
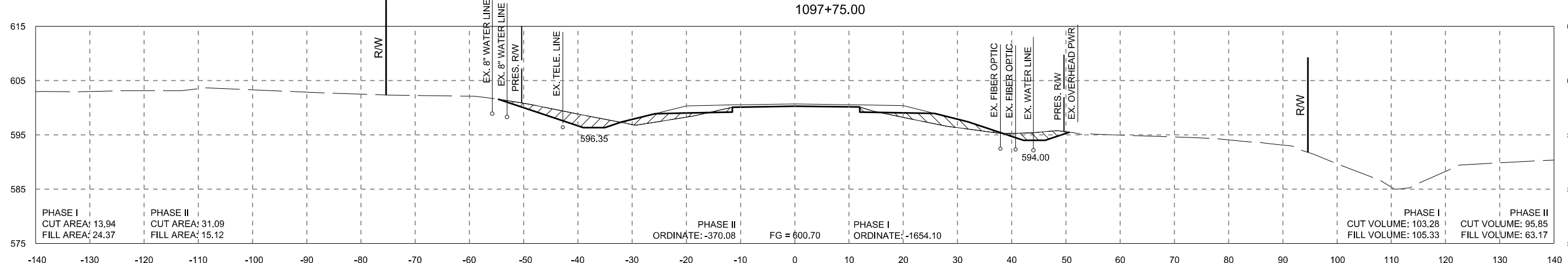
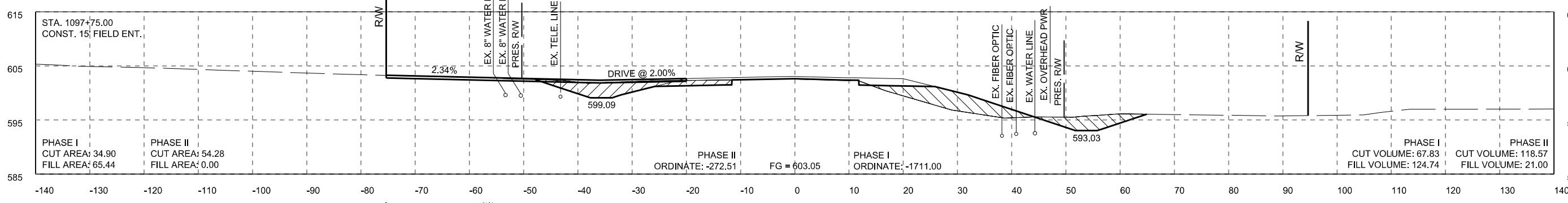
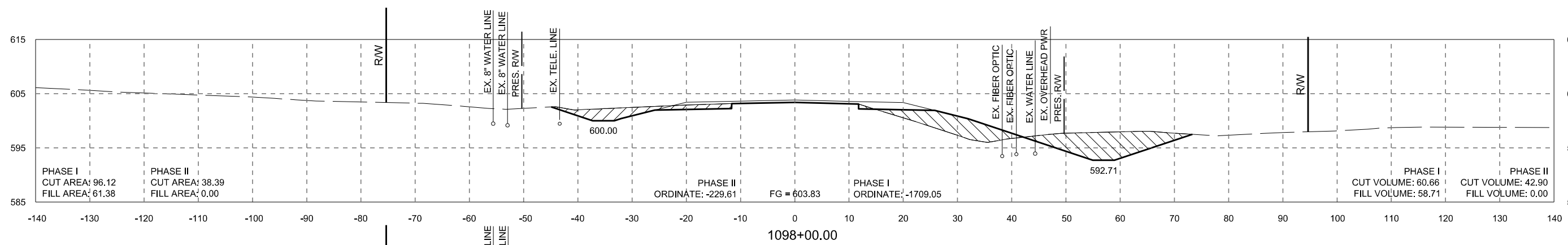
PRINT DATE: 8/22/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTION MAINLINE.dgn



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

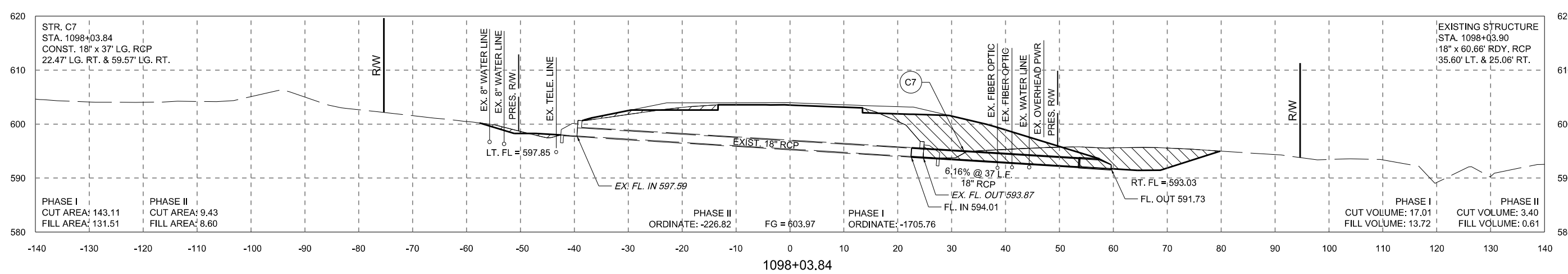
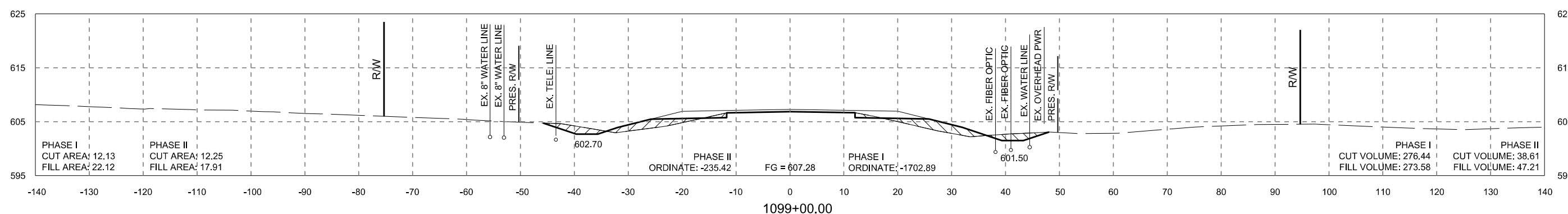
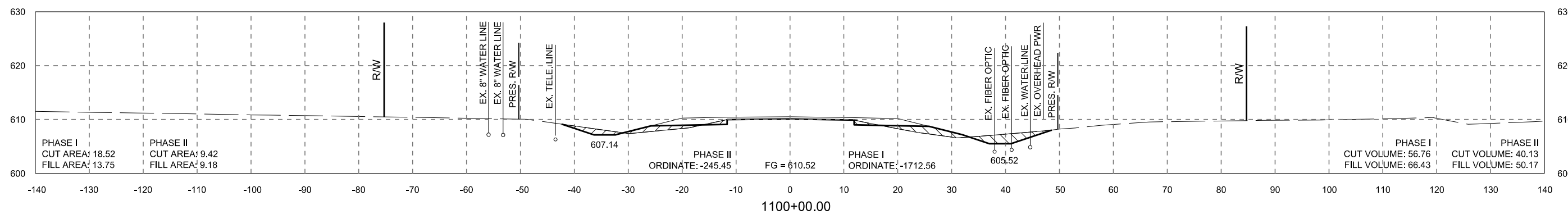
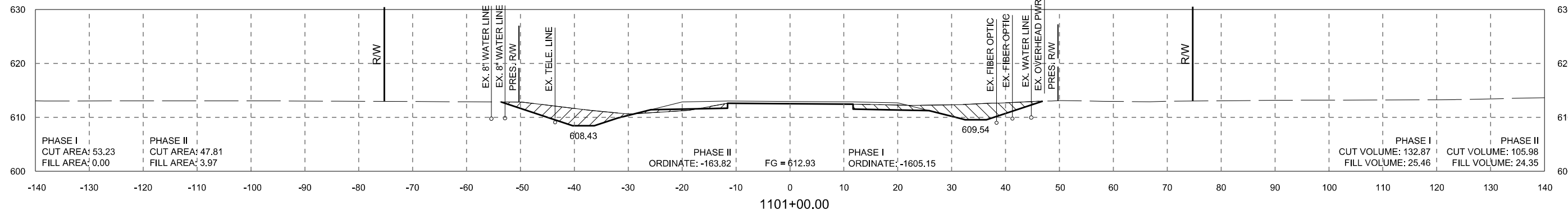
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DRAWN	MM	
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APPROVED	SL	
SQUAD	BKL, Inc.	

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DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X013
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

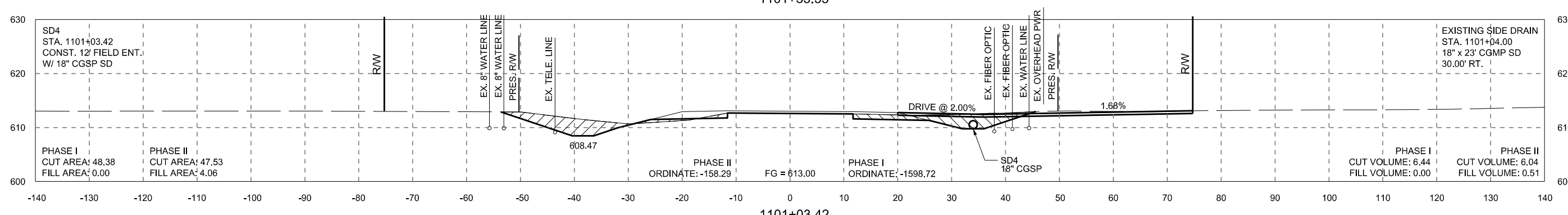
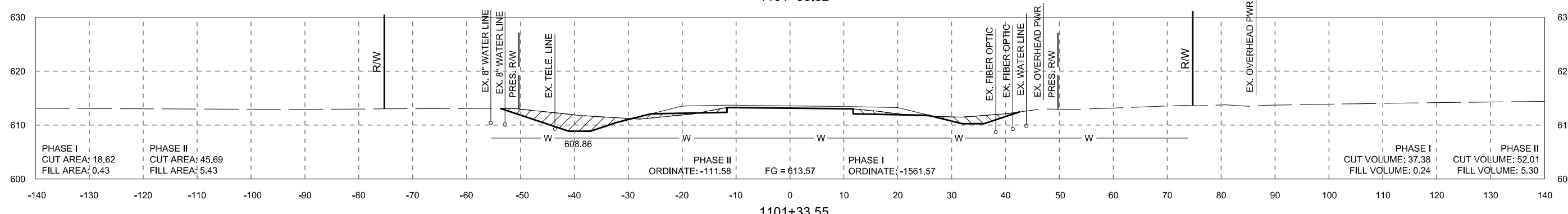
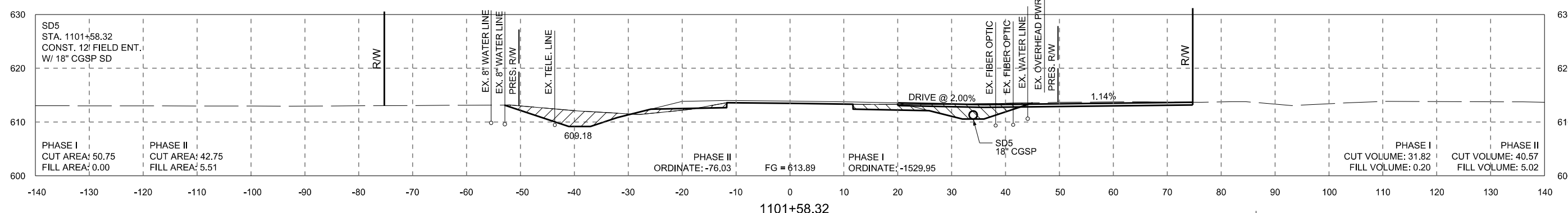
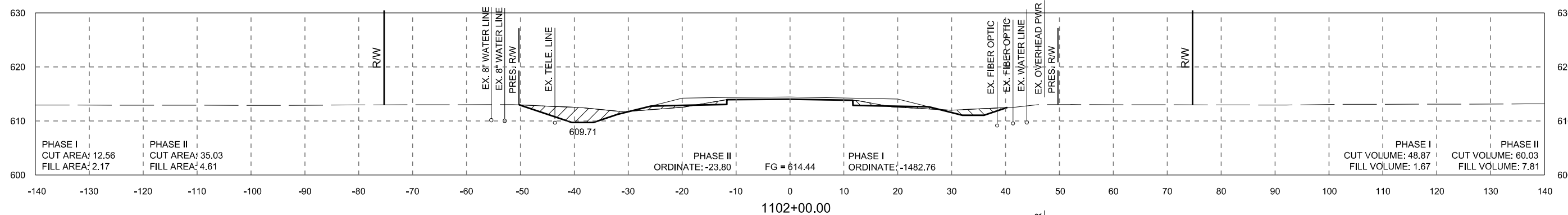


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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X014
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

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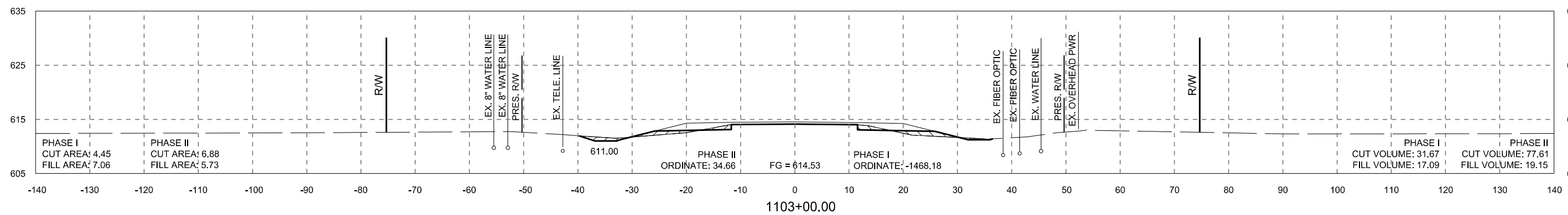
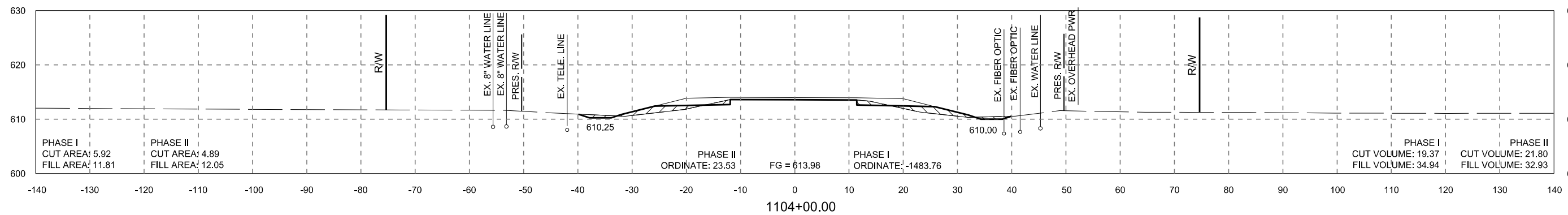
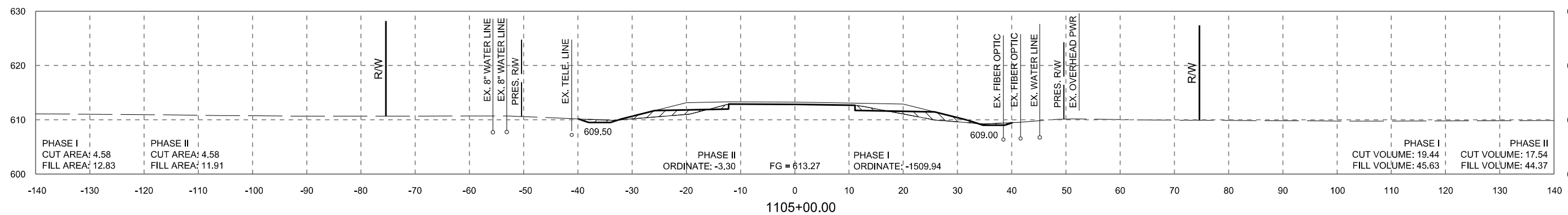
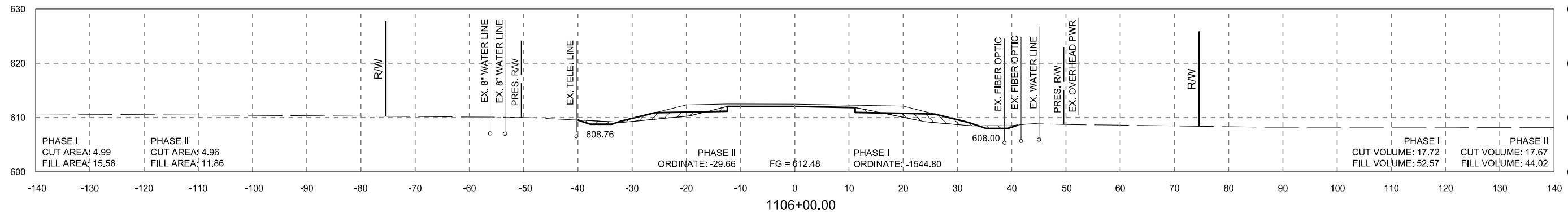


EXISTING SIDE DRAIN
 STA. 1101+04.00
 18" x 23' CGMP SD
 30.00' RT.

SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X015
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

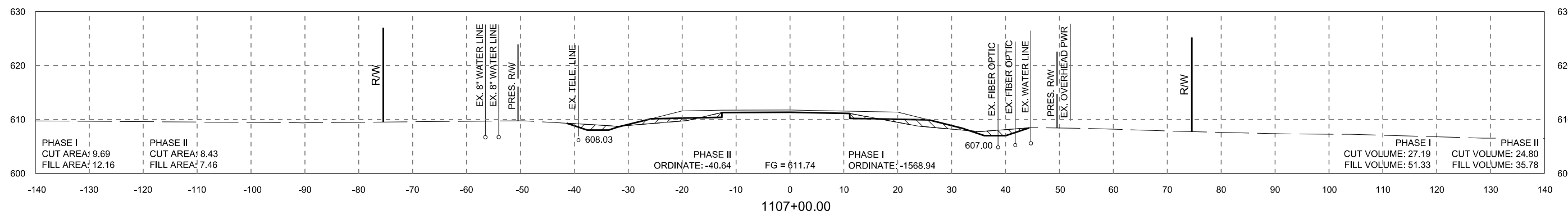
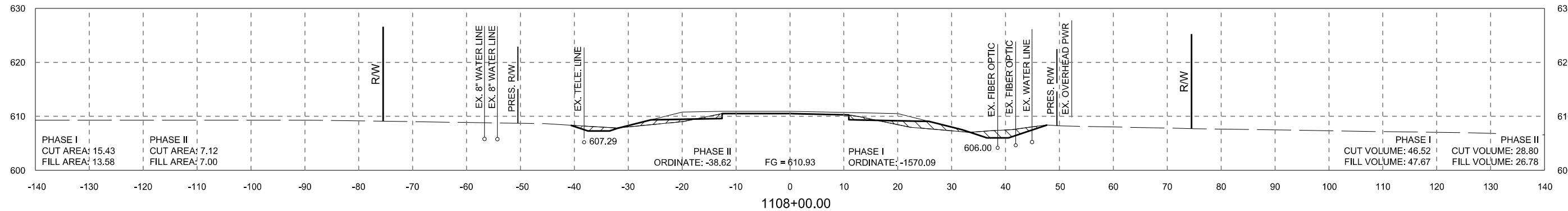
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

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APPROVED	SL	
SQUAD	BKL, Inc.	

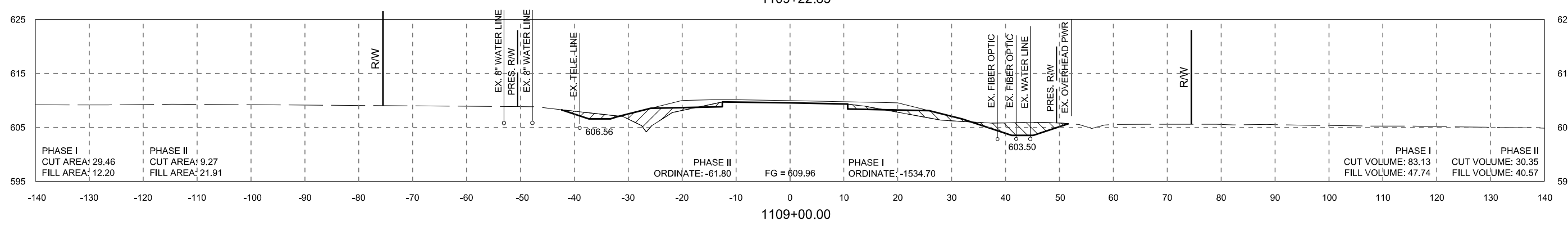
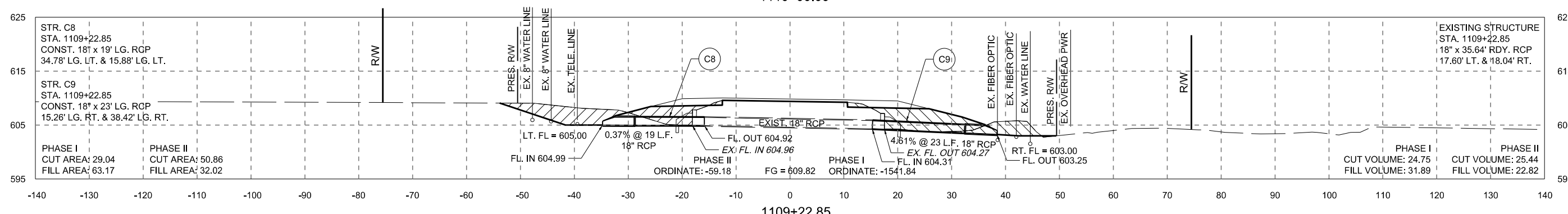
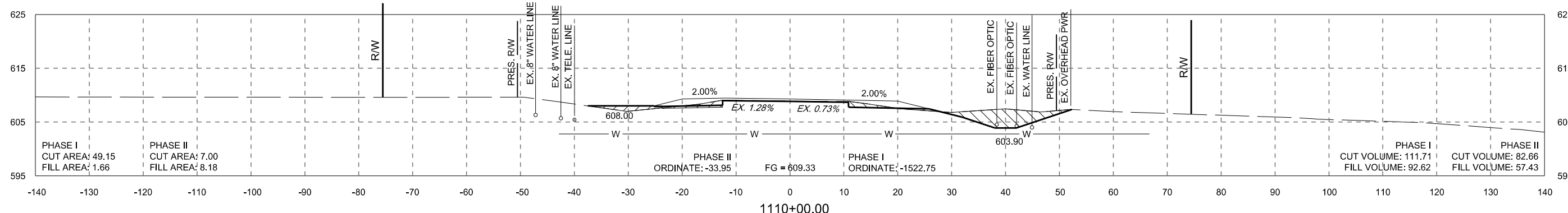
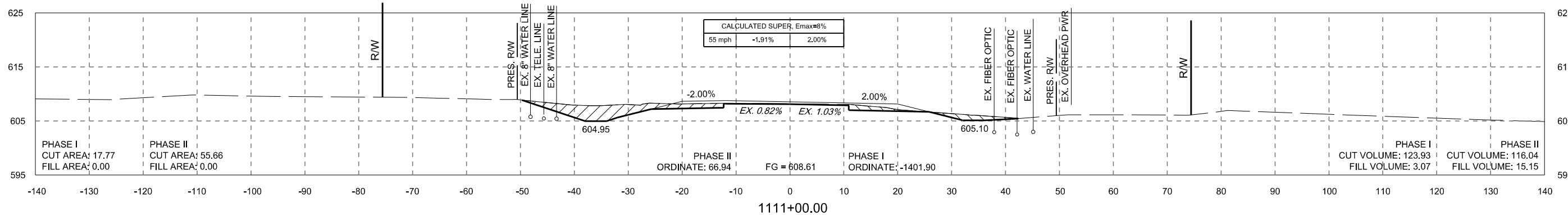
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X017
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

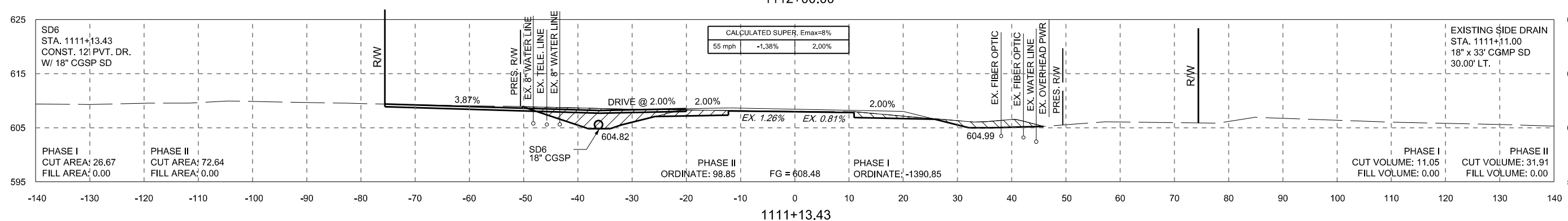
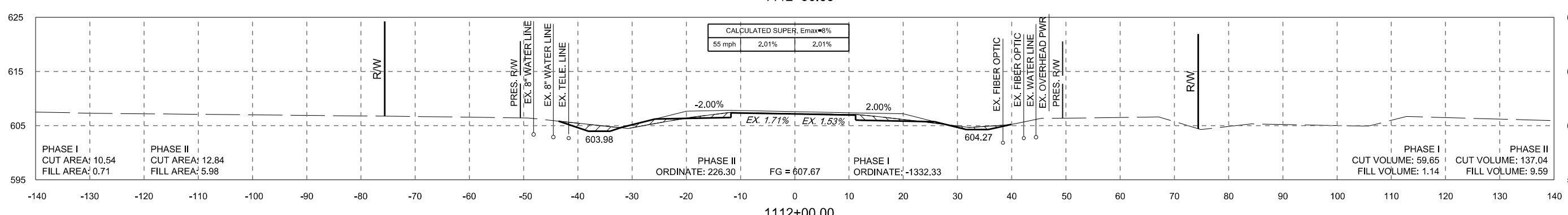
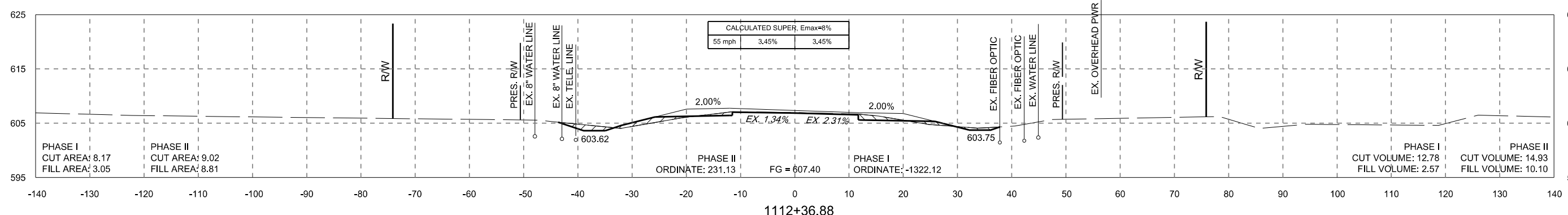
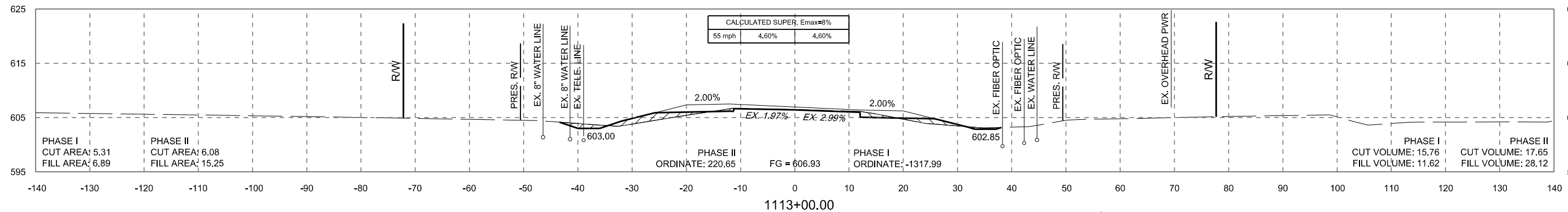
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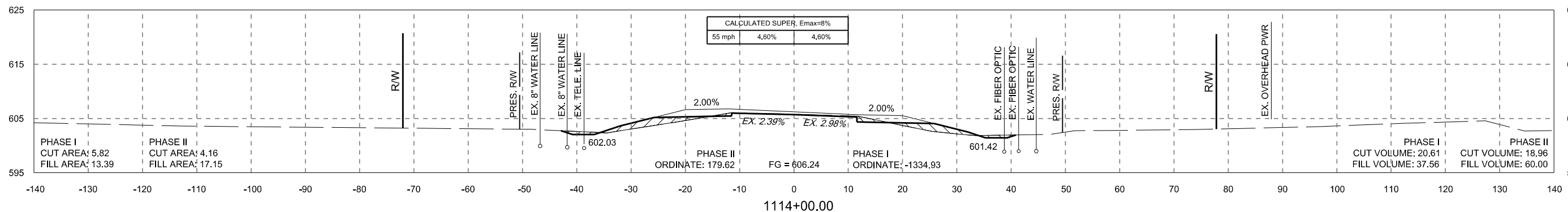
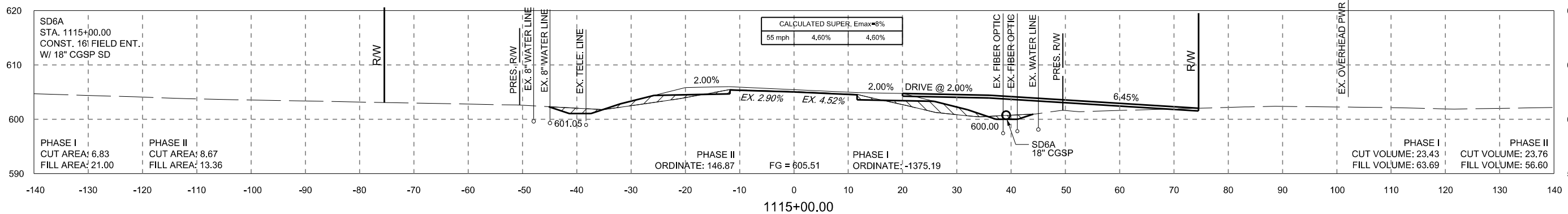
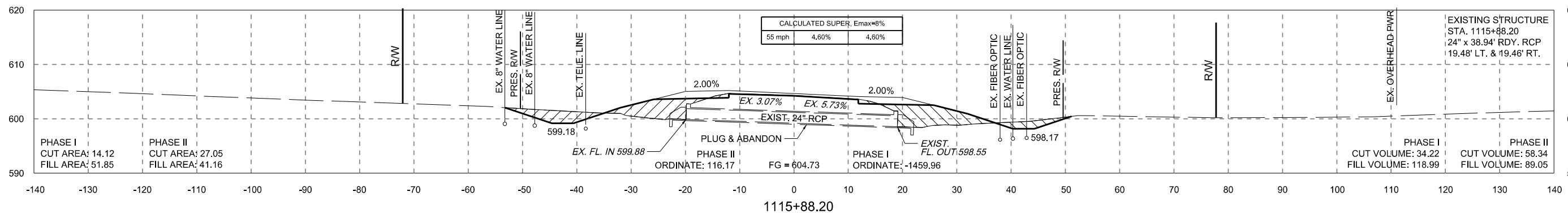
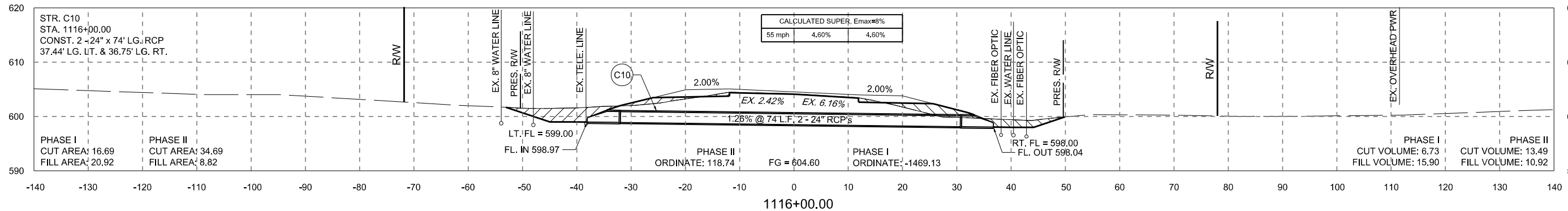
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DRAWN	MM	
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SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

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DRAWN	MM	
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SQUAD	BKL, Inc.	

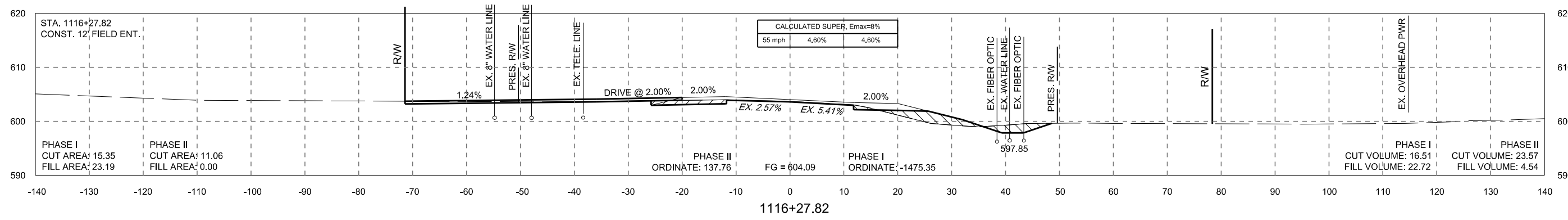
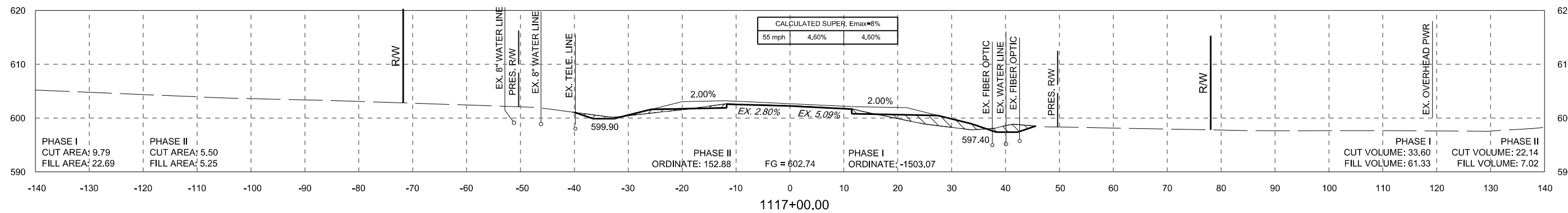
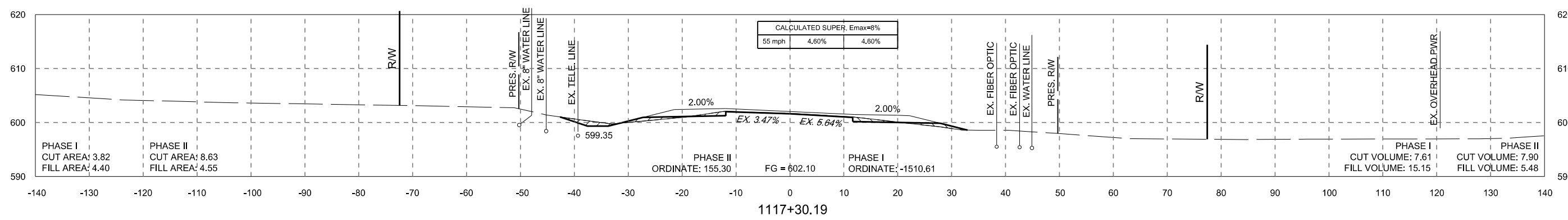
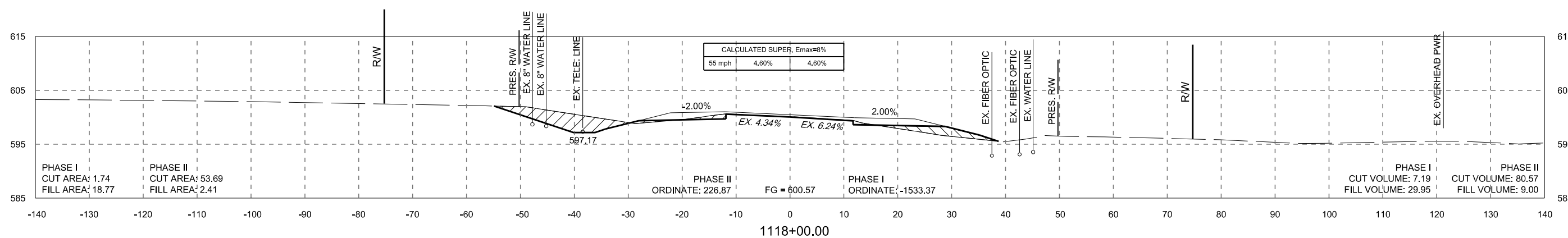


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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

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DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

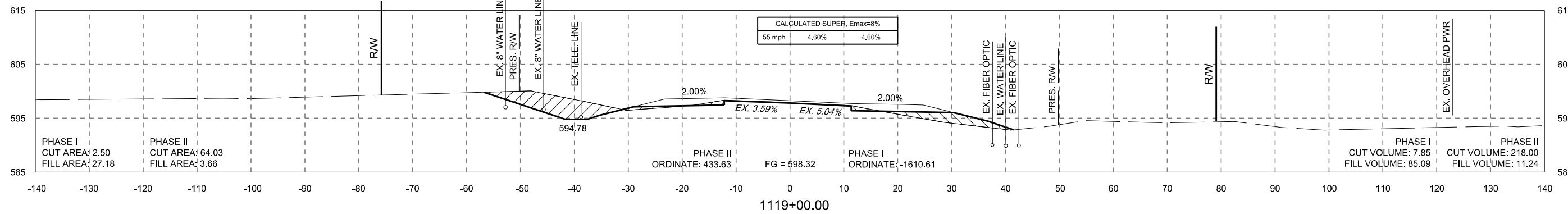
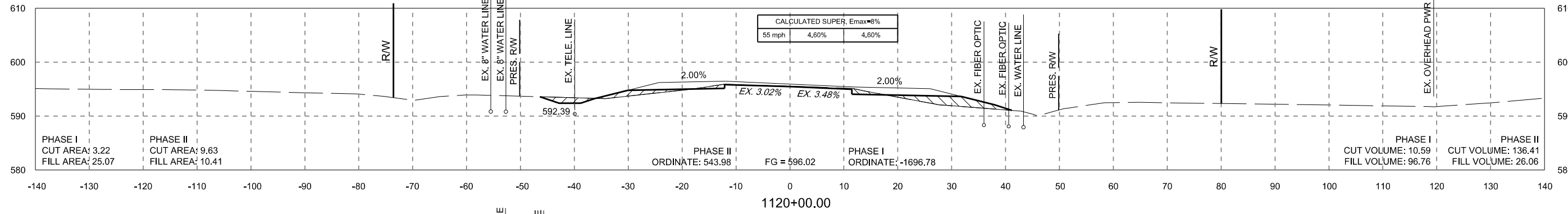
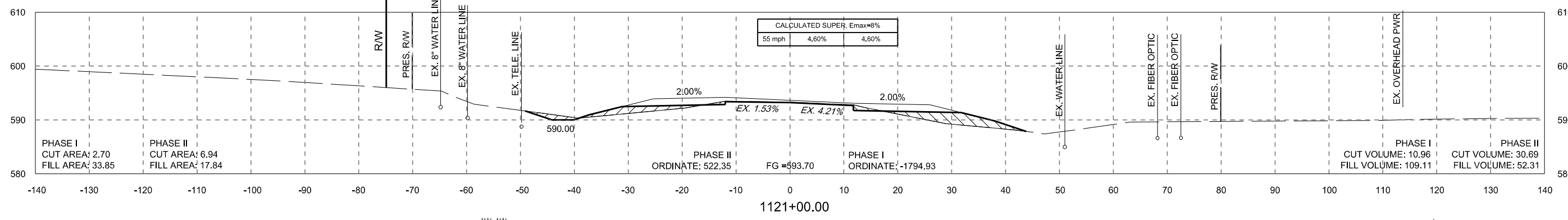
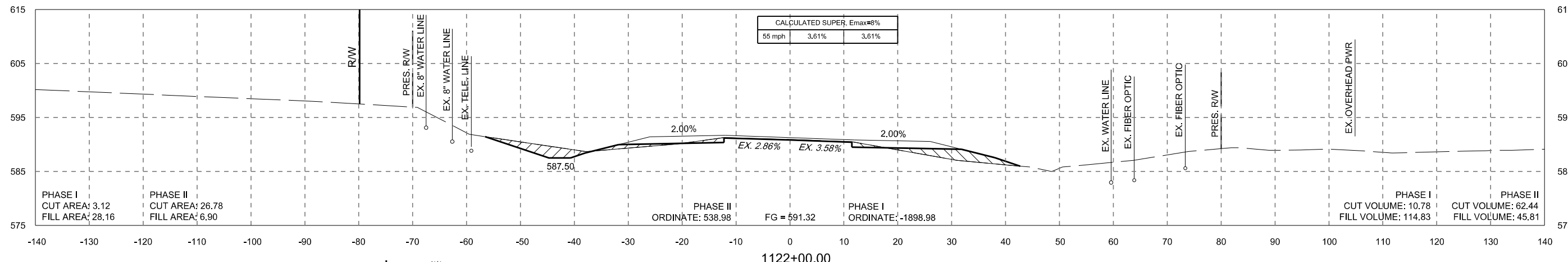
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SCALE: HORIZ. 1" = 1'-0"
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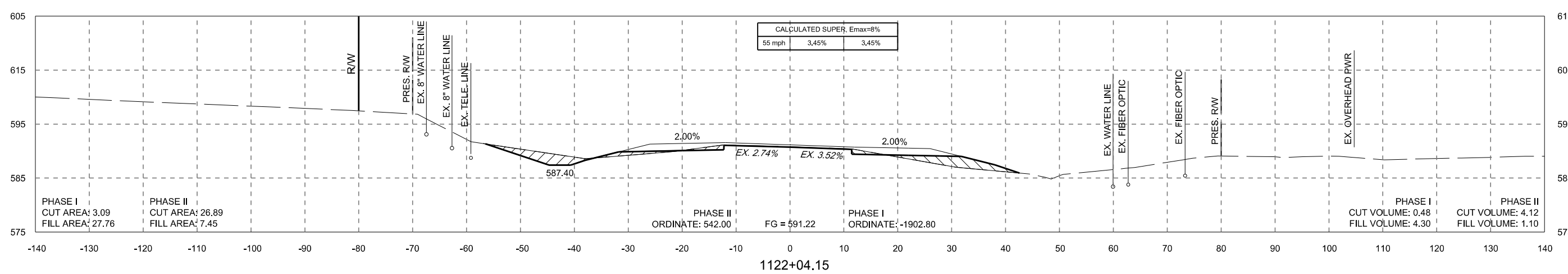
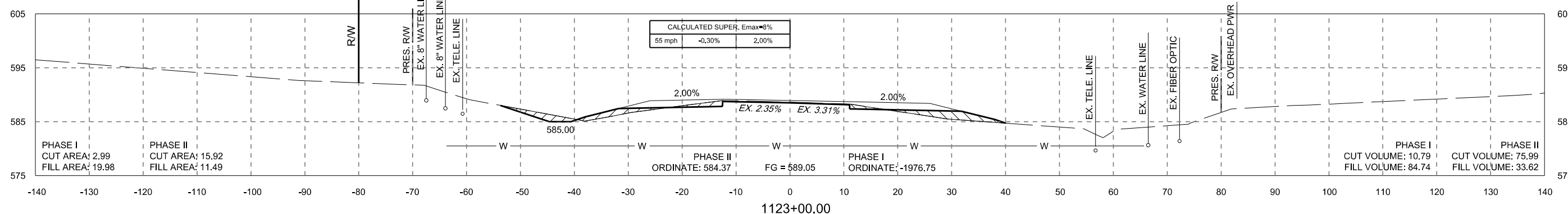
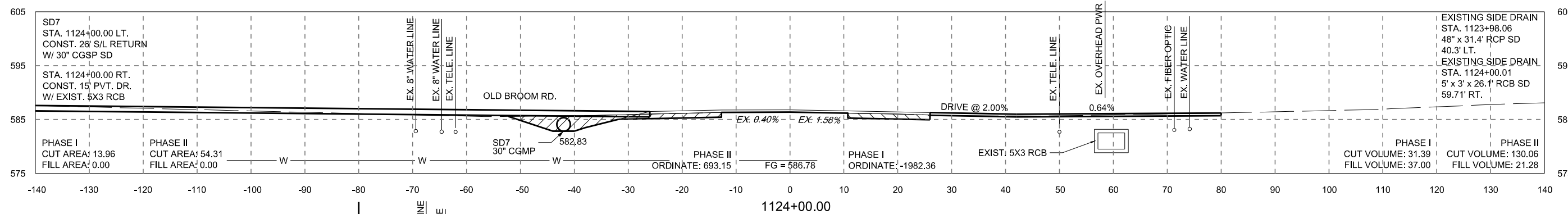
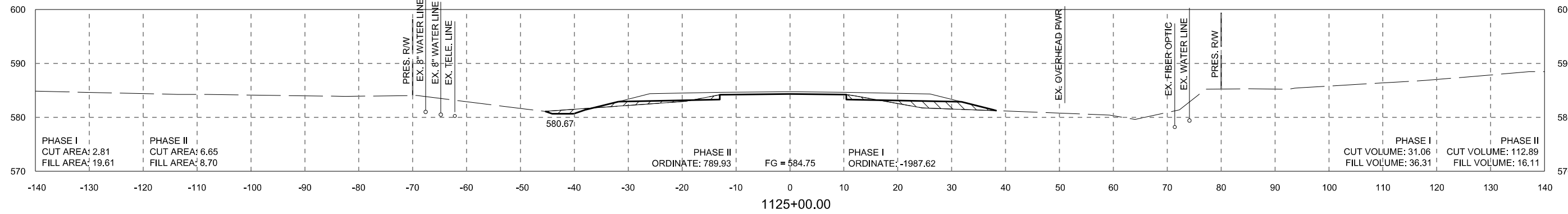
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APPROVED	SL	
SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

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SQUAD	BKL, Inc.	

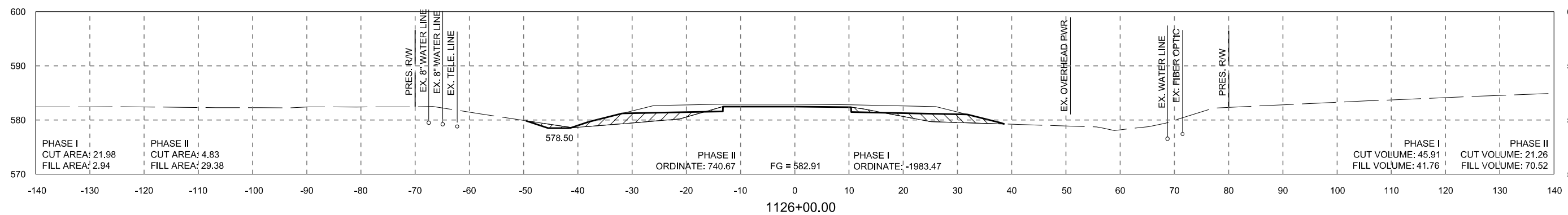
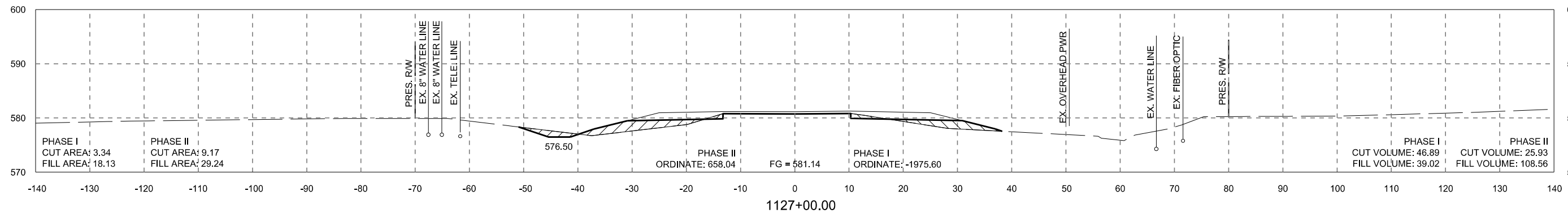


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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

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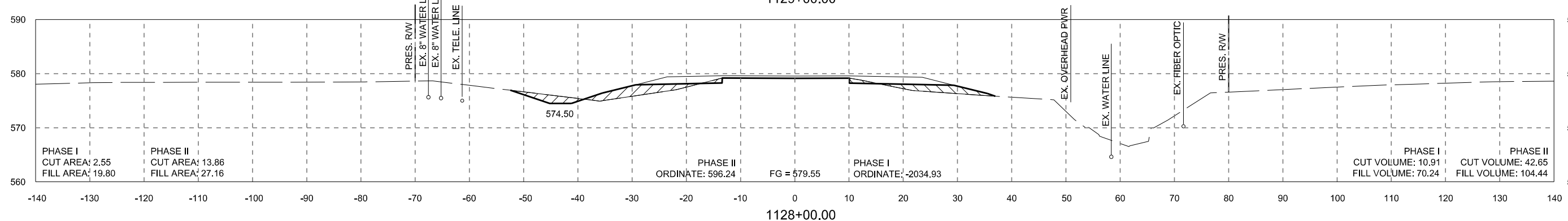
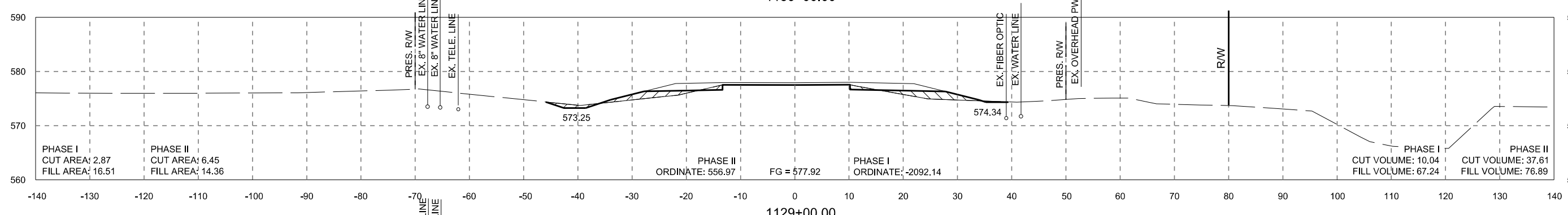
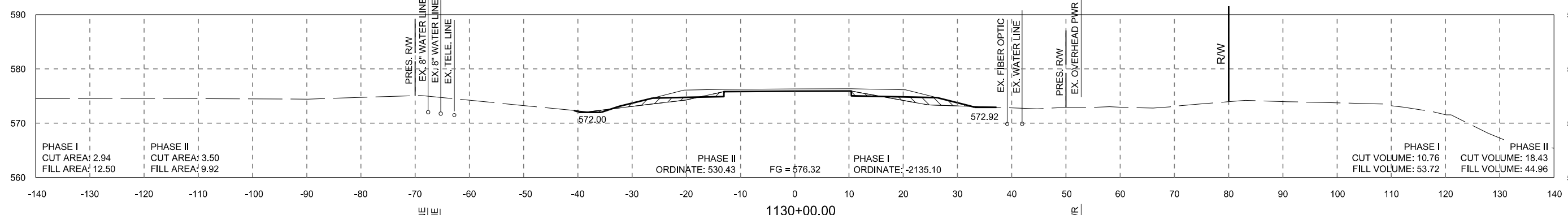
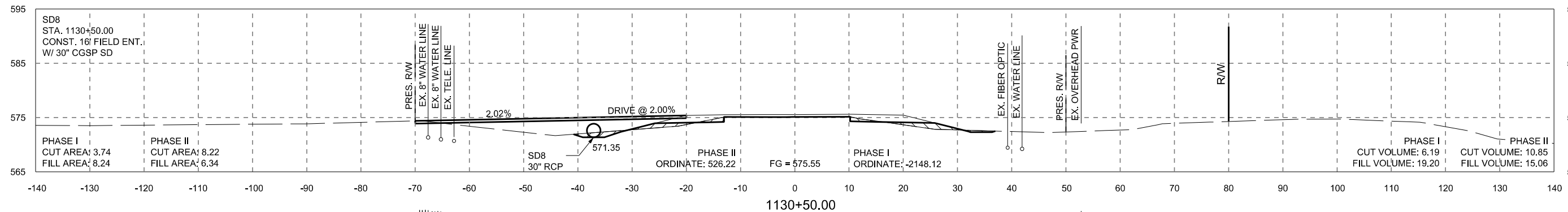
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

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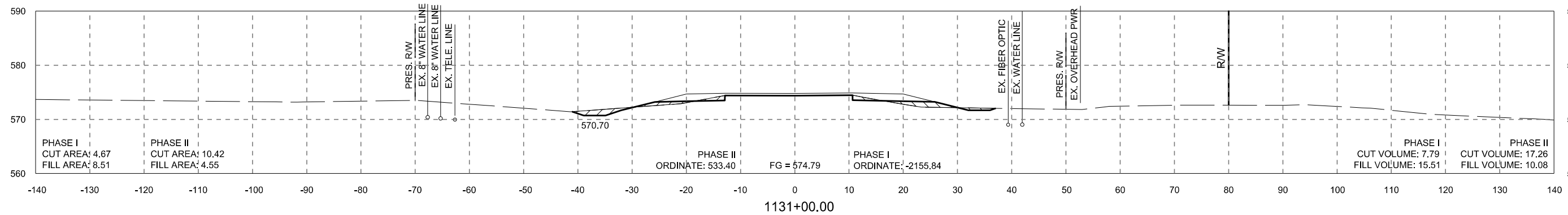
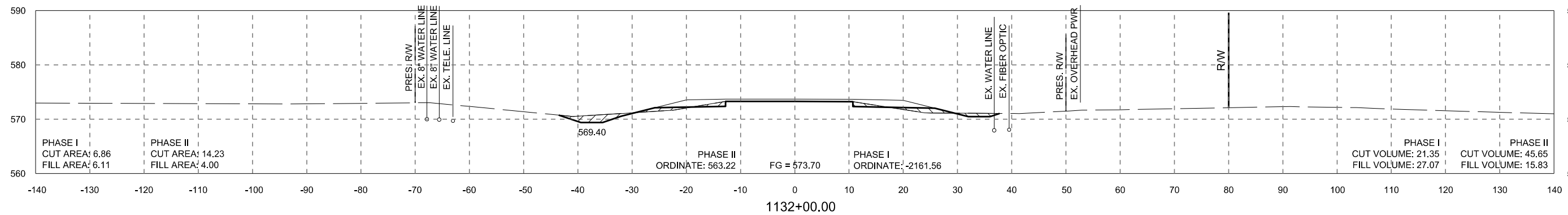
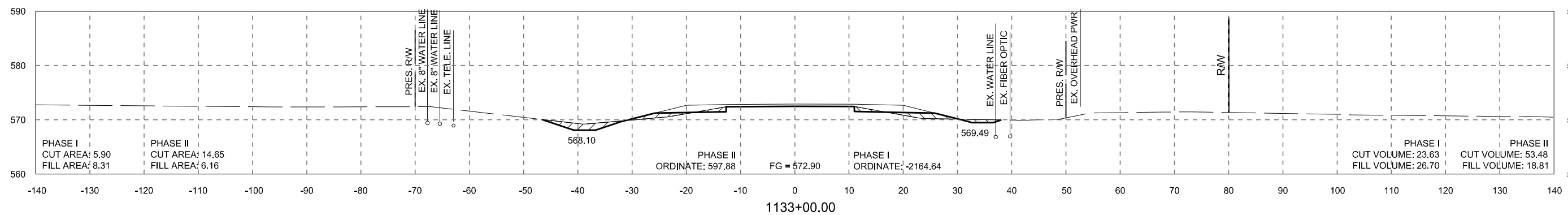
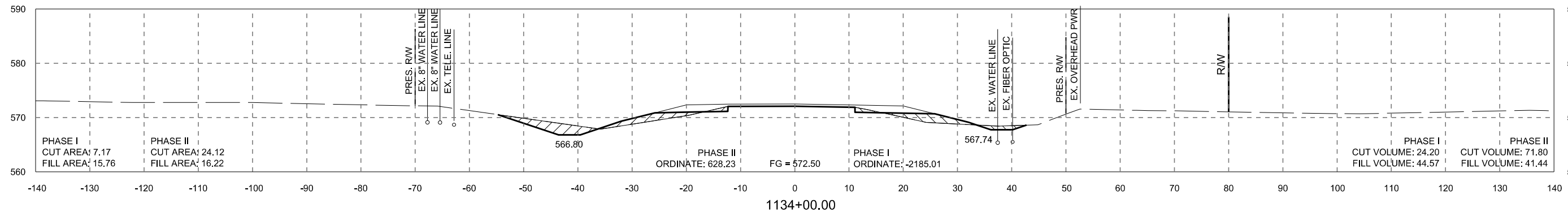
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X025
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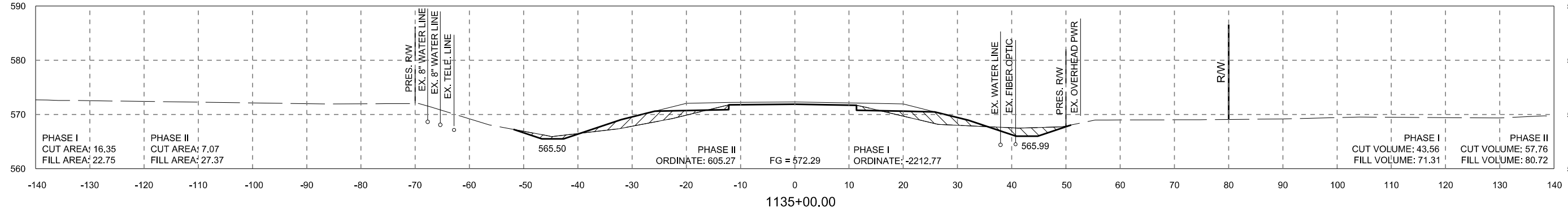
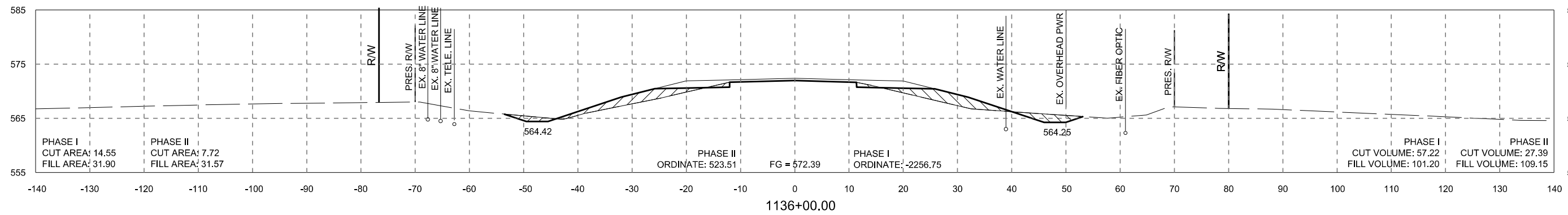
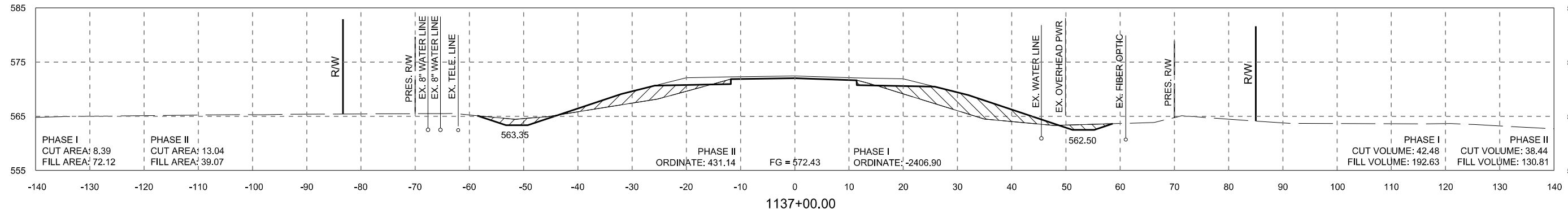
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X026
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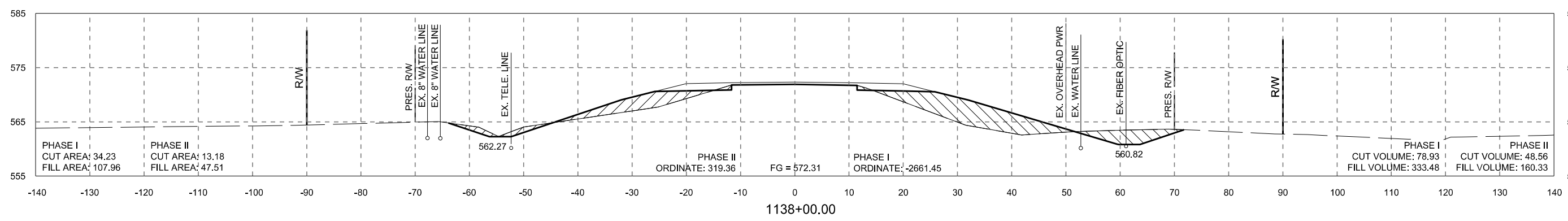
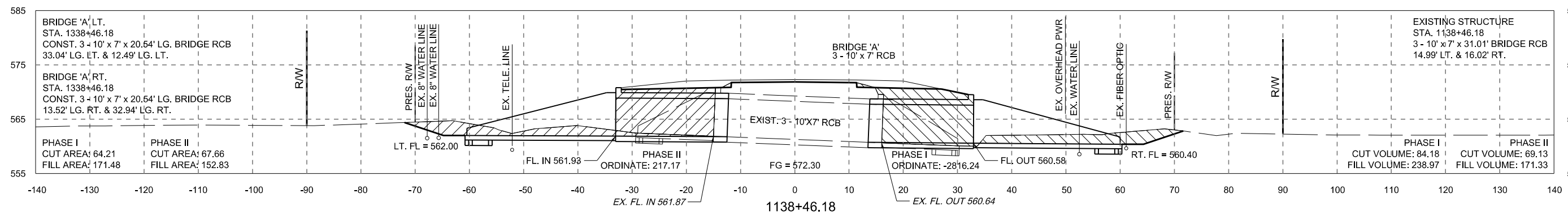
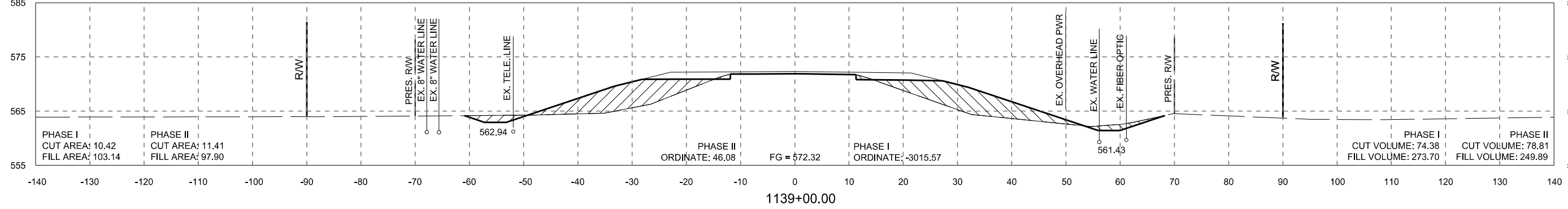
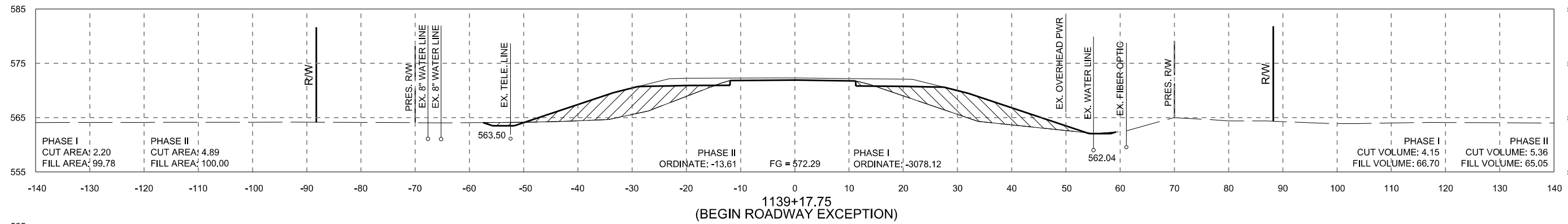
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X027
DRAWN	MM	
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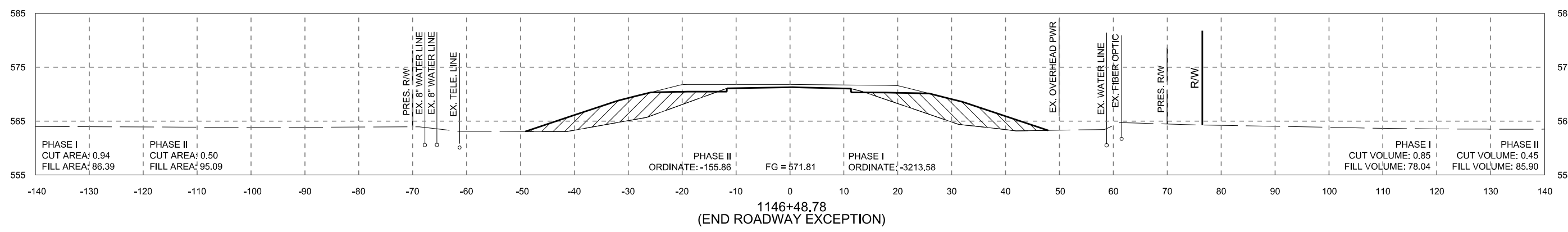
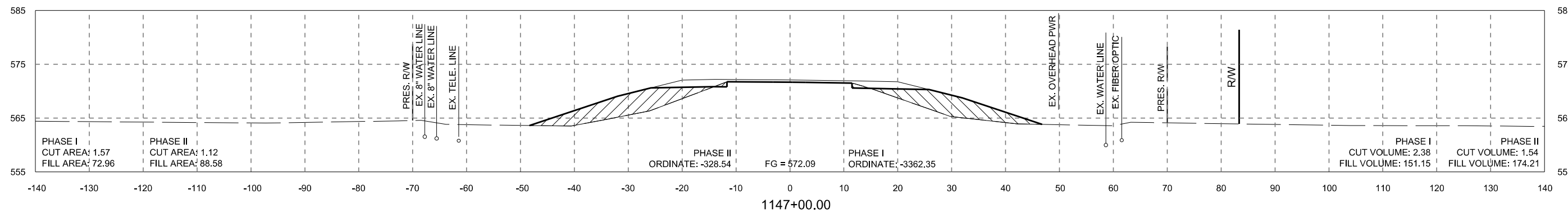
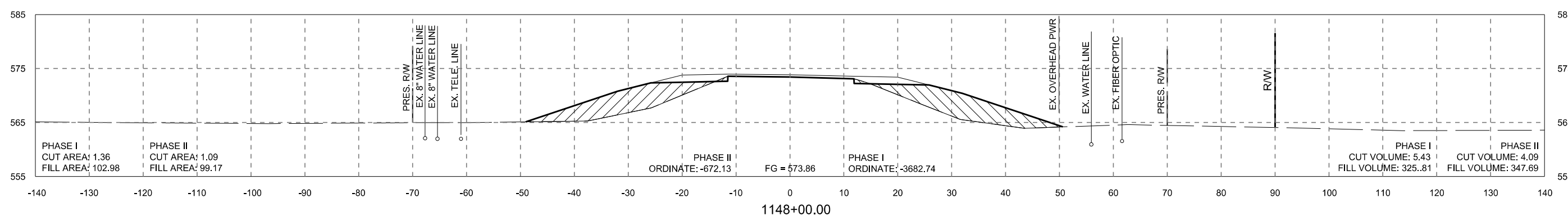
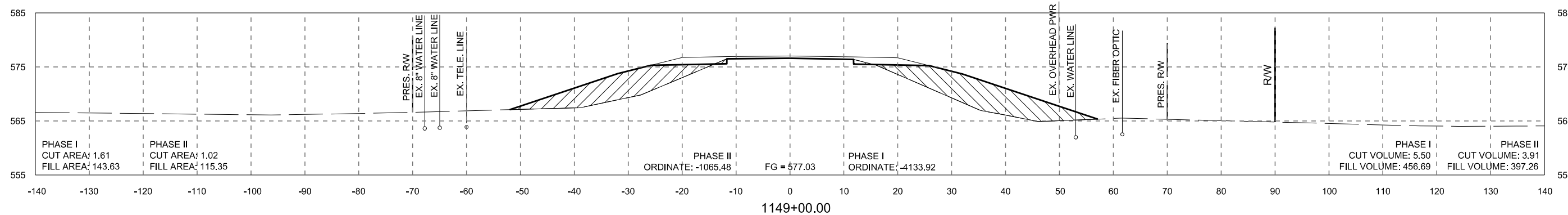
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X028
DRAWN	MM	
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SQUAD	BKL, Inc.	

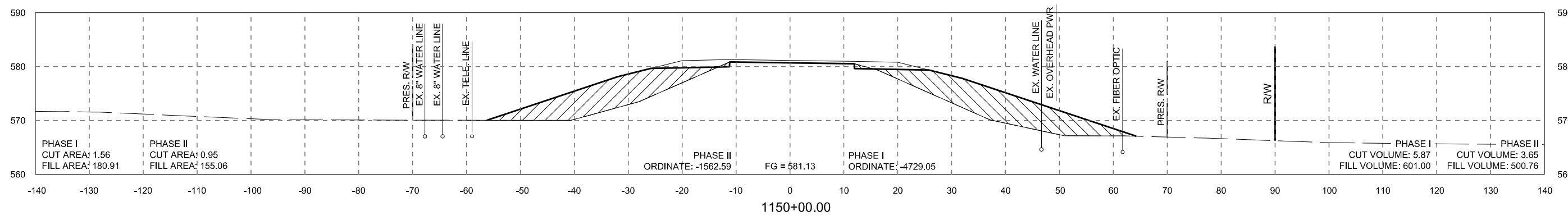
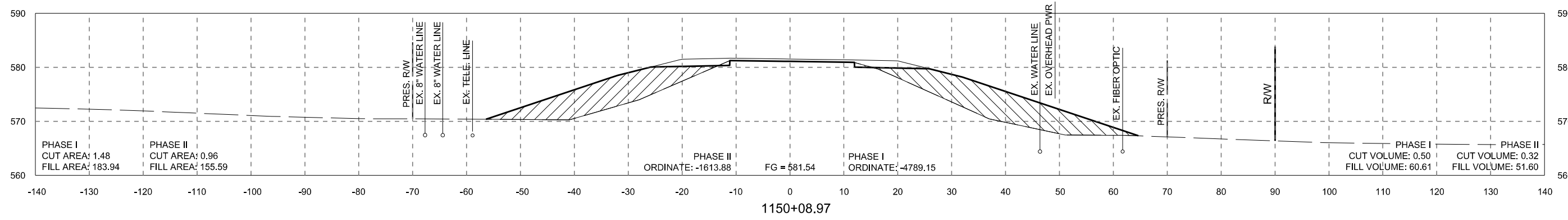
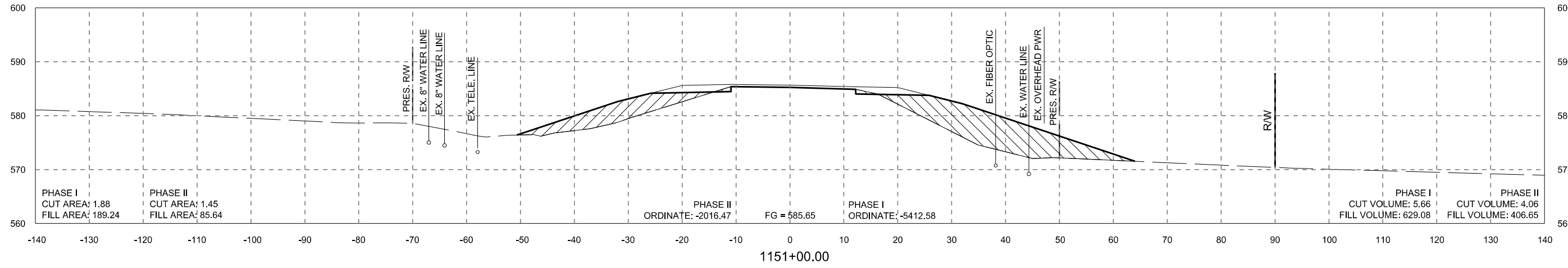
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SCALE: HORIZ. 1" = 1'-0"
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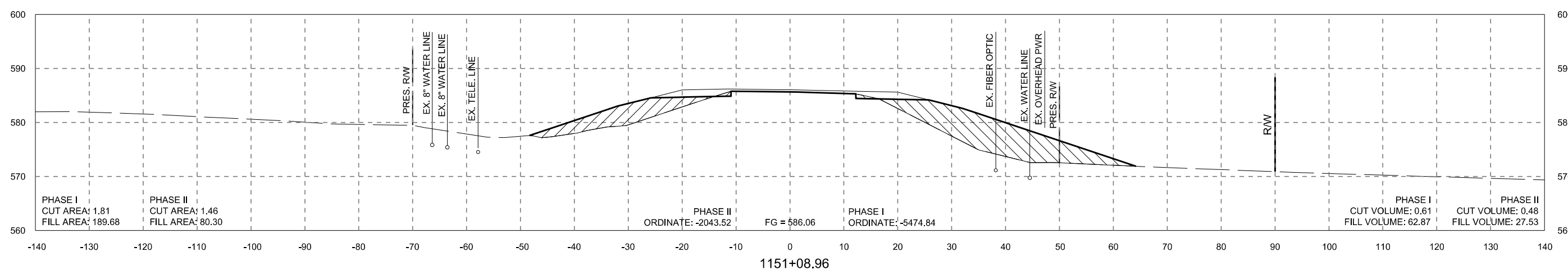
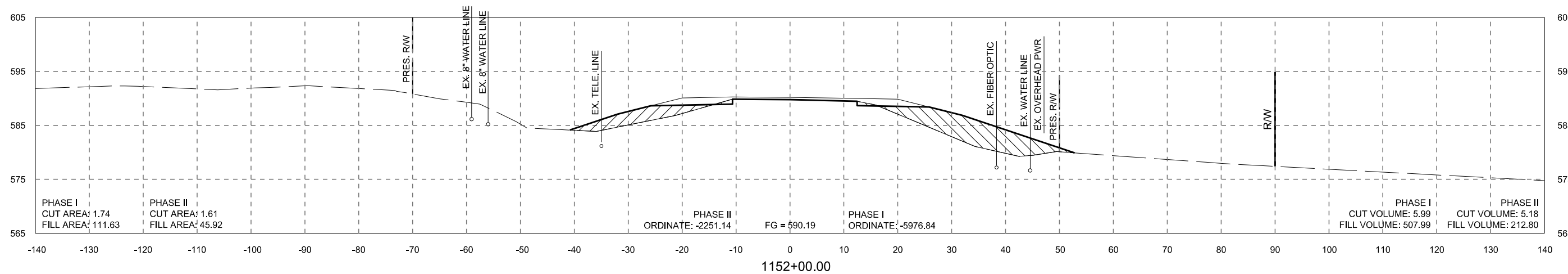
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APPROVED	SL	
SQUAD	BKL, Inc.	

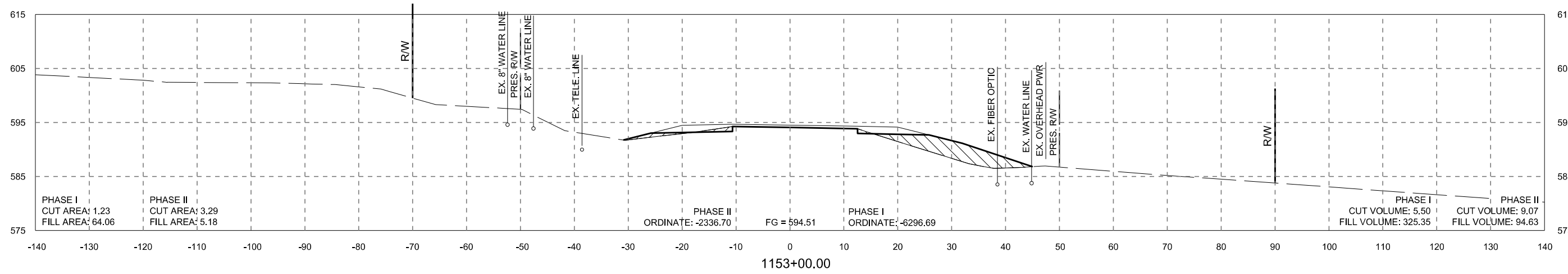
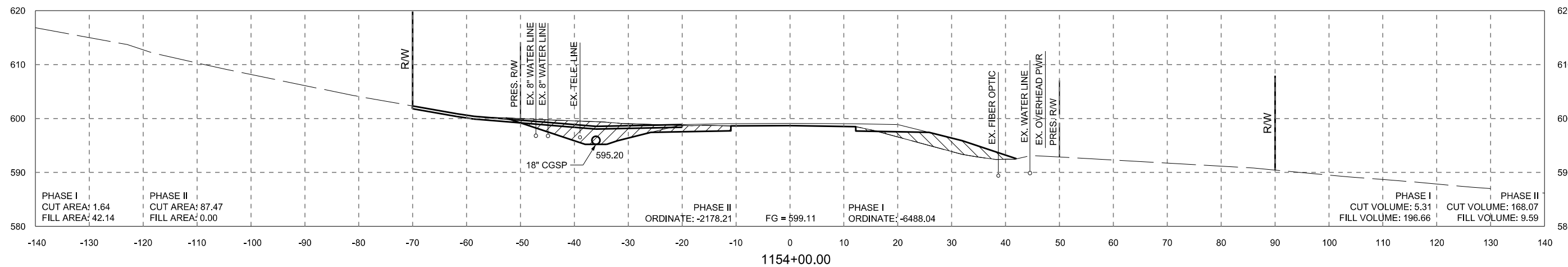
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

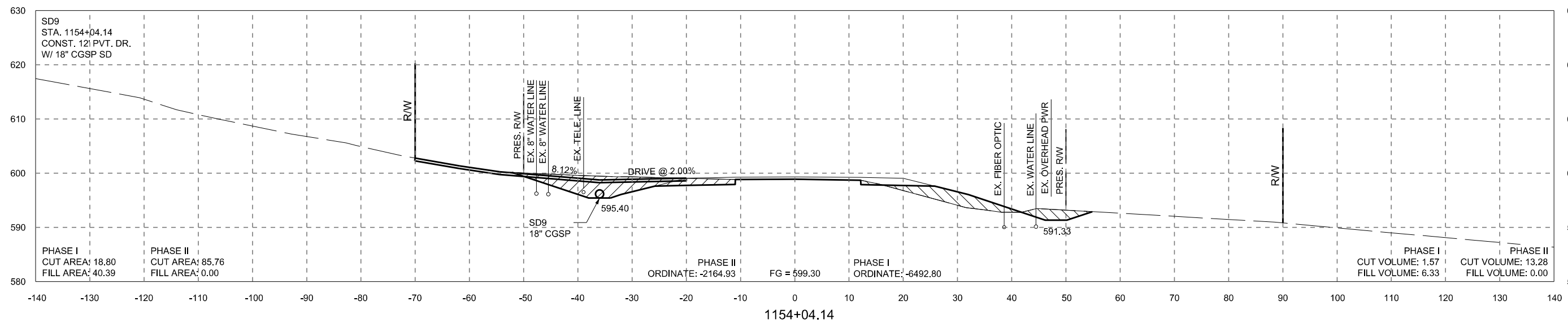
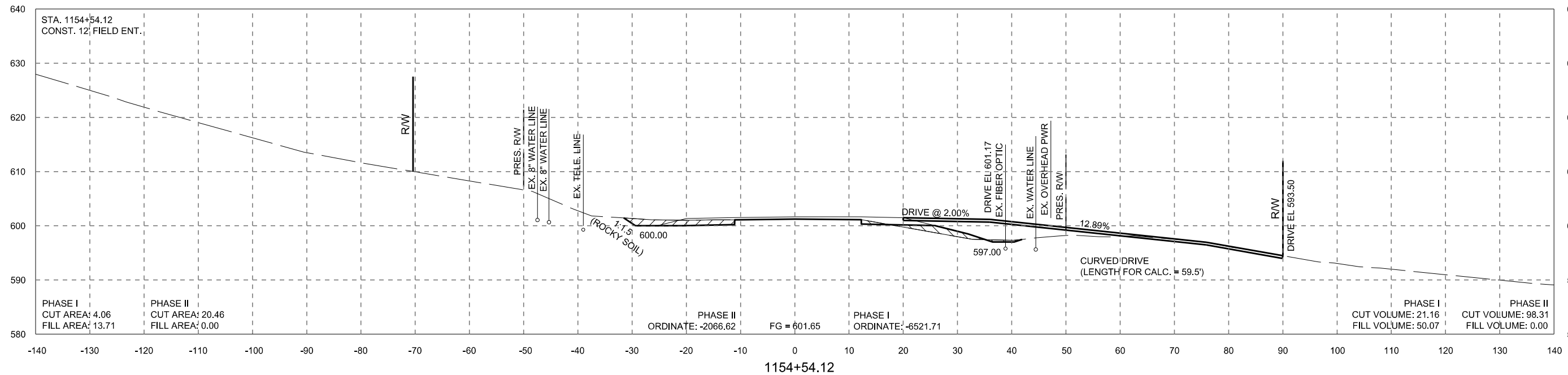
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CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
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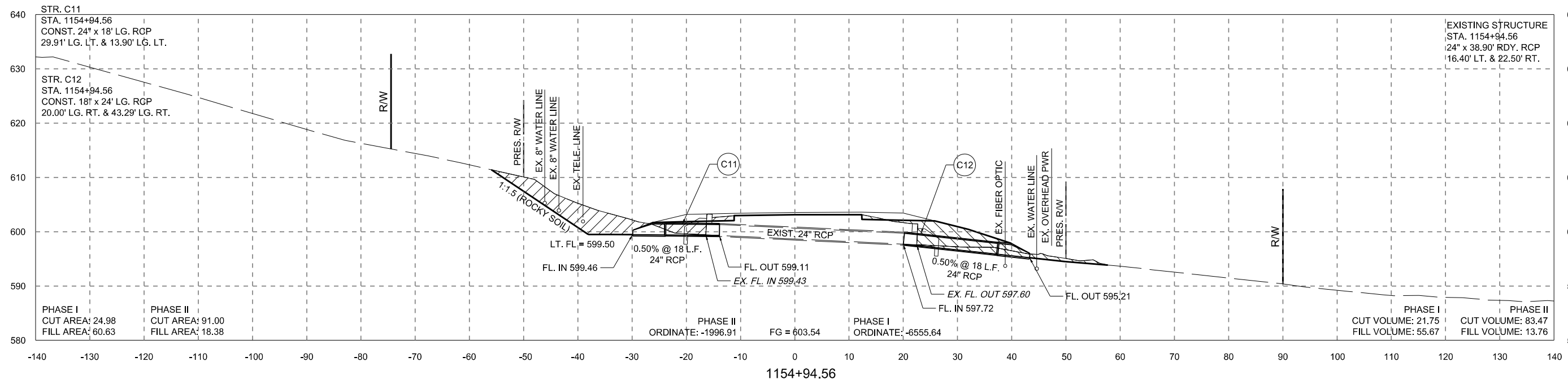
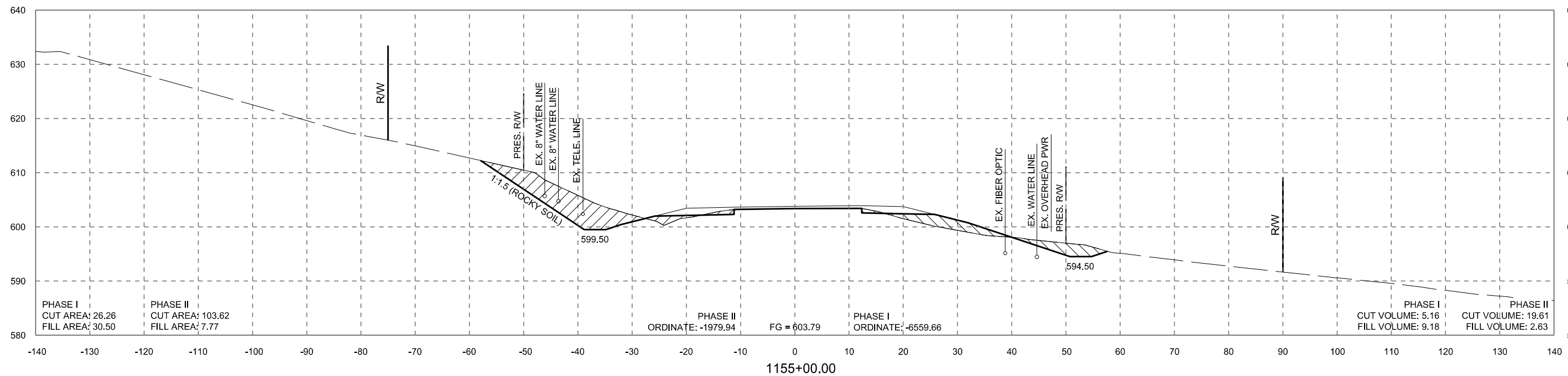
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CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	



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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X033
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CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

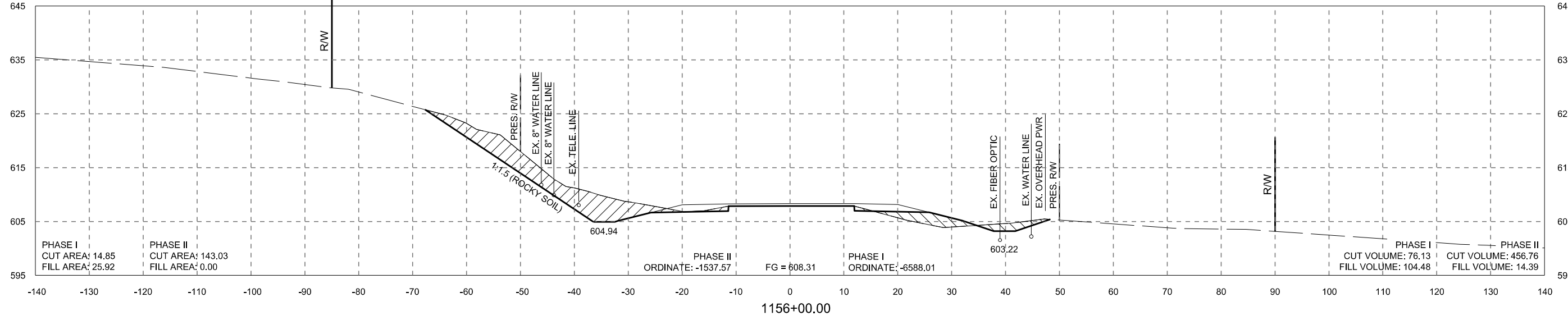
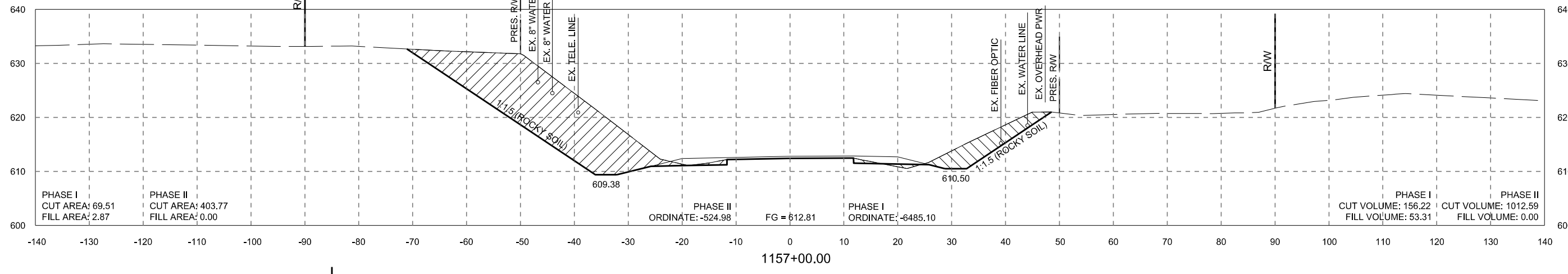
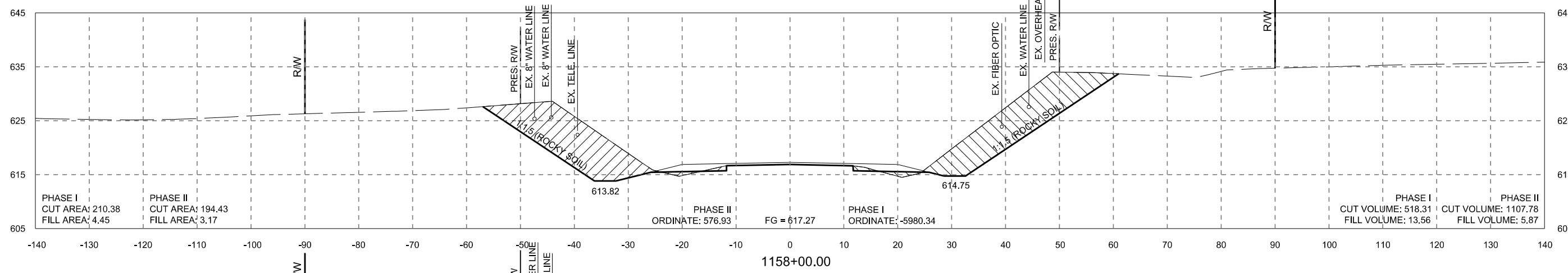


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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

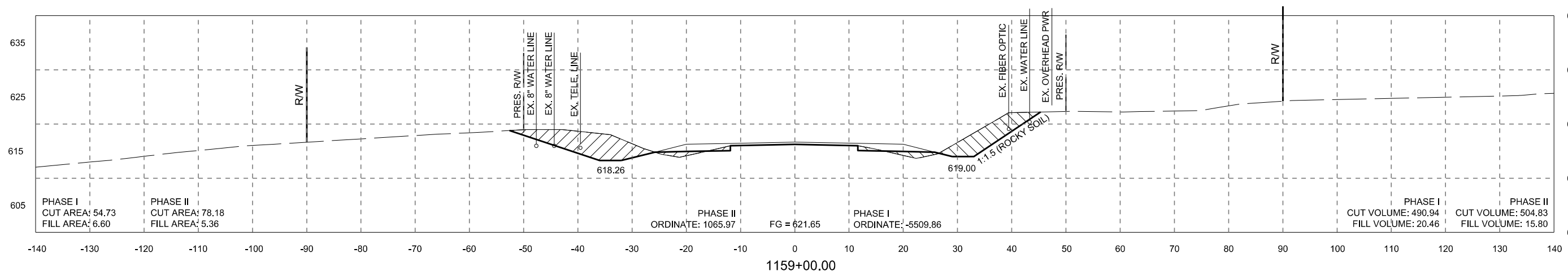
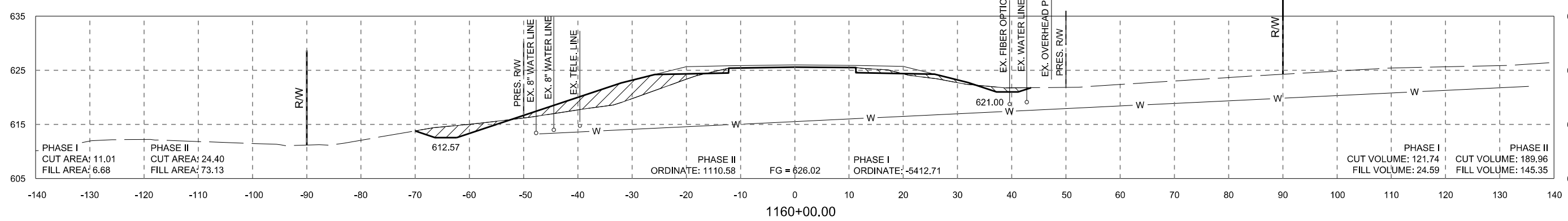
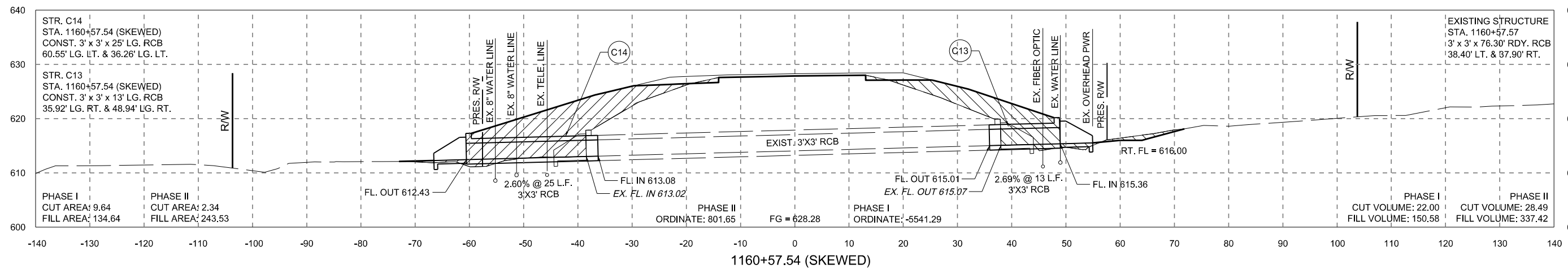
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DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X035
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CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

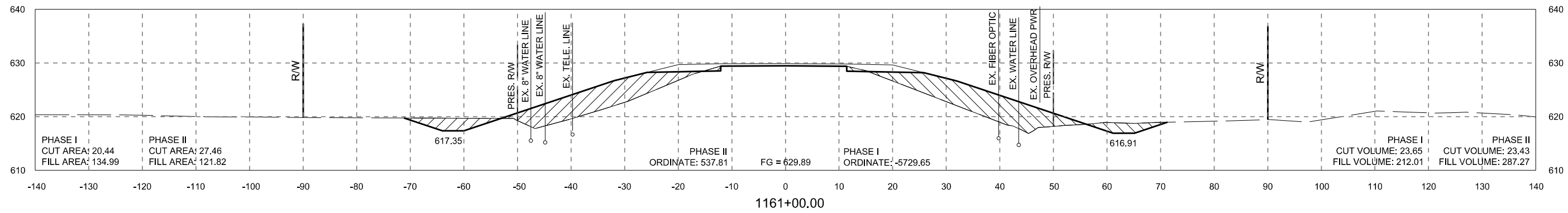
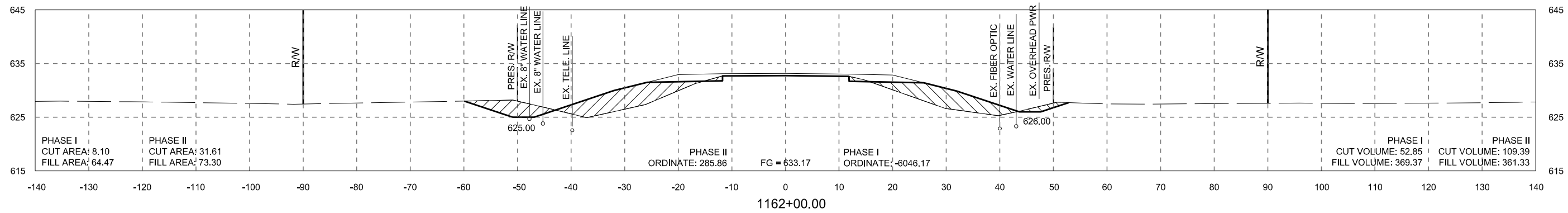
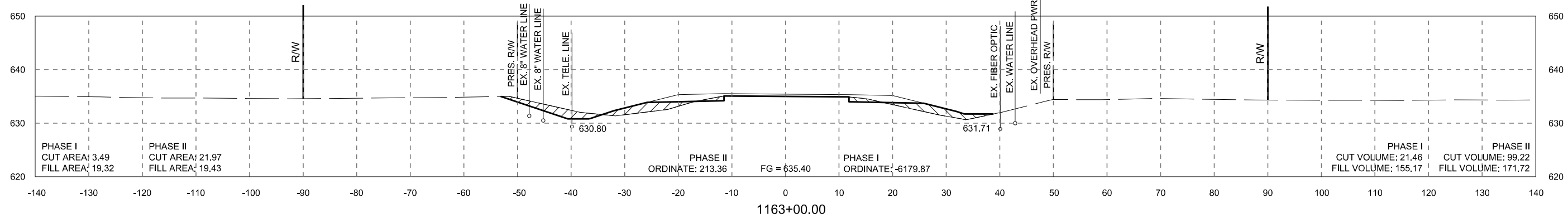


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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X036
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

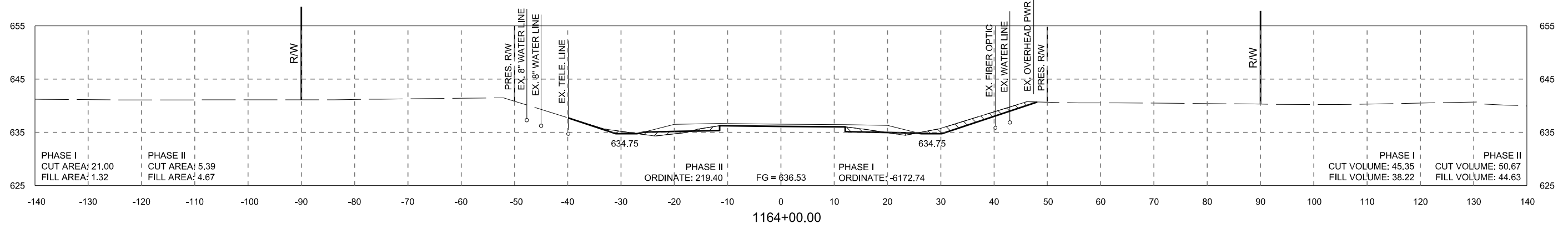
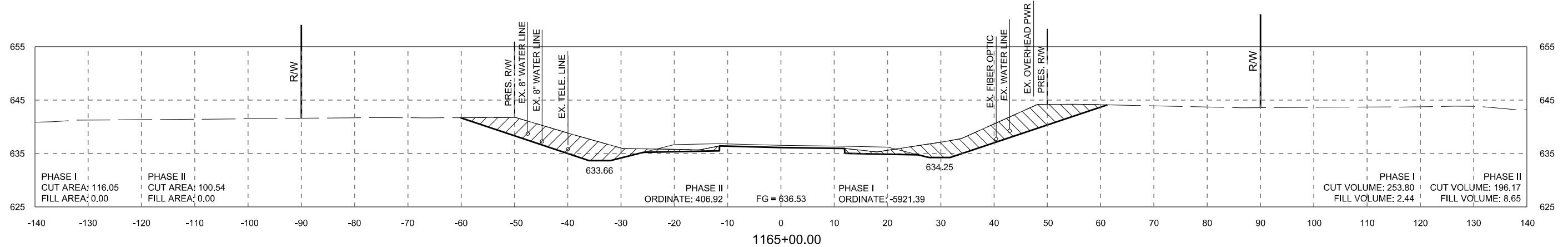
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X037
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

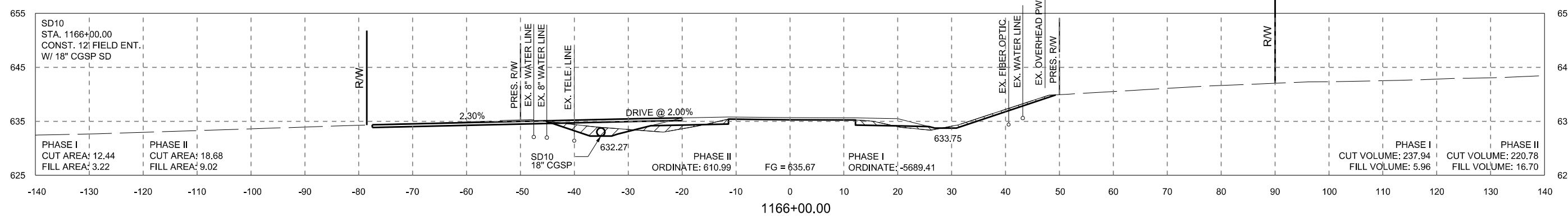
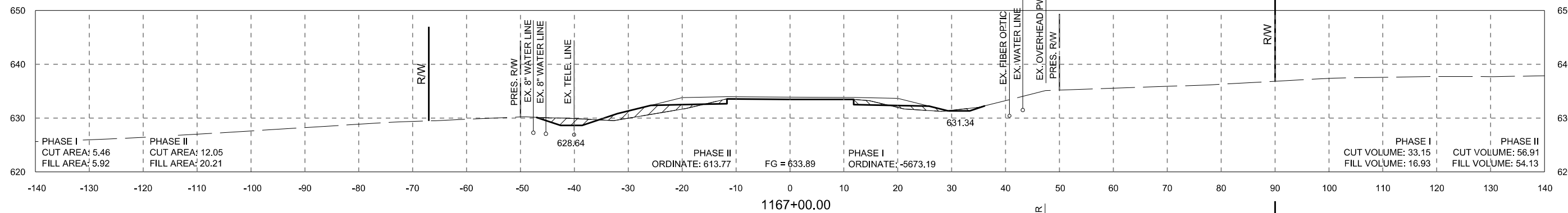
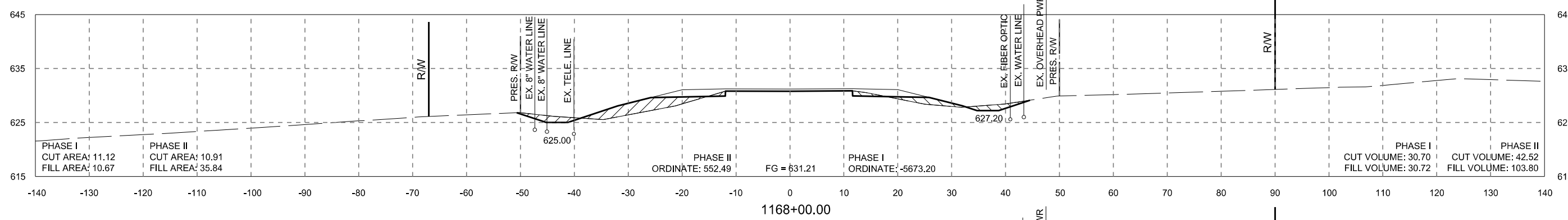
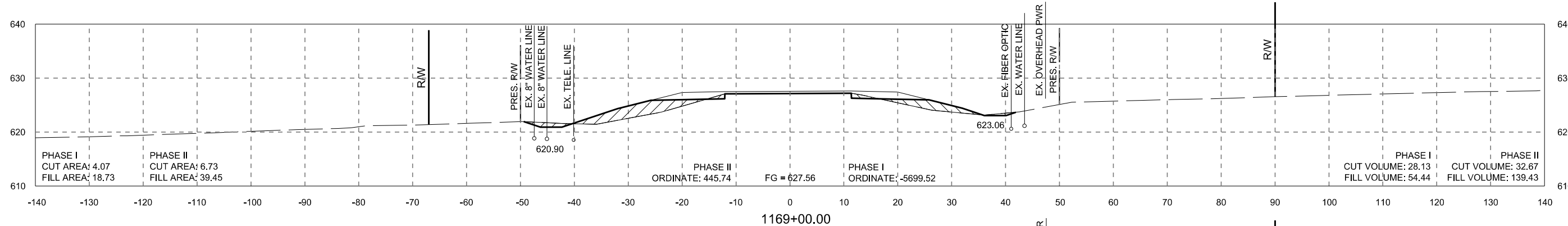
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X038
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

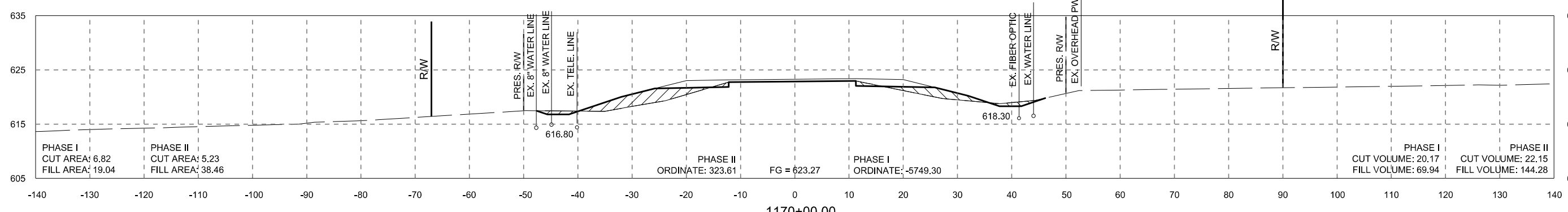
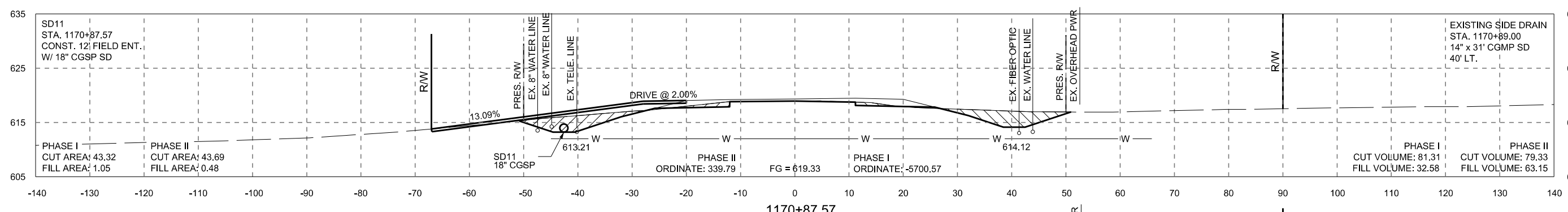
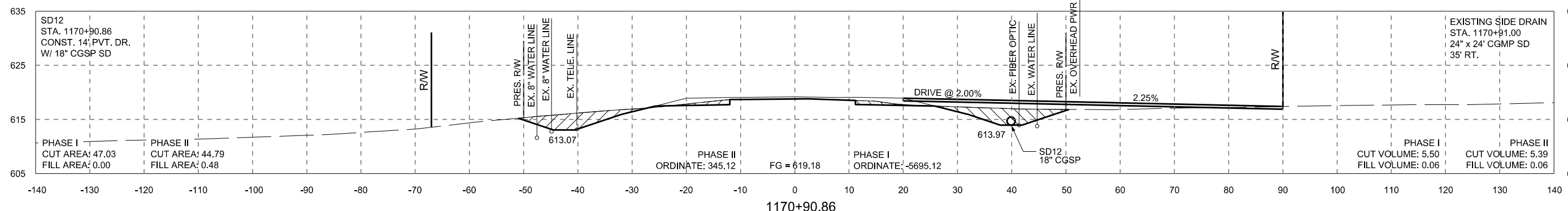
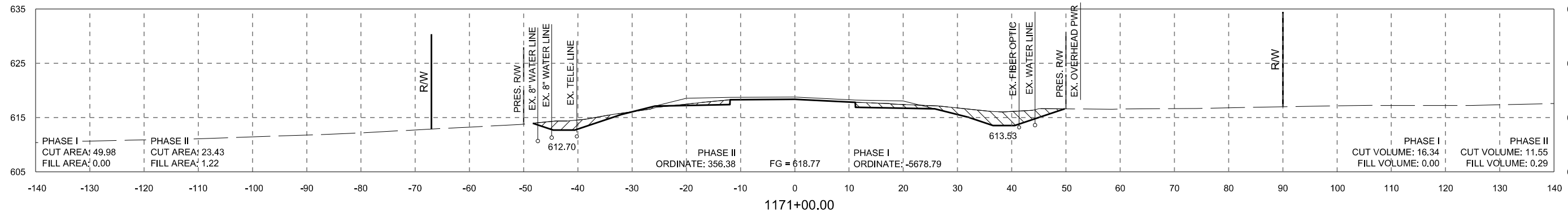
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X039
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

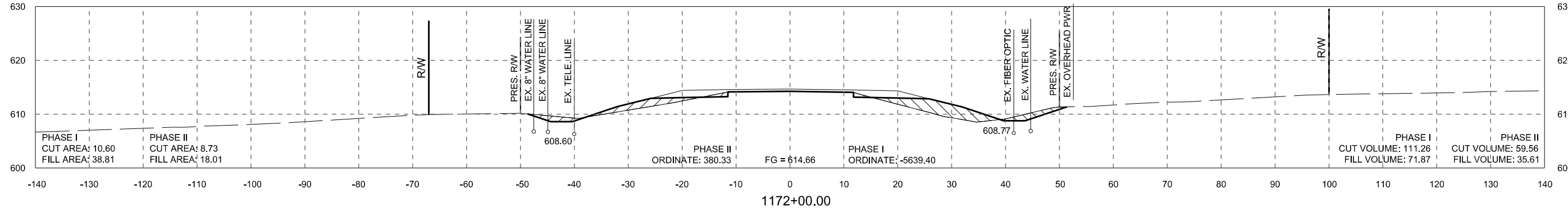
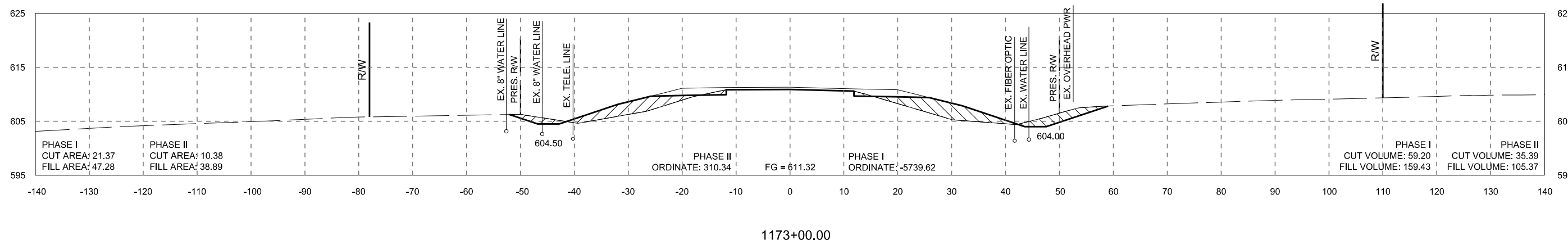
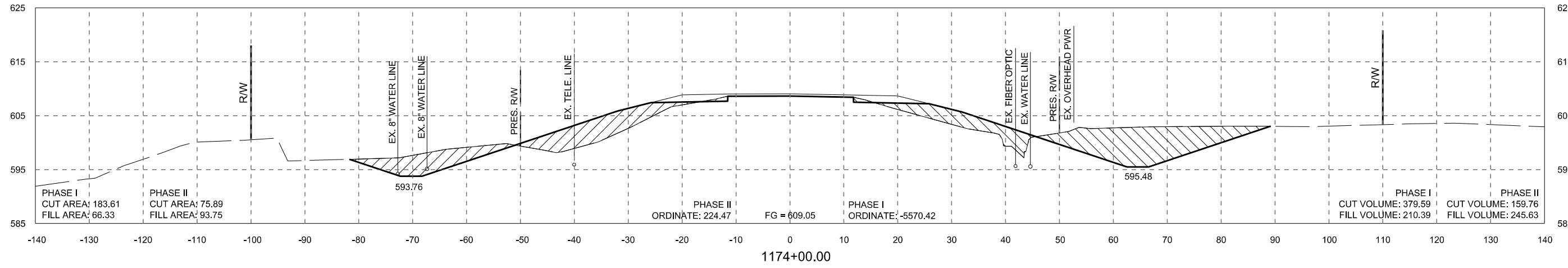
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SCALE: HORIZ. 1" = 1'-0"
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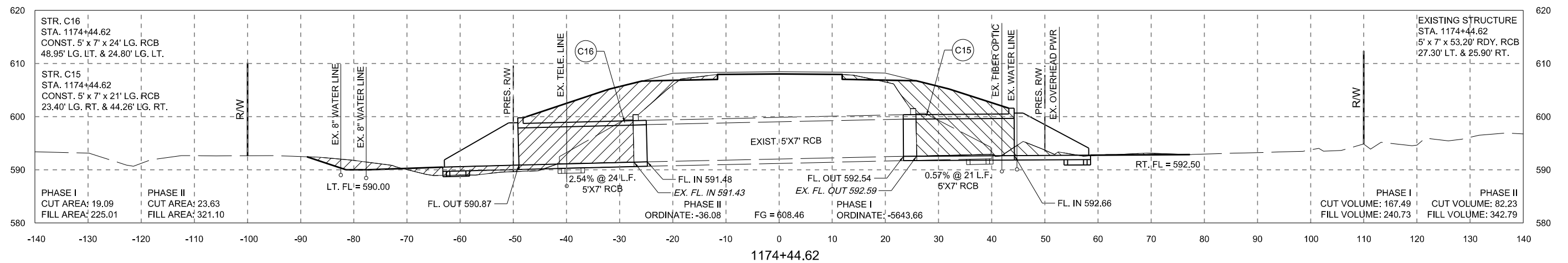
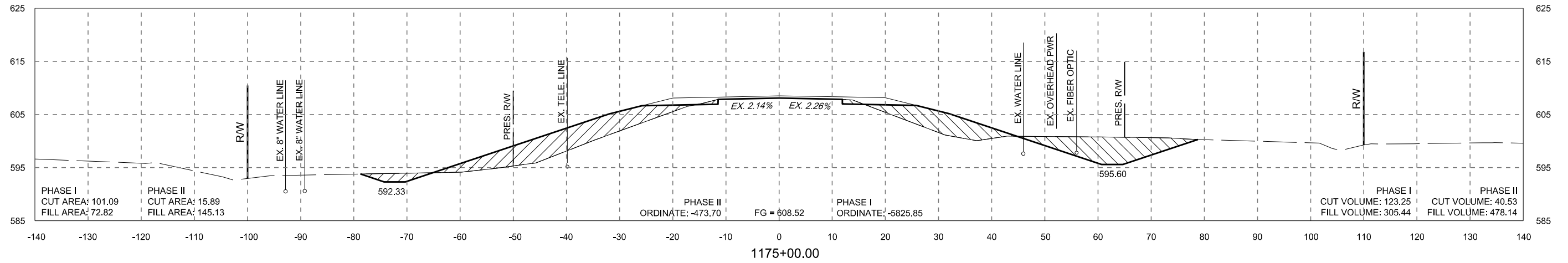
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DRAWN	MM	
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APPROVED	SL	
SQUAD	BKL, Inc.	

PRINT DATE: 8/22/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTION MAINLINE.dgn



SCALE: HORIZ. 1" = 1'-0"
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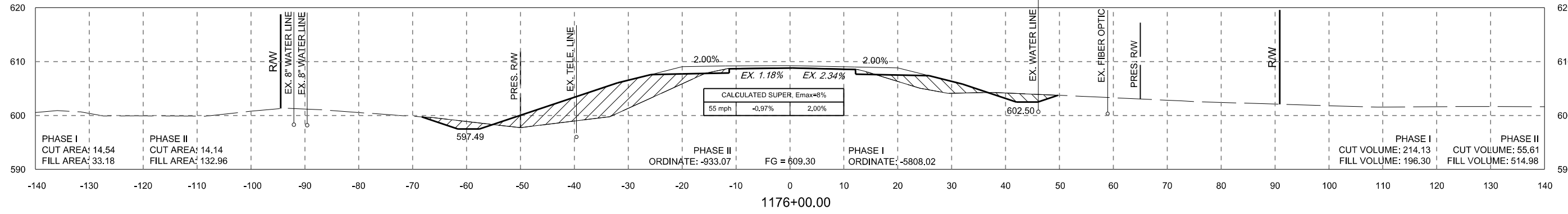
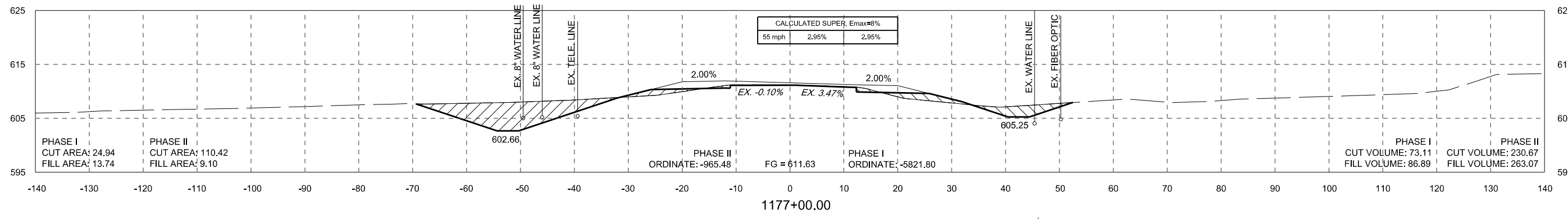
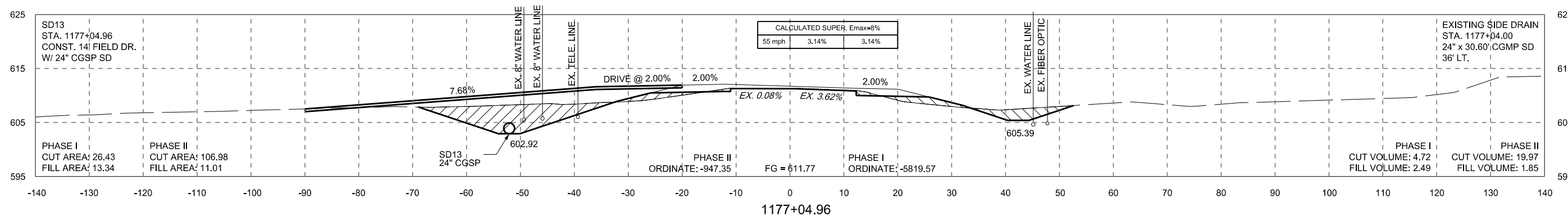
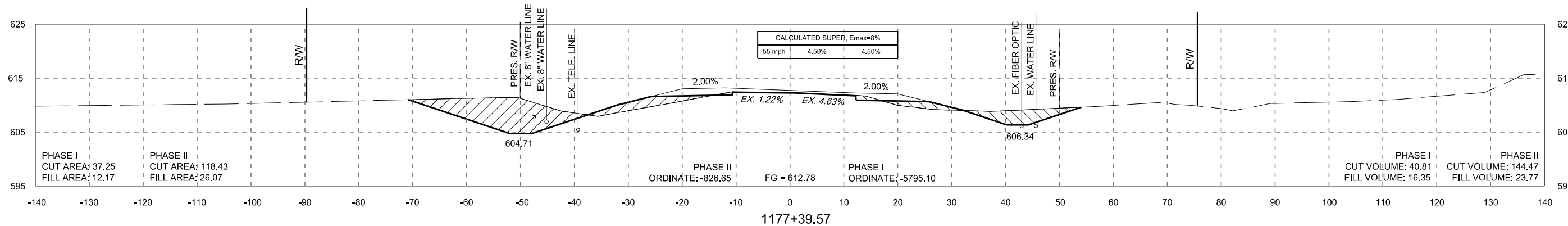


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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X042
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CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

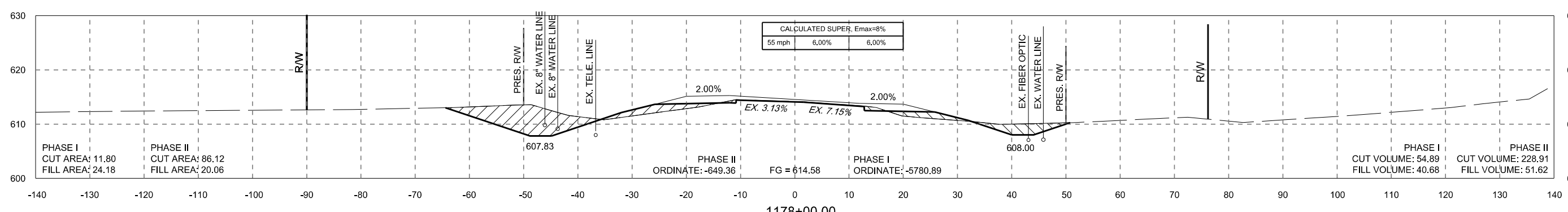
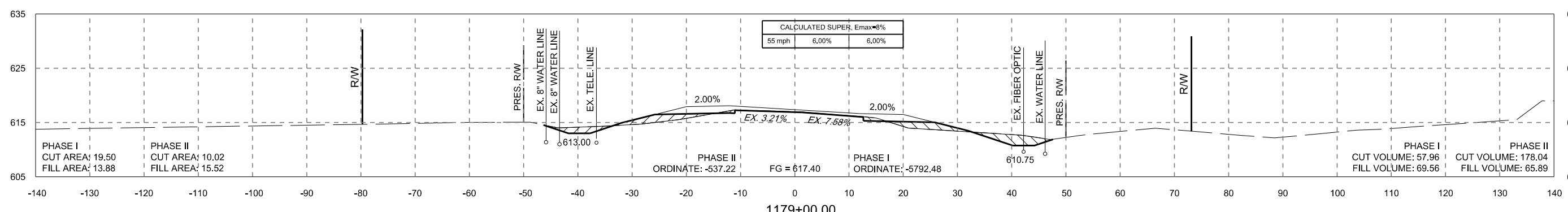
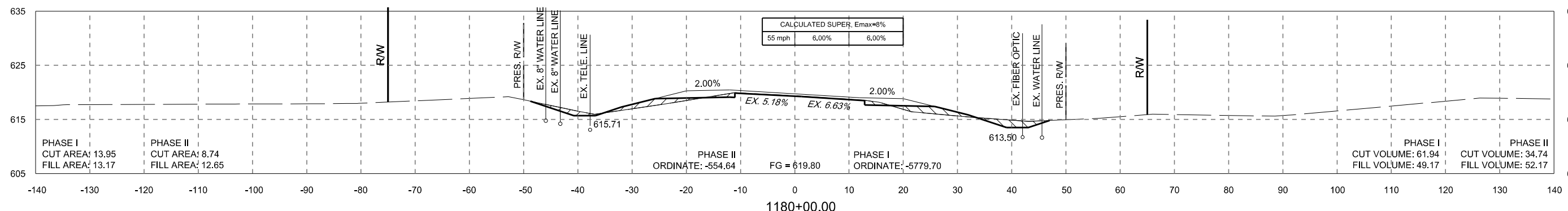
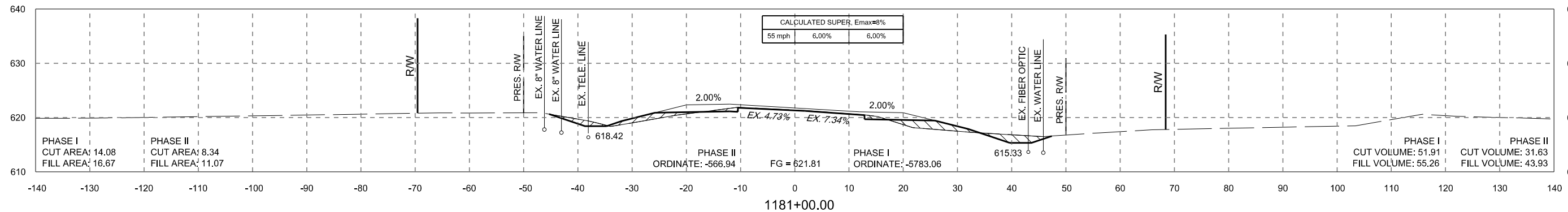
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X043
DRAWN	MM	
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SQUAD	BKL, Inc.	

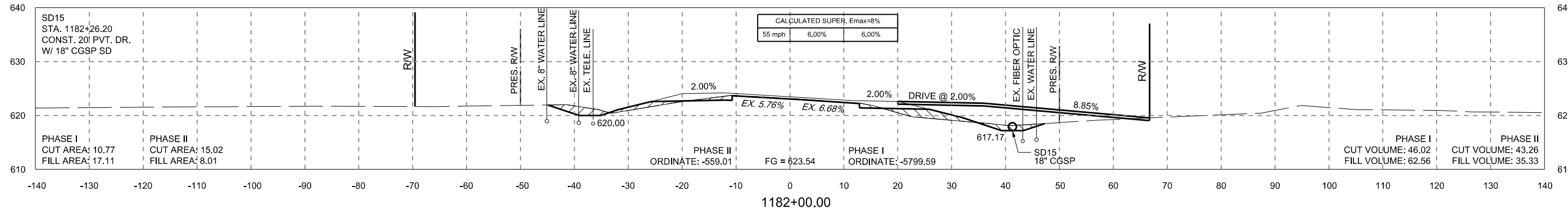
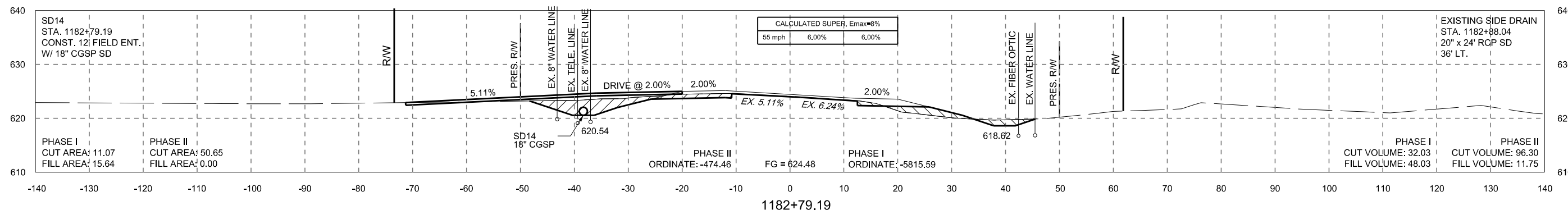
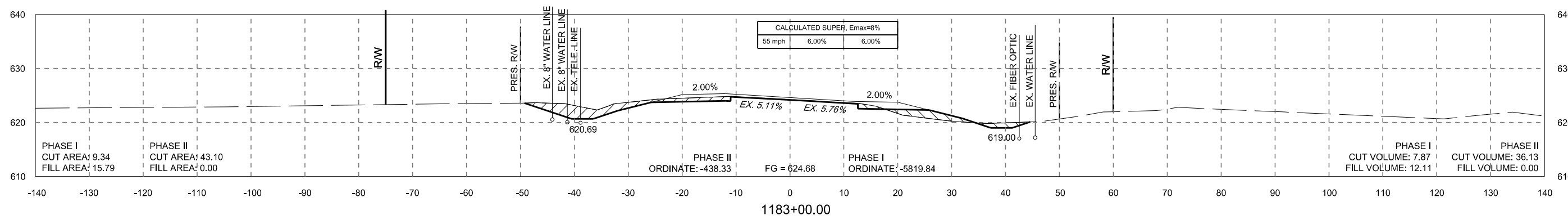
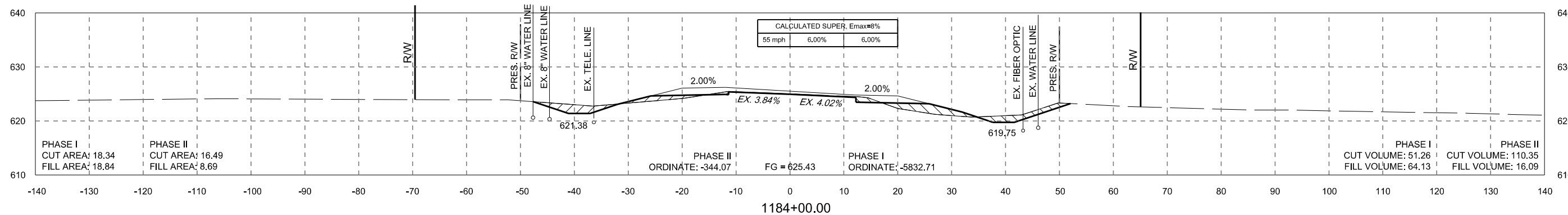
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X044
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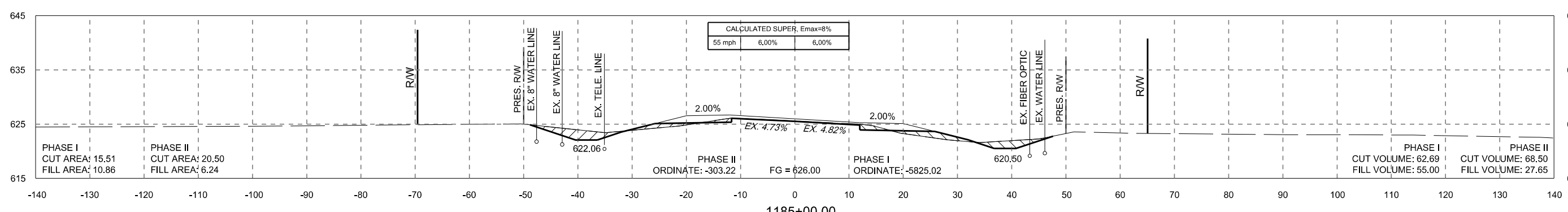
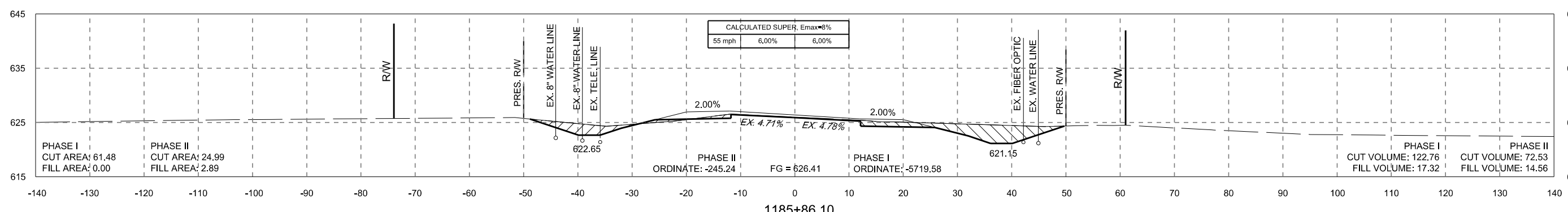
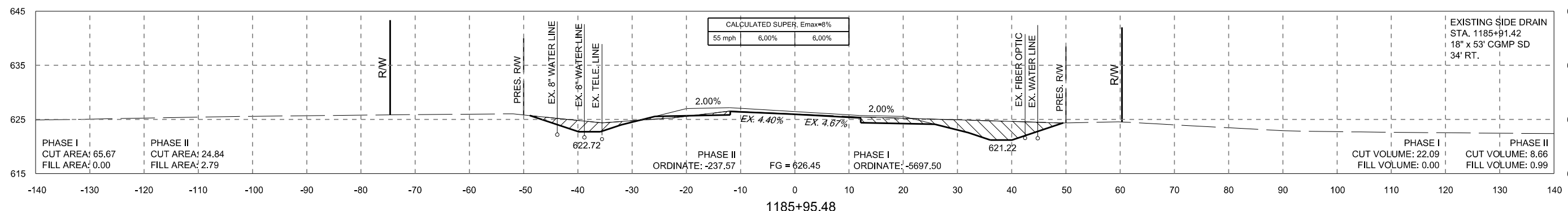
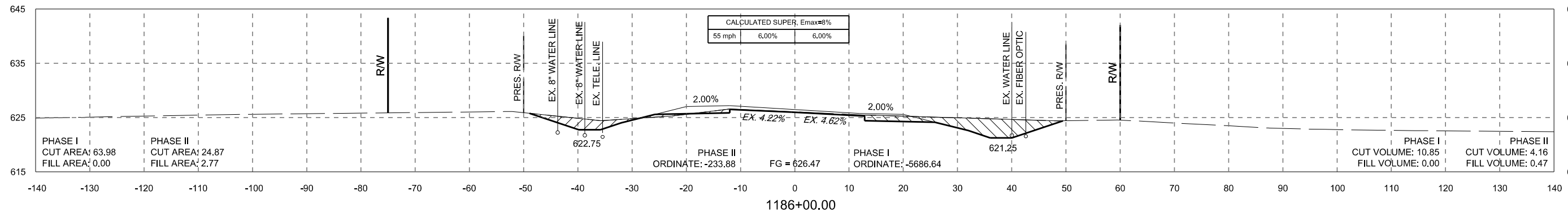
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X045
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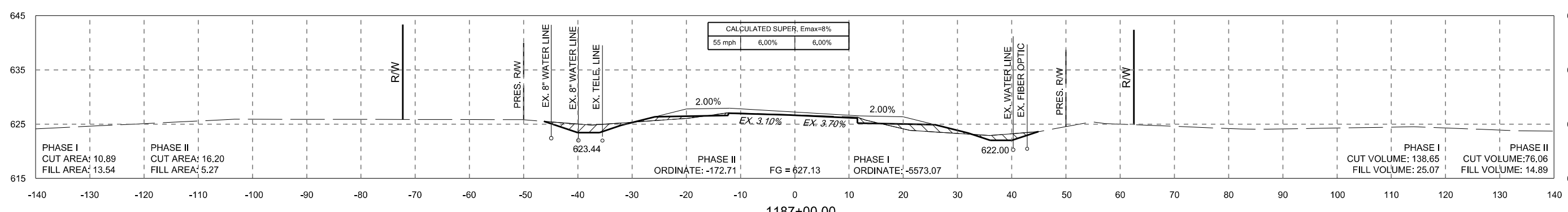
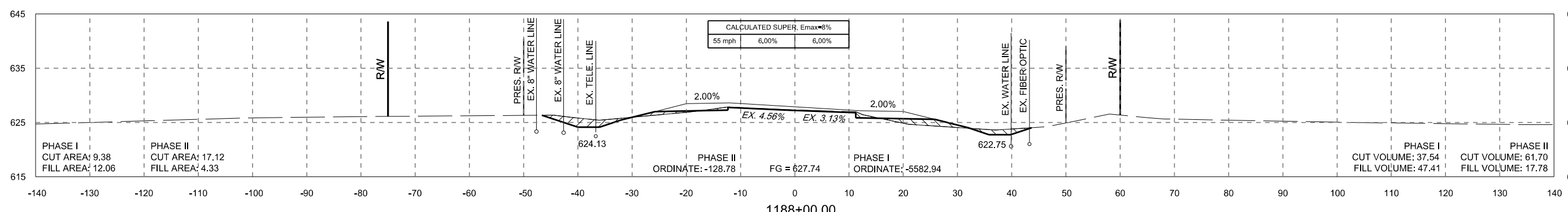
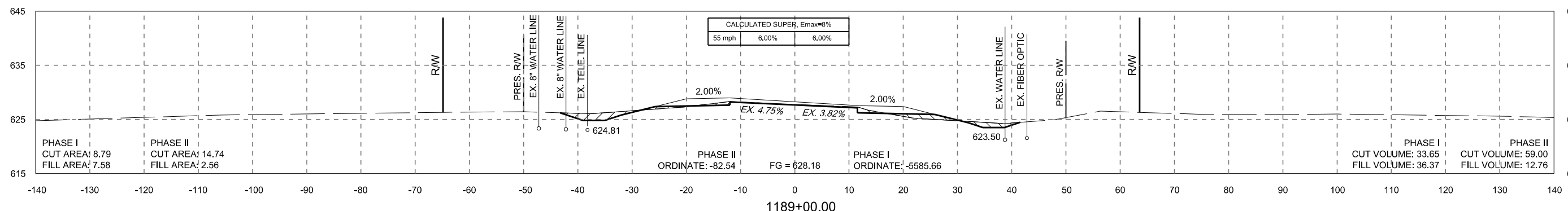
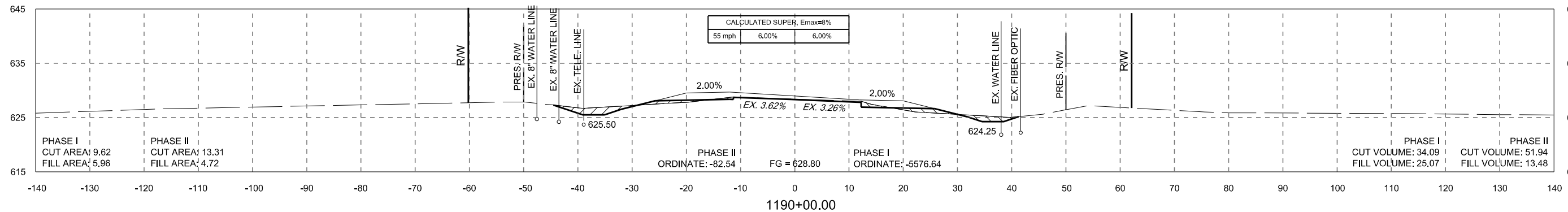
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SCALE: HORIZ. 1" = 1'-0"
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DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X046
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CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

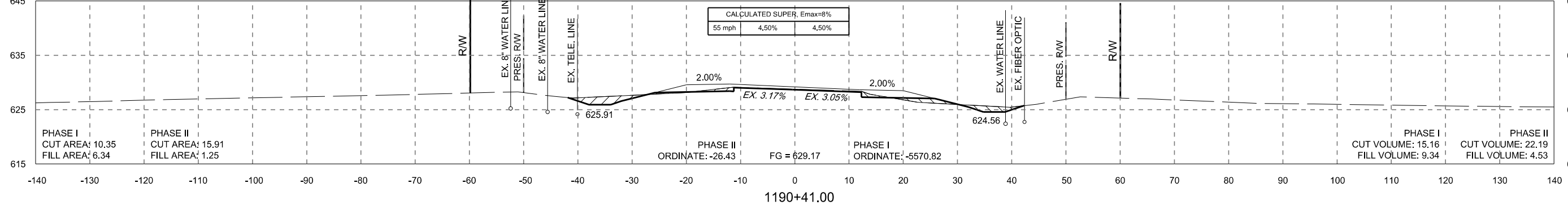
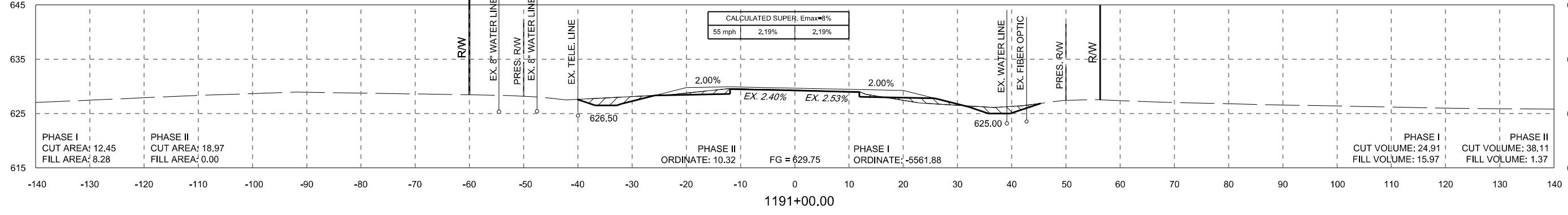
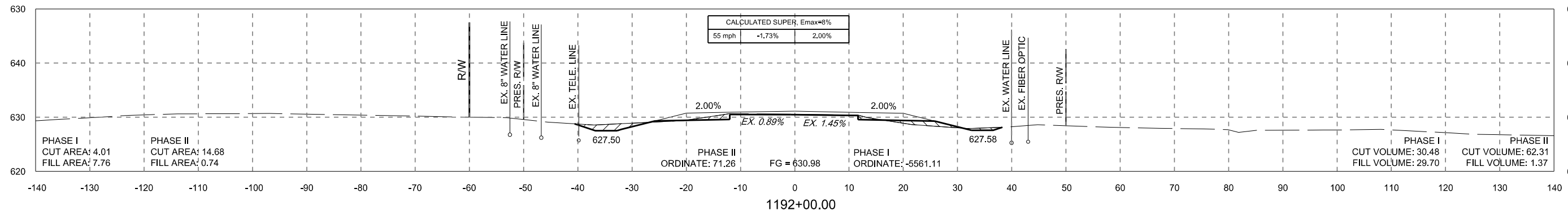
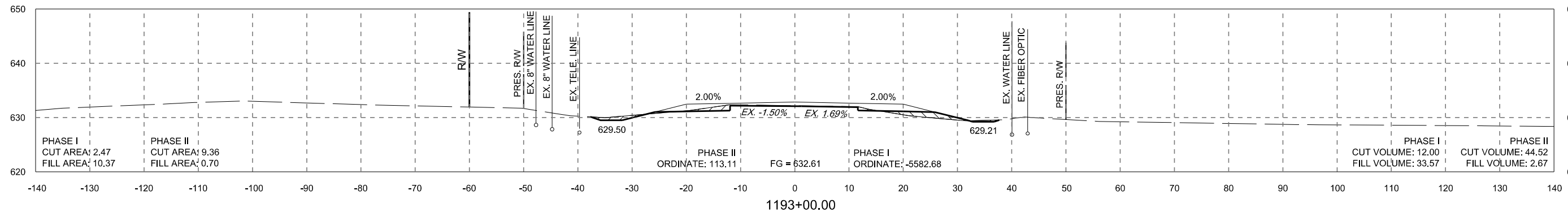
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X047
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

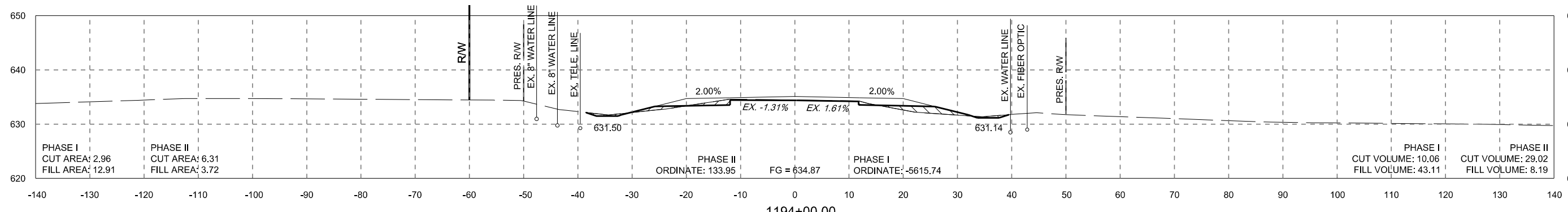
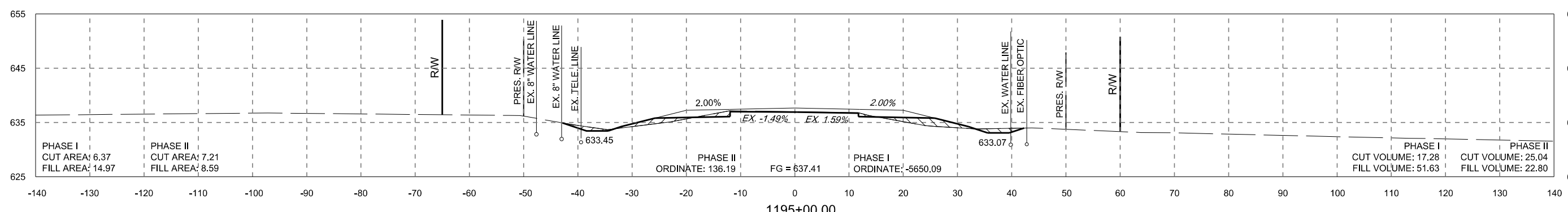
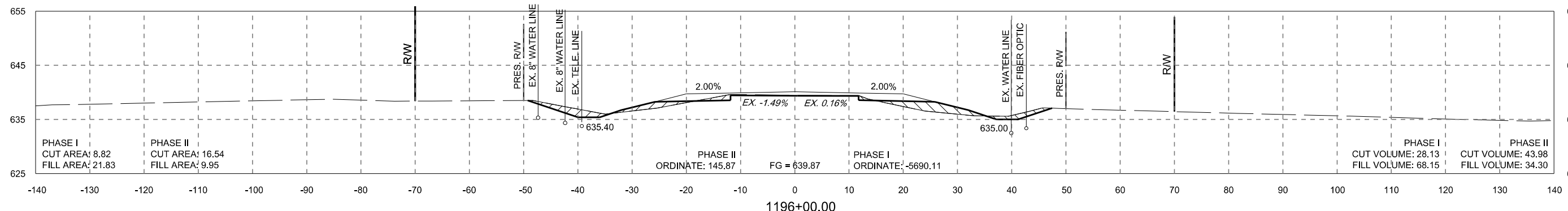
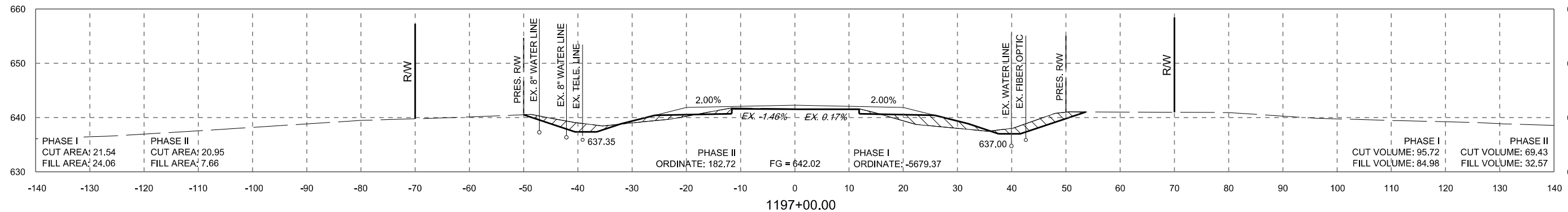
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X048
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APPROVED	SL	
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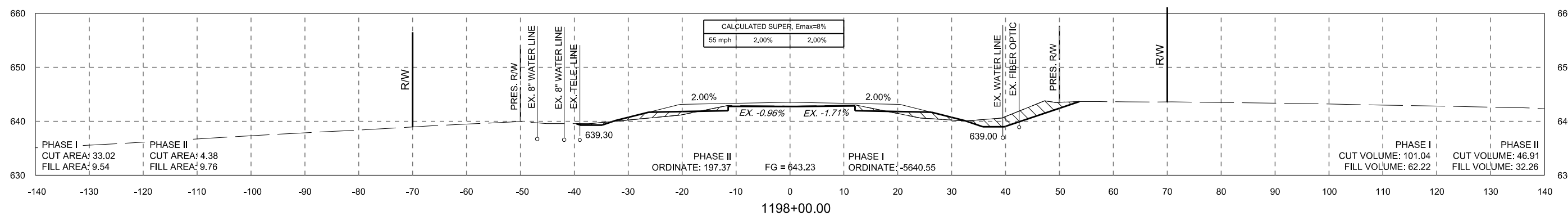
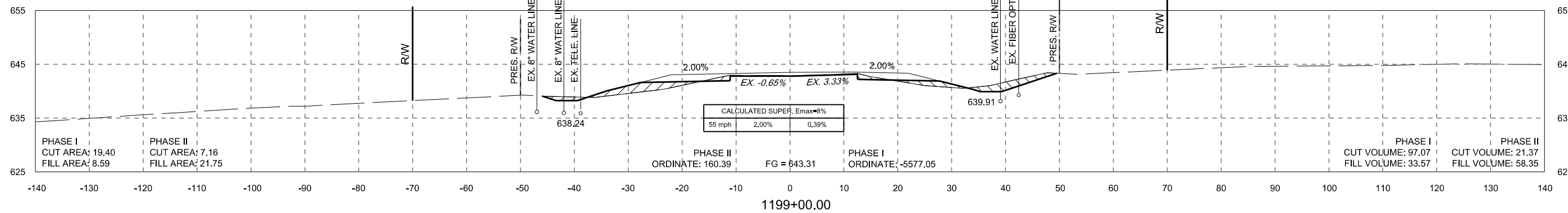
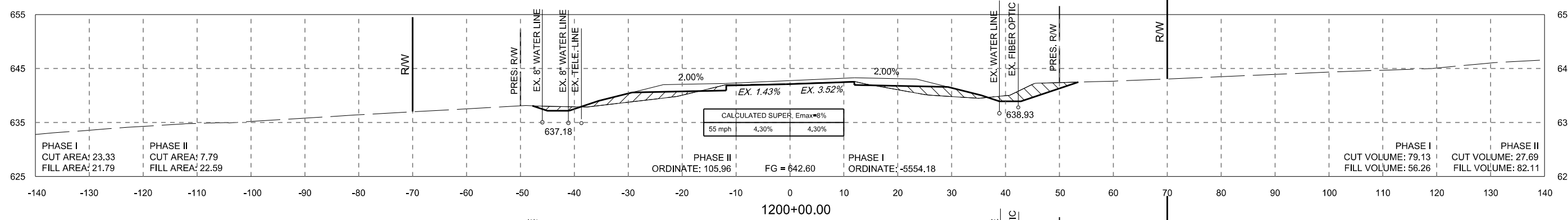
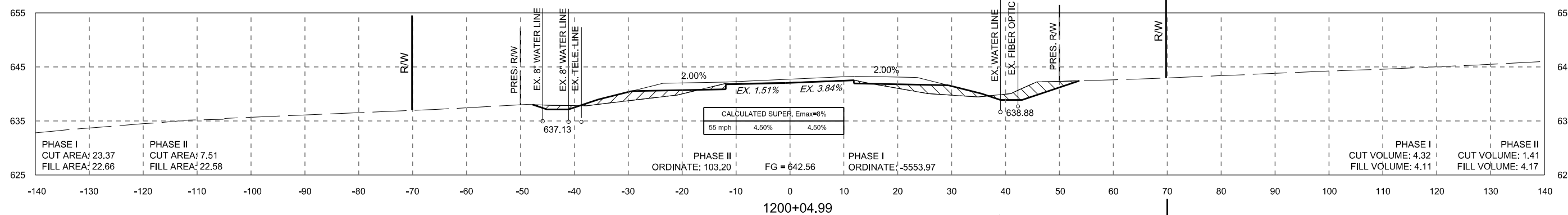
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SCALE: HORIZ. 1" = 1'-0"
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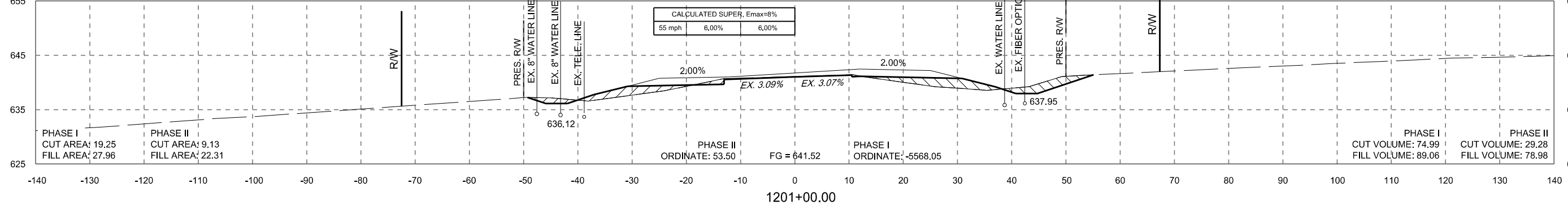
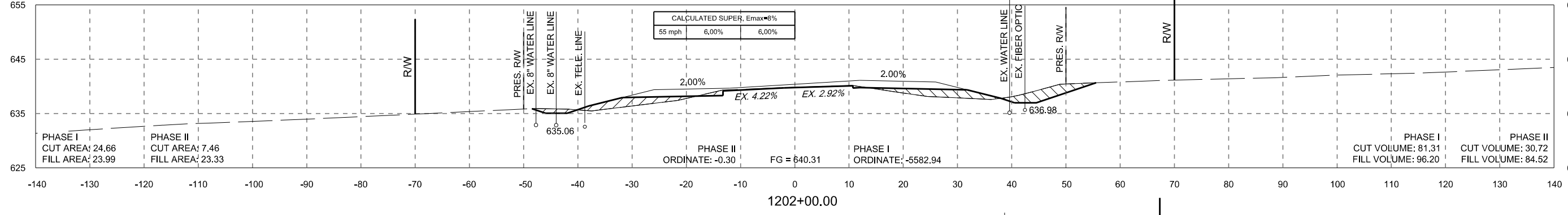
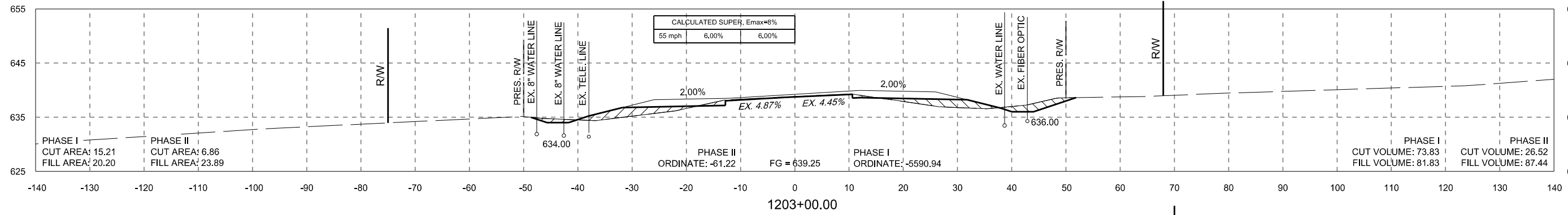
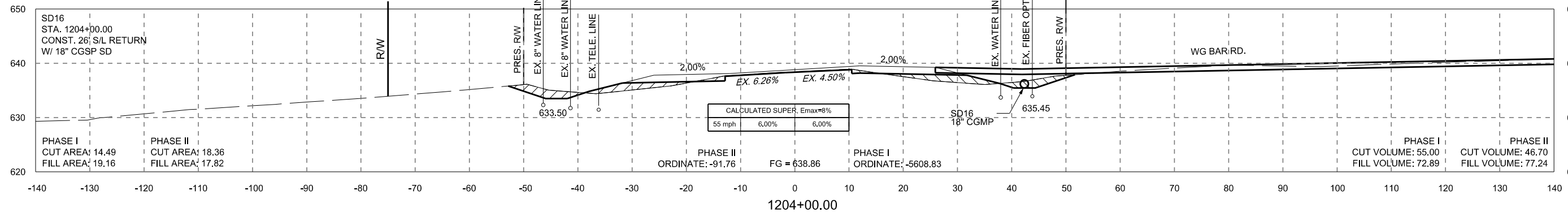
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APPROVED	SL	
SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X050
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SQUAD	BKL, Inc.	

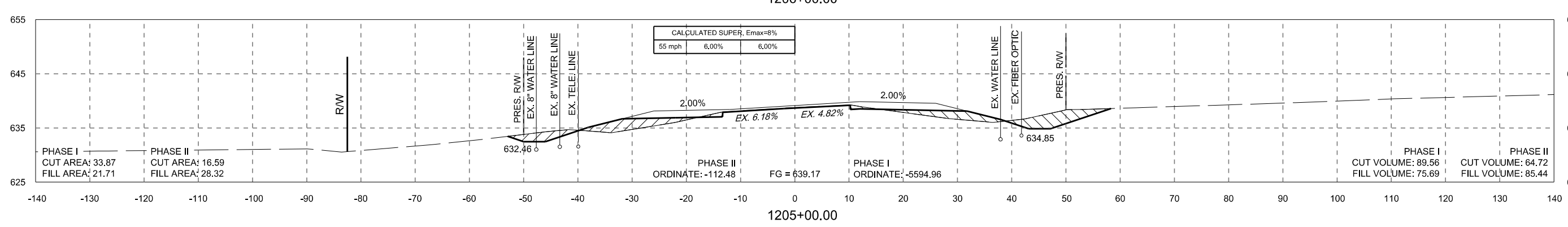
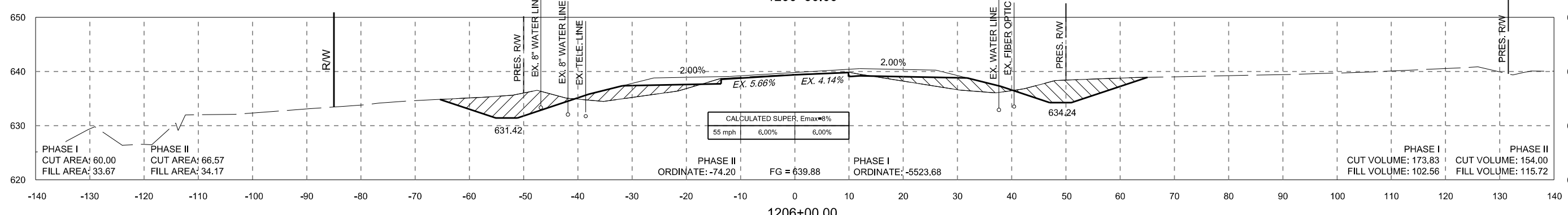
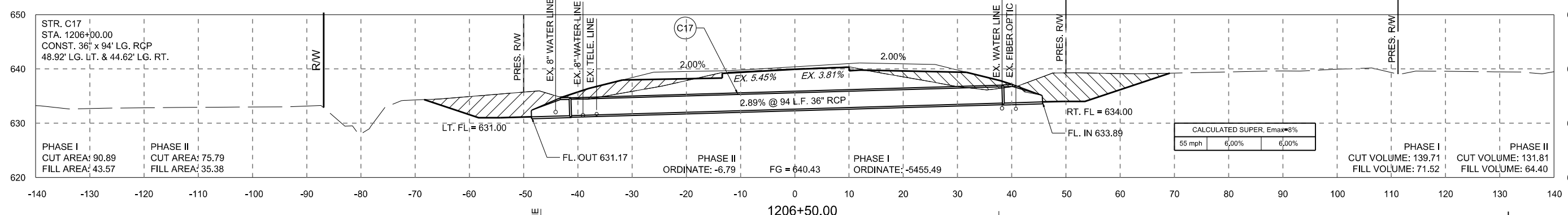
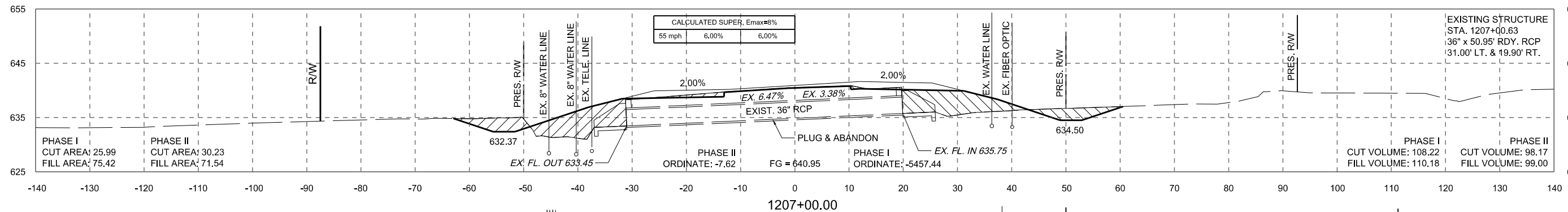


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SCALE: HORIZ. 1" = 1'-0"
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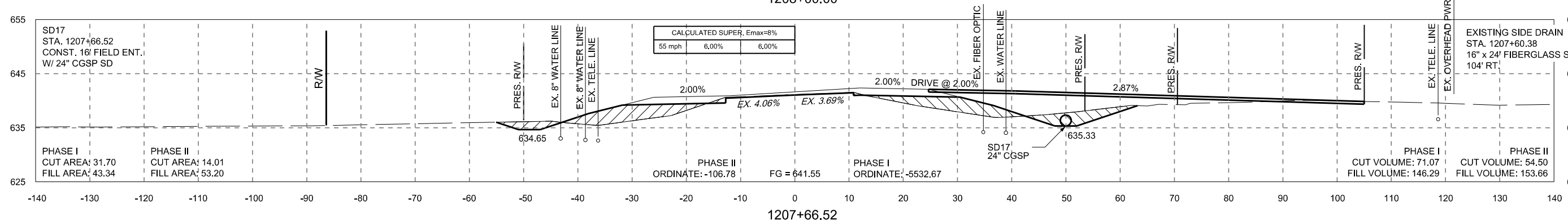
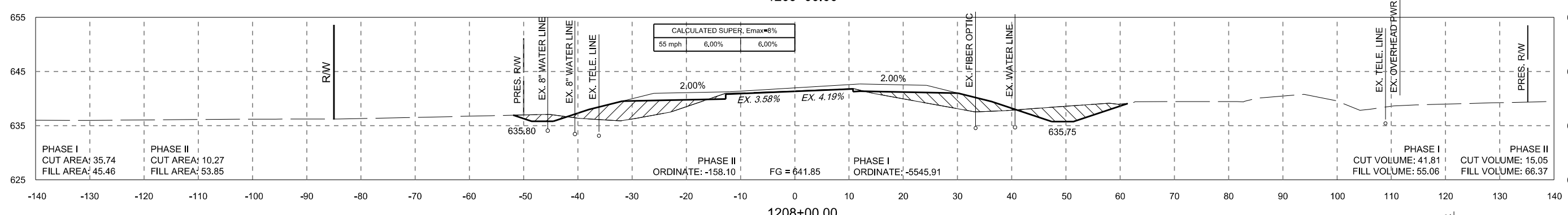
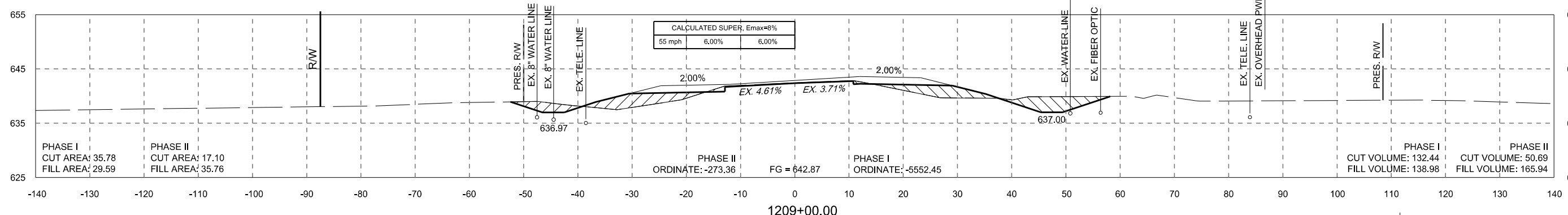
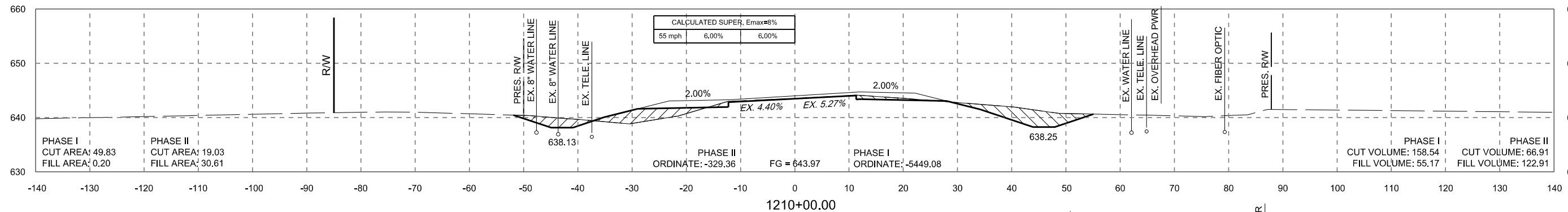
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

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APPROVED	SL	
SQUAD	BKL, Inc.	

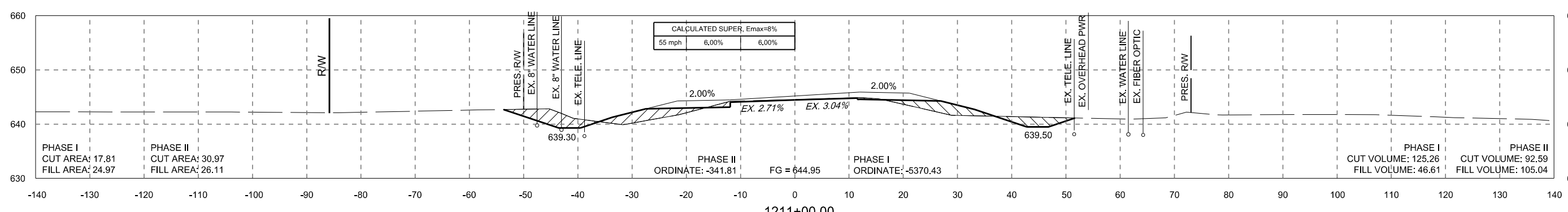
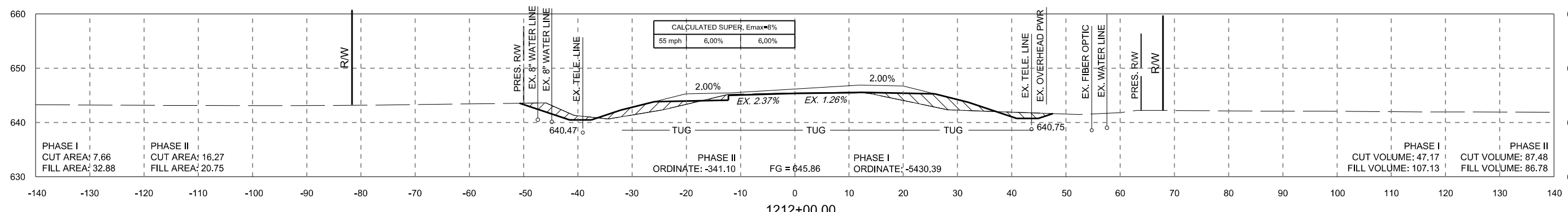
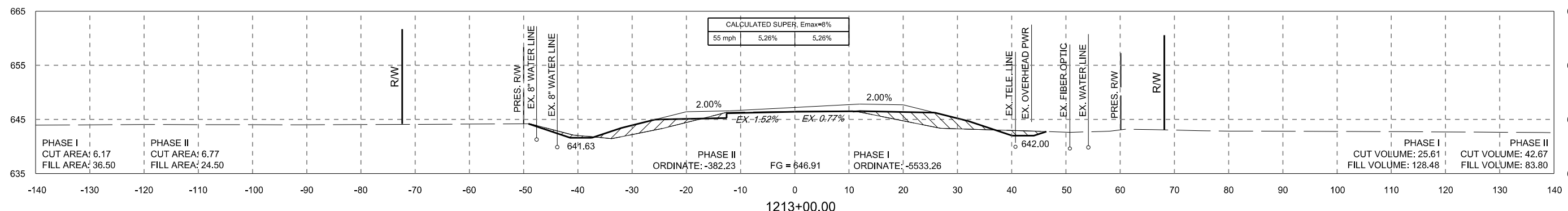
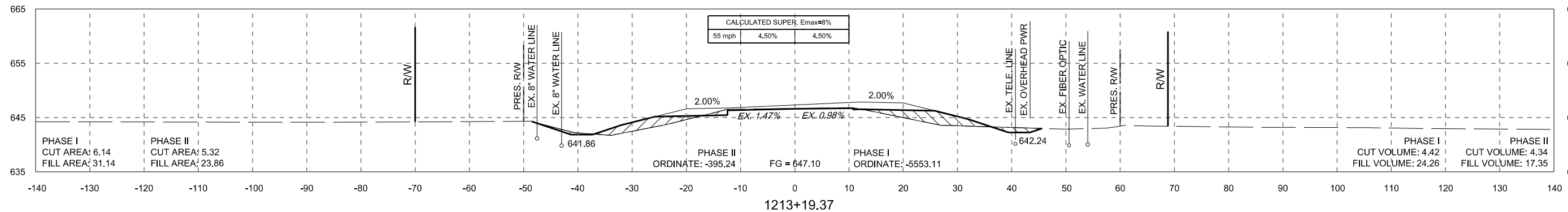
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X053
DRAWN	MM	
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APPROVED	SL	
SQUAD	BKL, Inc.	

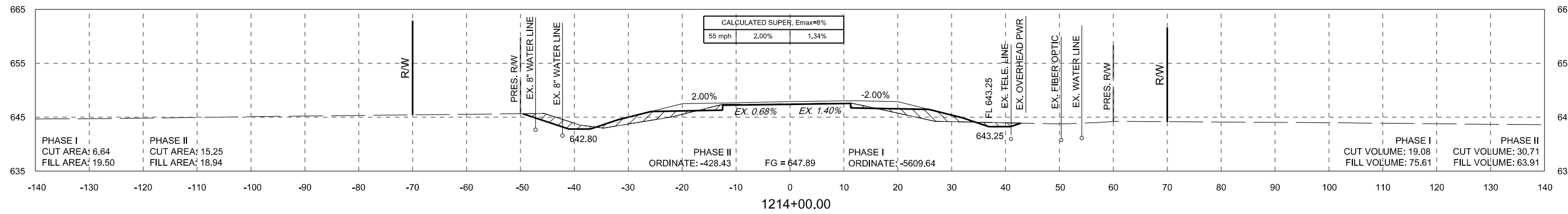
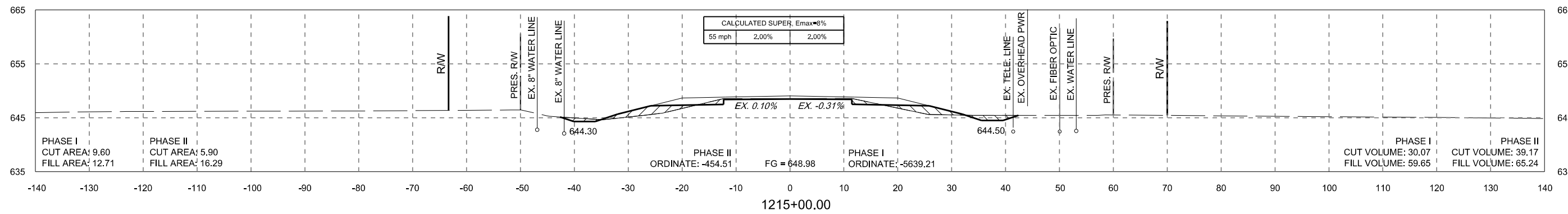
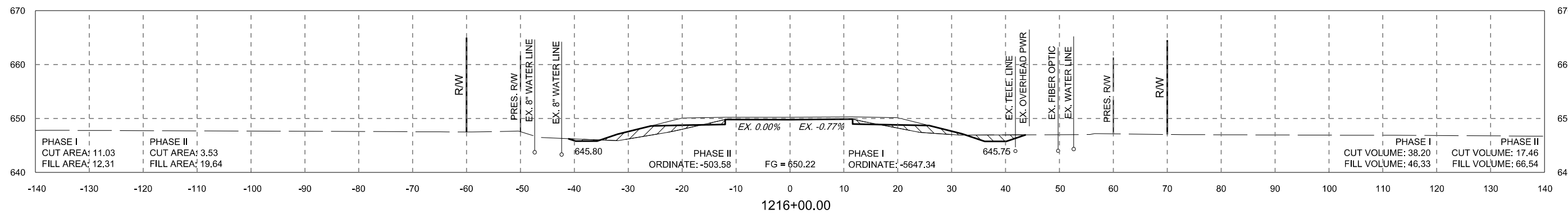
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CHECKED	DP	
APPROVED	SL	
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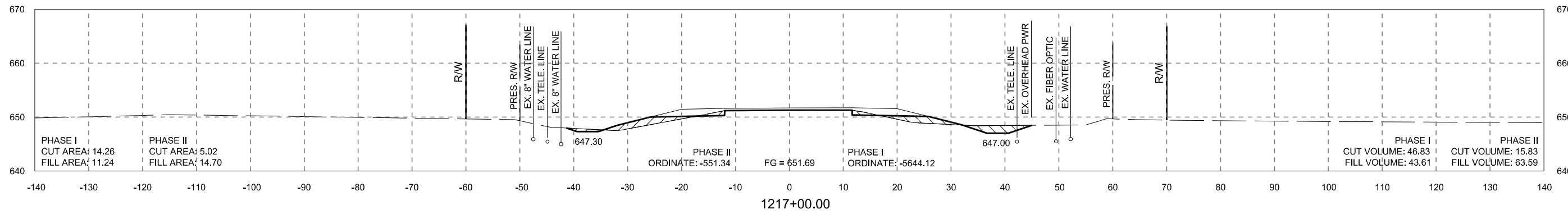
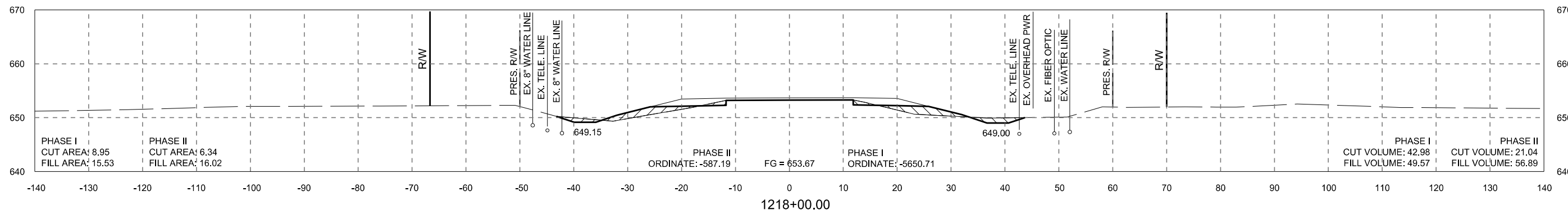
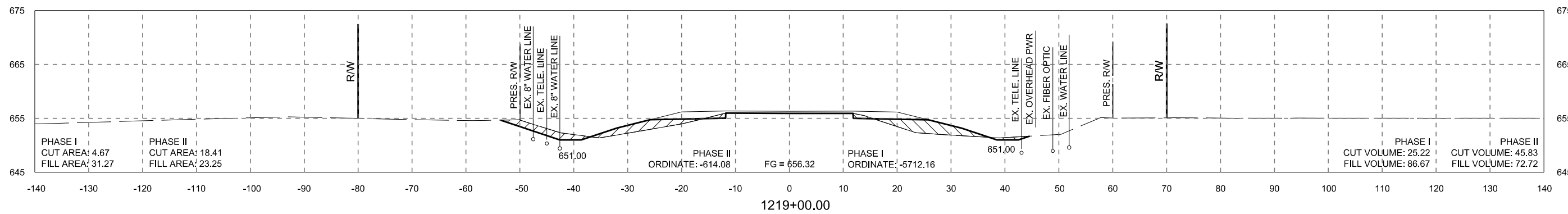
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X055
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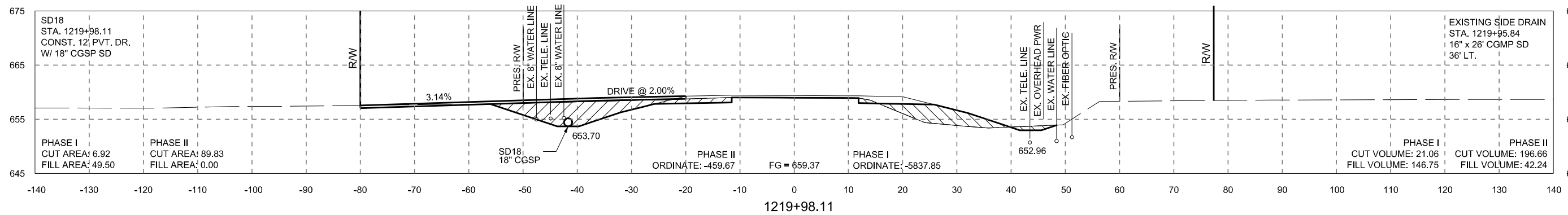
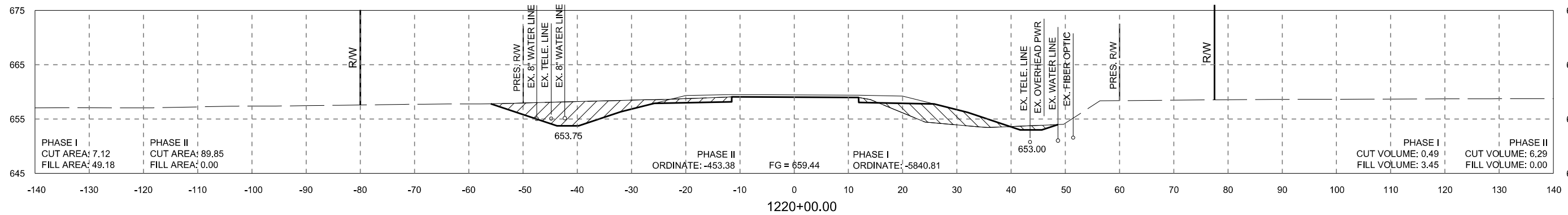
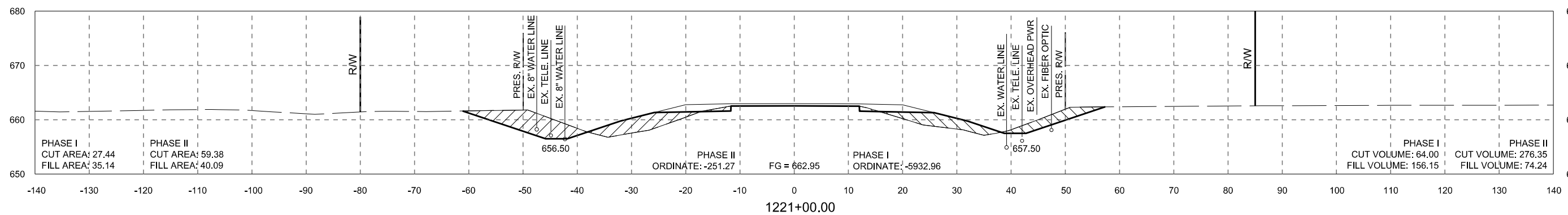
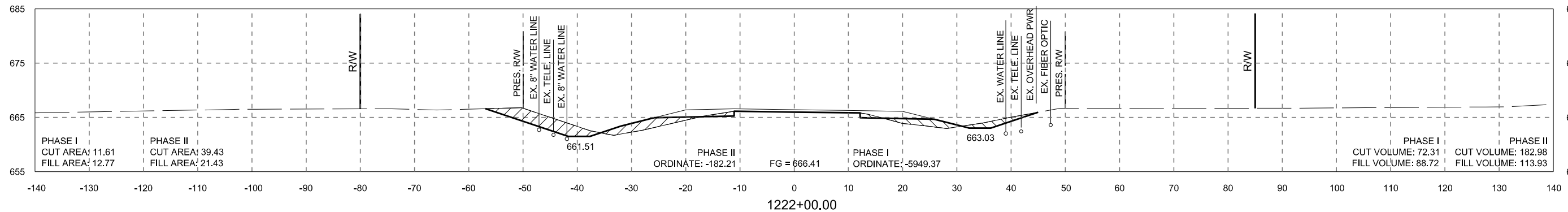
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VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X056
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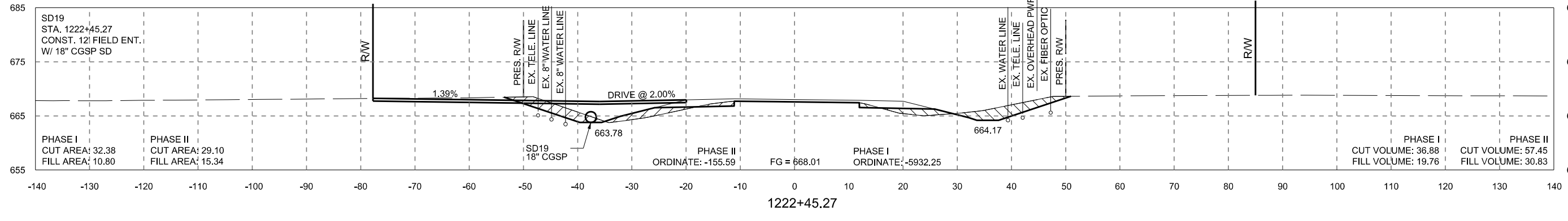
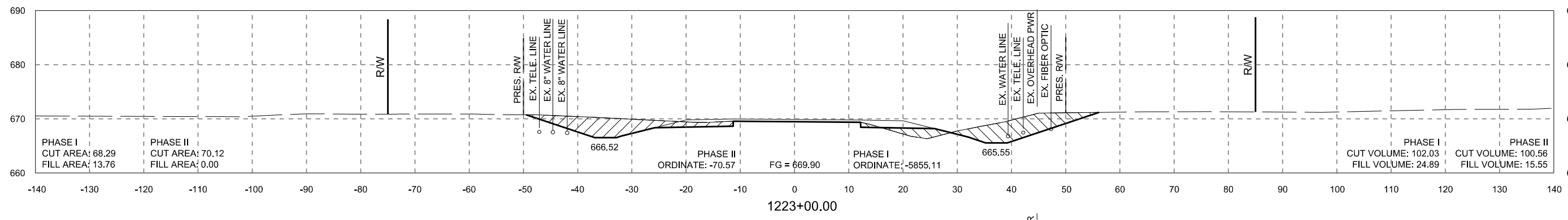
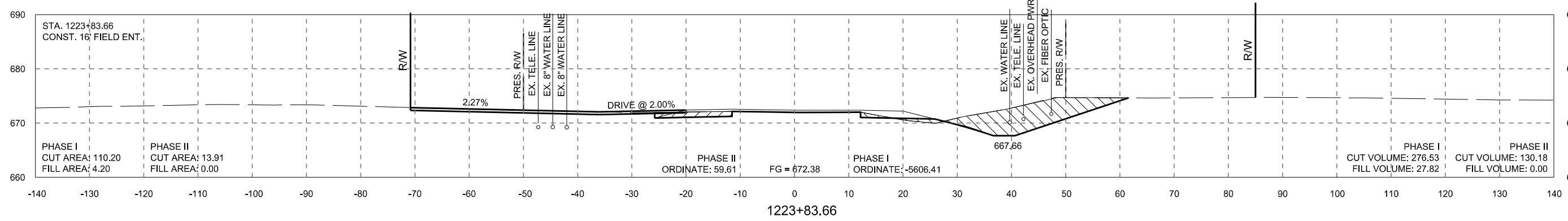
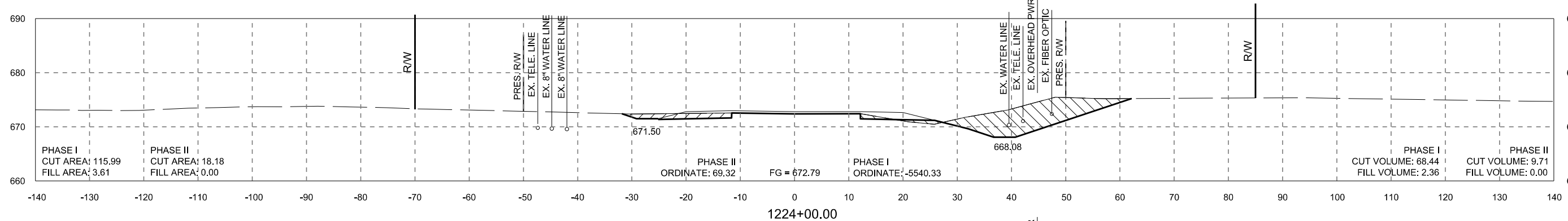
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X057
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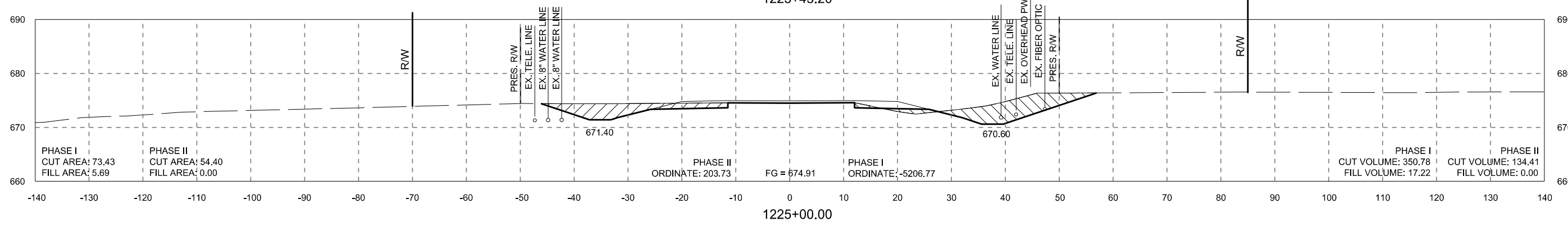
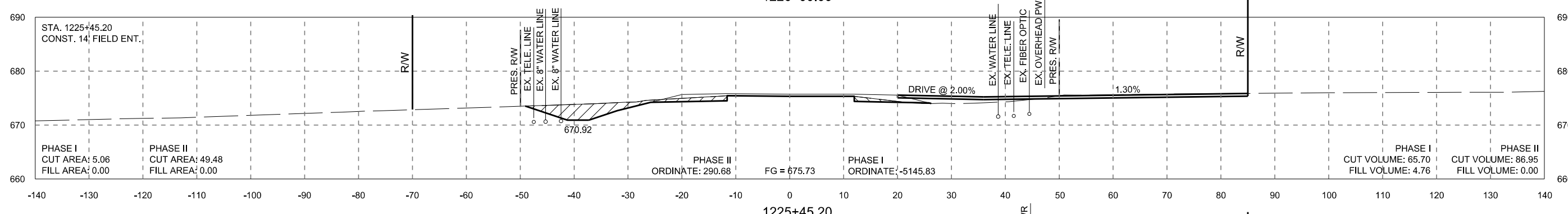
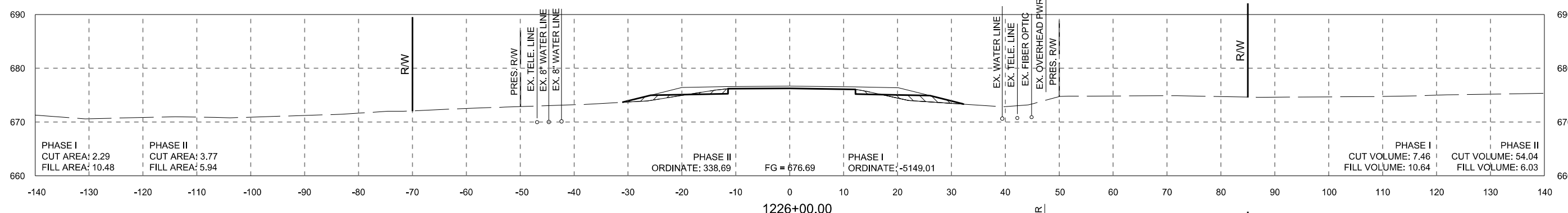
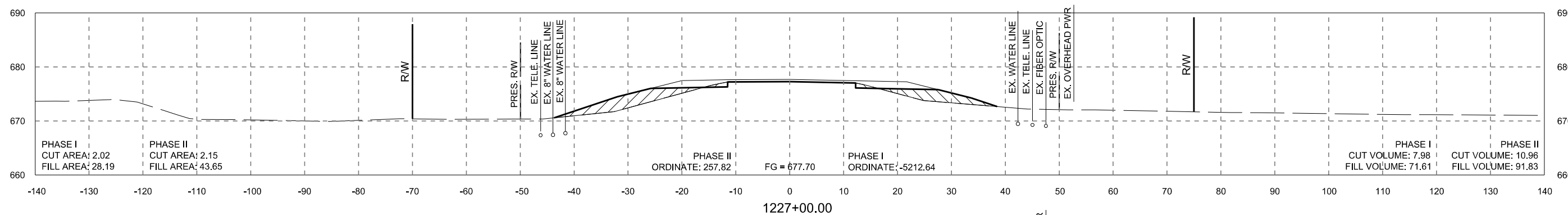
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SCALE: HORIZ. 1" = 1'-0"
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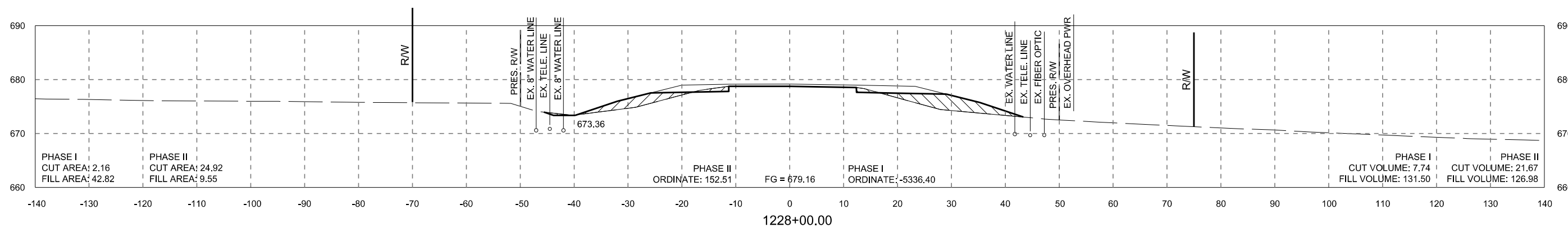
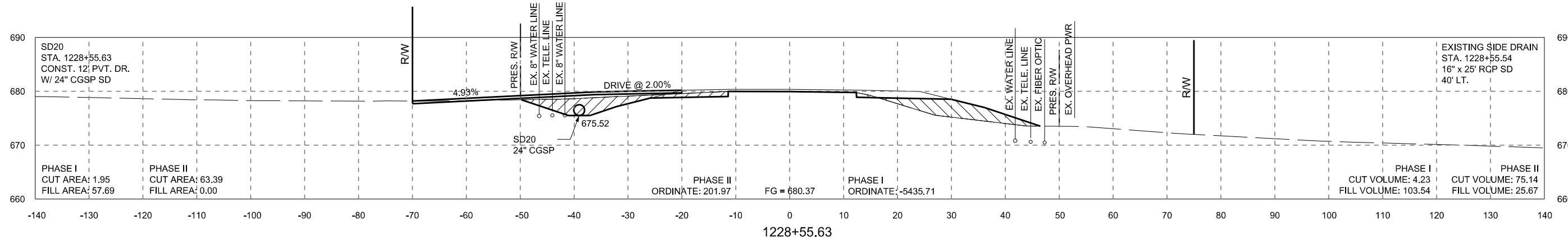
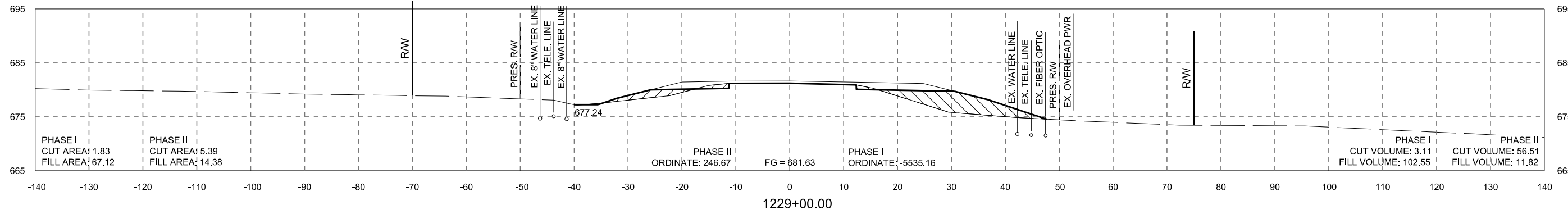
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SCALE: HORIZ. 1" = 1'-0"
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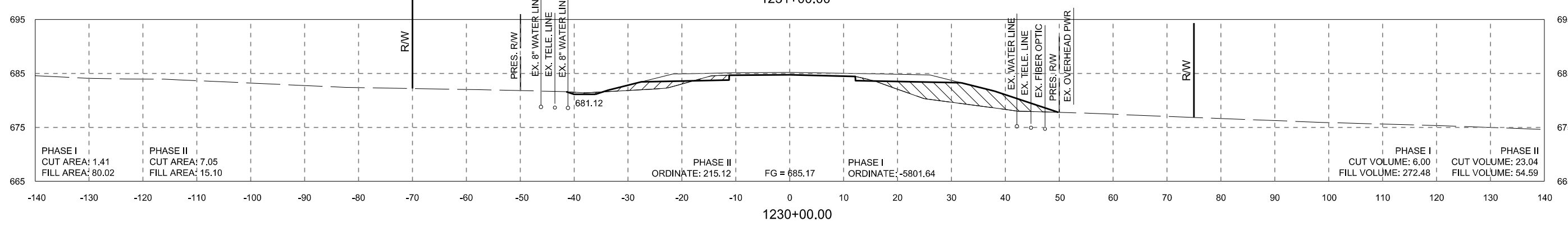
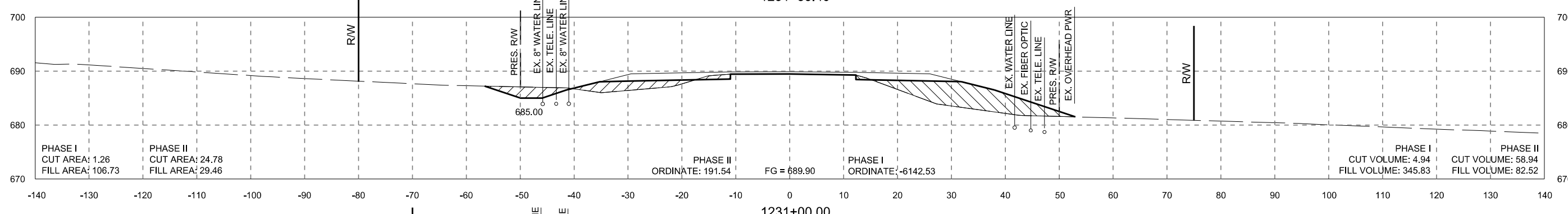
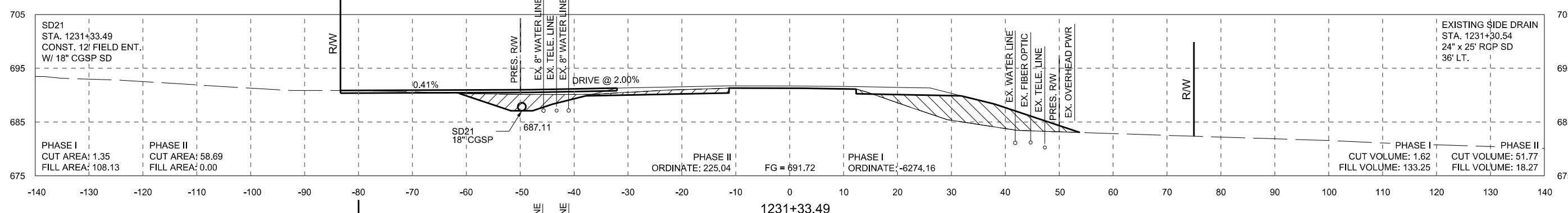
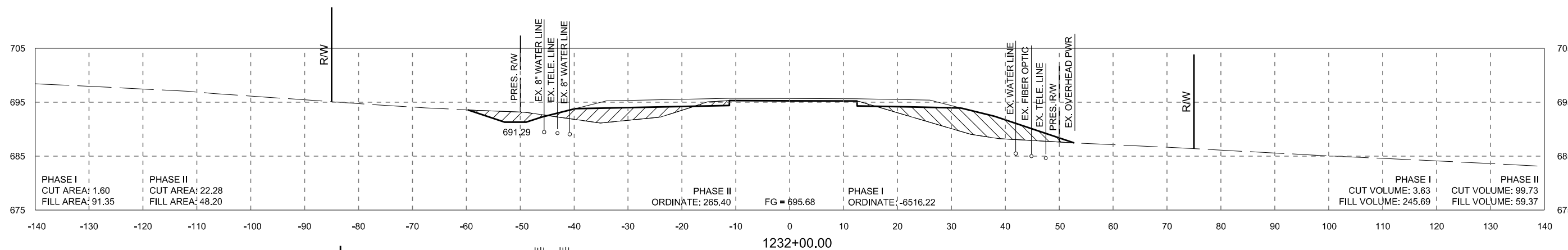
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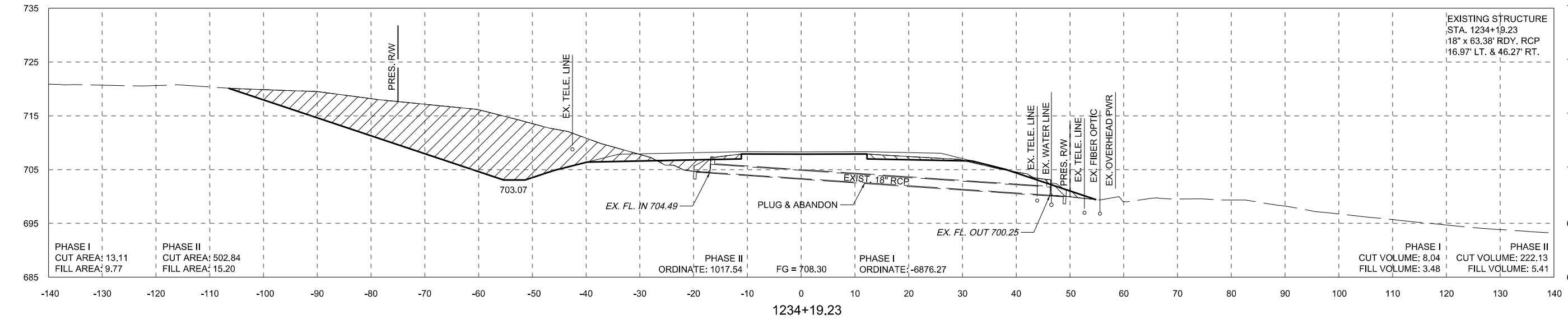
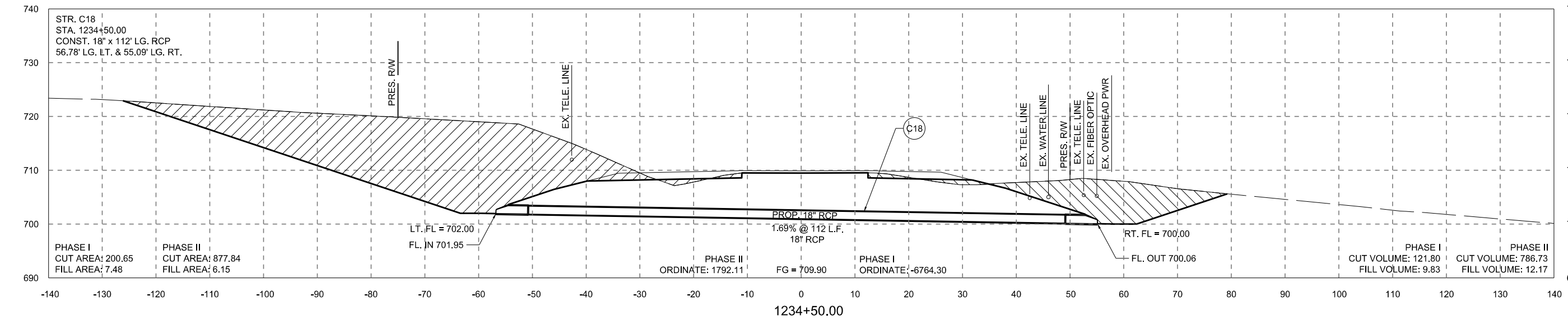
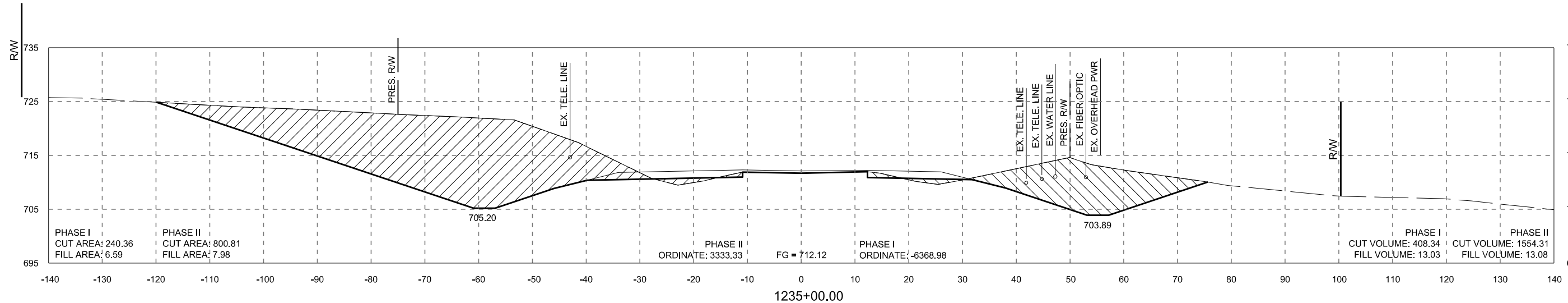
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SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X061
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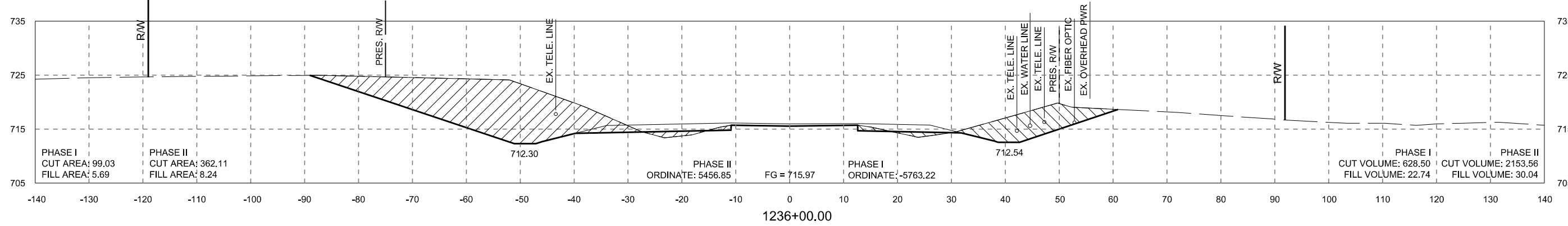
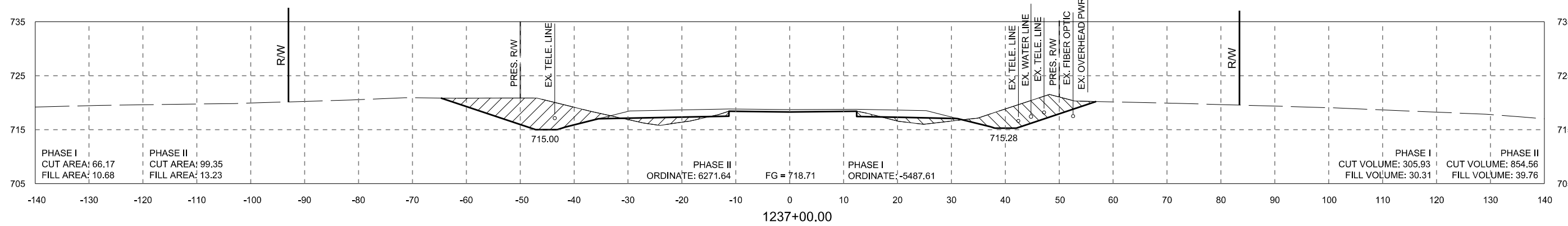
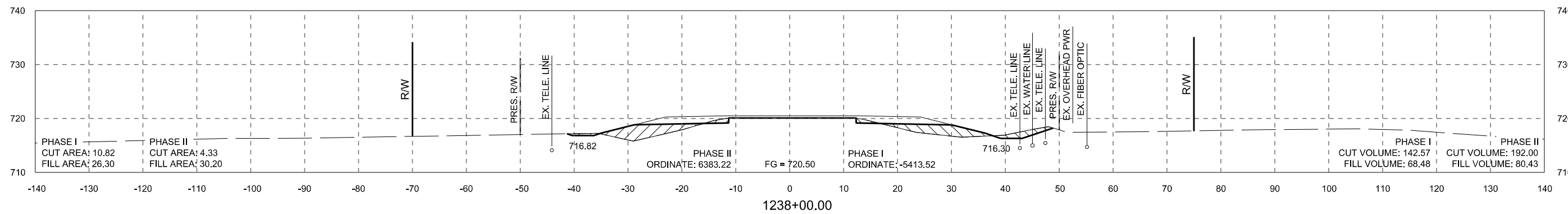


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SCALE: HORIZ. 1" = 1'-0"
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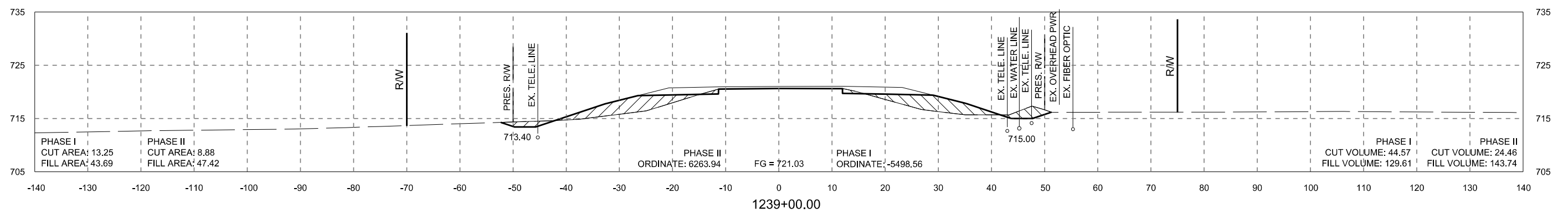
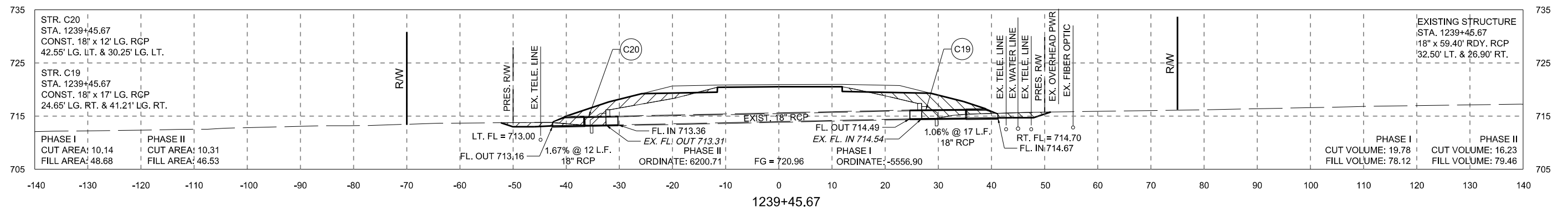
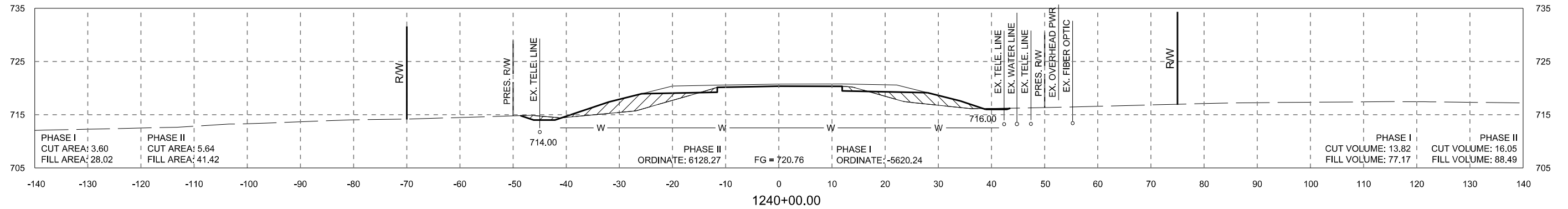
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SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
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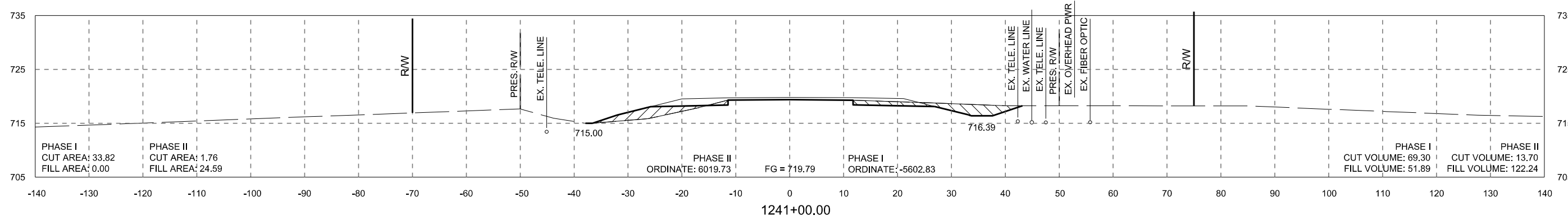
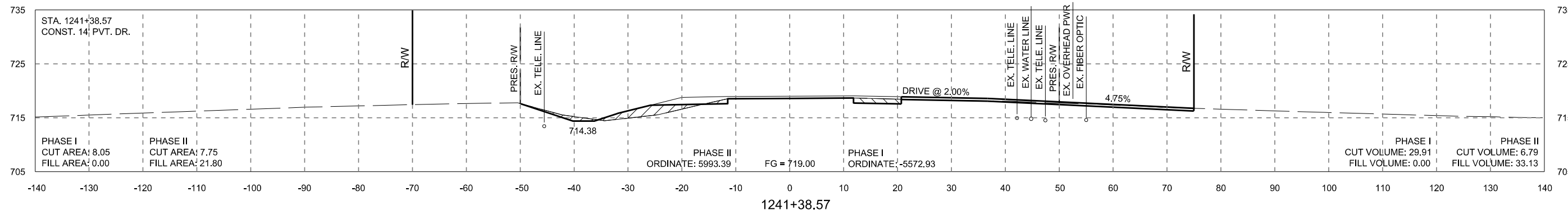
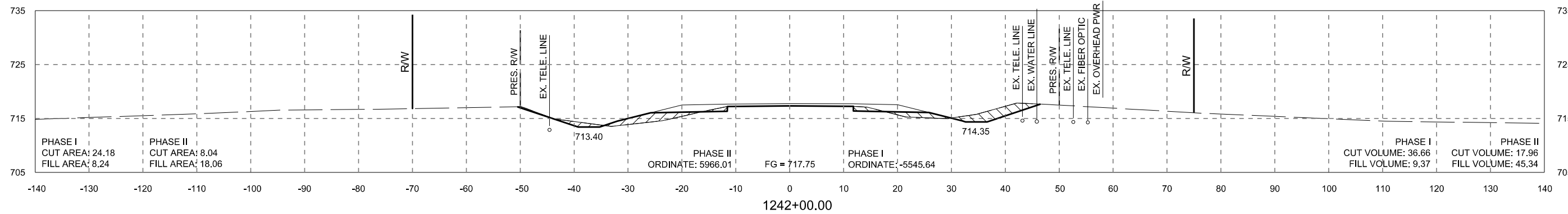


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SCALE: HORIZ. 1" = 1'-0"
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DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X065
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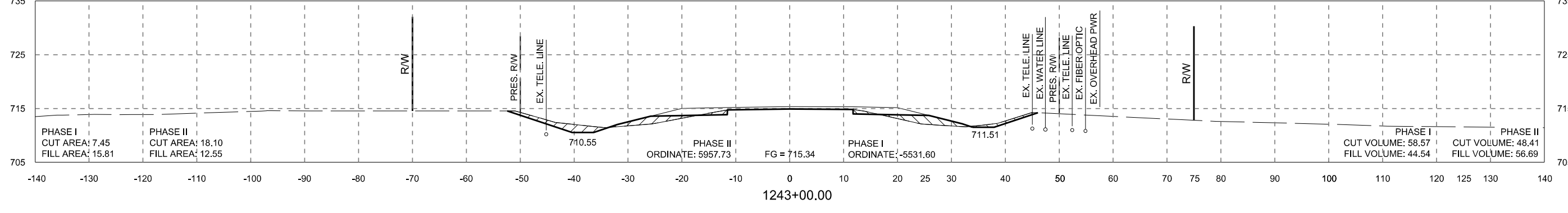
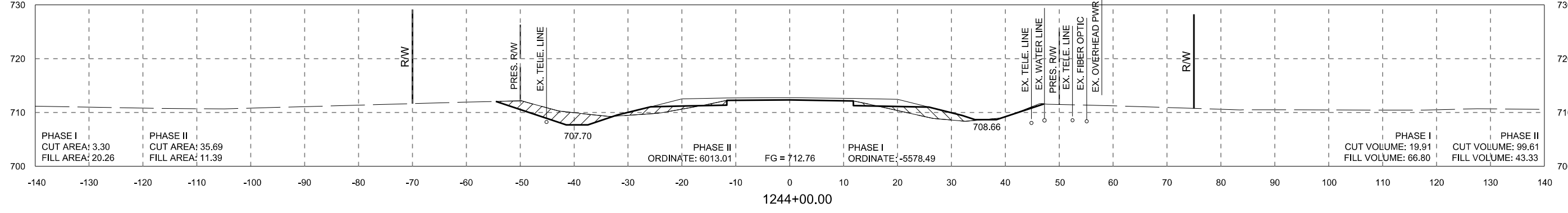
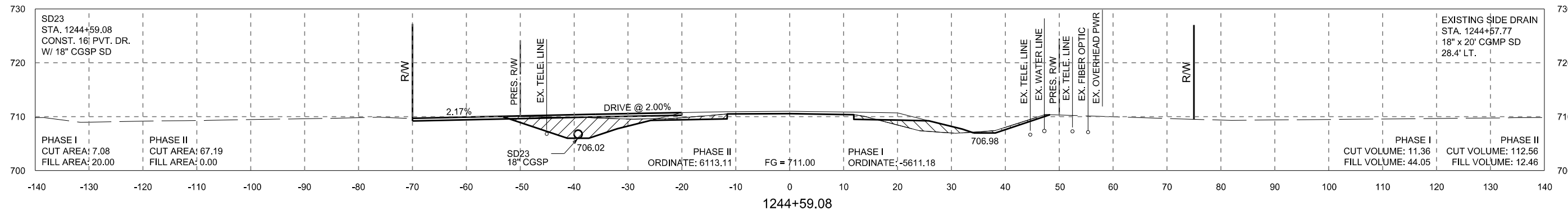
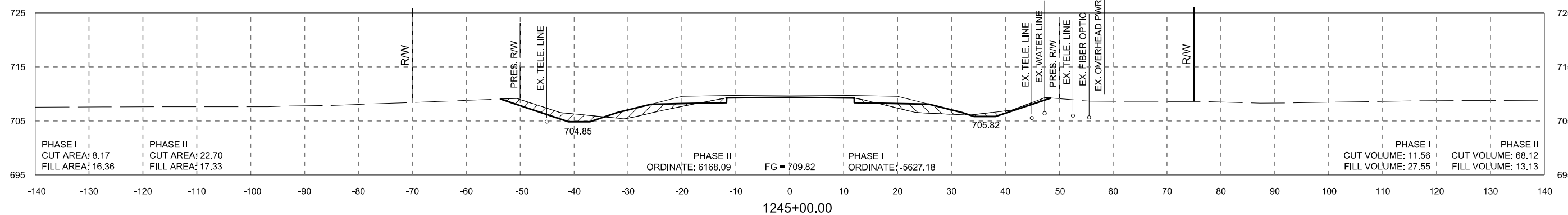
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X066
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SQUAD	BKL, Inc.	

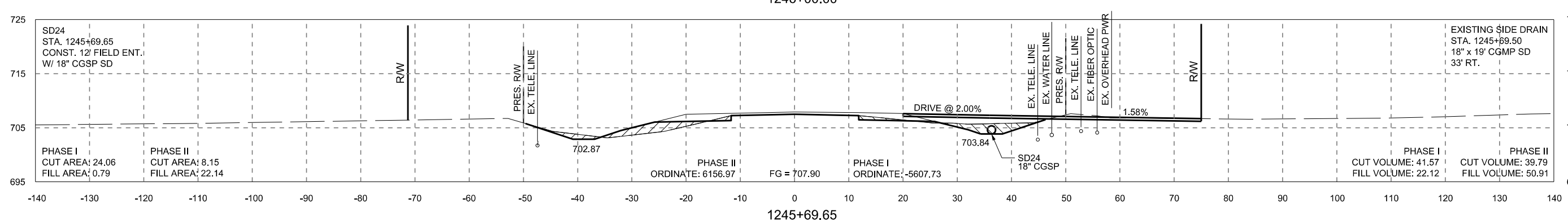
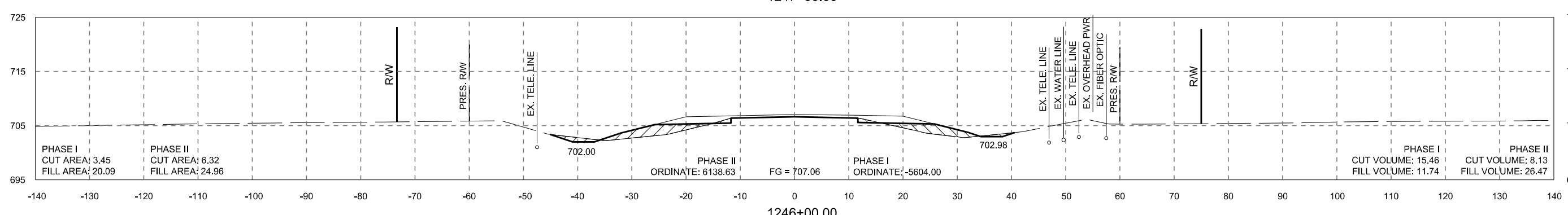
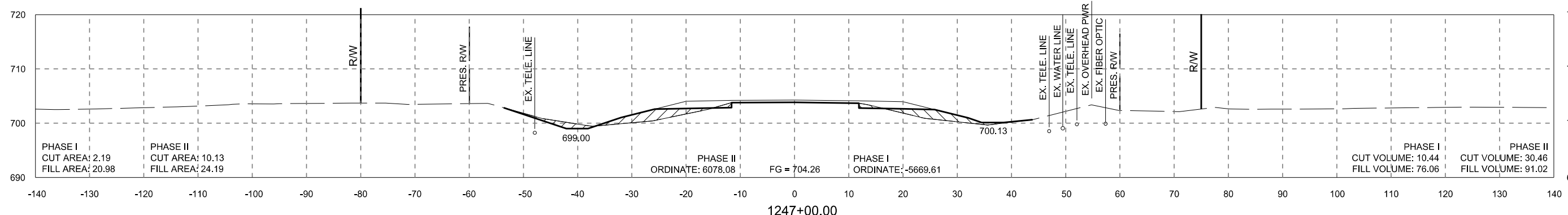
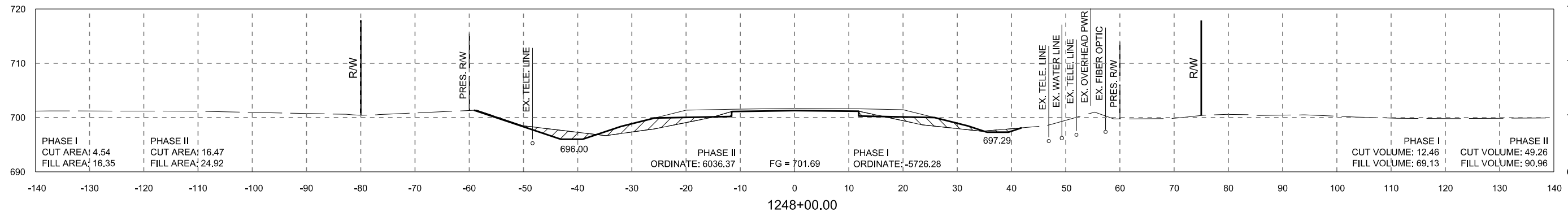
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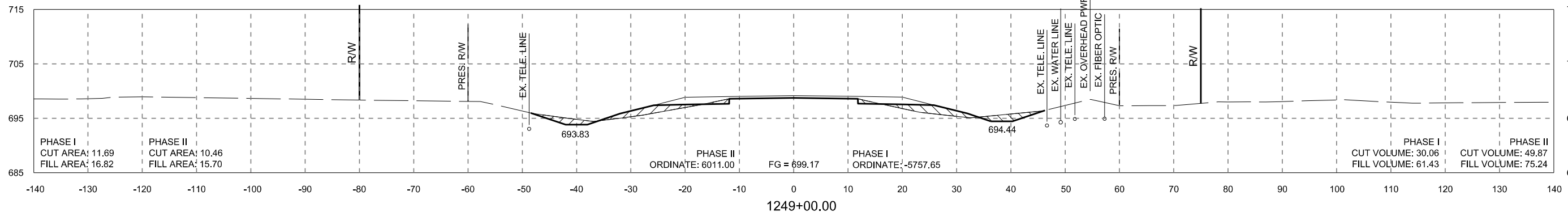
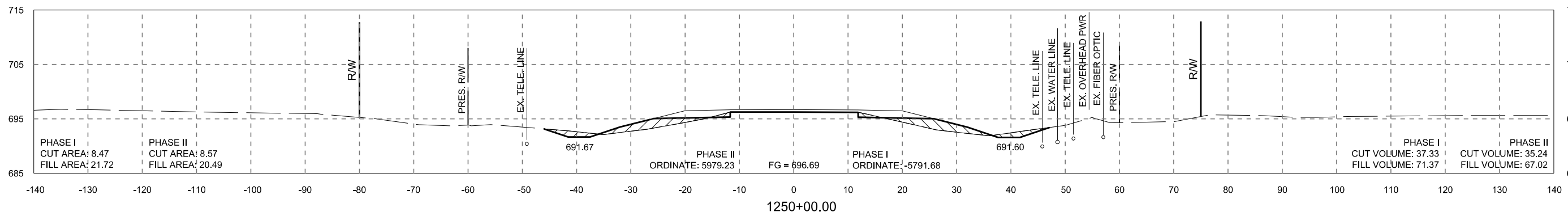
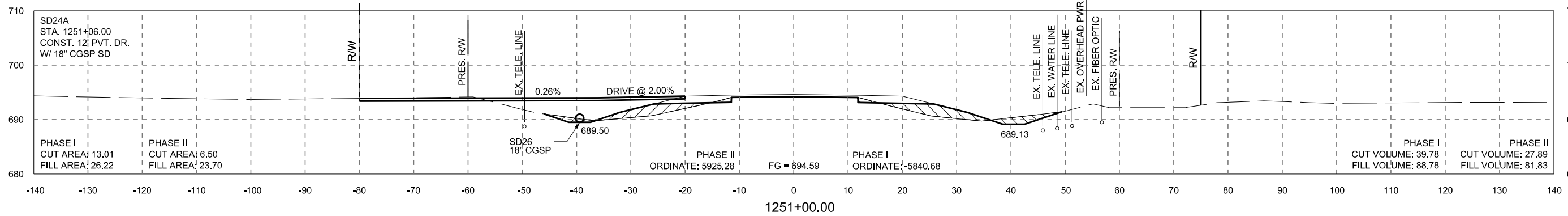
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SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

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APPROVED	SL	
SQUAD	BKL, Inc.	

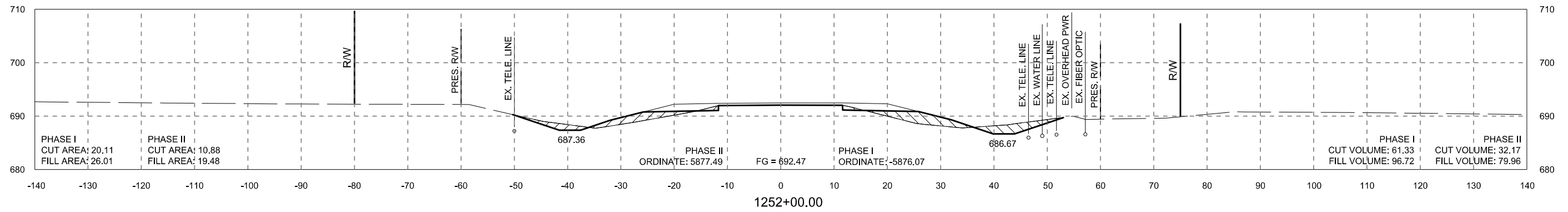
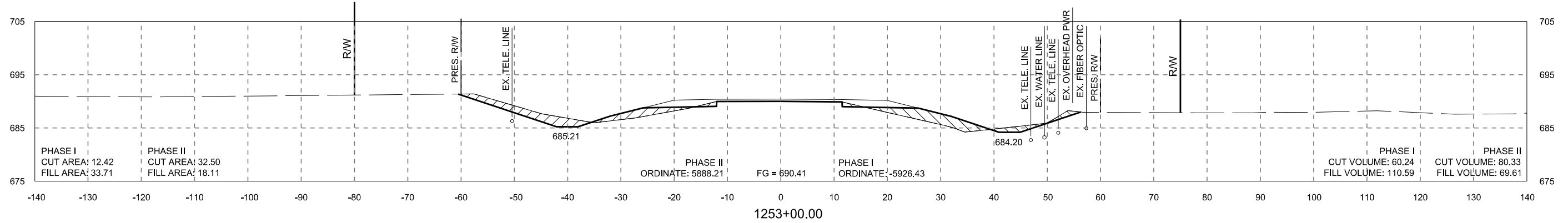
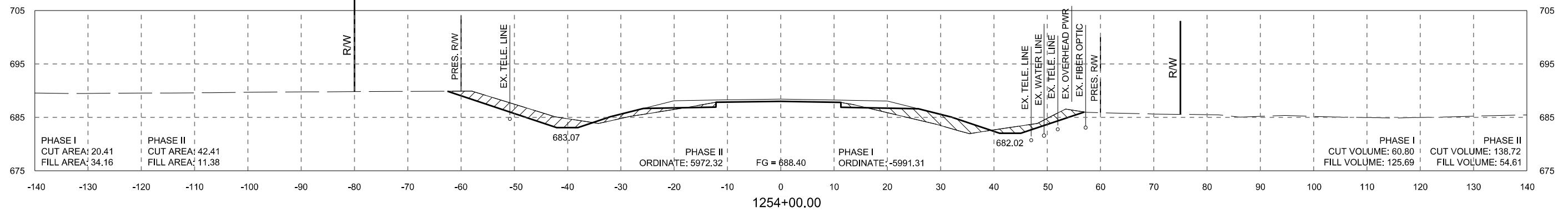


PRINT DATE: 8/22/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTION MAINLINE.dgn

SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X069
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

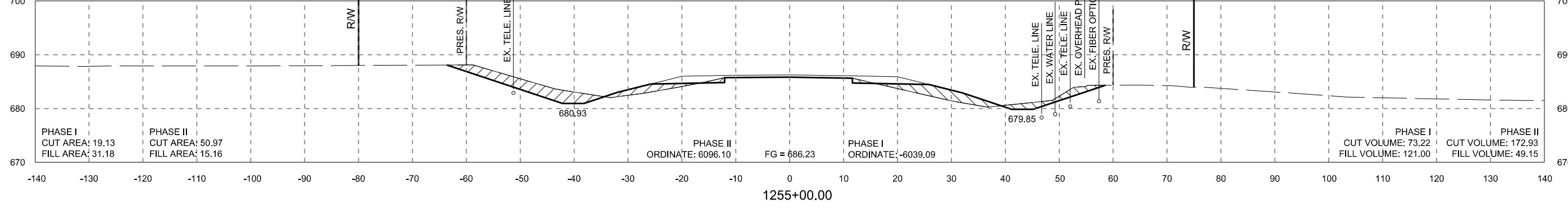
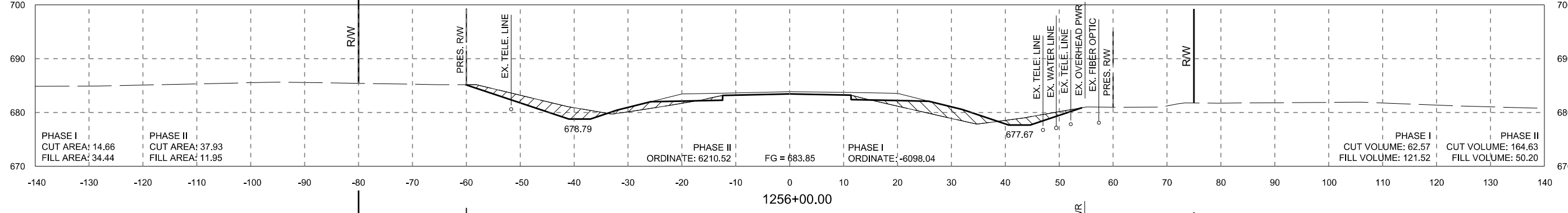
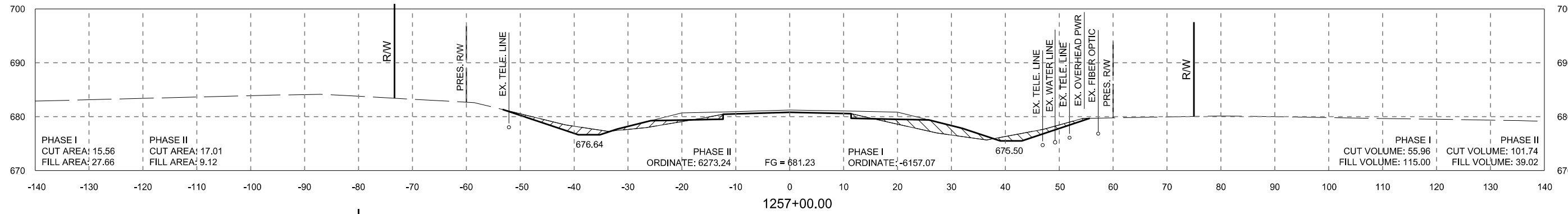
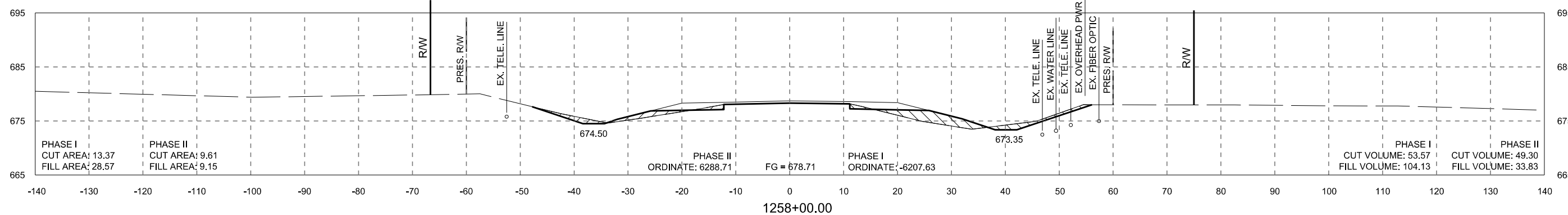
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X070
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CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

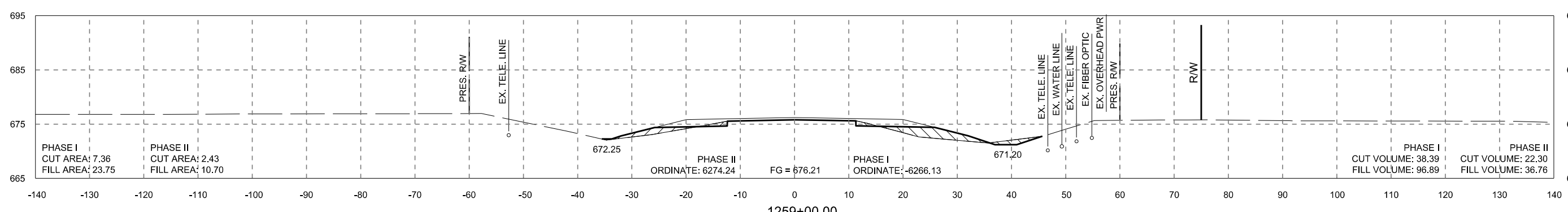
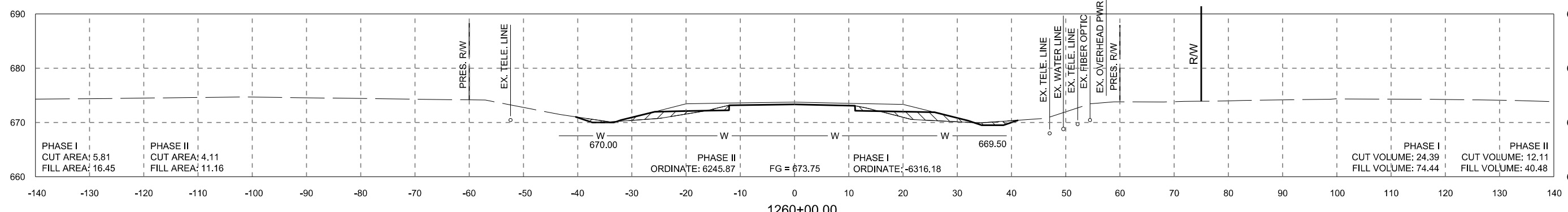
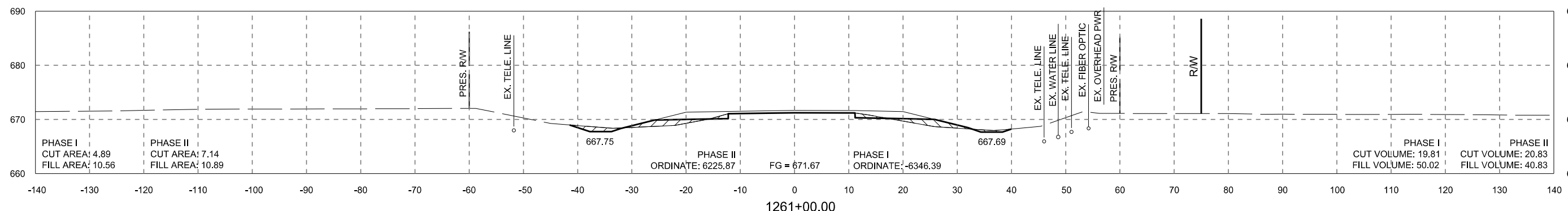
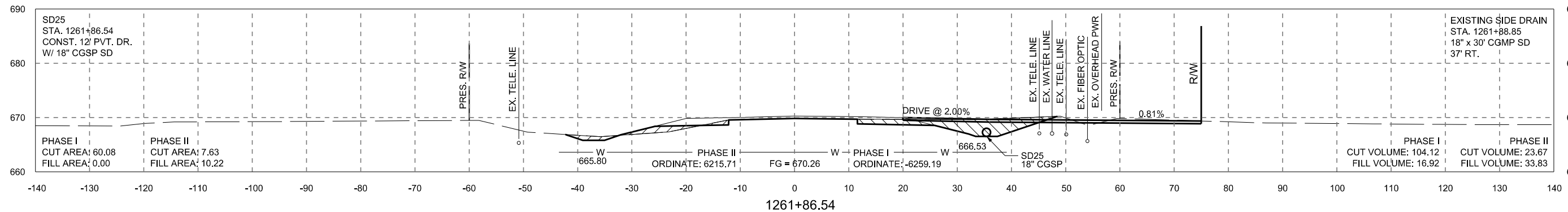
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X071
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

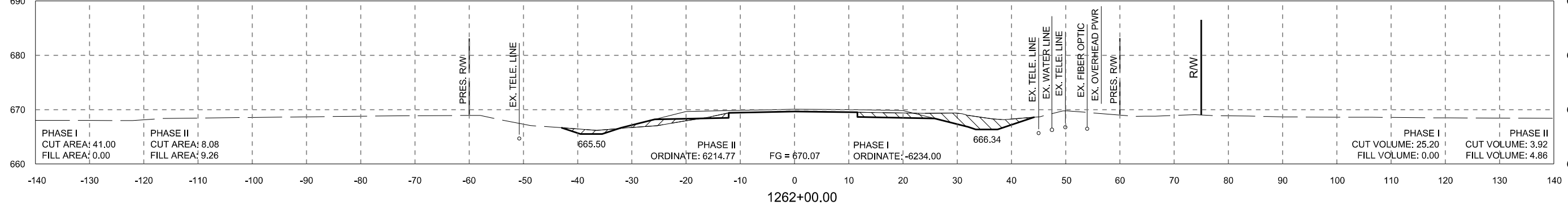
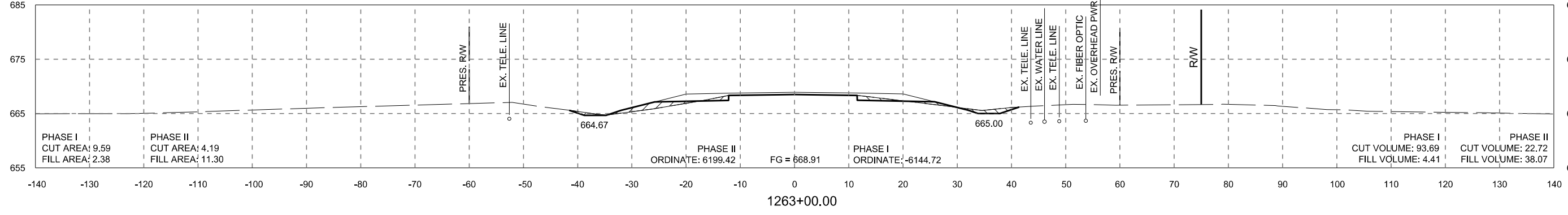
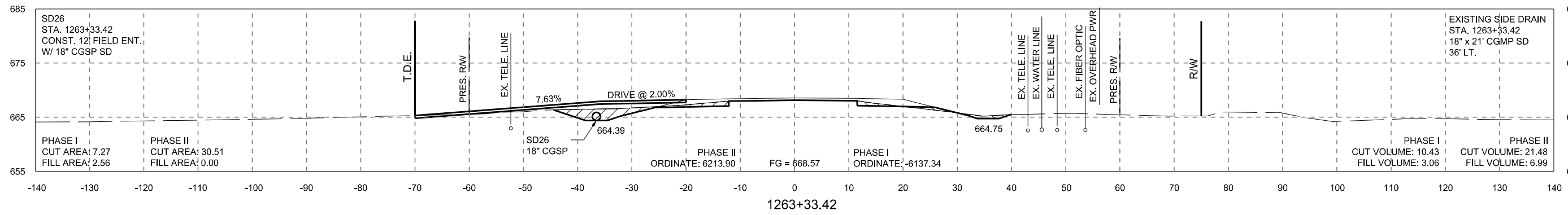
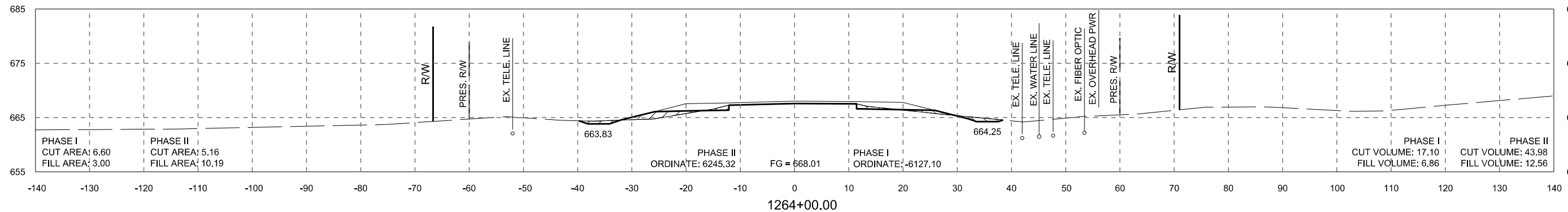
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X072
DRAWN	MM	
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APPROVED	SL	
SQUAD	BKL, Inc.	

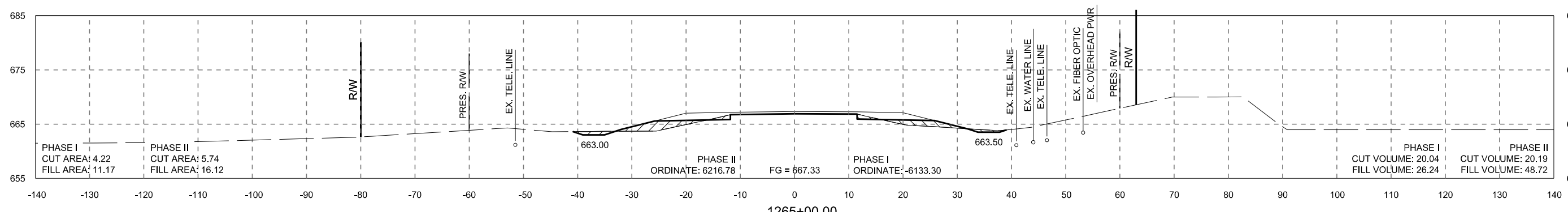
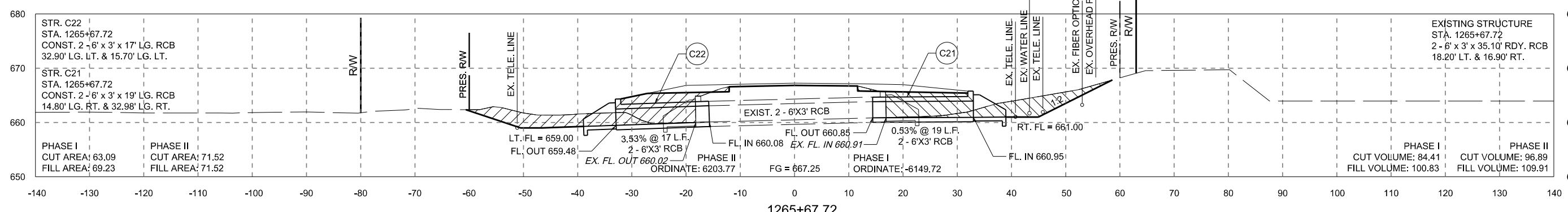
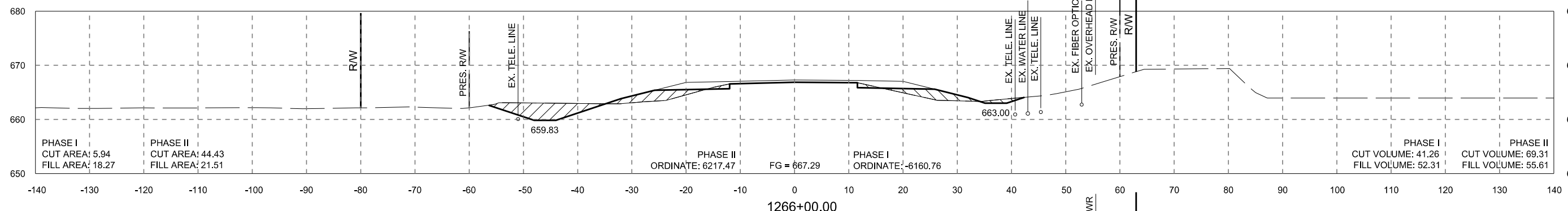
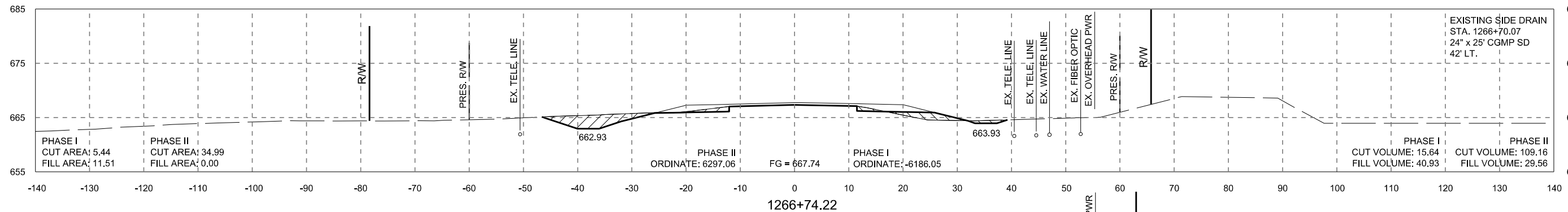
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X073
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

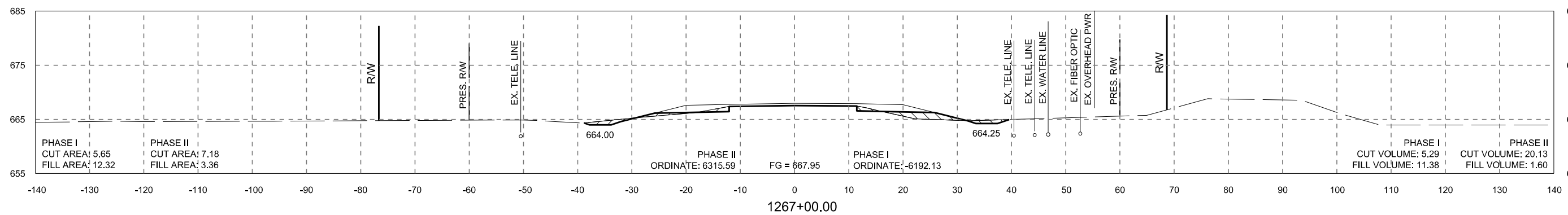
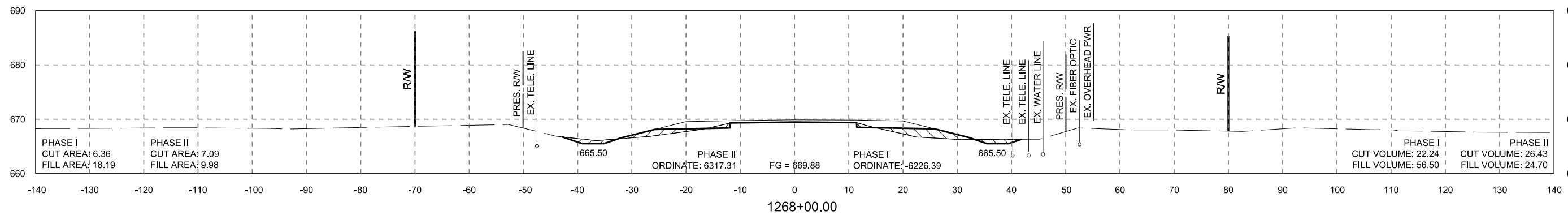
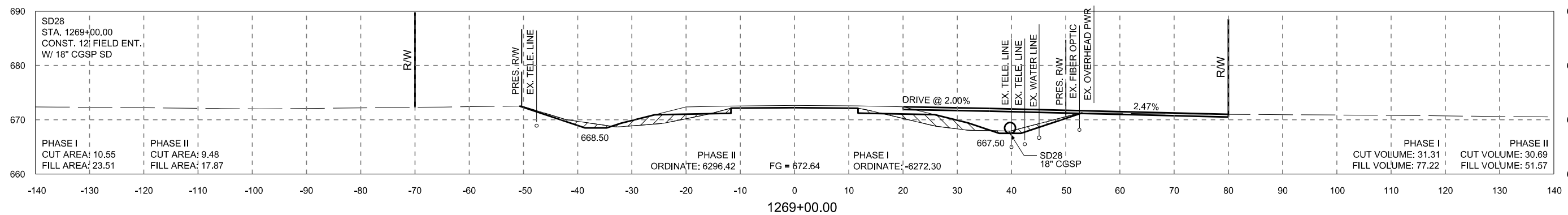
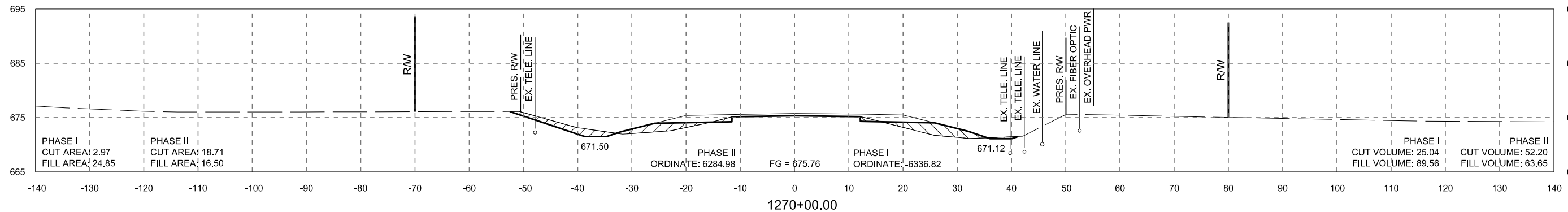
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X074
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CHECKED	DP	
APPROVED	SL	
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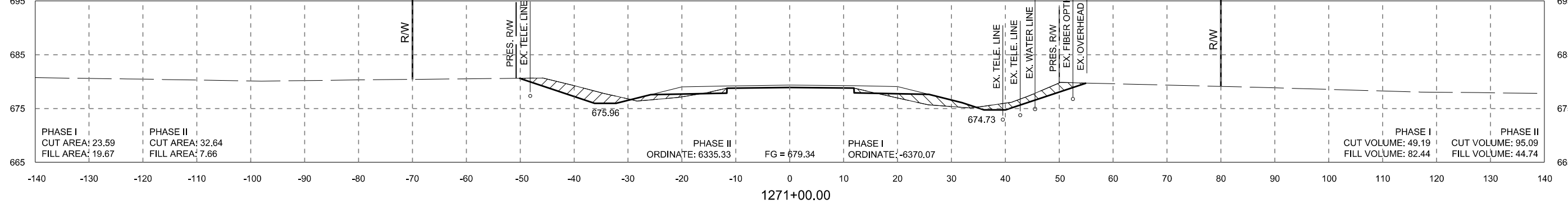
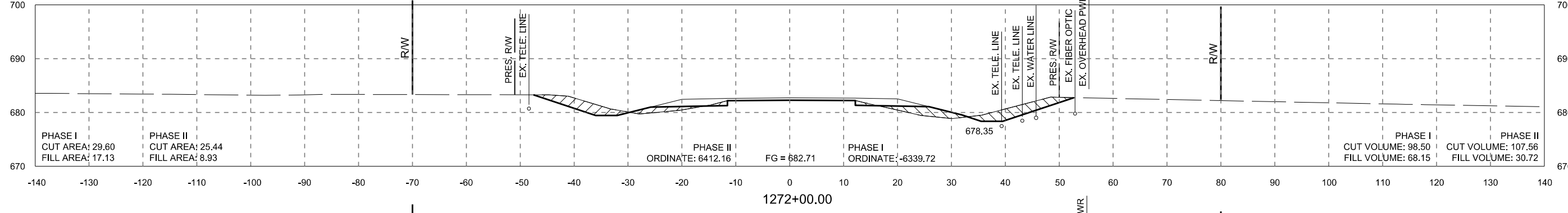
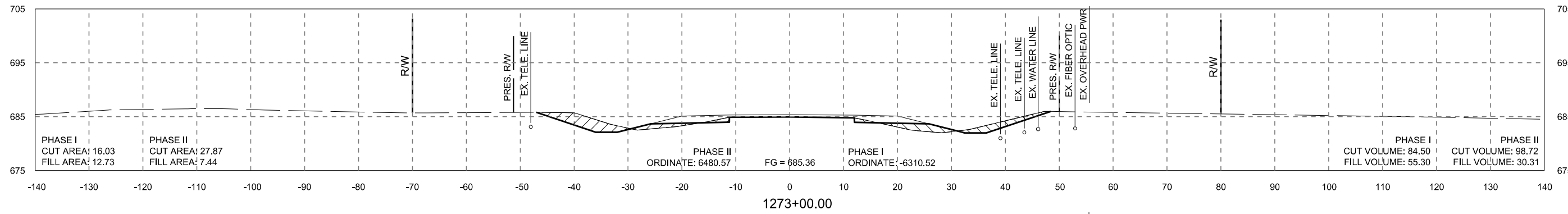
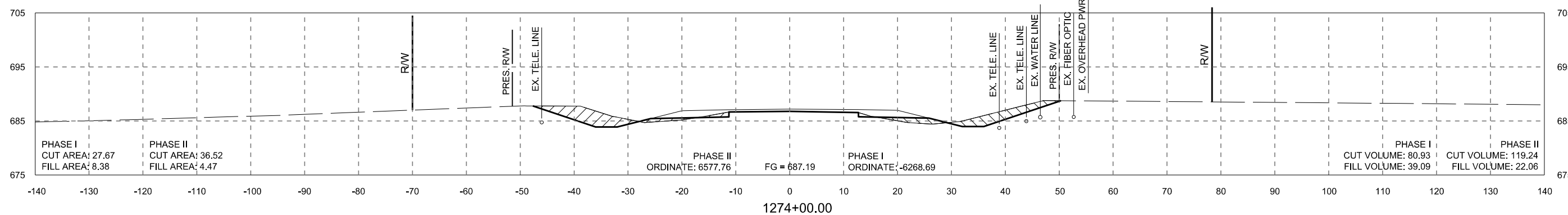
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X075
DRAWN	MM	
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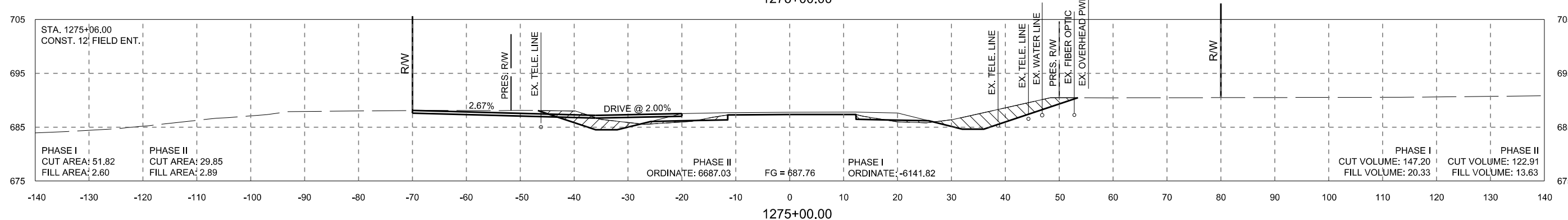
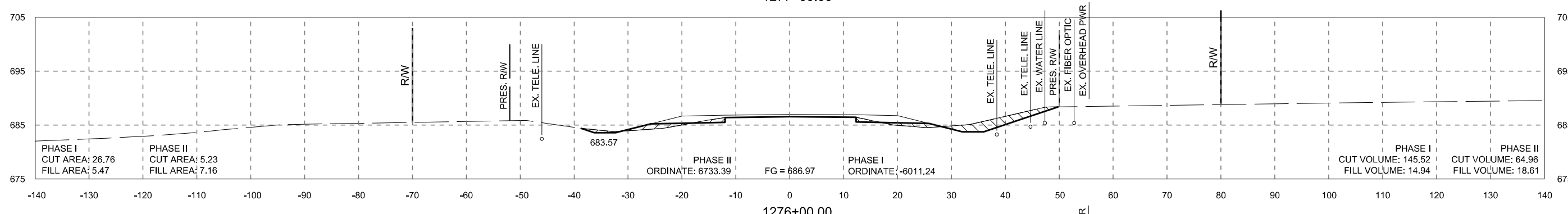
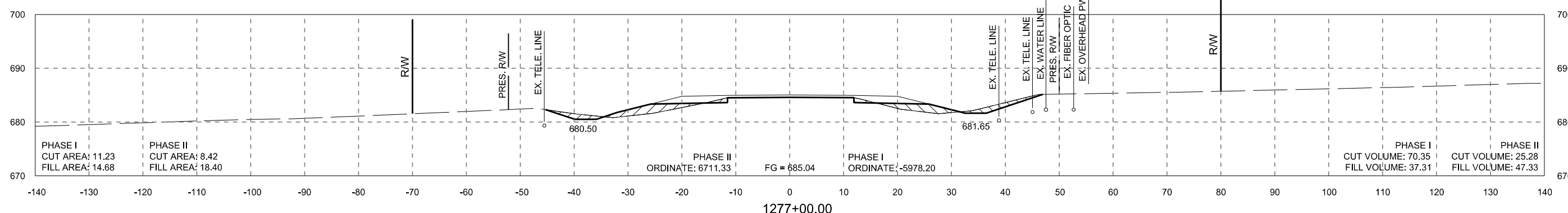
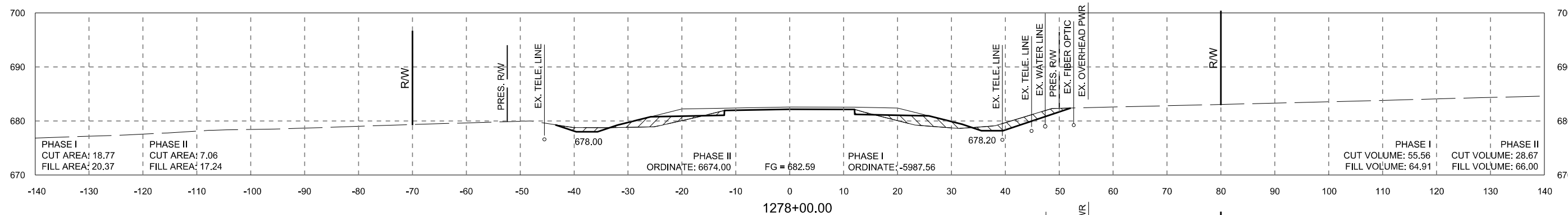
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X076
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
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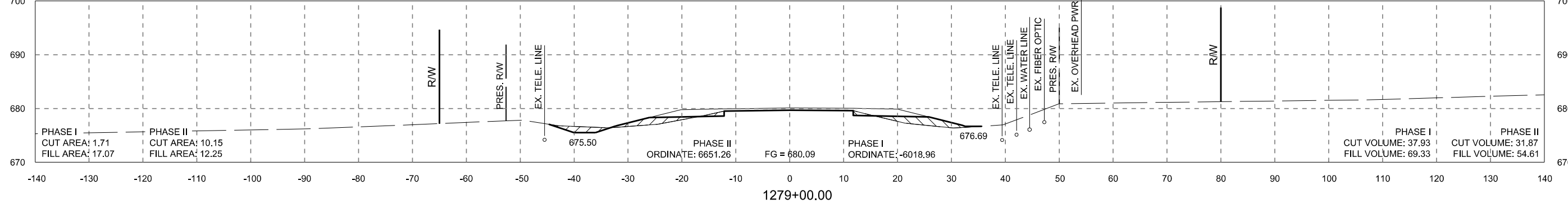
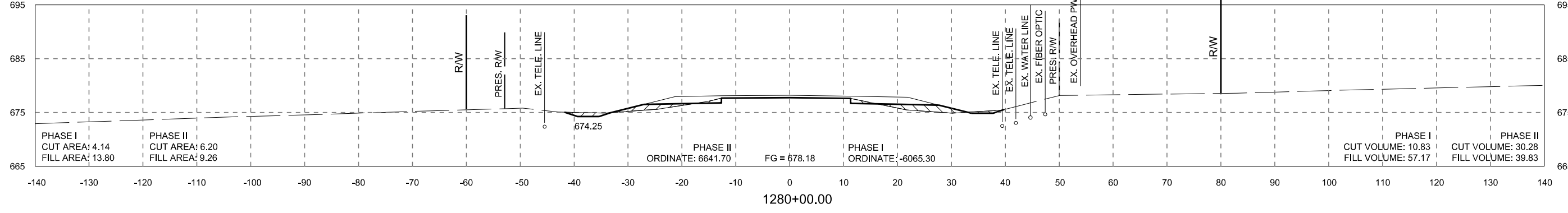
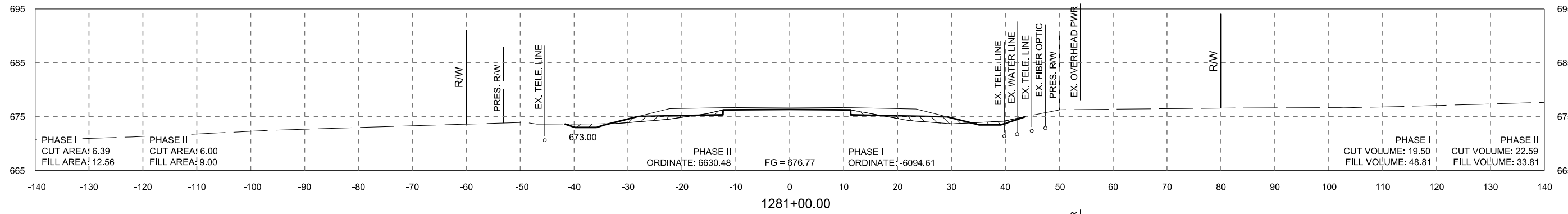
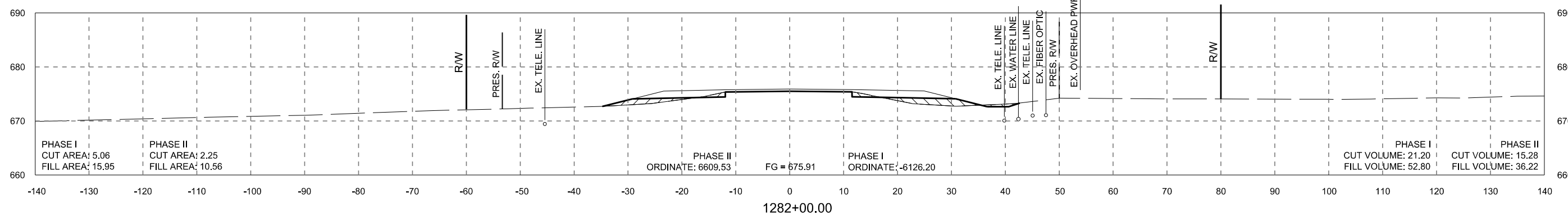
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X077
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

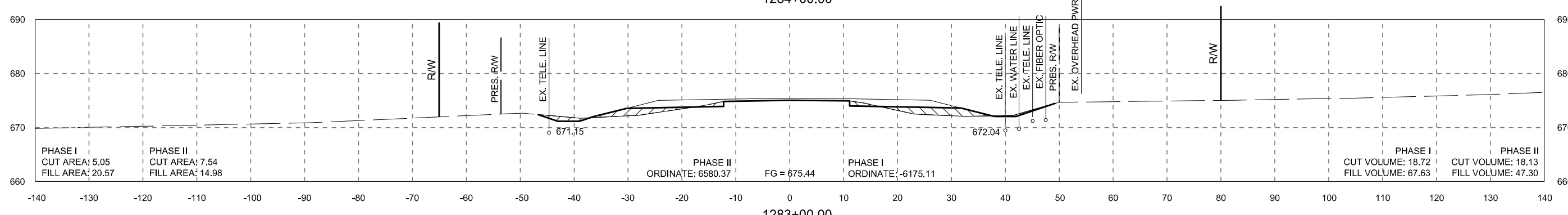
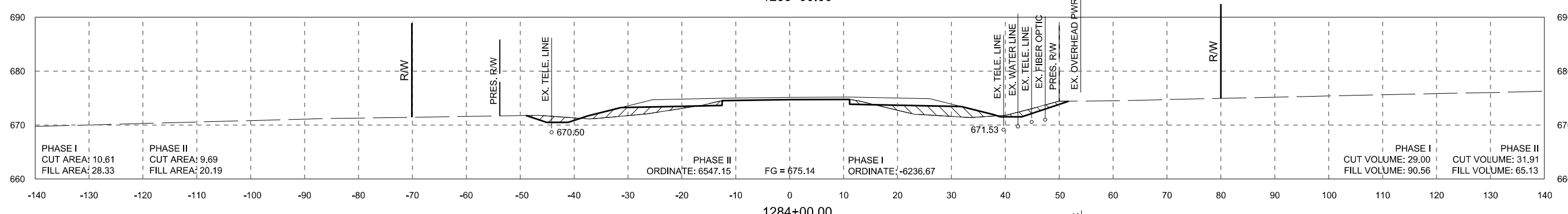
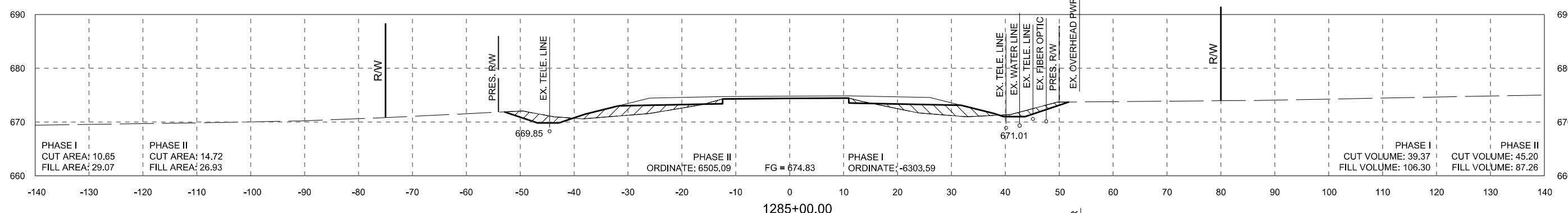
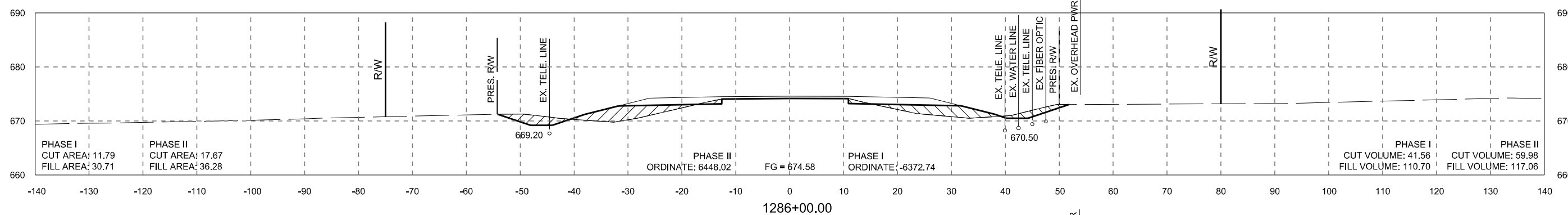
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X078
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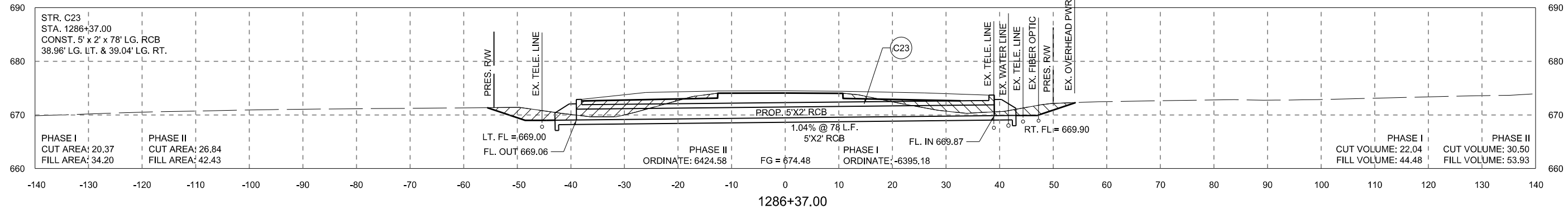
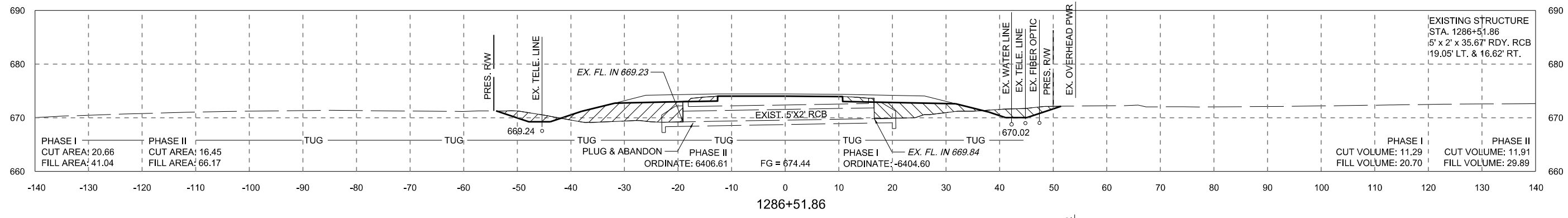
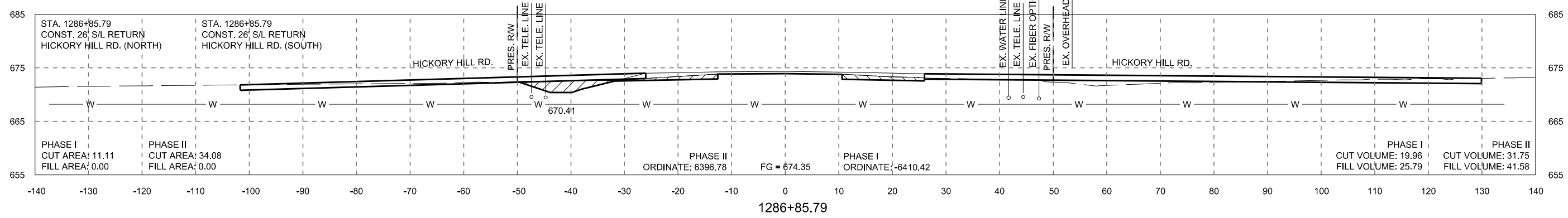
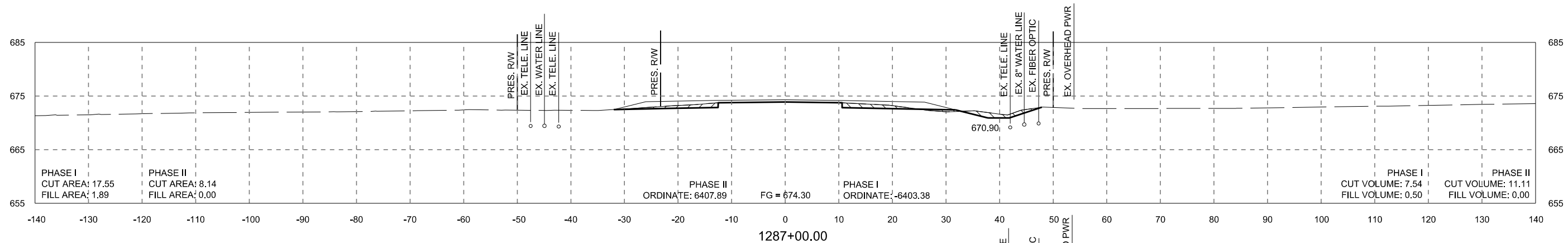
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X079
DRAWN	MM	
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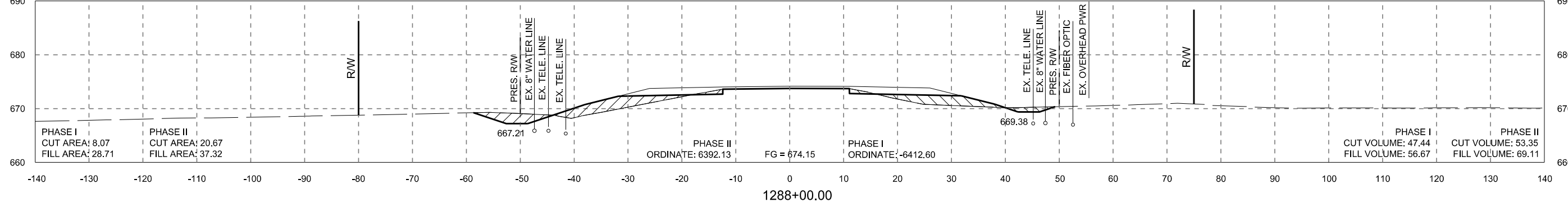
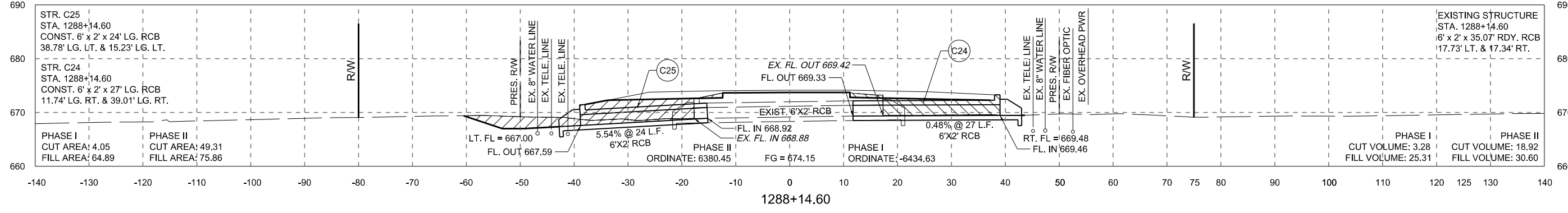
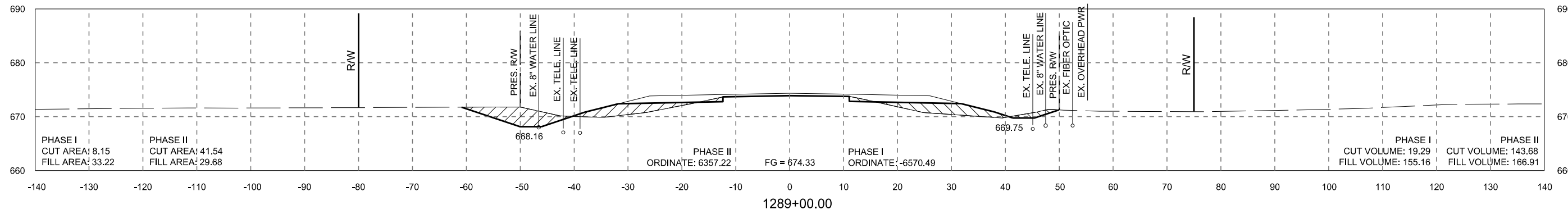
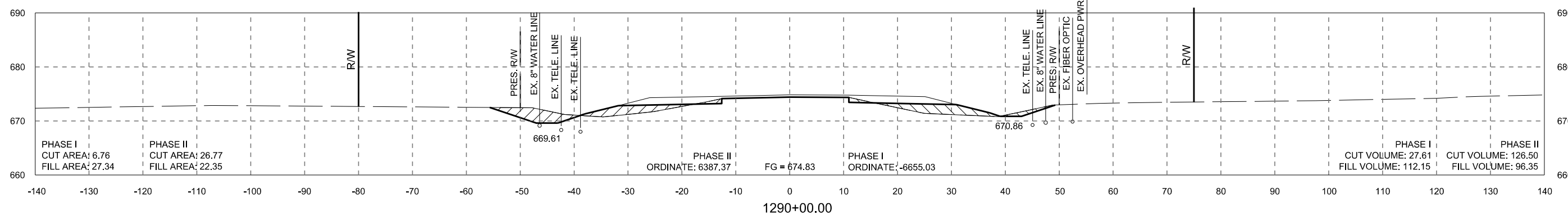
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X080
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

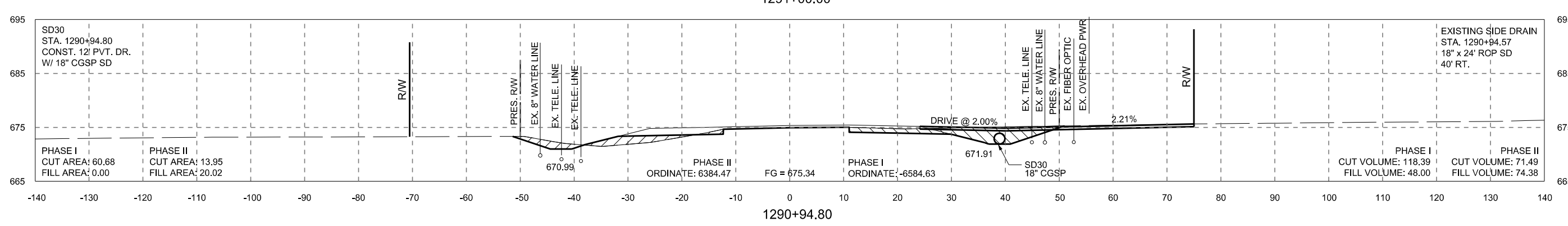
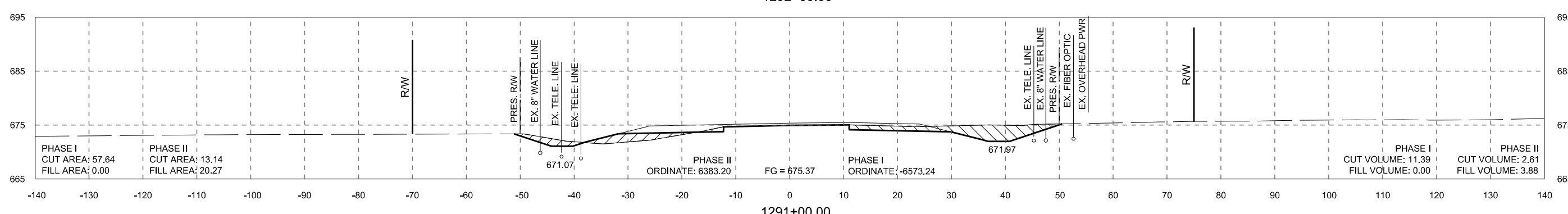
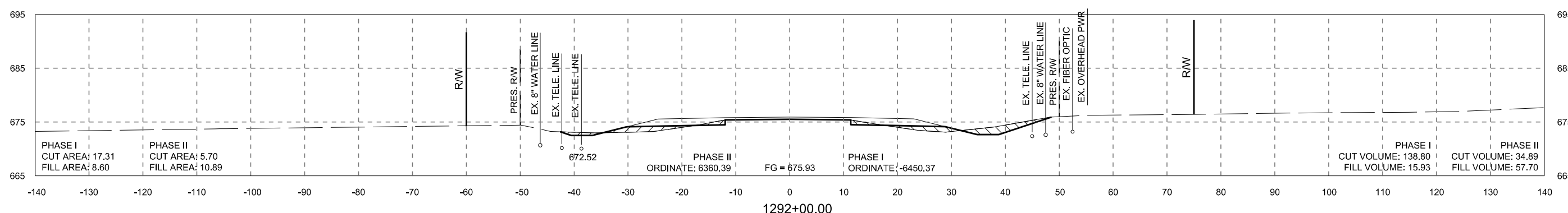
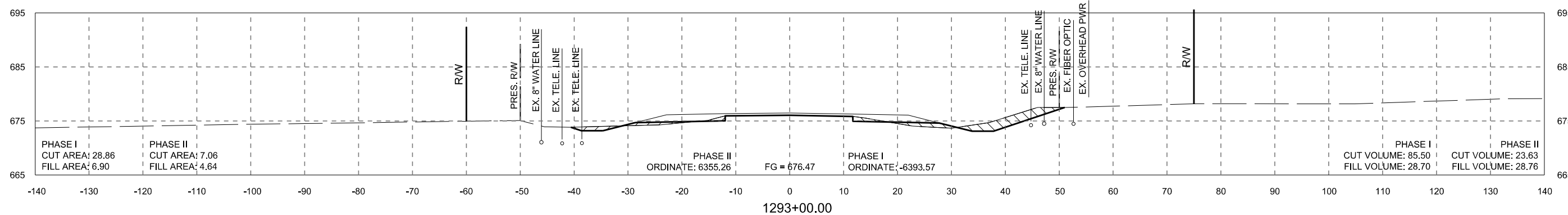
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X081
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APPROVED	SL	
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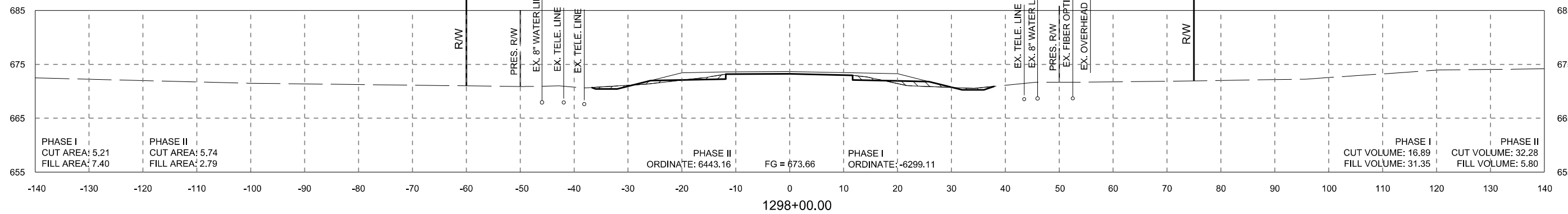
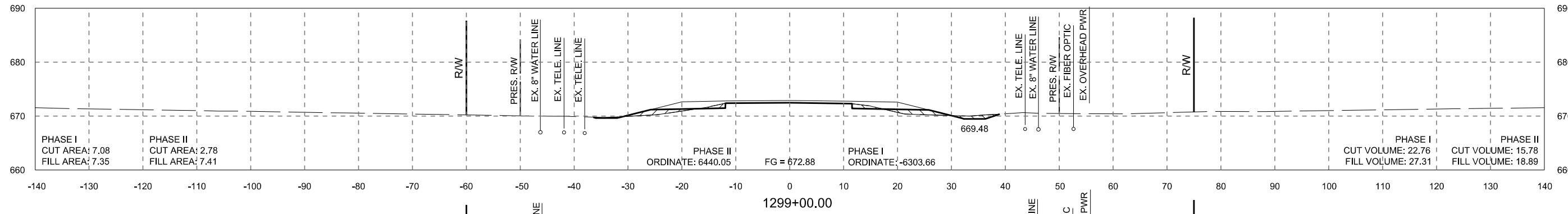
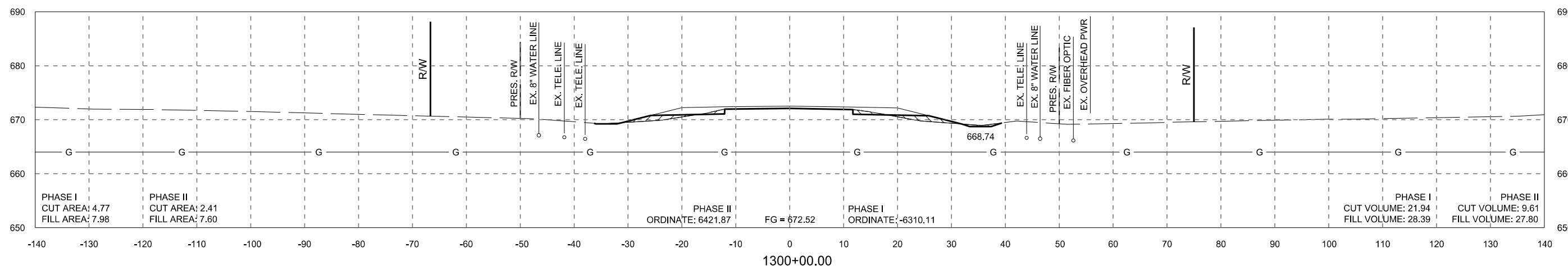
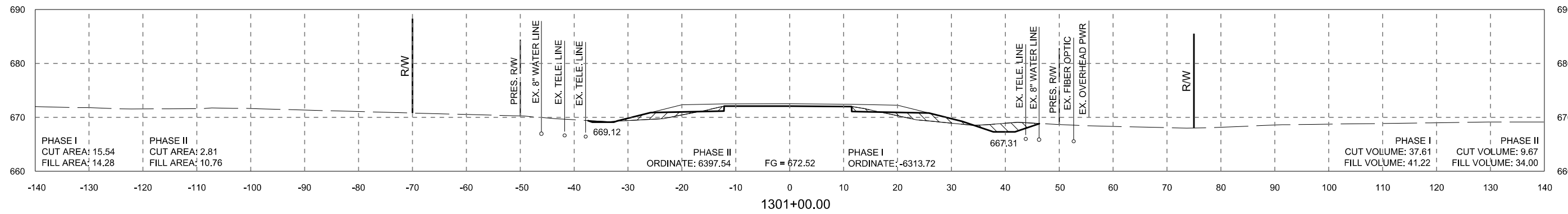
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X082
DRAWN	MM	
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APPROVED	SL	
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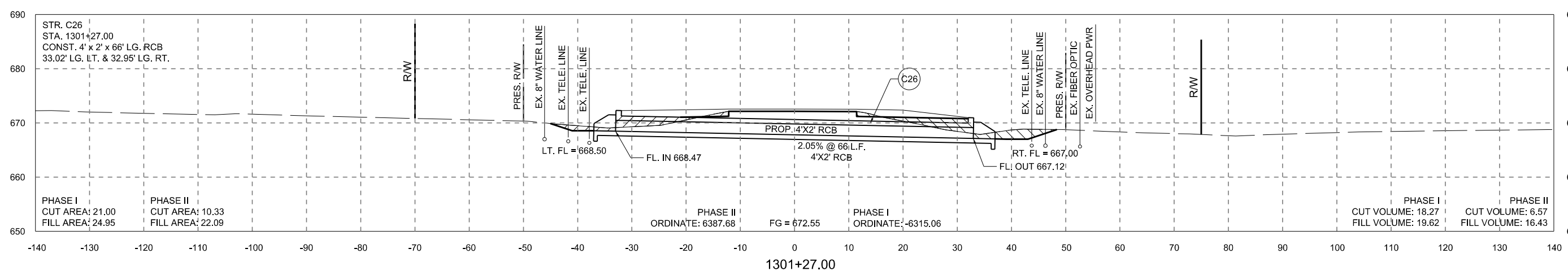
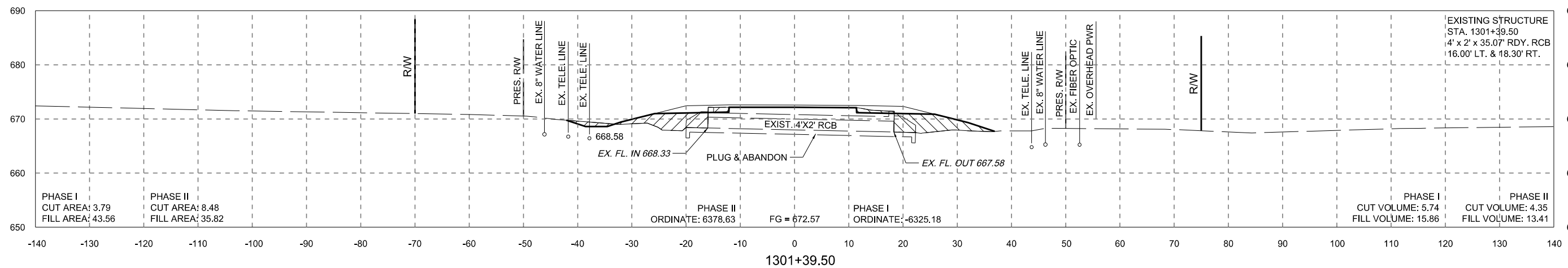
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

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DRAWN	MM	
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APPROVED	SL	
SQUAD	BKL, Inc.	

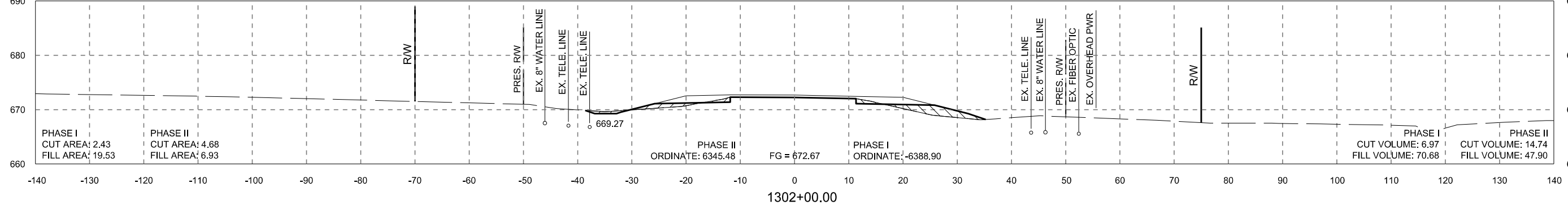
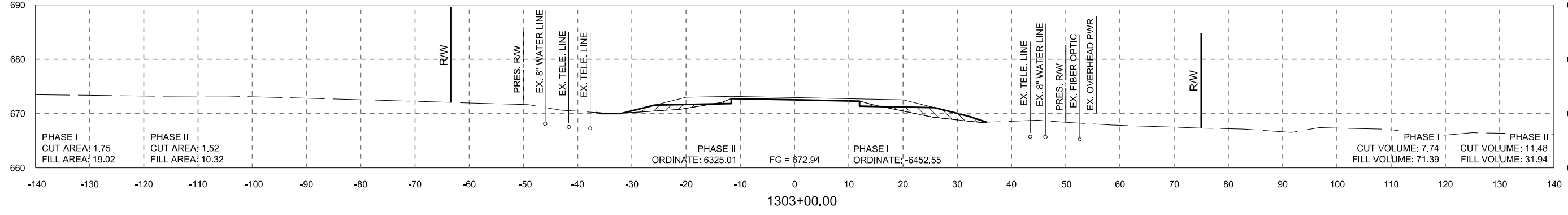
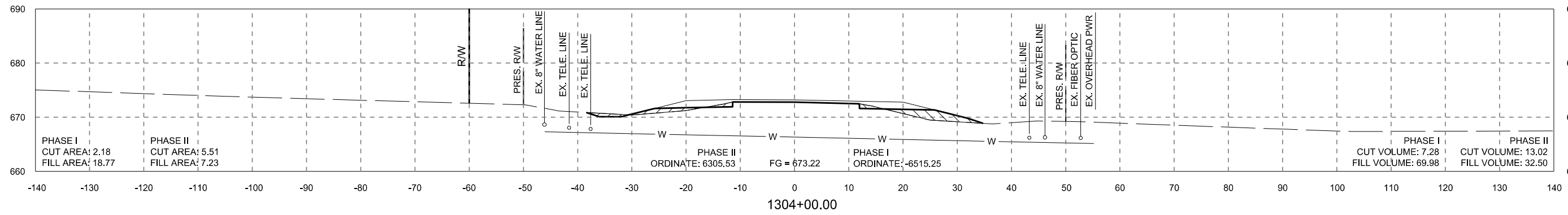
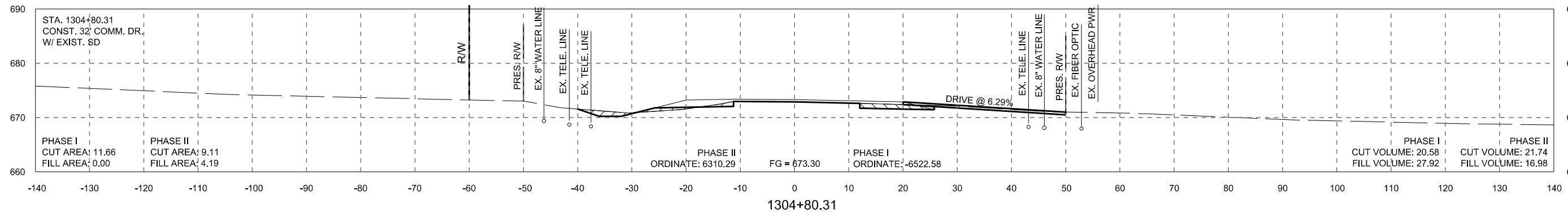
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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X085
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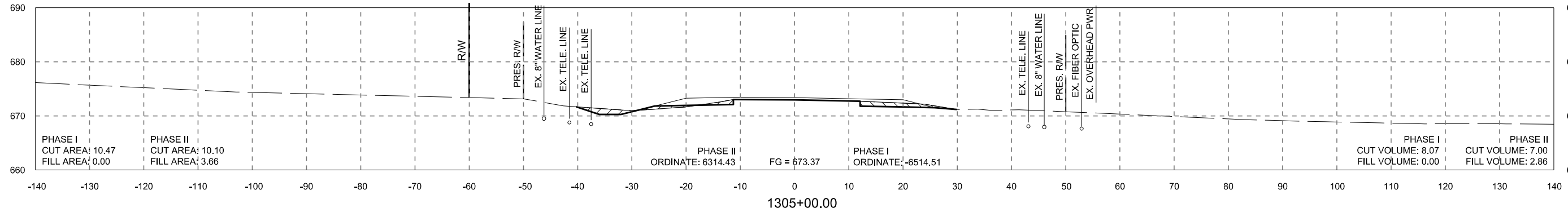
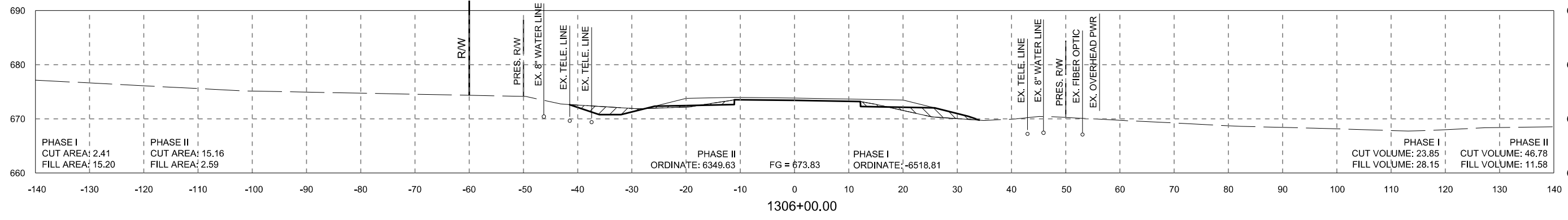
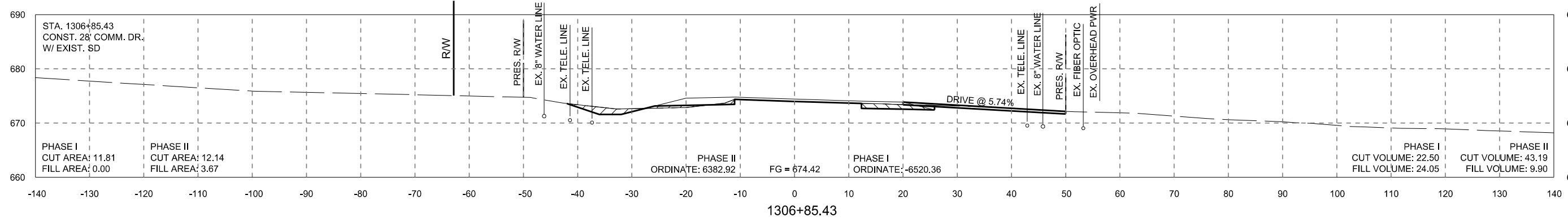
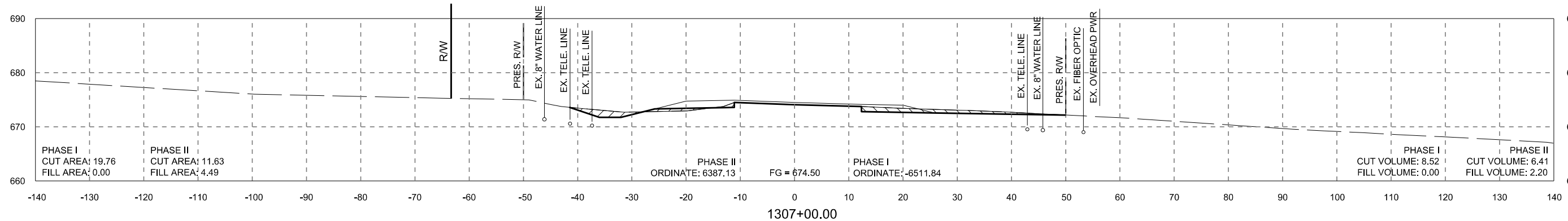
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SCALE: HORIZ. 1" = 1'-0"
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DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X086
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SQUAD	BKL, Inc.	

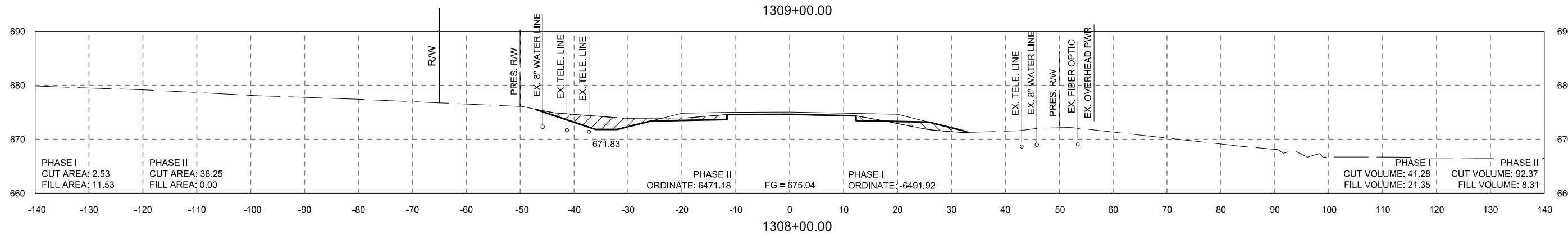
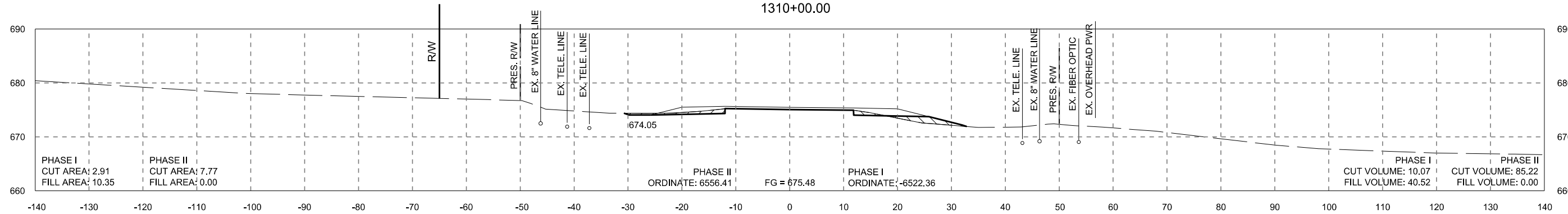
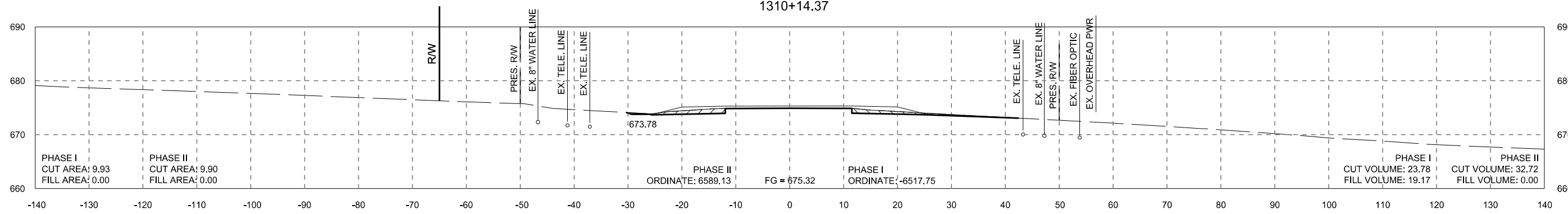
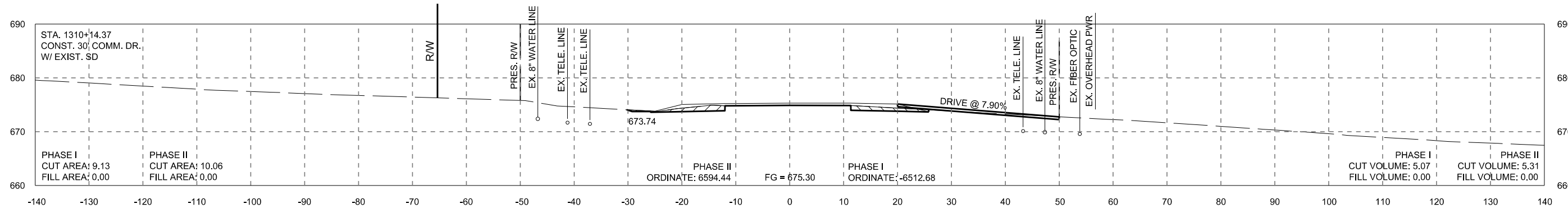
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X087
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

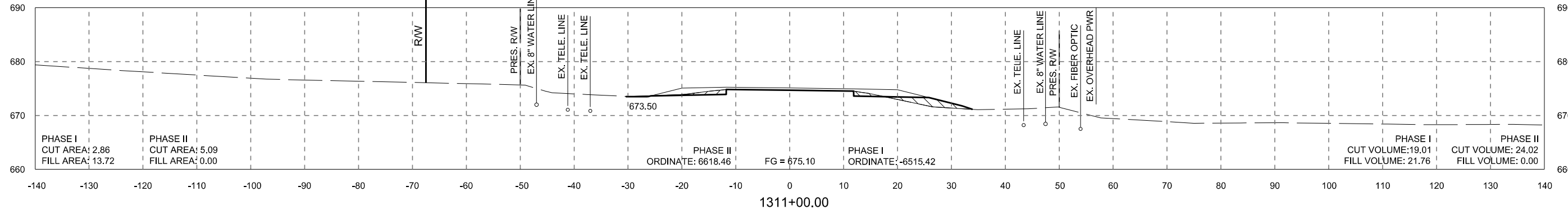
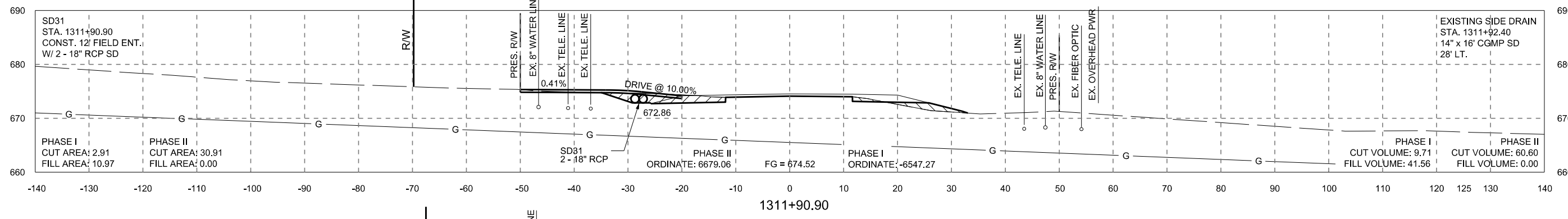
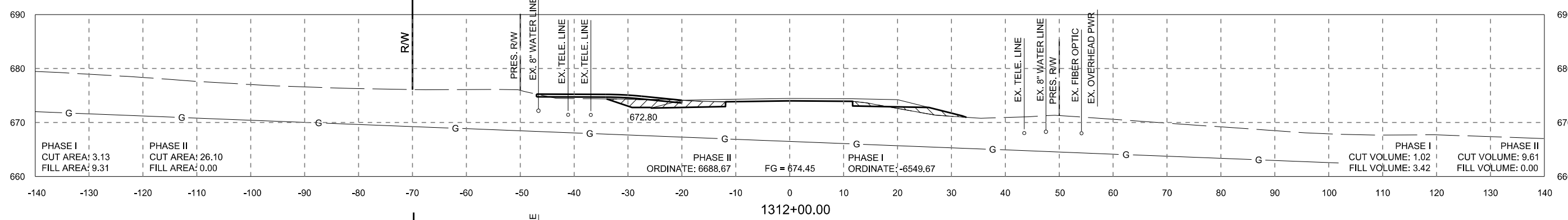
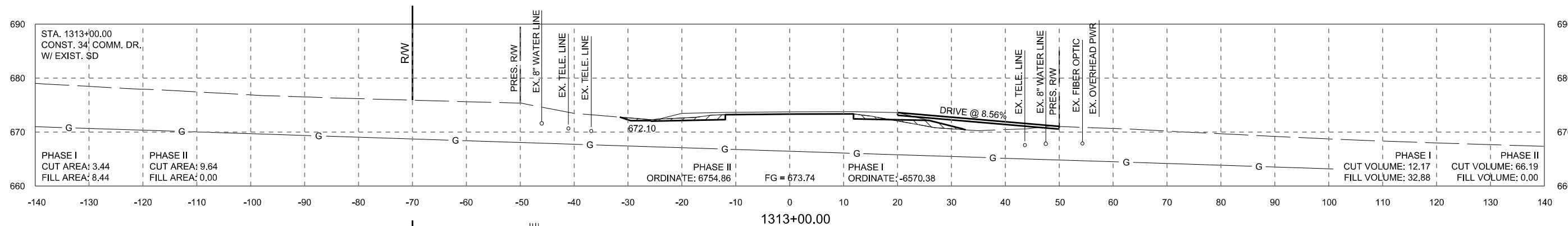
PRINT DATE: 8/22/2018 C:\Projects\650-SHT Final Plans\650-SHT Final Plans\650-SHT Final Plans\650-SHT Final Plans\SECTION MAINLINE.dgn



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X088
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

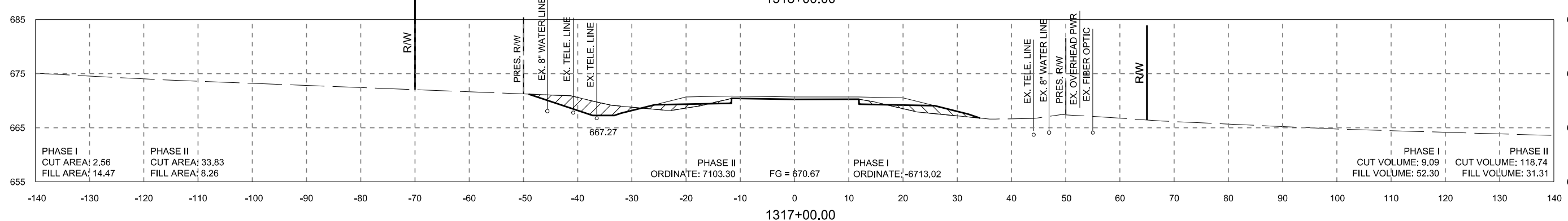
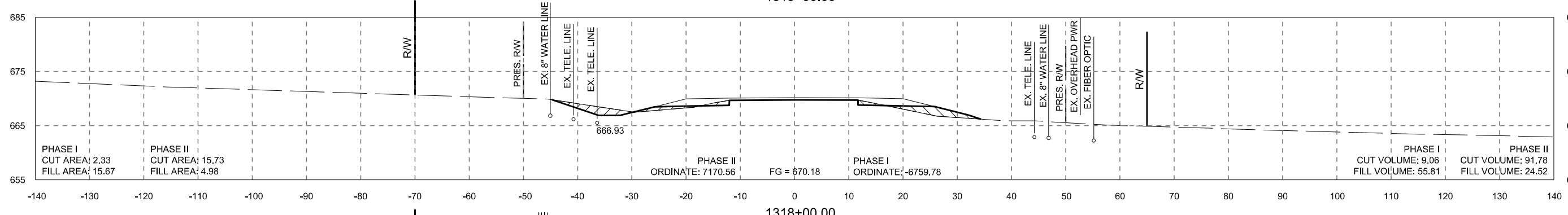
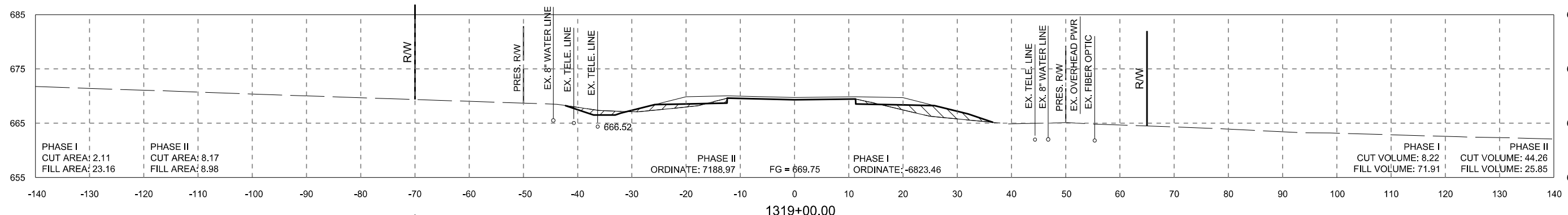
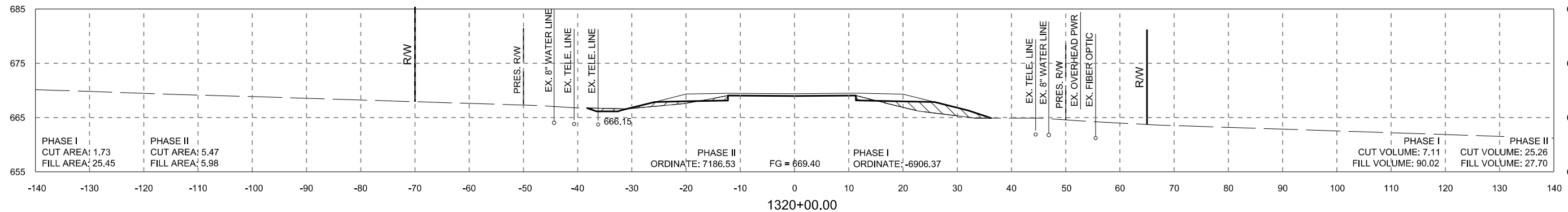
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

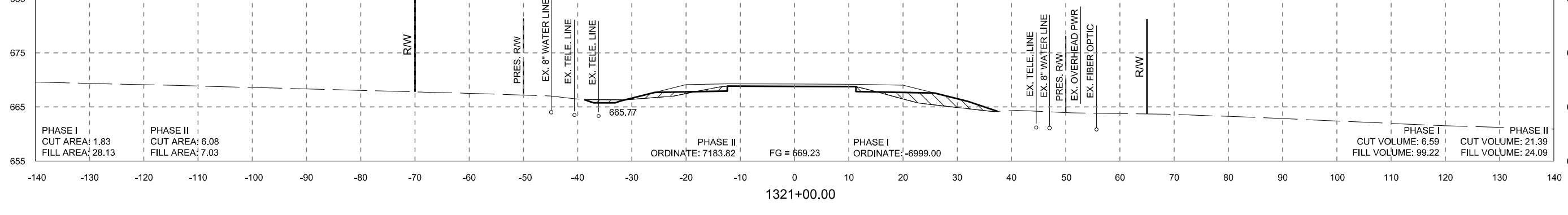
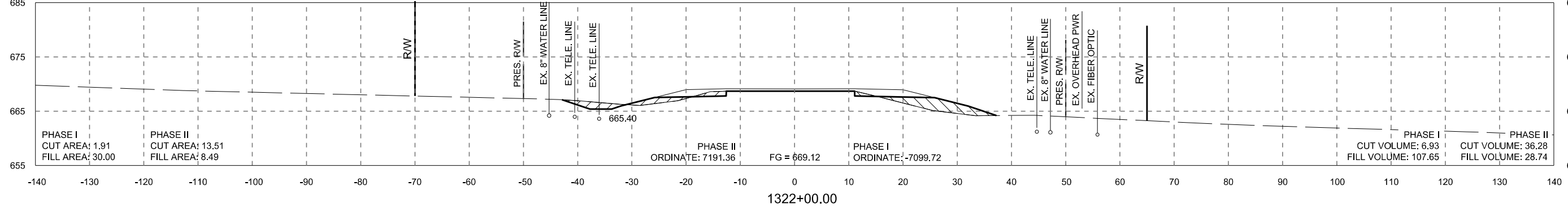
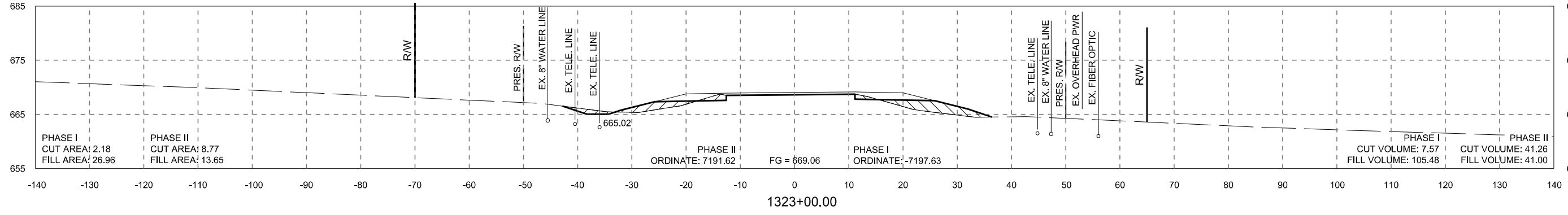
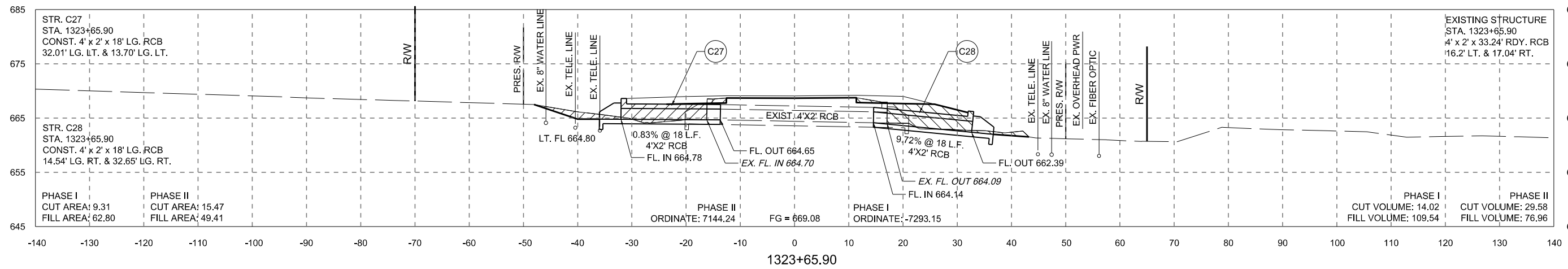
DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X089
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X091
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

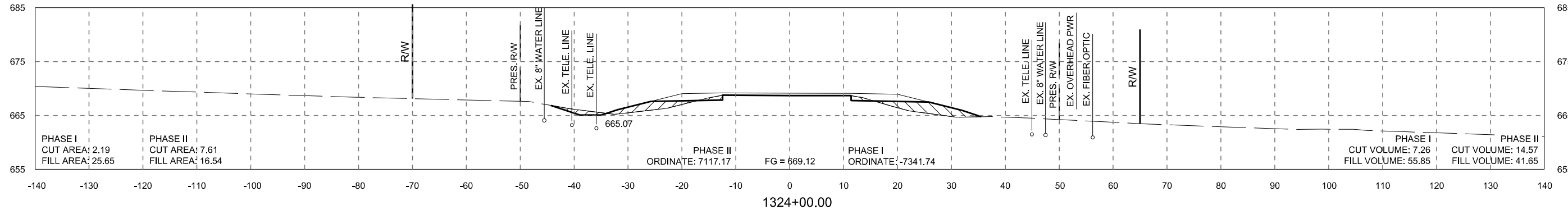
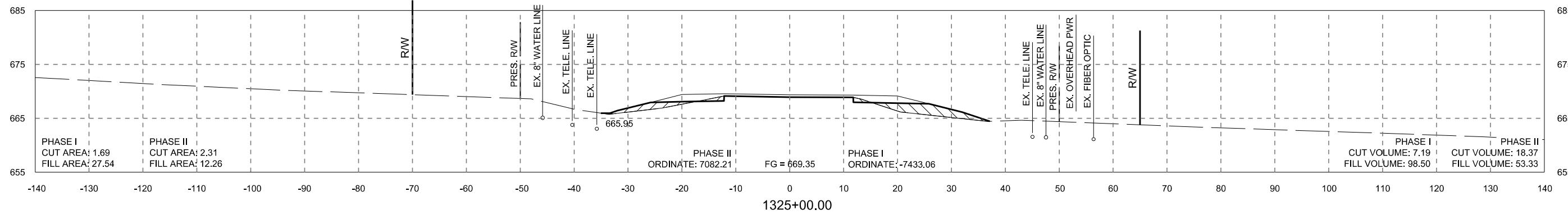
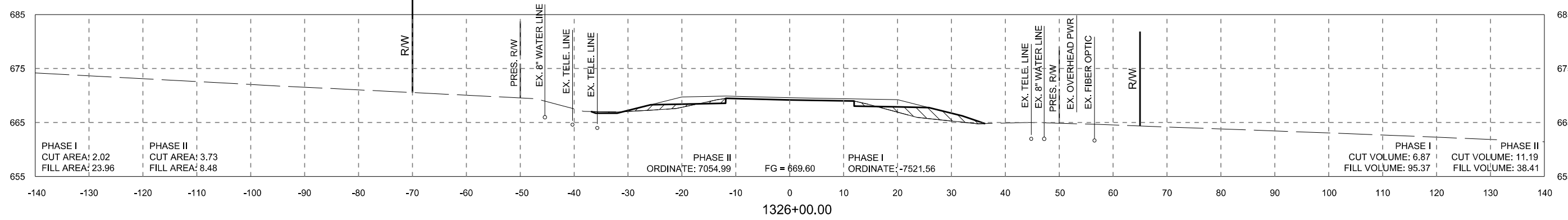
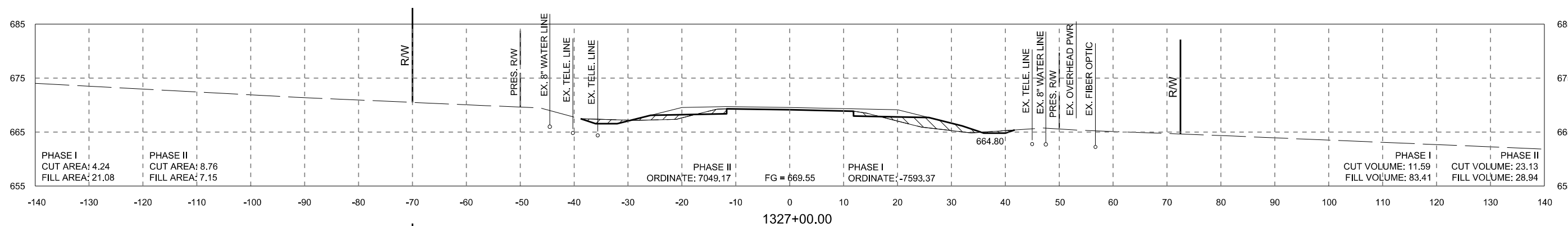


PRINT DATE: 8/22/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTION MAINLINE.dgn

SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X092
DRAWN	MM		
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APPROVED	SL		
SQUAD	BKL, Inc.		

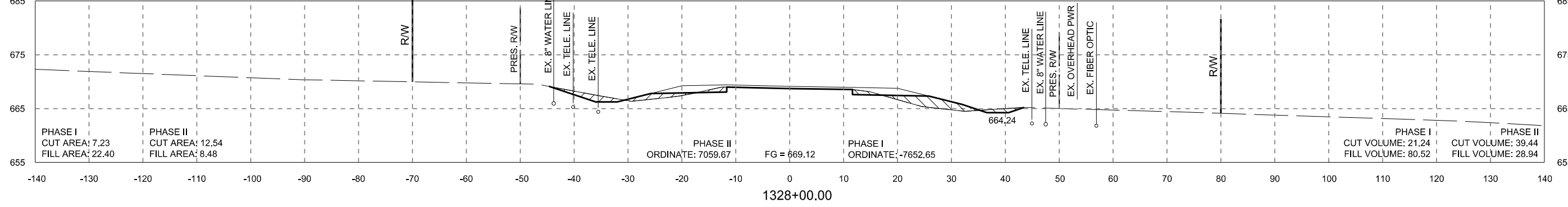
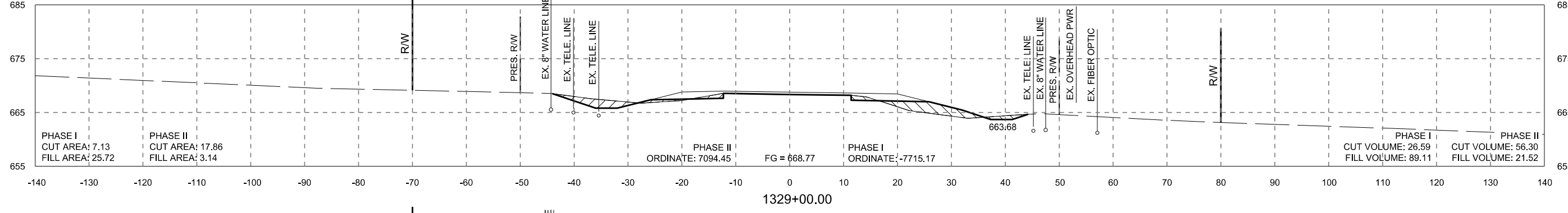
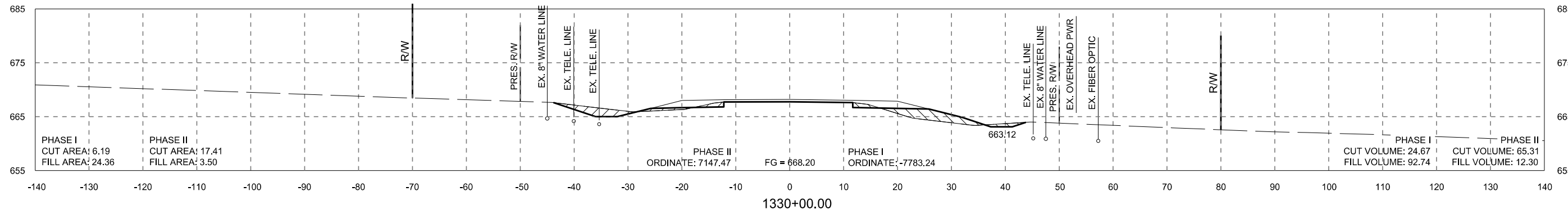
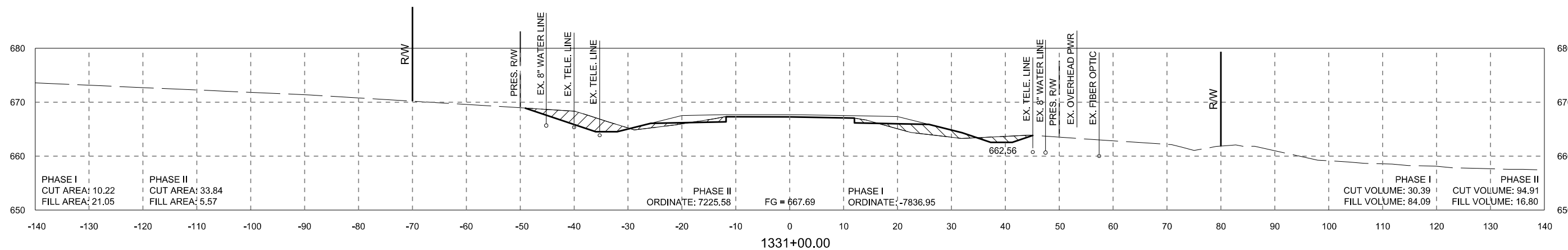
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

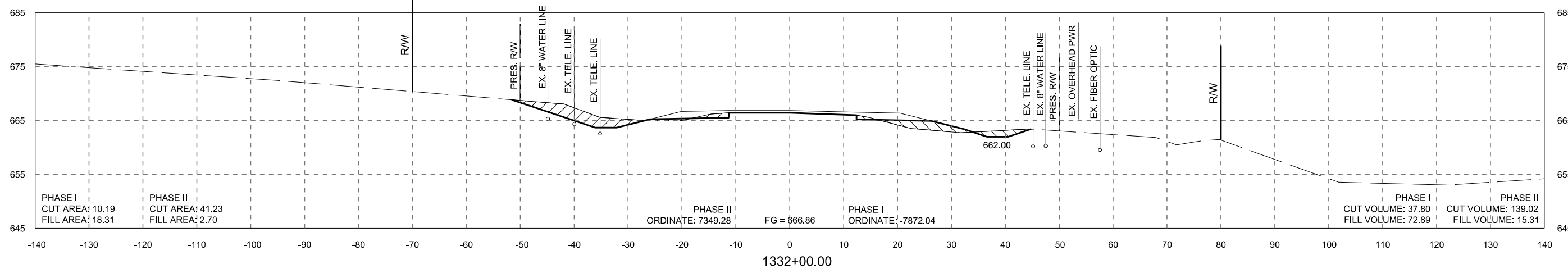
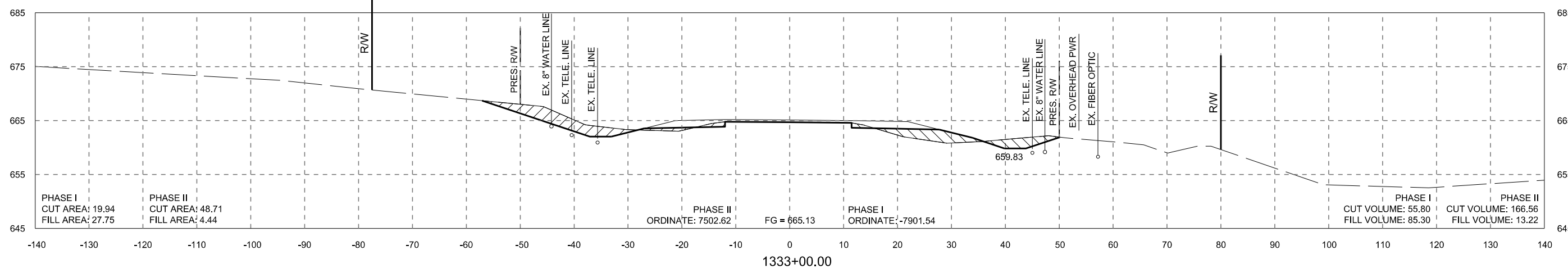
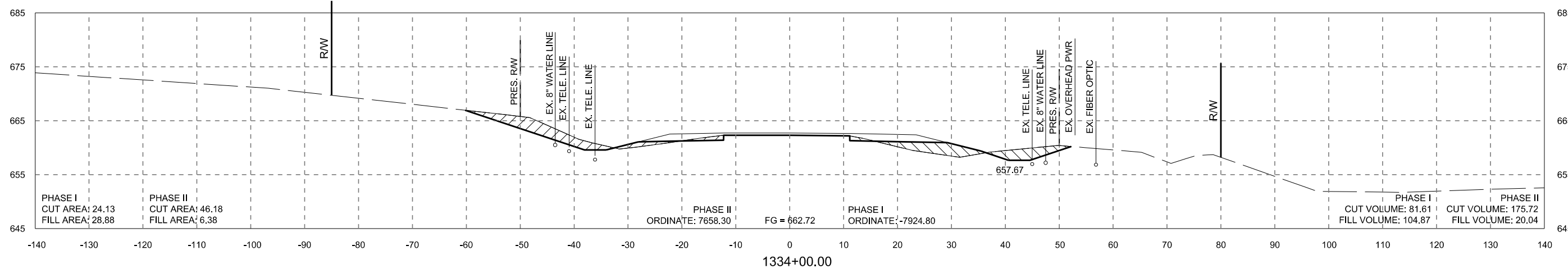
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DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X094
DRAWN	MM	
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SQUAD	BKL, Inc.	

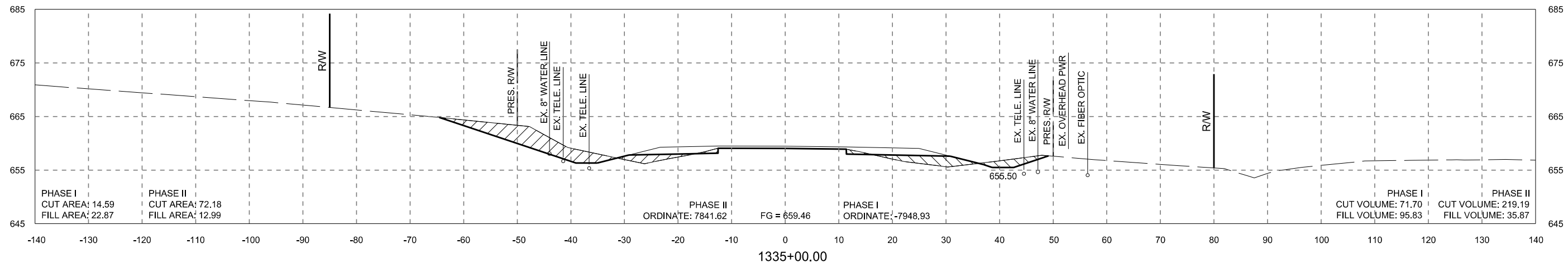
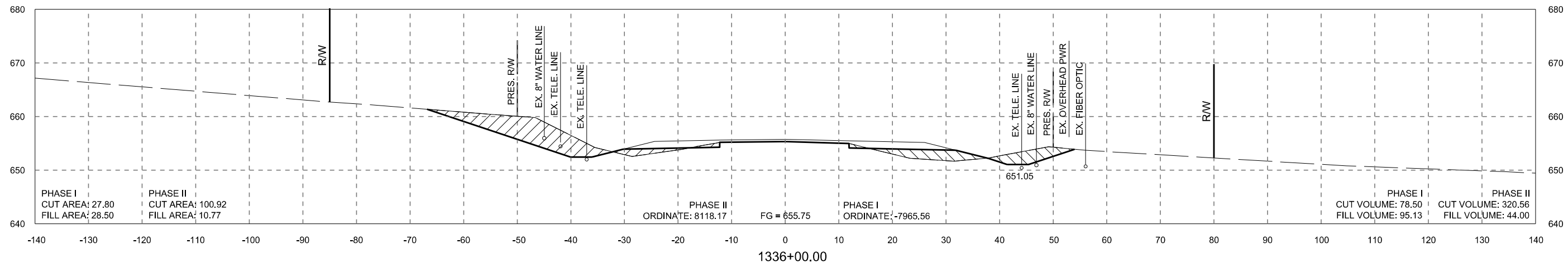
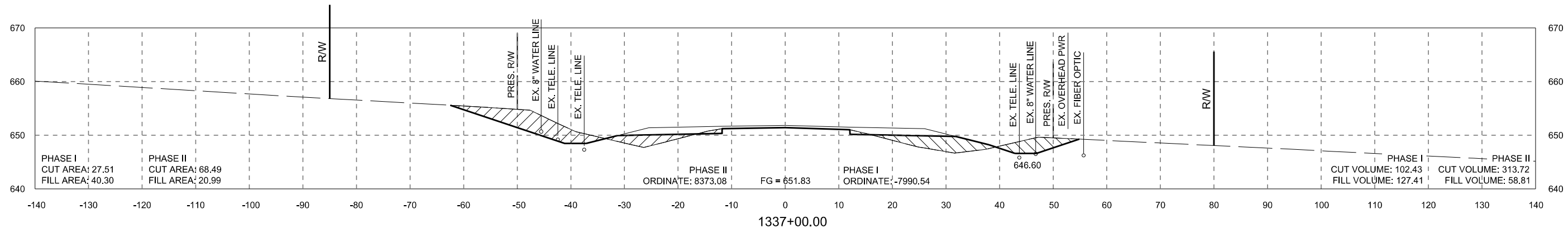


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SCALE: HORIZ. 1" = 1'-0"
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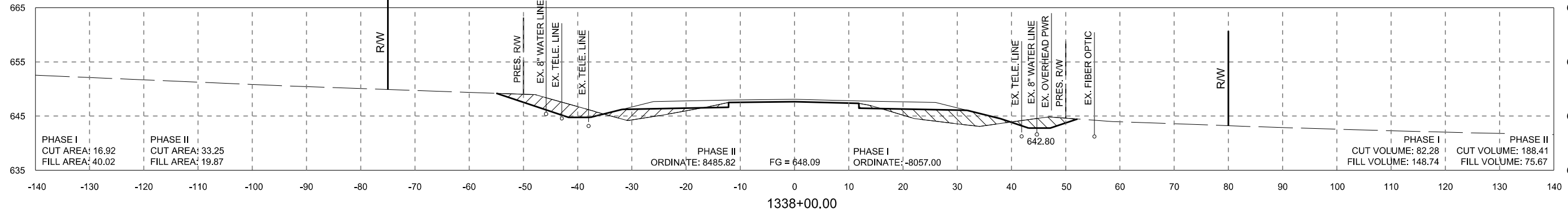
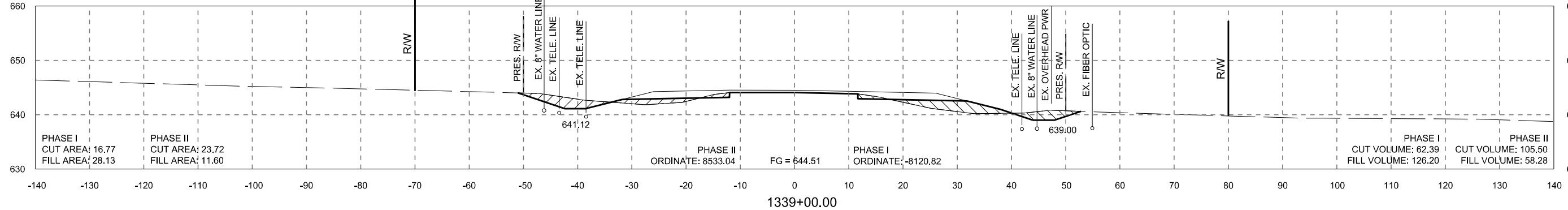
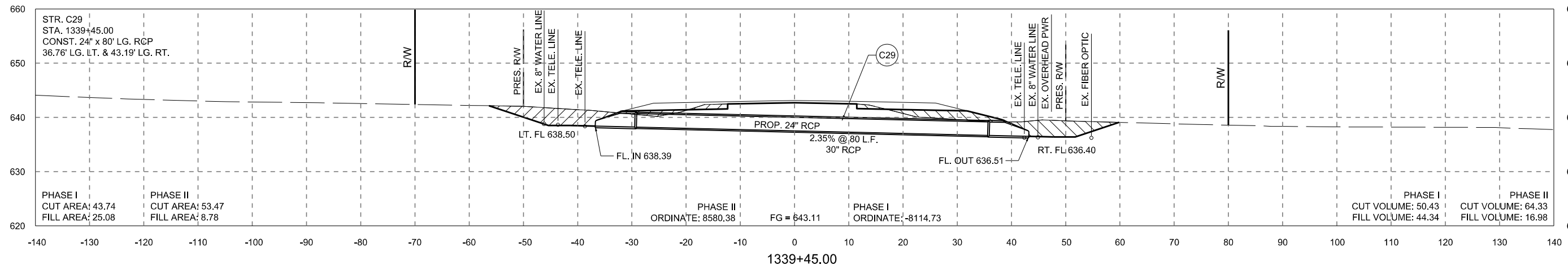
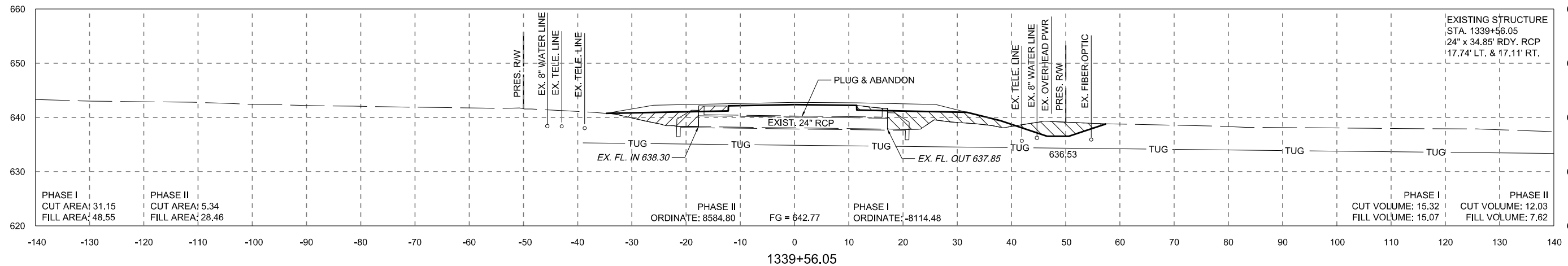
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CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

PRINT DATE: 8/22/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTION MAINLINE.dgn



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X096
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APPROVED	SL	
SQUAD	BKL, Inc.	

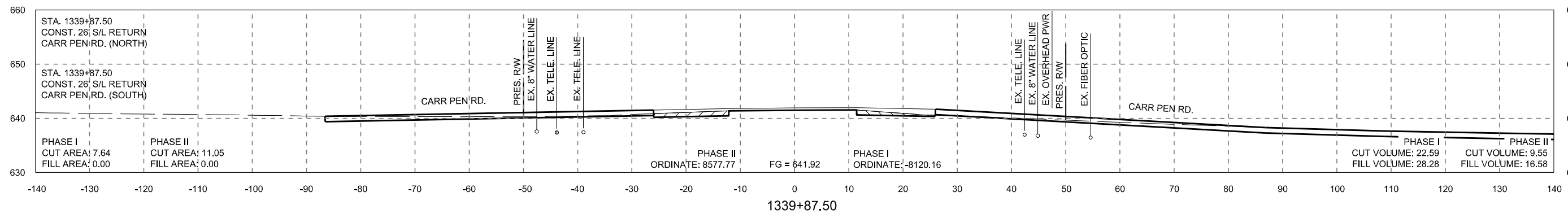
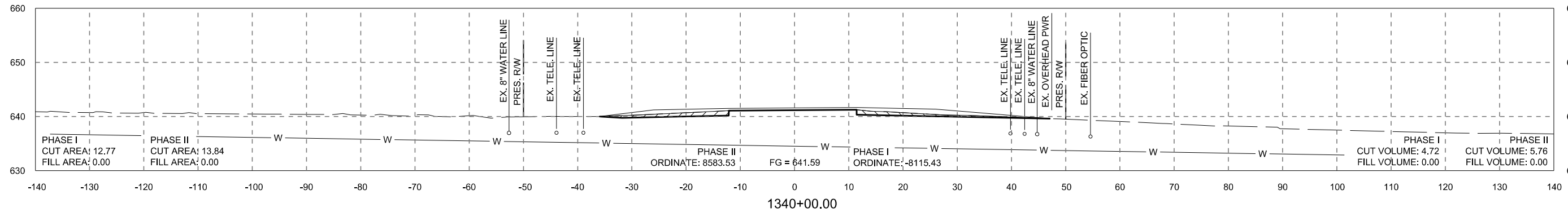
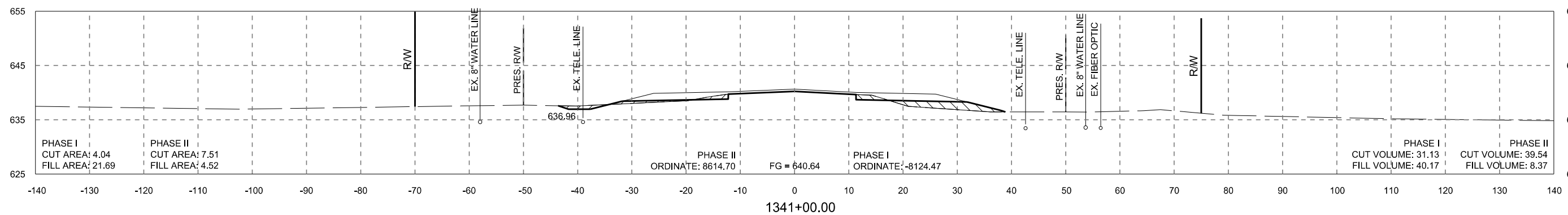
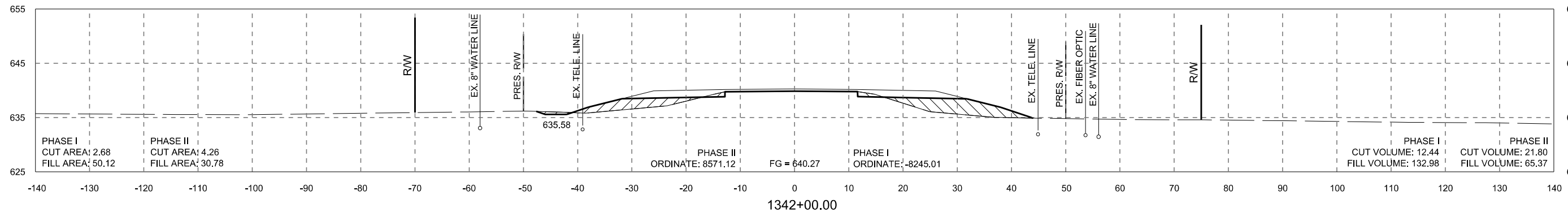


SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

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APPROVED	SL	
SQUAD	BKL, Inc.	

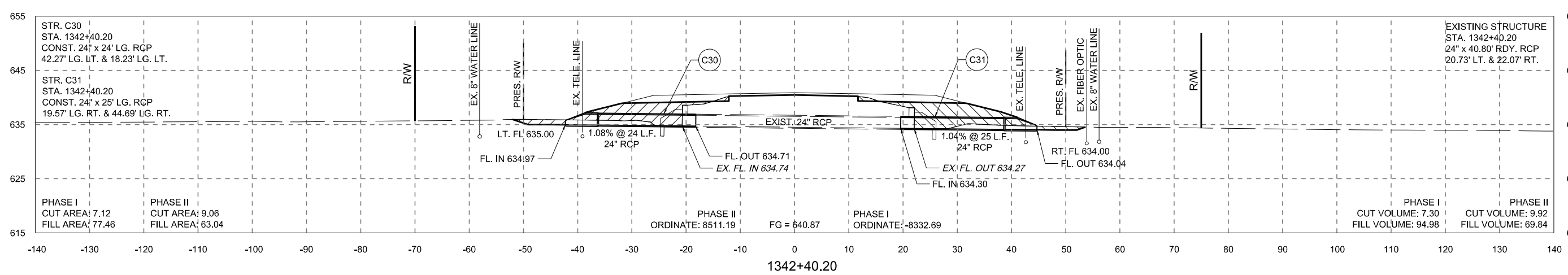
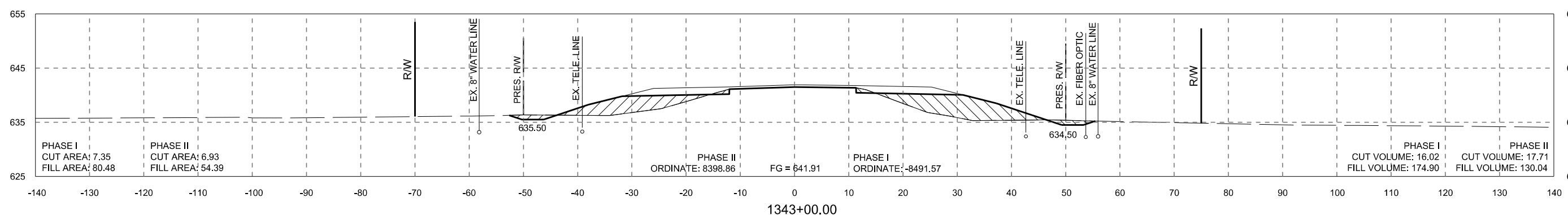
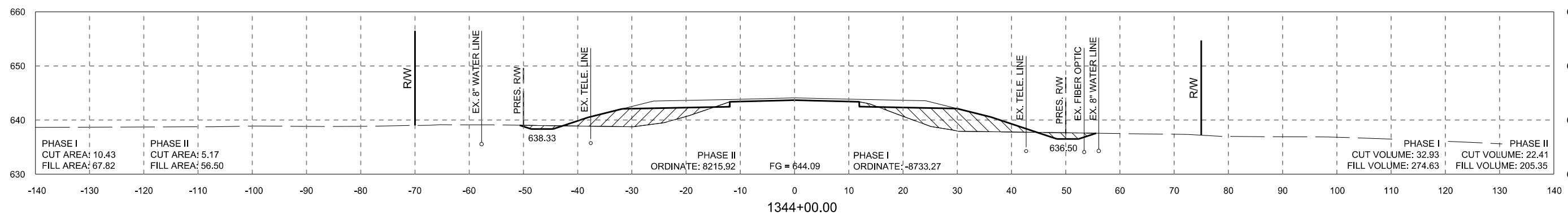
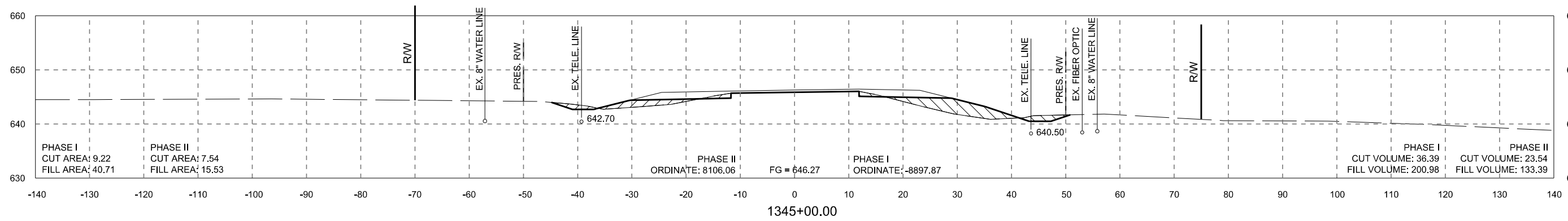
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SCALE: HORIZ. 1" = 1'-0"
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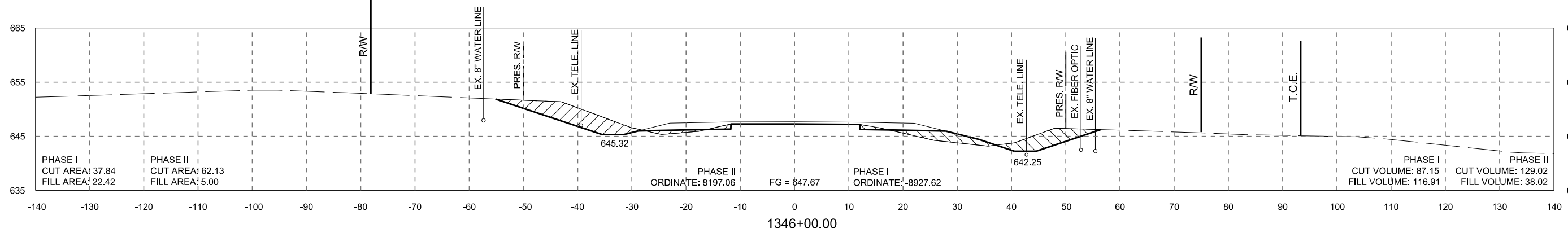
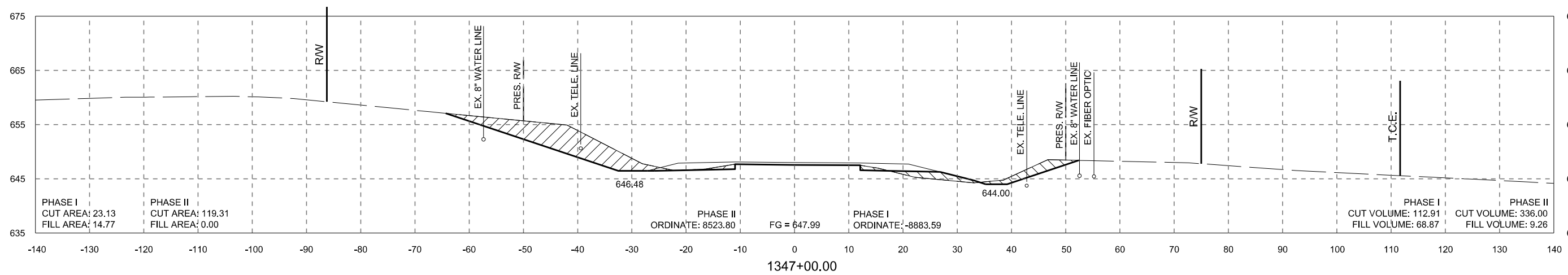
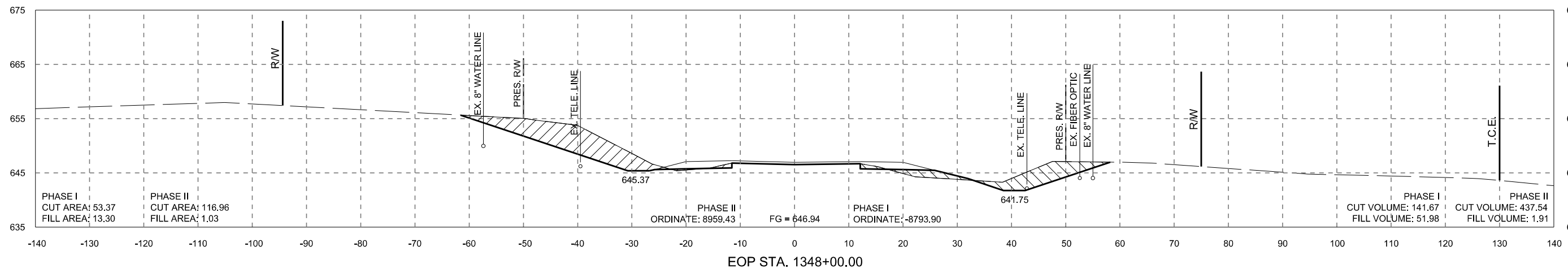
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DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	



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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X099
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CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

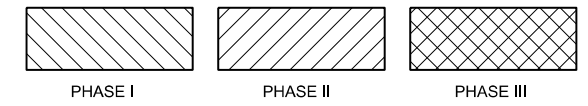
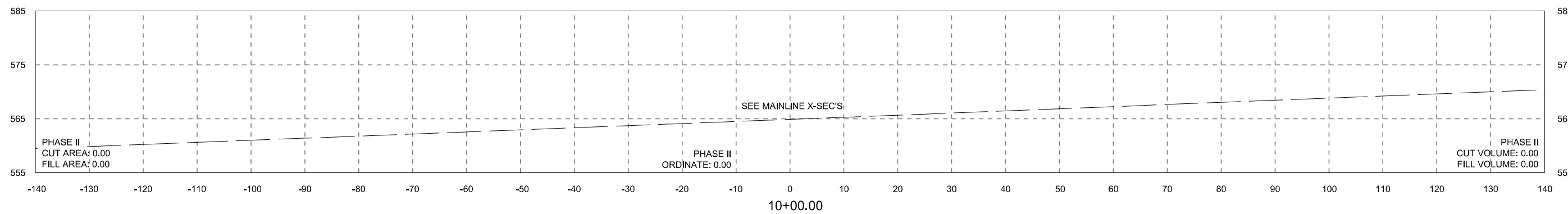
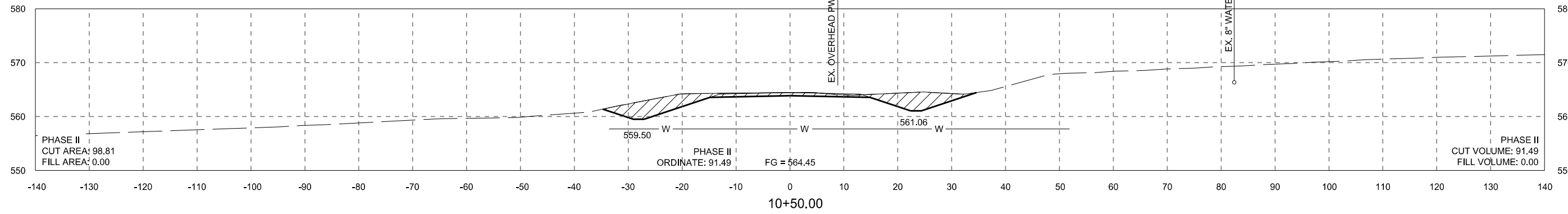
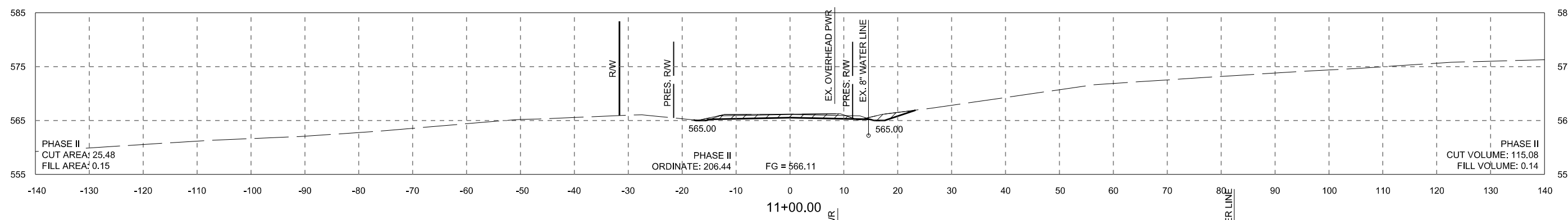
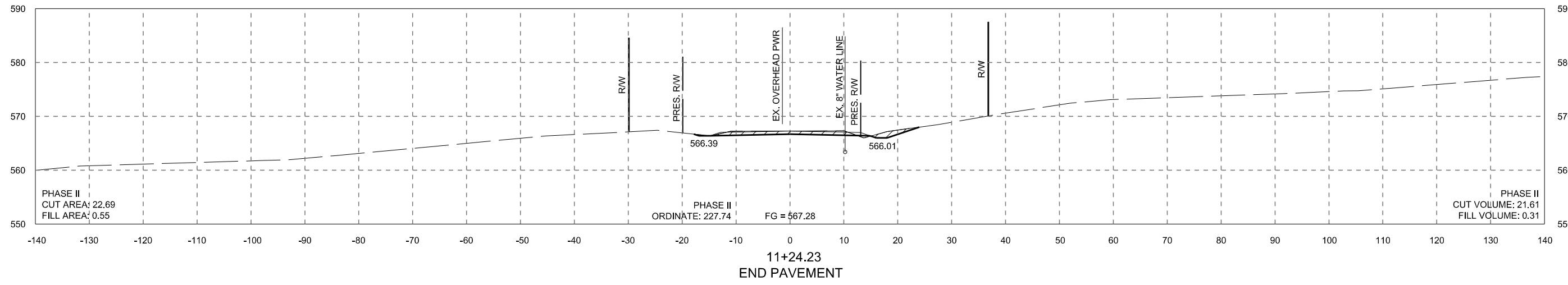


PRINT DATE: 8/22/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTION MAINLINE.dgn

SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION MAINLINE CROSS SECTIONS JOB PIECE NO. 24066(04) SHEET NO. X100
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CHECKED	DP	
APPROVED	SL	
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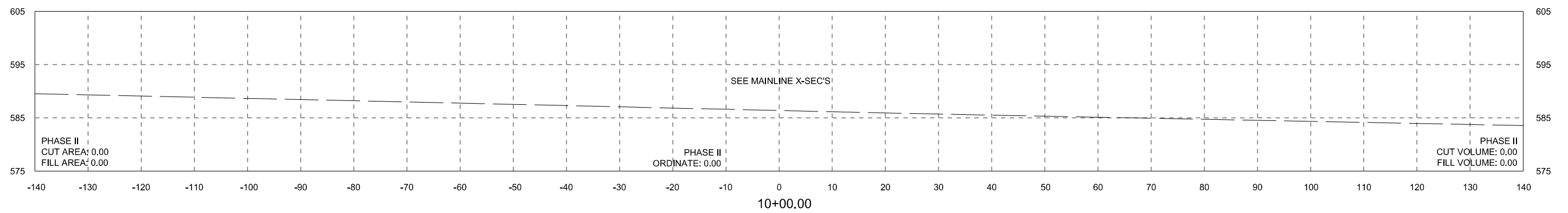
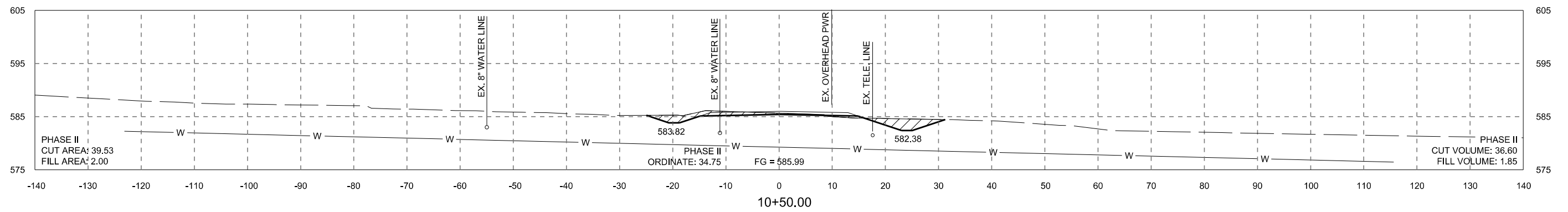
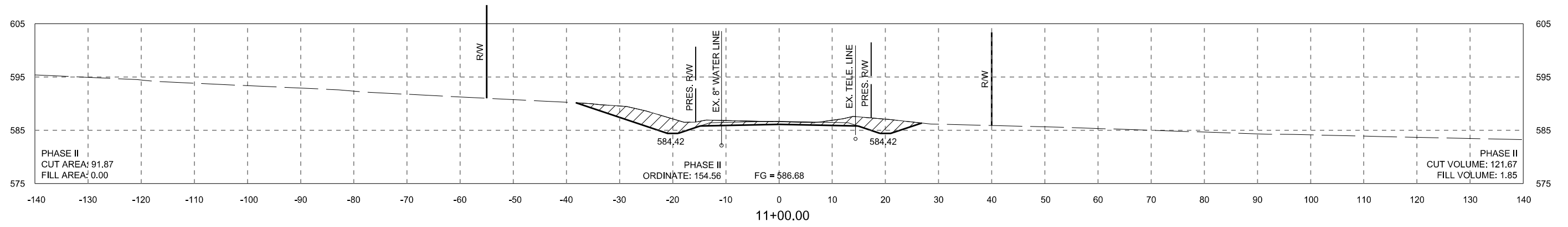
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS STANDING ROCK RD. JOB PIECE NO. 24066(04) SHEET NO. X101
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

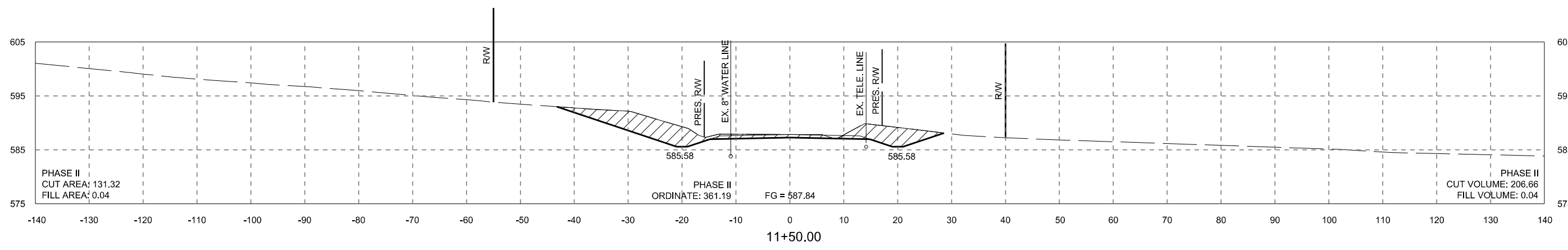
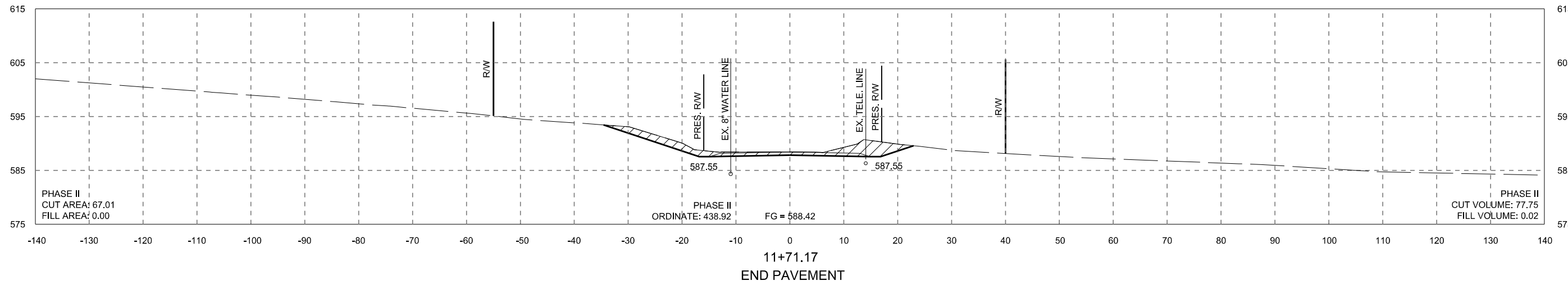
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

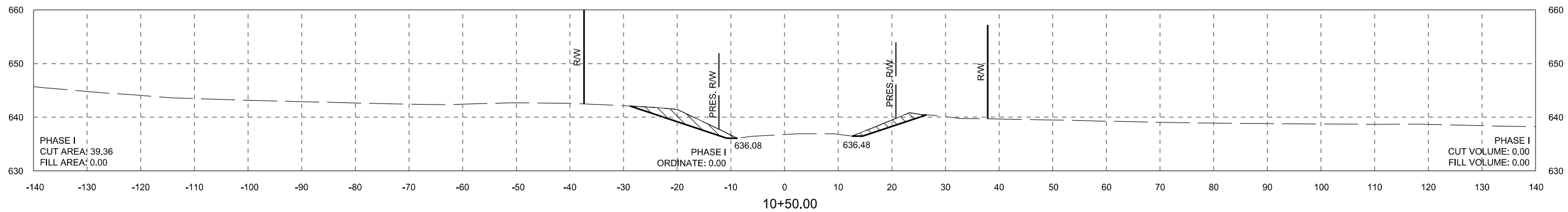
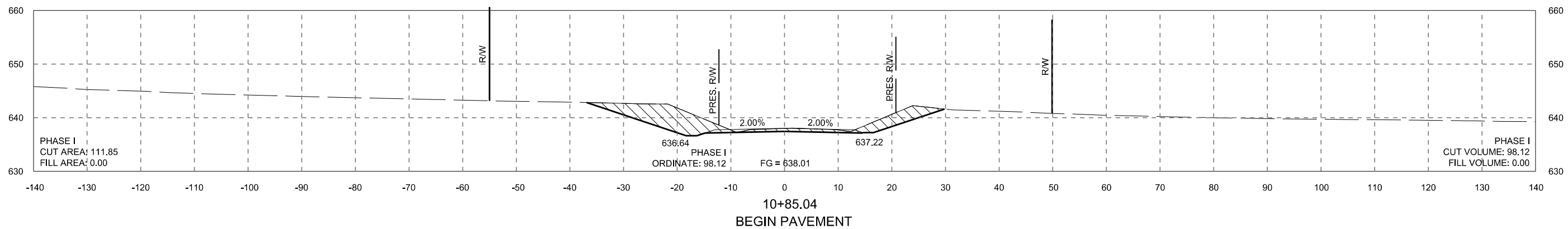
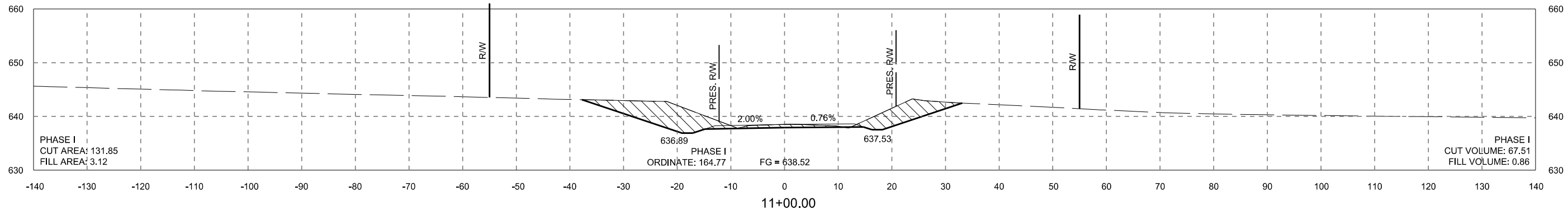
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DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

PRINT DATE: 8/23/2018 C:\Projects\650-SHT Final Plans\OGN\X101-24066(04)\X-SECTIONS-CC-RD.dwg



SCALE: HORIZ. 1" = 1'-0"
 VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS OLD BROOM RD. JOB PIECE NO. 24066(04) SHEET NO. X103
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

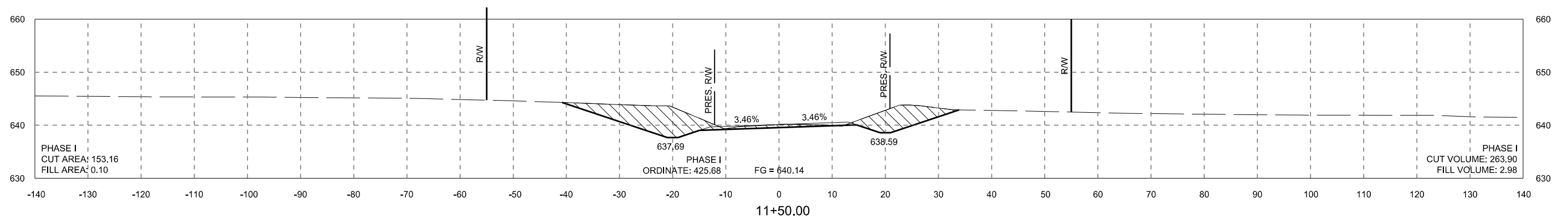
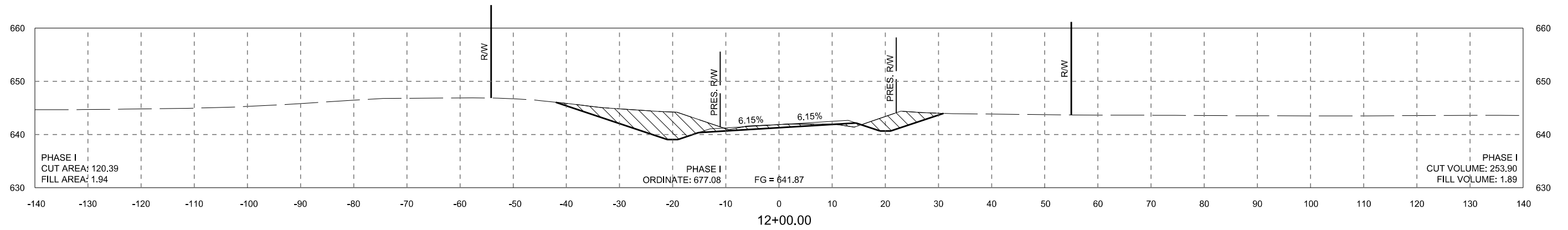
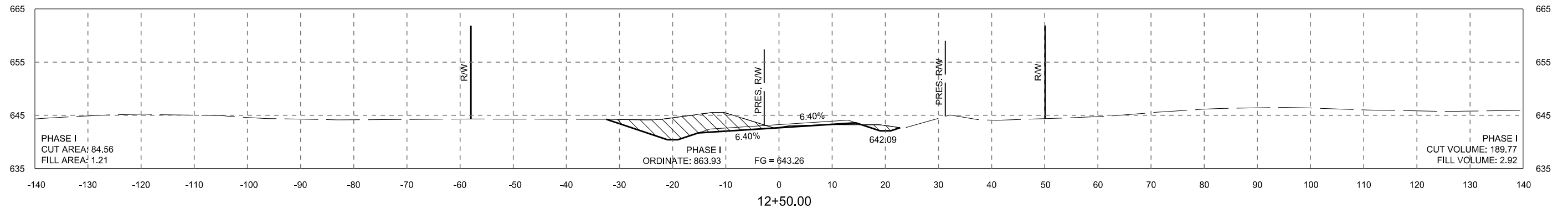


PRINT DATE: 8/23/2018 C:\Projects\150-SHT Final Plans\101-24066(04)-X-SECTIONS-CC-RD.dwg

SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS WG BAR RD. JOB PIECE NO. 24066(04) SHEET NO. X104
DRAWN	MM	
CHECKED	DP	
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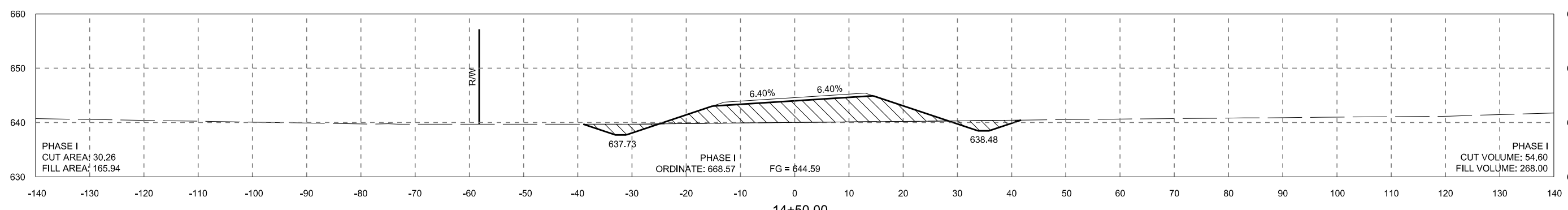
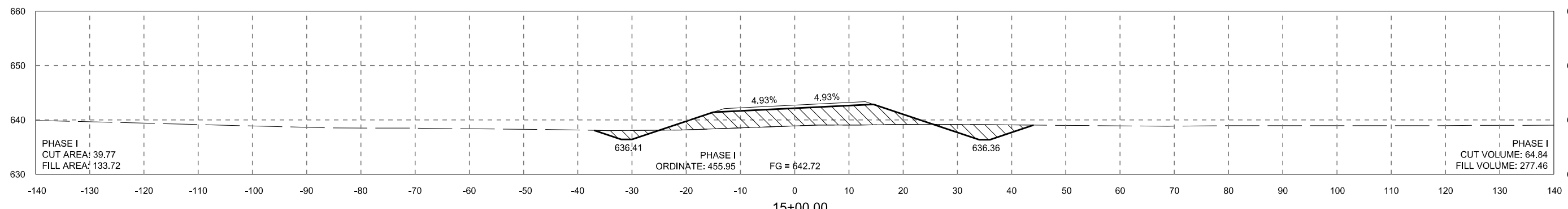
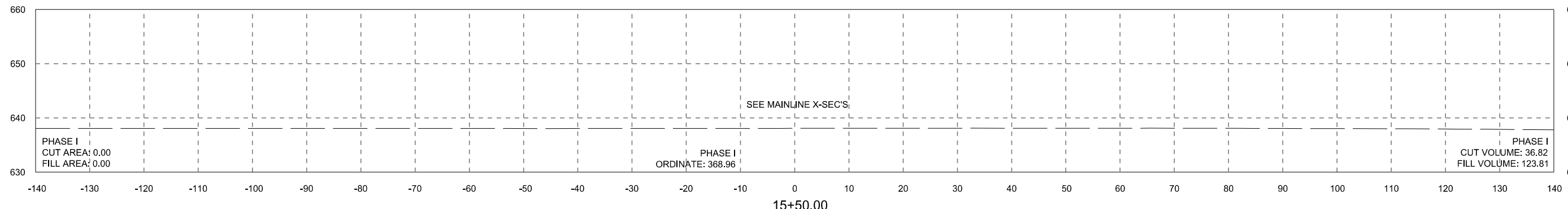
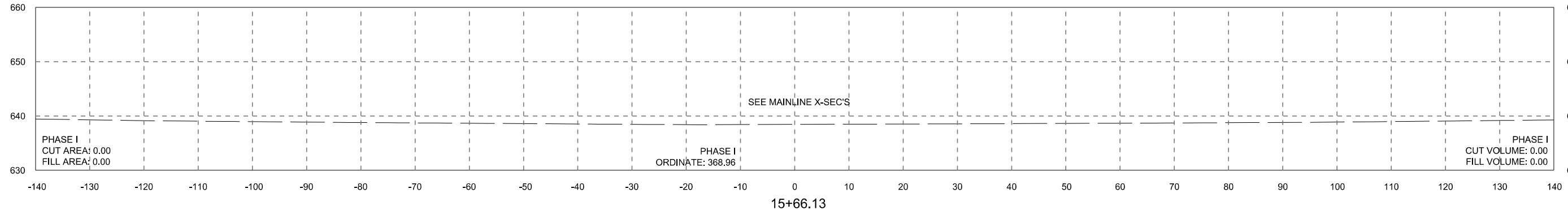
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

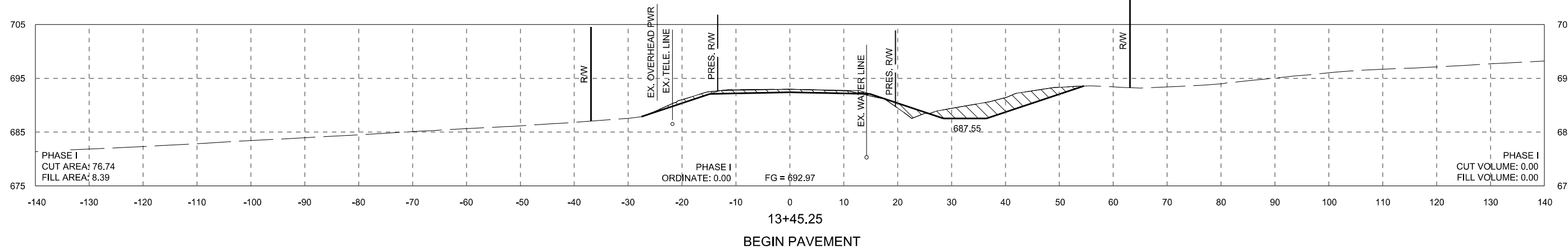
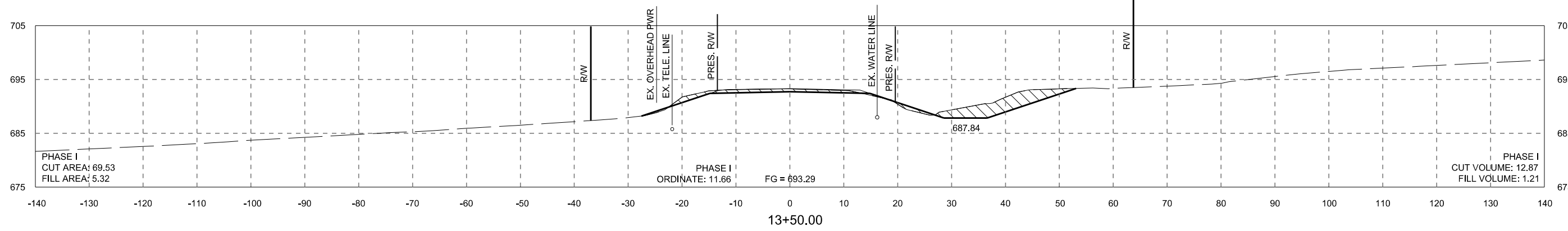
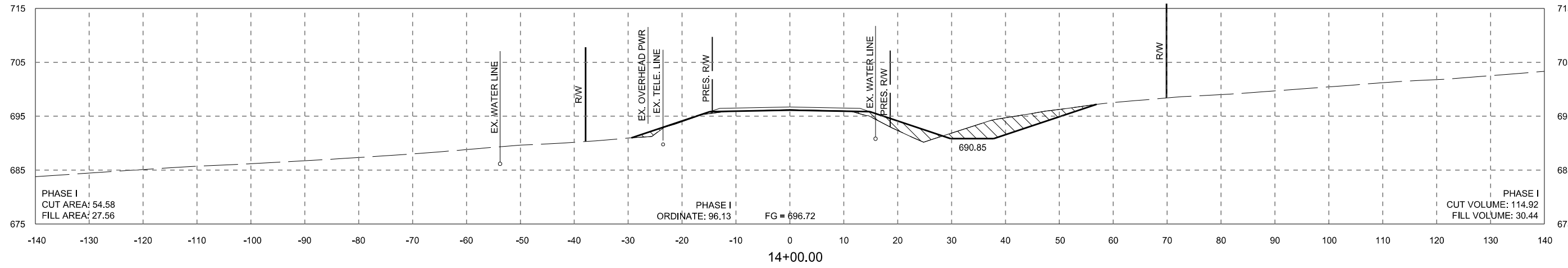
DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS WG BAR RD. JOB PIECE NO. 24066(04) SHEET NO. X105
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS WG BAR RD. JOB PIECE NO. 24066(04) SHEET NO. X107
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

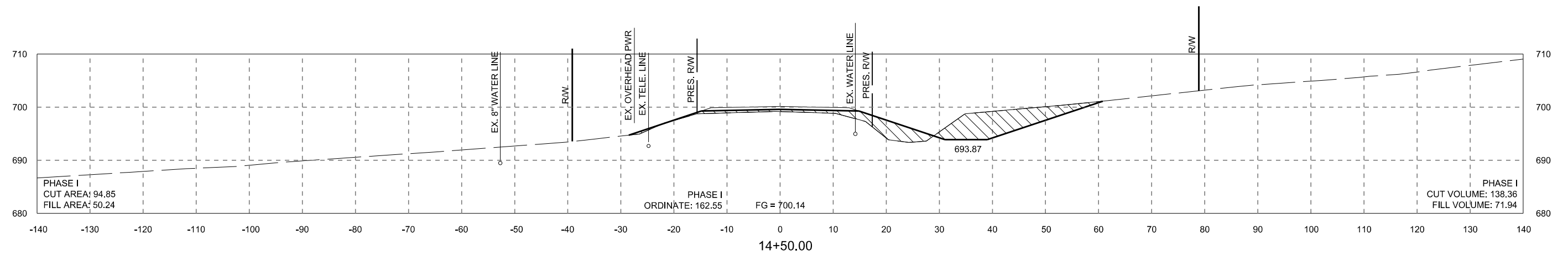
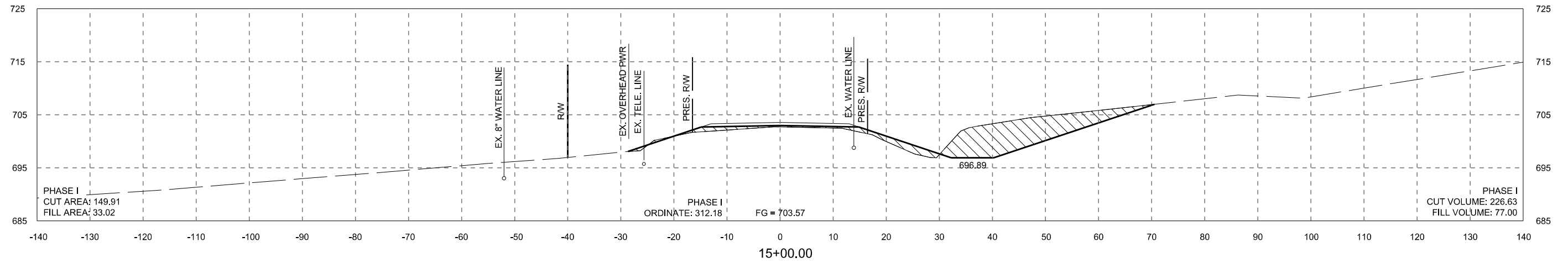


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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS WARDS CHAPEL RD. JOB PIECE NO. 24066(04) SHEET NO. X108
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

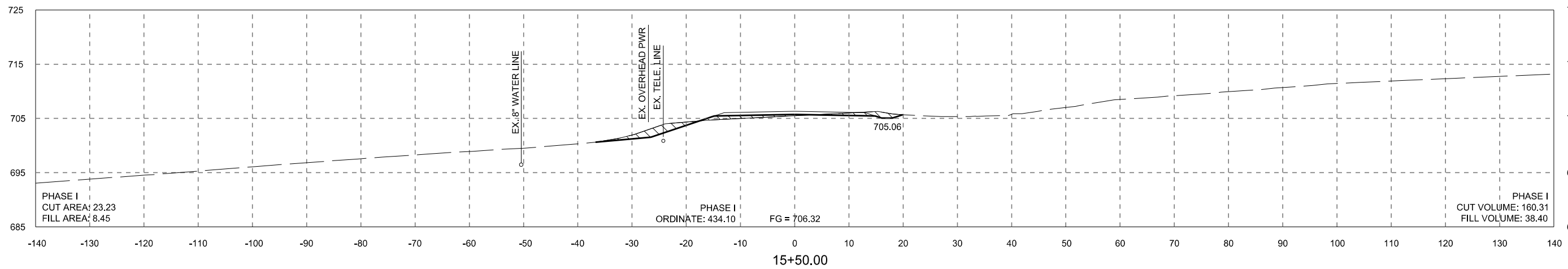
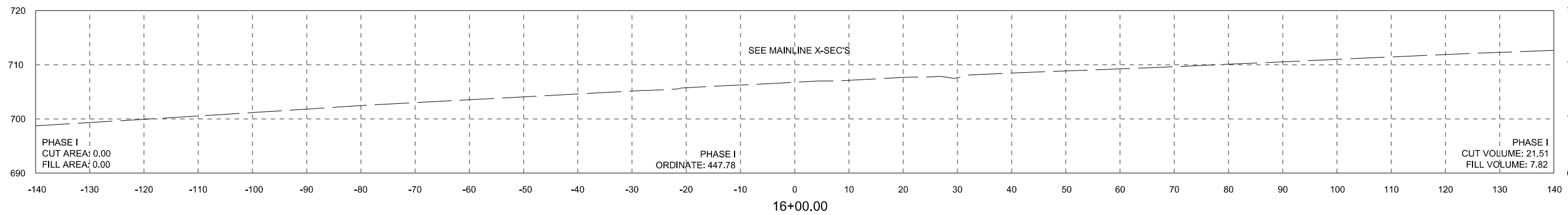
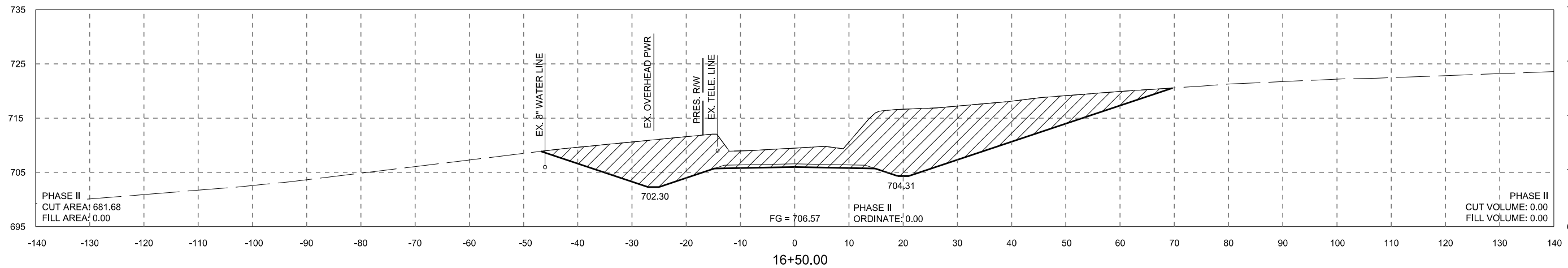
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS WARDS CHAPEL RD. JOB PIECE NO. 24066(04) SHEET NO. X109
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

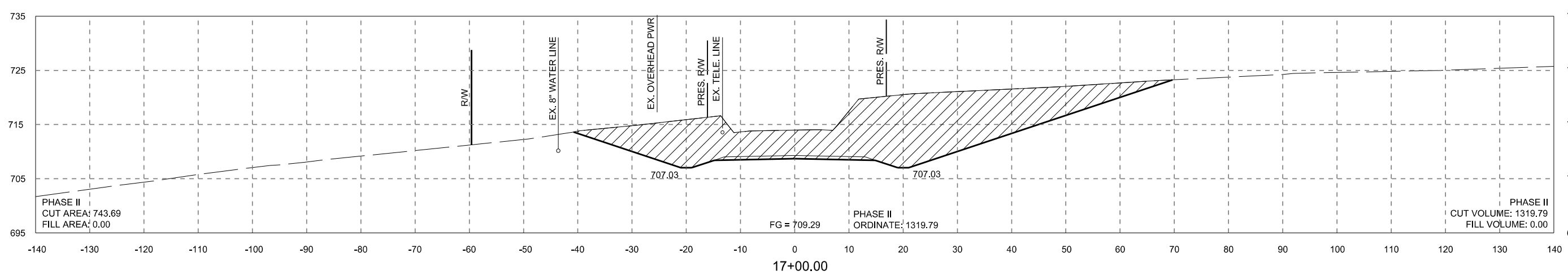
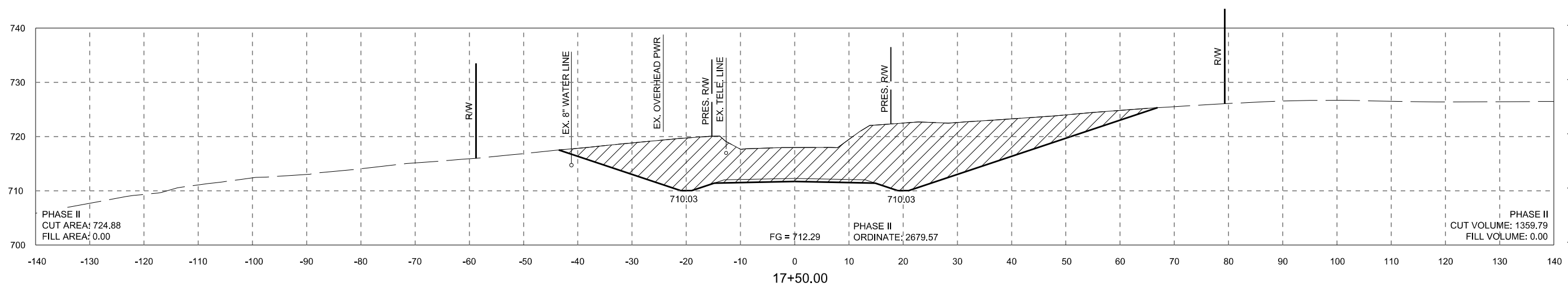
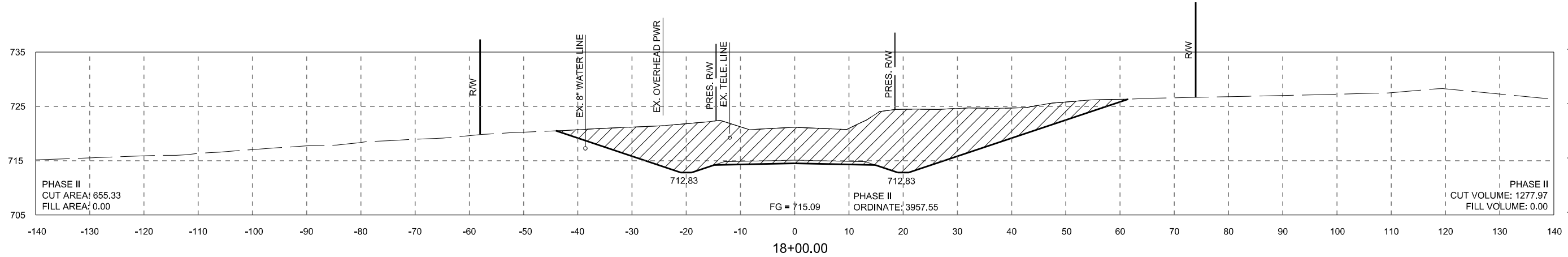
PRINT DATE: 8/23/2018 C:\Projects\150-SHT Final Plans\101-24066(04)-X-SECTIONS-CC-RD.dwg



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS WARDS CHAPEL RD. JOB PIECE NO. 24066(04) SHEET NO. X110
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

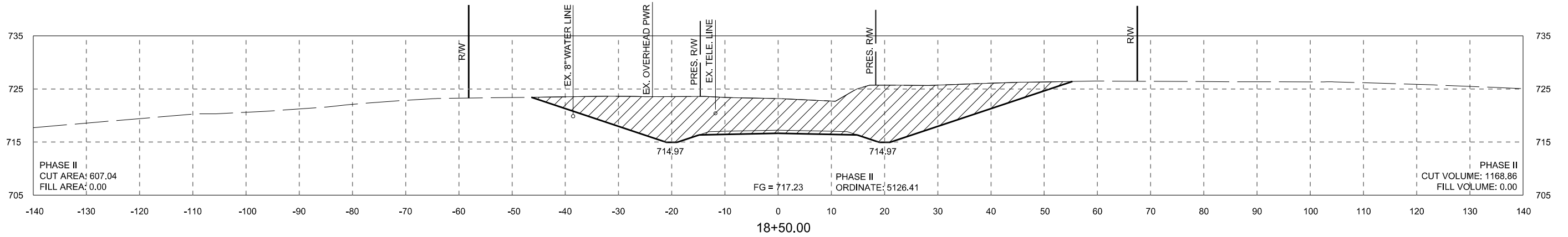
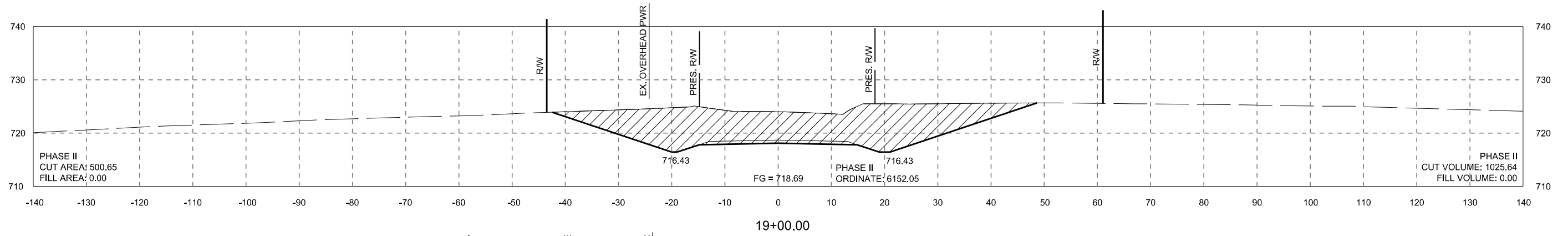
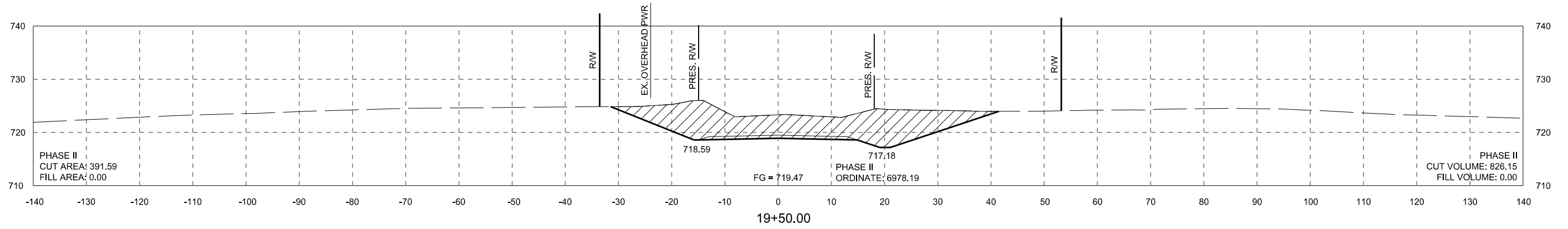
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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		
			CROSS SECTIONS WARDS CHAPEL RD.
			JOB PIECE NO. 24066(04) SHEET NO. X111

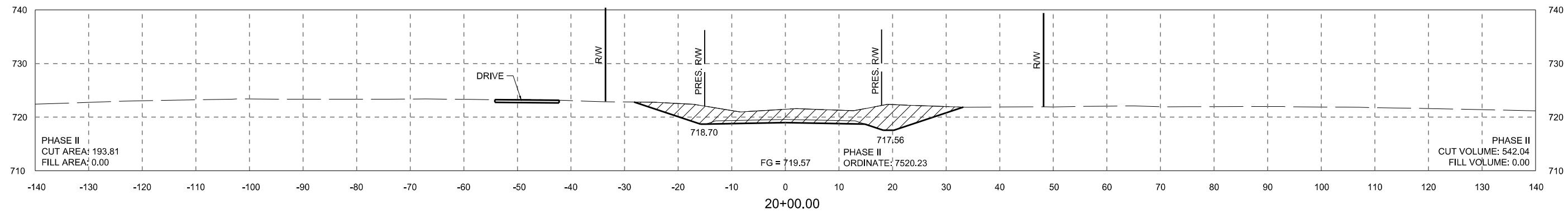
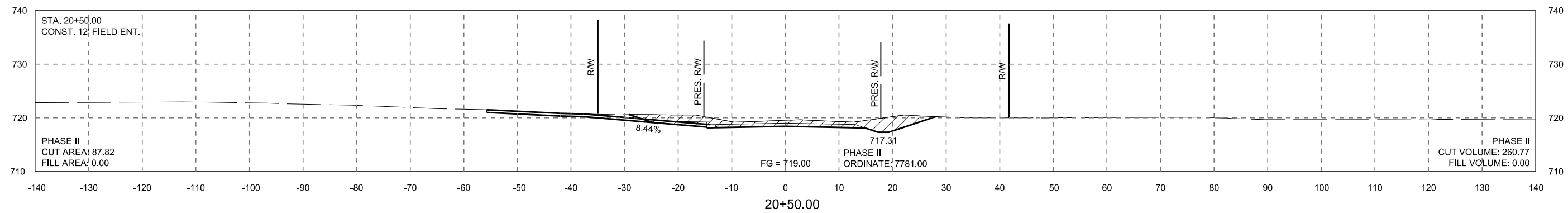
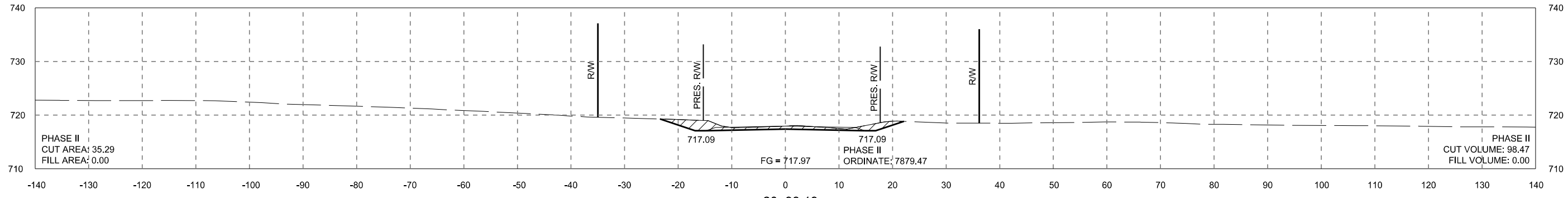
PRINT DATE: 8/23/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTIONS-CC-RD.dwg



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

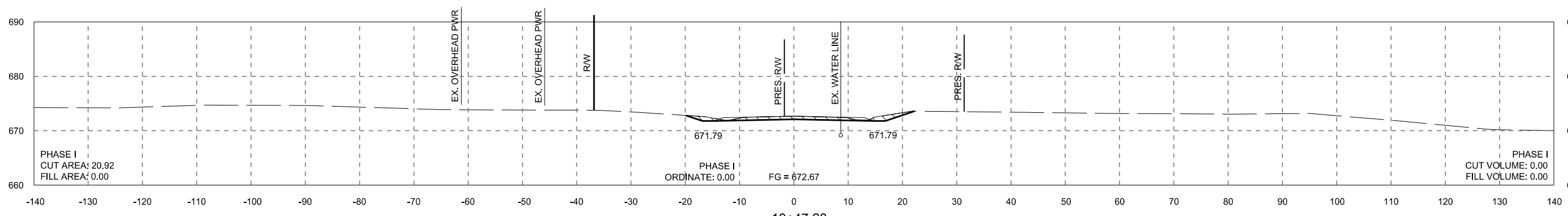
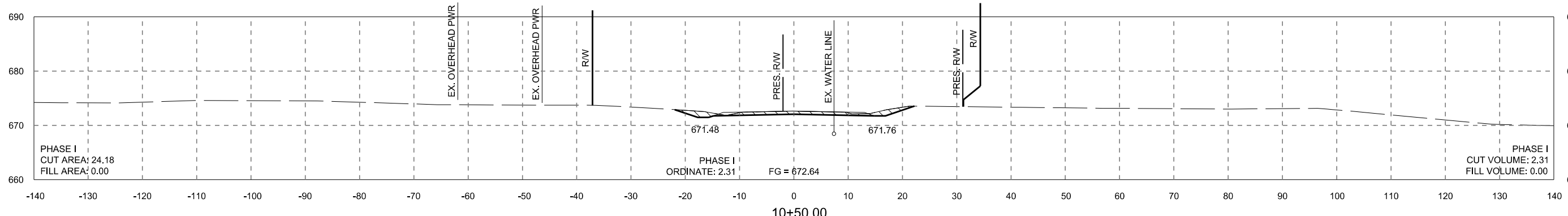
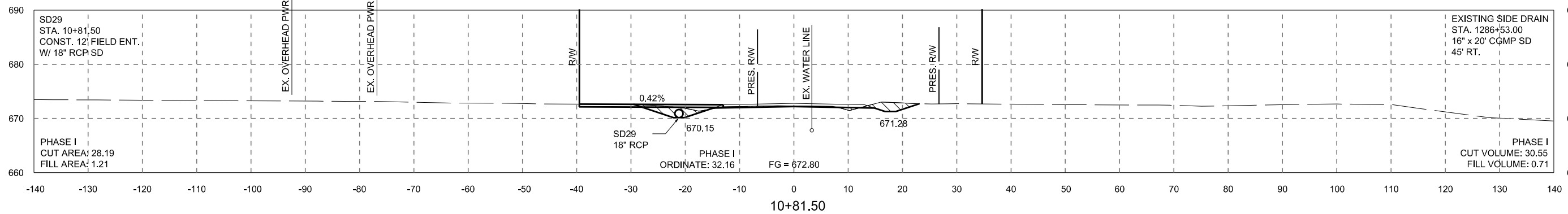
DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS WARDS CHAPEL RD. JOB PIECE NO. 24066(04) SHEET NO. X112
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

PRINT DATE: 8/23/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTIONS-CC-RD.dwg



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS WARDS CHAPEL RD. JOB PIECE NO. 24066(04) SHEET NO. X113
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

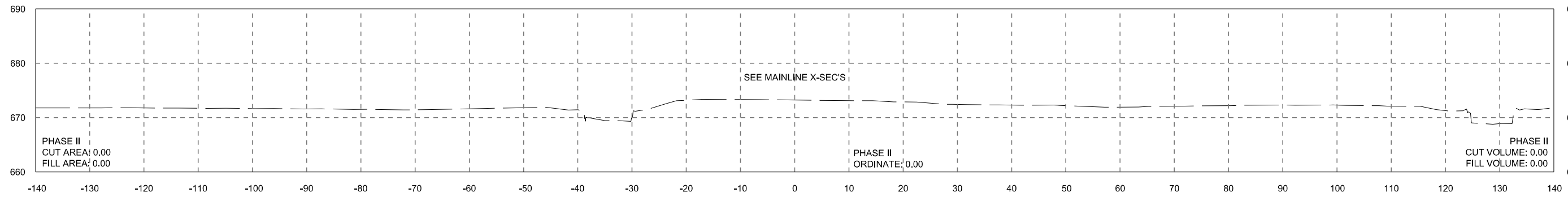


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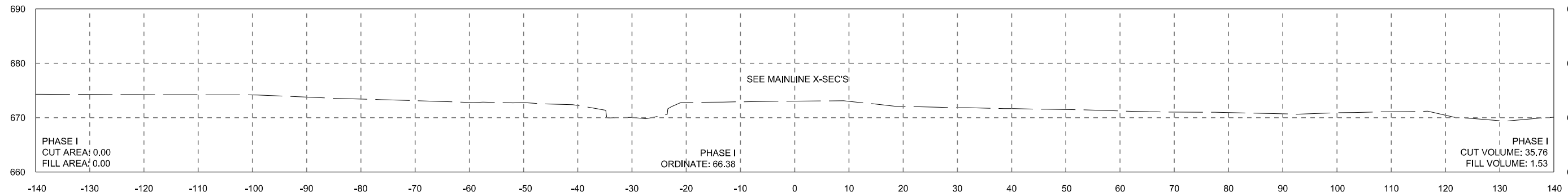
SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS HICKORY HILL RD. JOB PIECE NO. 24066(04) SHEET NO. X114
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 8/23/2018 C:\Projects\150-SHT Final Plans\101-24066(04)-X-SECTIONS-CC-RD.dwg



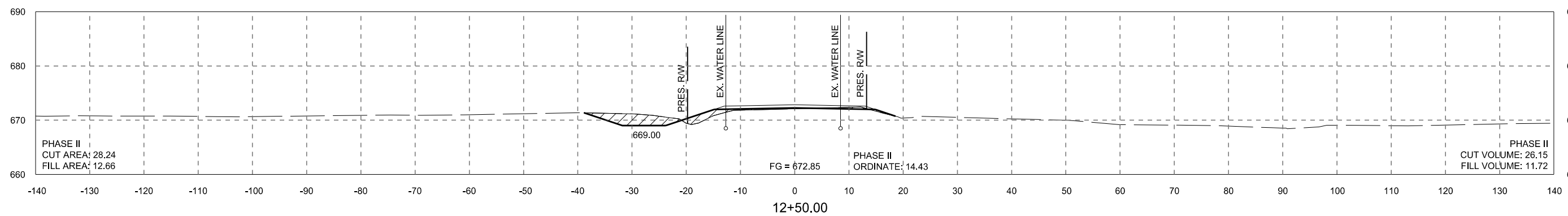
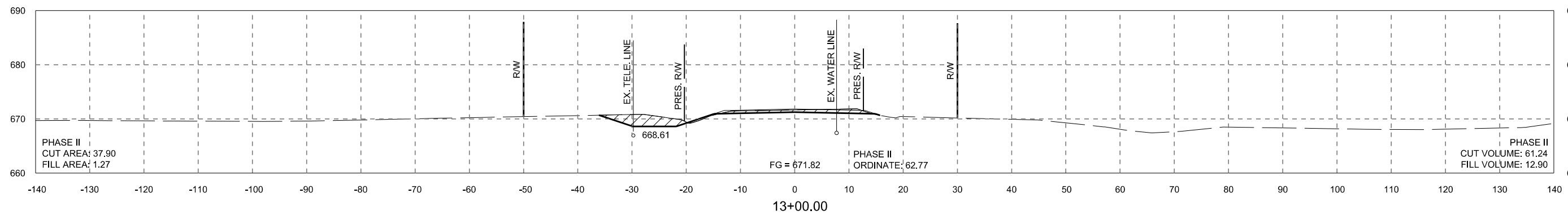
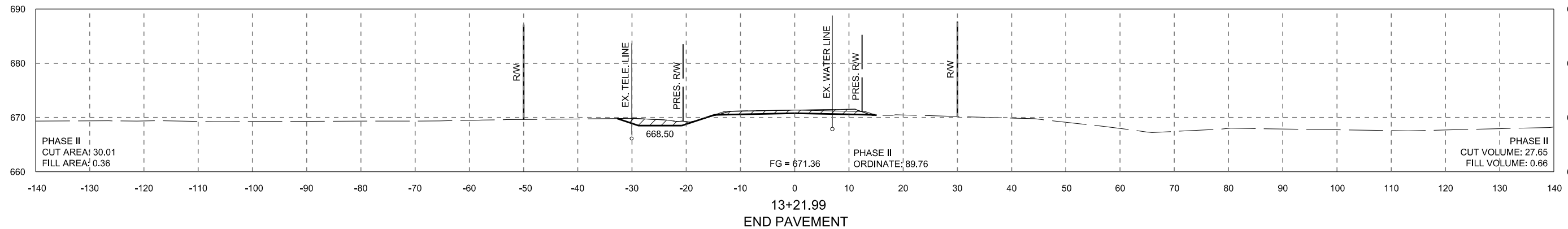
12+00.00



11+50.00

SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

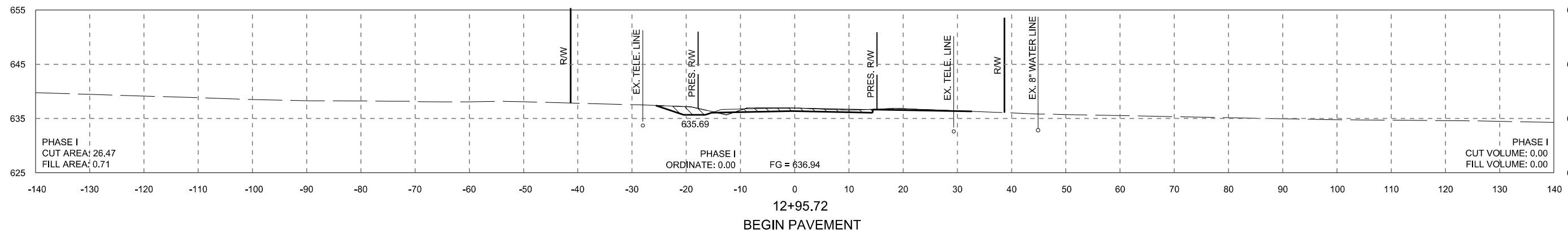
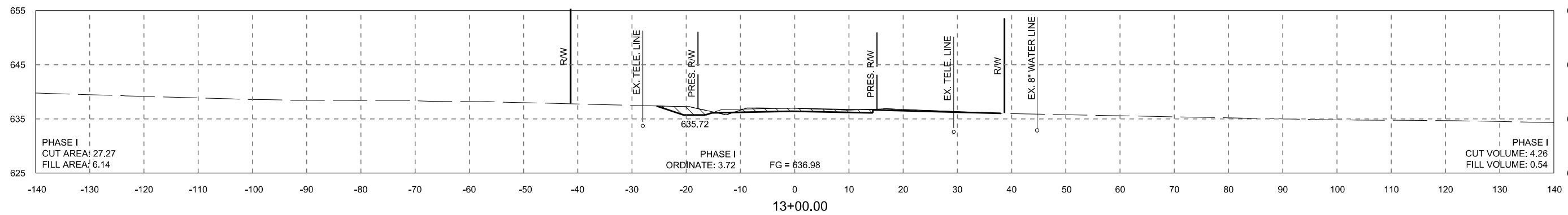
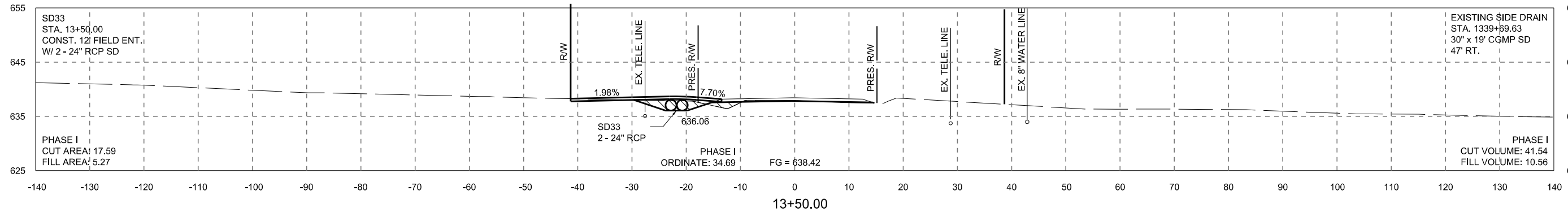
DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS HICKORY HILL RD. JOB PIECE NO. <u>24066(04)</u> SHEET NO. <u>X115</u>
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		



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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS HICKORY HILL RD. JOB PIECE NO. 24066(04) SHEET NO. X116
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

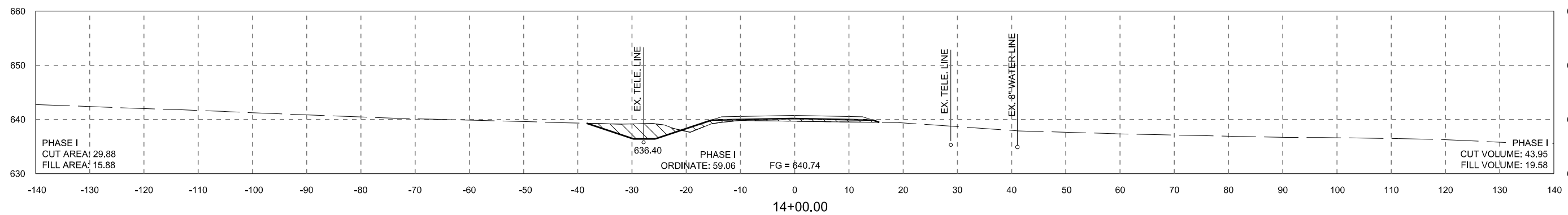
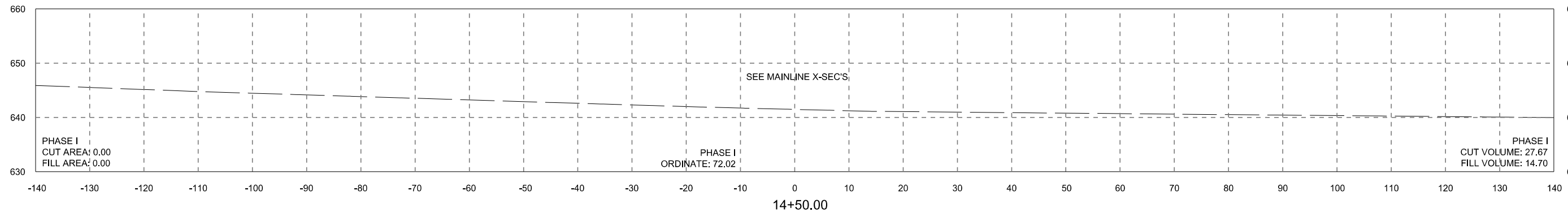


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SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS CARR PEN RD. JOB PIECE NO. 24066(04) SHEET NO. X117
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	

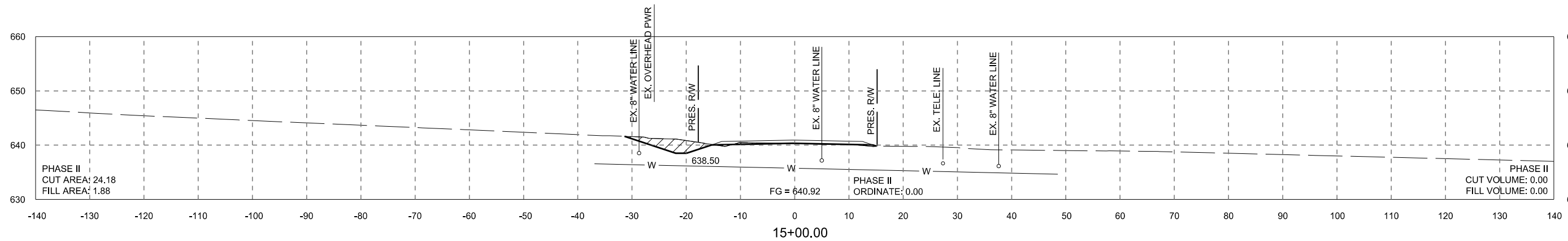
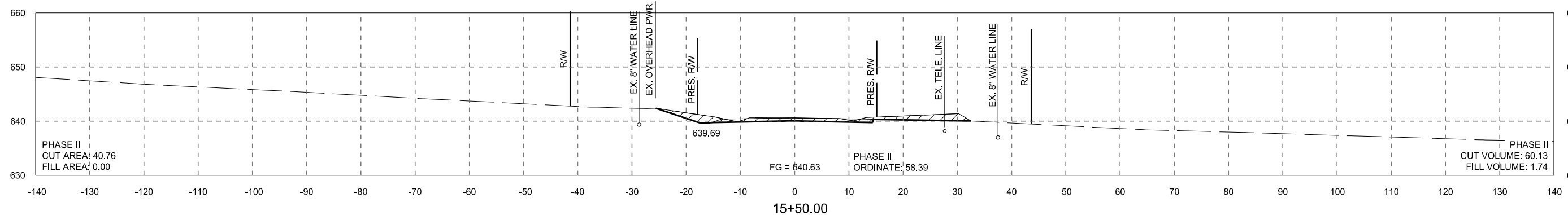
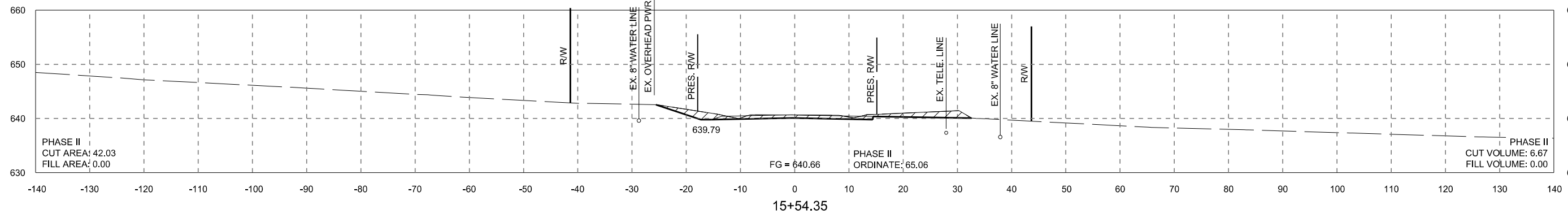
PRINT DATE: 8/23/2018 C:\Projects\650-SHT Final Plans\650-SHT\04-X-SECTIONS-CC-RD.dwg



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB		OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS CARR PEN RD. JOB PIECE NO. 24066(04) SHEET NO. X118
DRAWN	MM		
CHECKED	DP		
APPROVED	SL		
SQUAD	BKL, Inc.		

PRINT DATE: 8/23/2018 C:\Projects\150-SHT Final Plans\150-SHT Final Plans\04-X-SECTIONS-CC-RD.dwg



SCALE: HORIZ. 1" = 1'-0"
VERT. 1" = 1'-0"

DESIGN	RB	OKLAHOMA DEPARTMENT OF TRANSPORTATION DESIGN DIVISION CROSS SECTIONS CARR PEN RD. JOB PIECE NO. 24066(04) SHEET NO. X119
DRAWN	MM	
CHECKED	DP	
APPROVED	SL	
SQUAD	BKL, Inc.	